

December 20, 2021
Minutes – Regular Meeting of the Board of Commissioners

#### **WORK SESSION**

#### 1. 5:00 PM Discussion of the Moyock Regional Wastewater Treatment Plant

The Board of Commissioners met at 5:00 PM in the Board Meeting Room of the Historic Courthouse to discuss the County's options for operation of the Moyock Regional Wastewater Treatment Plant. Interim County Manager/County Attorney (ICM/CA) Ike McRee, recalled previous work session discussion when the Board directed staff to collect more data regarding the process of public private partnerships before moving forward with the county funding the Moyock Regional Wastewater Plant project or sell of system. He encouraged Board not to make a pressured decision and this issue could be viewed as a retreat item.

Assistant to the County Manager, Rebecca Gay, provided a presentation on the Public-private partnership (P3) defined through the N.C General Statue § 143-128.1C describing the P3 process and outlining the role of a private sector. Mrs. Gay included the variety of contract structures and financing in comparison of Traditional design build, P3, and sale. Mrs. Gay provided examples highlighting the aspects of each project and limitations of the P3 along with a charted timeline of the P3 steps that would be evaluated.

During discussion ICM/CA Ike McRee recommended the Board continue discussion as an Item during the Commissioners Retreat.

Commissioner White motioned to conduct further discussion as a retreat item and move item for action to a regular meeting in February 2022. Commissioner Mary Etheridge seconded. Motion carried 7-0

Discussion concluded and the Work Session was adjourned.

RESULT: CONTINUED [UNANIMOUS] Next: 2/7/2022 6:00 PM

MOVER: Bob White, Commissioner

**SECONDER:** Mary "Kitty" Etheridge, Commissioner

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S.

Jarvis, Commissioner, Kevin E. McCord, Commissioner, Bob White,

Commissioner

#### 6:00 PM CALL TO ORDER

The Currituck County Board of Commissioners held a regular meeting at 6:00 PM in the Board Meeting Room of the Historic Courthouse, 153 Courthouse Road, Currituck, North Carolina.

Attendee Name	Title	Status	Arrived
Michael H. Payment	Chairman	Present	
Paul M. Beaumont	Vice Chairman	Present	
J. Owen Etheridge	Commissioner	Present	
Mary "Kitty" Etheridge	Commissioner	Present	
Selina S. Jarvis	Commissioner	Present	
Kevin E. McCord	Commissioner	Present	
Bob White	Commissioner	Present	

Chairman Payment called the meeting to order and announced the earlier work session discussion on Moyock Wastewater Treatment Plant.

#### A) Invocation & Pledge of Allegiance

Commissioner Jarvis offered the invocation and led the Pledge of Allegiance.

#### B) Approval of Agenda

Commissioner White moved for approval. Commissioner McCord seconded the motion. The motion carried. 7-0

RESULT: APPROVED [UNANIMOUS]

MOVER: Bob White, Commissioner

SECONDER: Kevin E. McCord, Commissioner

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner, Bob White, Commissioner

#### **PUBLIC COMMENT**

Please limit comments to matters other than those appearing on this agenda as a Public Hearing. Public comments are limited to 3 minutes.

Chairman Payment opened the Public Comment period.

Susan Powers, Green View resident expressed her concerns regarding the Eagle Creek Sewer issues and the consideration of the wastewater plant sell to a private sector.

Richard Shortway with Corolla Volunteer Fire Department provided a letter discussing the recently Board approved grant application.

No others were signed up nor wished to speak and the Public Comment period was closed.

#### **COMMISSIONER'S REPORT**

Commissioner Jarvis attended the Chamber of Commerce Leadership Day event with Interim County Manager/County Attorney, Ike McRee, and expressed the desire for the Currituck Citizens Academy to return.

Commissioner Mary Etheridge, attended the Senior Center Christmas luncheon and extended gratitude to Senior Center Director Stacey Joseph and the Senior Center's staff. She also attended the ABC Commissions meeting providing the yearly independent audit revenue and expressed gratitude to the ABC staff for the dedication of hard work.

Commissioner White, addressed the quarterly County Newsletters congratulating Tourism Director Tameron Kugler for being selected as a VP of the North Carolina Travel Industry Association. He encouraged the community to sign up for the monthly E-newsletter, and addressed the development in Currituck County as a positive outlook.

Chairman Payment echoed Commissioner White regarding economic growth of the County and shared the upcoming businesses in the Lower end of the County. Mr. Payment thanked all of the Fire departments for the community outreach of bringing Santa Claus around.

Commissioner Beaumont attended the Corolla Volunteer Awards Dinner and expressed gratitude for the invitation. Commissioner Beaumont shared a NCDOT letter regarding the Mid-county bridge is to move forward. He praised the Crawford breakfast with Santa and the community outreach.

Commissioner McCord echoed praises to all the Fire departments for sharing Santa Claus with the community. He shared the success of "Operation Santa" and "Shop with a Cop". Mr. McCord shared county office closings for the holidays.

Commissioner J.O. Etheridge echoed Commissioner Jarvis desire for the Currituck Citizens Academy.

Commissioners conveyed Merry Christmas to all.

#### **COUNTY MANAGER/COUNTY ATTORNEY REPORT**

Interim County Manager/ County Attorney, (ICM/CA) Ike McRee, reported the decision by the court of appeals in the 85 Sunny, LLC vs. Currituck County.

#### **PUBLIC HEARINGS**

#### A. PB 21-25 Flora Farms:

Parties were sworn in and Planner, Tammy Glave, reviewed the use permit application to

the Board. Ms. Glave used a powerpoint to review the site location and site plan. Recording dates were presented and student generation and school capacities were reviewed. Outstanding issues, including several that the Technical Review Committee (TRC) recommended be addressed prior to approval were presented, and Ms. Glave said the TRC recommended denial. She presented the findings of fact and responded to questions related to the TRC comments included in the agenda documents.

Jamie Schwedler, Attorney for the applicant, called several witnesses to provide testimony and provided Special Use Permit/Preliminary Plat witness Exhibits.

Engineer, Mark Bissell, was questioned by Ms. Schwedler as to his role in developing the project site plan configuration and stormwater plan. Mr. Bissell discussed the reviews and modifications. The 1st phase of date 12/1/2020 County Staff reports have pushed back 6 months. Commission McCord questioned the wastewater line connectivity with the Eagle Creek lines. Bissell clarified that for a short distance the lines run parallel, but A wastewater collection system will be constructed for the entire development and managed by Currituck Water and Sewer.

Mike Myers explained how the sewer systems are designed and constructed to handle the capacity needed to serve projects need and the process of an engineer's review. Mr. Myers stated a system is under capacity it is harder to operate due to the of the lack of bacteria food. The Flora development is planned to tie into the Eagle Creek wastewater treatment system own by Sandler Utilities. He indicated that the Eagle Creek collection system has no impact on the operation of the plant.

Lyle Overcash, Raleigh, was tendered as an expert by the Board and testified on behalf of the applicant. Responding to questioning by Ms. Schwedler, he provided his background in traffic engineering and transportation. He reviewed a traffic analysis performed at the site location.

Chairman Payment called a recess at 7:47 PM. The meeting reconvened at 7:56 PM.

Hunter Howell, Cary, was tendered as an expert by the Board and testified on behalf of the applicant as a licensed state certified appraiser. Mr. Howell provided an analysis to determine the effects of the proposed Flora projects development.

Jonathan Wells, a certified planner, was tendered as an expert by the Board and testified on behalf of the applicant in school planning and capacity. He summarized his review and analysis and testified in support of the findings of fact for approval relative to the adequate public facilities ordinance for school capacities. Commissioner Jarvis questioned the exhibits capacity information provided in 2019 to current capacity data.

Matt Lutz, Superintendent Sworn in. Dr. Lutz responded to questions from Commissioners and Attorney for the applicant, Jamie Schwedler related to adequate student capacity and school rezoning.

Chairman Payment opened the Public Hearing.

Jenna Kehoe, Green View resident, requested a moratorium on the Flora or any subdivision that may attached to the Wastewater Treatment plant expressed concerns

regarding the Eagle Creek sewer system

Brian Dodd, Green View resident expressed concerns regarding the effects on Eagle Creek that may occur with the Flora or any subdivision approval.

David Roberson, Ranchland resident addressed a retention pond backs up to his property that is not deemed wetland but is flooding due to surrounding development.

No others were signed up nor wished to speak. Public Hearing was closed.

Ms. Schwedler provided a summary of the witness testimony and stated the applicant has met the burden to support the findings of fact and asked the Board to approve the Special Use Permit.

Commissioner White motioned to continue item to next meeting January 3, 2022, for further action. Chairman Payment seconded the motion. Motion carried, 5-2. Commissioner Jarvis and Commissioner Mary Etheridge opposed.

RESULT: CONTINUED [5 TO 2] Next: 1/3/2022 6:00 PM

MOVER: Bob White, Commissioner
SECONDER: Michael H. Payment, Chairman

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Kevin E. McCord, Commissioner, Bob White,

Commissioner

NAYS: Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner

#### B. PB 18-23 Fost, Phases 6-9, Preliminary Plat/Special Use Permit:

Parties were sworn in and Planner, Tammy Glave, reviewed the use permit application to the Board. Ms. Glave used a powerpoint to review the site location and site plan. Outstanding issues, including several that the Technical Review Committee (TRC) recommended be addressed prior to approval were presented, and Ms. Glave said the TRC recommended denial. She presented the findings of fact and responded to questions related to the TRC comments included in the agenda documents.

Jamie Schwedler, Attorney for the applicant, called several witnesses to provide testimony and provided Preliminary Plat/ Special Use Permit Witness Exhibits.

Mark Bissell, Professional Civil Engineer explained his role in the Fost project. Mr. Bissell discussed the reviews and modifications.

Mike Myers explained how the sewer systems are designed and constructed to handle the capacity needed to serve projects need and the process of an engineer's reviews. The Fost development is planned to tie into the Eagle Creek wastewater treatment system owned by Sandler Utilities. He indicated that the Eagle Creek collection system has no impact on the operation of the plant.

Lyle Cash explained his role in the Fost project of analyzing the traffic impacts.

Mr. Hunter Howell explained his role in the Fost project as a licensed state certified appraiser. Mr. Howell provided an analysis to determine the effects of the proposed Fost projects development.

Jonathan Wells explained his role in the Fost Project as certified planner in the public-school capacity,

attendance, advocate for facilities and state standards.

Chairman Payment opened the Public Hearing. No one signed up nor wished to speak and the Public Hearing was closed.

Ms. Schwedler provided a summary of the witness testimony and stated the applicant has met the burden to support the findings of fact and asked the Board to approve the Special Use Permit.

Commissioner McCord motioned to table the item to the January 3, 2022, regular meeting for Action. Commissioner Owen Etheridge seconded. The motion carried 5-2. Commissioner Jarvis and Commissioner Mary Etheridge opposed.

RESULT: TABLED [5 TO 2] Next: 1/3/2022 6:00 PM

MOVER: Kevin E. McCord, Commissioner SECONDER: J. Owen Etheridge, Commissioner

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Kevin E. McCord, Commissioner, Bob White,

Commissioner

NAYS: Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner

#### **NEW BUSINESS**

### A. Consideration and Action on a Grant Agreement Between Currituck County and Corolla Volunteer Fire Department for the Purchase of Fire Apparatus.

ICM/CA, Mr. McRee, discussed the Resolution for the purchase of Fire Apparatus for Corolla Volunteer Fire Department and the Board requested to modify the agreement adding the county as the dual owner of the apparatus.

Commissioner White motioned for approval. Chairman Payment seconded the motion. Motion carried 7-0.

RESULT: APPROVED [5 TO 2]

**MOVER:** Kevin E. McCord, Commissioner **SECONDER:** J. Owen Etheridge, Commissioner

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Kevin E. McCord, Commissioner, Bob White,

Commissioner

NAYS: Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner

#### B) Consent Agenda

Commissioner Jarvis moved for approval of the Consent Agenda. Commissioner McCord seconded the motion. The motion carried, 7-0.

RESULT: APPROVED [UNANIMOUS]

MOVER: Selina S. Jarvis, Commissioner

SECONDER: Kevin E. McCord, Commissioner

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S.

Jarvis, Commissioner, Kevin E. McCord, Commissioner, Bob White,

Commissioner

#### 1. Budget Amendments

				Debit	(	Credit	
			Decrea	se Revenue or	Increase	e Revenue or	
Account Number		Account Description	Increa	ase Expense	Decrea	se Expense	
10535-516000		Repairs and Maintenance	\$	1,410			
10535-557100		Software		·	\$	1,410	
			\$	1,410	\$	1,410	
Explanation:	Сс	ommunications (10535) - Transfer budgeted	funds for unanti	cipated communicat	ion equipment	repairs.	
Net Budget Effec	ct:	Operating Fund (10) - No change.					
				Debit	(	Credit	
			Decrea	se Revenue or	Increase	e Revenue or	
Account Number		Account Description	Increa	ase Expense	Decrea	se Expense	
10510-590000		Capital Outlay	\$	17,000			
10380-488000		ABC Education Donations			\$	12,000	
10380-488200		ABC Law Enforcement Distribution			\$	5,000	
			\$	17,000	\$	17,000	
Explanation:	1	 neriff (10510) - Increase appropriations to pu	rchase a golf ca	art to be used for the	drunk driving	orogram at	
	the	e high school.					

				Debit		Credit
			Doere	ease Revenue or	Incres	ase Revenue or
Account Number		Account Description		ease Expense		ease Expense
Account Number		Account Description	IIICI	ease Expense	Deci	ease Expense
51848-597008		MES Modular Unit - Dec 2021	\$	940,000		
51390-495041		T F - School Capital Reserve			\$	940,000
41990-587051		TT - School Construction	\$	940,000		
41390-499900		Appropriated Fund Balance	Ψ	040,000	\$	940,000
41330-433300		Appropriated Fund Balance			Ψ	940,000
			\$	1,880,000	\$	1,880,000
	_					
Explanation:		chool Capital Construction (51848) - Increas		ns to purchase and ins	stall a modu	lar unit at the
	IVIC	byock Elementary School during the renovar	tion process.			
Net Budget Effec	ct.	School Capital Reserve Fund (41) - Increas	sed by \$940.0	00		
Not Badget Ellet	J	School Construction Fund (51) - Increased		00.		
			, , , , , , , , , , , , , , , , , , , ,	Debit	Credit	
			Decre	ease Revenue or	Increa	ase Revenue or
Account Number		Account Description	Incr	ease Expense	Decr	rease Expense
51848-597008		MES - Addition 2021	\$	13,159,800		
51390-495041		T F - School Capital Reserve			\$	3,150,000
51390-495042		T F - Transfer Tax Capital Reserve			\$	10,009,800
41990-587051		TT - School Construction	\$	1,500,000		
41390-499900		Appropriated Fund Balance			\$	1,500,000
42450-587051		T T - School Capital Construction	\$	10,009,800		
42390-499900		Appropriated Fund Balance			\$	10,009,800
			\$	24,669,600	\$	24,669,600
			Ψ	24,009,000	Ψ	24,009,000
Explanation:	Sc	: chool Capital Construction (51848) - Increas	e appropriatio	ns for addition to the N	Movock Fler	mentary
p.uu.u.u		chool. The Guaranteed Maximum Price (GN				
		expected contingencies outside of the GMF				
		sign.				
Net Budget Effec	ct:	School Capital Reserve Fund (41) - Increas				
		Transfer Tax Capital Fund (42) - Increased				
		School Construction Fund (51) - Increased	by \$13,159,8	00.		

			Debit			Credit
			Decrease Reve	nue or	Increas	e Revenue o
Account Number		Account Description	Increase Expe			ise Expense
10430-503000		Salaries - Part Time	\$	1,500		
10430-503430		Poll Workers	\$ 2	1,000		
10430-505000		FICA	\$	1,725		
10340-450500		Administration and Filing Fees			\$	24,225
			\$ 2	4,225	\$	24,225
			Ψ 2	4,223	Ψ	24,220
Explanation:	7	ections (10430) - Increase appropriations f	or additional part-time sal	aries and fo	poll workers	for the spring
	ele	ections.				
Net Budget Effe	ct:	Operating Fund (10) - Increased by \$24,2				
			Debit			Credit
			Decrease Reve	nue or	Increas	e Revenue o
Account Number		Account Description	Increase Expe	ense	Decrea	se Expense
10410-553000		Dues & Subscriptions	\$	1,500		
10410-557100		Software License Fees	\$	16		
10410-506000		Health Insurance Expense			\$	1,516
			\$	1,516	\$	1,516
				1,010		.,
Explanation:	Ac	lministration (10410) - Transfer budgeted f	unds for increase in dues	and software	e license fees	for FY 2022
Nat Dadas ( Effe	-4.	On another French (40). No allower				
Net Budget Effe	CT:	Operating Fund (10) - No change.	Debit			Credit
			Debit			Oledit
			Decrease Reve		Increas	e Revenue o
Account Number		Account Description	Increase Expe	ense	Decrea	se Expense
61818-511010		Data Transmission	\$ 1	0,000		
61818-561000		Professional Services		1,000		
61818-506000		Health Insurance Expense	7	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	\$	5,000
61818-590000		Capital Outlay			\$	6,000
			\$ 1	1,000	\$	11,000
Explanation:	7	ainland Water (61818) - Transfer budgeted	line items for increased of	osts of data	transmission	and
	pro	ofessional services.				
Net Budget Effe		Mainland Water Fund (61) - No change.				

#### 2. Project Ordinance-Moyock Elementary School Modular

## COUNTY OF CURRITUCK CAPITAL PROJECT ORDINANCE

BE IT ORDAINED by the Currituck County Board of Commissioners, North Carolina that pursuant to Section 13.2 of Chapter 159 of the General Statutes of North Carolina, the following capital project ordinance is hereby adopted:

**SECTION 1.** The project authorized is purchase and set-up of modular classrooms at Moyock Elementary School.

**SECTION 2.** The following amounts are appropriated for the project:

Moyock Elementary Modular Unit - Dec 2021

\$ 940,000

\$ 940,000

**SECTION 3.** The following funds are available to complete this project:

Transfer from School Capital Reserve

\$ 940,000

\$ 940,000

**SECTION 4.** The Finance Director is hereby directed to report, on a quarterly basis, on the financial status of each project element delineated in Section 2 above.

#### **SECTION 5. SPECIAL APPROPRIATIONS AND RESTRICTIONS**

The Budget Officer is hereby authorized to transfer appropriations within the fund as contained herein under the following conditions:

a. He may transfer amounts between object line items within the fund up to One Thousand dollars (\$1,000).

#### SECTION 6. CONTRACTUAL OBLIGATIONS

The County Manager is hereby authorized to execute contractual documents under the following conditions:

- a. He may execute contracts for construction or repair projects which do not require formal competitive bid procedures.
- b. He may execute contracts for (1) purchases of apparatus, supplies, and materials, or equipment which are within the budgeted departmental appropriations; (2) leases of personal property for a duration of one year or less and within budgeted departmental appropriations; and (3) services which are within budgeted departmental appropriations.
- c. He may execute contracts, as the lessor or lessee of real property, which are of a

duration of one year or less which are within the budgeted departmental appropriations.

#### SECTION 7. USE OF BUDGET ORDINANCE

The Budget Officer and the Finance Director shall use this capital project ordinance for administration of the budget and for the accounting system.

ADOPTED this 20th day of December 2021.	
	Michael Payment, Chairman Board of Commissioners
ATTEST:	
Samantha Evans Deputy Clerk to the Board	

3. Resolution Authorizing the Purchase of Powered Ambulance Stretchers and Stair Chairs From Stryker Corporation Through Sole Source Purchase Pursuant To N.C GEN. STAT. §143-129(e)(6)

#### COUNTY OF CURRITUCK

RESOLUTION AUTHORIZING THE PURCHASE OF POWERED AMBULANCE STRETCHERS AND STAIR CHAIRS FROM STRYKER CORPORATION THROUGH SOLE SOURCE PURCHASE PURSUANT TO N.C. GEN. STAT. ?143-129(e)(6)

WHEREAS, N.C. Gen. Stat. ?143-129(e)(6) authorizes a unit of local government to purchase apparatus, supplies, materials or equipment when standardization or compatibility is an overriding consideration; and

WHEREAS, Currituck County Fire-EMS needs two (2) ambulance stretchers and two (2) stair chairs; and

WHEREAS, Currituck Fire-EMS must comply with North Carolina Office of Emergency Medical Services rules and regulations for equipment; and

WHEREAS, Stryker Corporation is the only entity capable of providing Fire-EMS with ambulance stretchers and stair chairs compatible with current North Carolina Office of Emergency Medical Services rules and regulations; and

WHEREAS, the County has been using Stryker Corporation to purchase emergency response, power loaded stretchers and ambulance equipment; and

WHEREAS, as the sole and exclusive distributor of the Stryker Power-PRO XT and Stair-PRO, Stryker Corporation is supplying Fire-EMS with two (2) Power-PRO XT

ambulance stretchers and two (2) Stair Chairs and accessories at a cost of \$86,788.81; and

WHEREAS, the total cost of the purchase is \$86,788.81.

NOW, THEREFORE, BE IT RESOLVED by the Board of Commissioners for Currituck County, North Carolina as follows:

Section 1. The County of Currituck is authorized to enter into a contract in the amount of \$86,788.81 with Stryker Corporation for the sole source purchase of Power-PRO XT ambulance stretchers and Stair-PRO chairs in accordance with the sole source provision requirements set forth by N.C. Gen. Stat. ?143-129(e)(6). Further, the County Manager is authorized to execute the agreement with Stryker for the acquisition of apparatus, materials, and equipment described in this resolution and the proposed contract.

Section 2. This resolution shall be effective upon its adoption.

This the 20th day of December 2021.

ATTEST: Michael H. Payment,
Chairman
Samantha Evans Board of Commissioners
Deputy Clerk to the Board of Commissioners
(COUNTY SEAL)

- 4. Resolution Authorizing the Moyock Volunteer Fire Department to Expend \$8000 of Capital Project Fund to Replace Flooring
- 5) Approval Of Minutes for 12-6-2021
  - 1. Approval of 12-6-2021 Minutes

#### **ADJOURN**

#### **Motion to Adjourn Meeting**

The Board had no further business and Commissioner White moved to adjourn. Commissioner Beaumont seconded the motion and the motion carried, 7-0. The regular meeting of the Board of Commissioners adjourned at 11:42PM

RESULT: APPROVED [UNANIMOUS]

MOVER: Michael H. Payment, Chairman

SECONDER: Paul M. Beaumont, Vice Chairman

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S.

Jarvis, Commissioner, Kevin E. McCord, Commissioner, Bob White,

Commissioner



# Currituck County Agenda Item Summary Sheet

Agenda ID Number - (ID # 3282)

Agenda Item Title: 5:00 PM Discussion of the Moyock Regional Wastewater Treatment Plant

**Submitted By:** Samantha Evans – County Manager

Presenter of Item:

**Board Action:** Discussion

**Brief Description of Agenda Item:** 

Is this item regulated by plan, regulation or statute?

**Manager Recommendation:** 



# Currituck County Agenda Item Summary Sheet

**Agenda ID Number** – 3298

**Agenda Item Title:** PB 21-25 Flora Farms:

Submitted By: Tammy Glave - Planning & Community Development

Item Type: Quasi-Judicial

Presenter of Item: Tammy Glave

**Board Action:** Action

#### **Brief Description of Agenda Item:**

Preliminary Plat/Special Use Permit for 277 lots located in Moyock on the east and west side of Survey Road, Tax Map 15, Lots 85A, 85B, and 85C, Moyock Township.

**Planning Board Recommendation:** 

**Staff Recommendation:** 

**TRC Recommendation:** 



# STAFF REPORT PB 21-25 FLORA FARMS PRELIMINARY PLAT/ SPECIAL USE PERMIT, BOARD OF COMMISSIONERS DECEMBER 20, 2021

APPLICATION SUMMARY	
Property Owners:	Applicant:
John Flora III	North-South Development, LLC
PO Box 369	C/O Justin Old
Moyock NC 27958	417-D Caratoke Hwy
	Moyock NC 27958
Mary Nell Brumsey	
117 Puddin Ridge Rd	
Moyock NC 27958	
Case Number: PB 21-25	<b>Application Type:</b> Preliminary Plat/Special Use Permit
Parcel Identification Numbers:	Existing Use: Single-Family Dwelling, farmland
0015-000-085A-0000	
0015-000-085B-0000	
0015-000-085C-0000	
Land Use Plan Classification: Full Service	Parcel Size (Acres): 202.44
Moyock Small Area Plan Classification:	Zoning: C-MXR
Limited Service and Full Service	
Number of Units: 277	Overall Project Density (units/acre): 1.37
Required Open Space (Acres):	Provided Open Space (Acres):
60.04 (Residential)	74.92 (Residential)
.23 (Commercial)	1.07 (Commercial)

SURROUNDING PARCELS		
	Land Use	Zoning
North	Single-family dwellings, retail	AG, GB
South	Single-family dwellings, Solar Energy Facility	AG, SFM
East	Planned Development under construction	PD-R
West	Single-family dwellings, cultivated farmland, Middle School	AG, SFM

#### STAFF ANALYSIS

The Board of Commissioners approved the rezoning for this development on March 1, 2021. The developer notes that the goal of the development is to build a community that has a creative design, with single-family lots in an attractive arrangement, with an efficient use of open space that promotes an active lifestyle and strong sense of community. Limited, neighborhood commercial uses are also proposed on a 2.3 acre portion of the property.

The Phased Recording Schedule estimates earliest recording dates as shown:

		PHASING SCHEDULE	
PHASE	# LOTS	EARLIEST RECORDATION*	OTHER IMPROVEMENTS
1	55	6/1/2022	SURVEY RD. IMPROVEMENTS, PONDS 1 & 6, DOG PARK, ROUND-A-BOUT, NORTH MAIL KIOSK, TEMP. RV PARKING, AND WEST PROPERTY LINE DRAINAGE IMPROVEMENTS.
2	76	1/1/2023	CLUBHOUSE AND PONDS 3-5 & 7.
3	55	7/1/2023	SWIMMING POOL, POND 2, AND PERMANENT RV PARKING.
4	54	1/1/2024	SOUTH MAIL KIOSK AND PONDS 8 & 10.
5	37	7/1/2024	PONDS 9 & 11, PICNIC AREA, AND NATURE OVERLOOK.
TOTAL	277		

<sup>\*</sup> MINIMUM 6-MONTHS BETWEEN RECORDING DATES.

INFRASTRUCTURE	
MINAGINGGIONE	
Water	Public
Sewer	Private Centralized System (on-site at Eagle Creek)
Transportation	Pedestrian: A series of sidewalks and pedestrian trails; The developer must install an 8' multi-use path along the Caratoke Highway property line as required by the Connect Currituck Plan.  Connectivity Score: Minimum = 1.4 Proposed = 1.41  • One connection to Survey Road  • Five connections to Fost, PD-R
Stormwater/Drainage	See attached map for required stormwater/drainage improvements (PB 18-23 Fost PD-R Rezoning Stormwater Conditions dated 7/2019 prepared by engineering staff). The Flora off-site improvements on the Flora tract have not been completed to date and must be completed. Staff recommends that a final plat not be approved for this development until those drainage improvements are complete.
Design Standards	Non-residential design standards apply to the commercial development.
Lighting	Street lights are proposed and must be full cut-off fixtures
Landscaping	Farmland buffers, street trees, major arterial screening, and site landscaping are required
Compatibility	The BOC determined that this development was compatible with the Land Use Plan and the Moyock Small Area Plan at its March 1, 2021 meeting.
Recreation and Park Area	The applicant will be paying approximately \$48,548.87 as a fee-in-
Dedication	lieu of recreation and park area dedication for this development.
Riparian Buffers	Required for the wetlands in these phases.

#### **COMMUNITY MEETING**

The developer held a community meeting on June 22, 2021 at the Eagle Creek Pavilion. There were approximately eleven people in attendance. Most of the questions were regarding drainage and traffic. A community meeting summary prepared by the applicant is attached to this staff report.

#### **SCHOOLS**

This development is split between the Shawboro and Moyock Elementary School districts. The portion closest to Survey Road is in the Moyock Elementary attendance area and the rear portion of the development is in the Shawboro Elementary School attendance area. There are Adequate Public Facility concerns that Shawboro Elementary is at 100% *Actual Capacity* for this school year. (*Dr. Lutz, October 2, 2021 BOC meeting*). There is not capacity for the 26 additional students project to attend Shawboro Elementary. The Adequate Public Facilities ordinance requires that school capacity be in place at the time of approval or within two years of approval of the special use permit for the development. There are no plans in place to expand Shawboro Elementary School in the future.

There is a substantial completion date of August 2023 for the expansion of Moyock Elementary School.

ADEQUATE PUBLIC FACILITIES – SCHOOLS <sup>1</sup>				
School	2021-2022 Actual Capacity <sup>2</sup>	Committed Capacity <sup>3</sup>	Proposed Capacity Changes Number of Students	
Moyock Elementary Shawboro Elementary Central Elementary	128% 99% 96%	131%	+ 26 Shawboro +42 Moyock	
Moyock Middle Currituck Middle	94% 62%	92%	+22	
Currituck High JP Knapp Early College	88% 86%	105%	+38	

<sup>&</sup>lt;sup>1</sup>Does not include minor subdivisions, exempt subdivisions, and subdivisions approved prior to the adoption of the adequate public facilities ordinance (October 1994)

Based upon questions at a previous meeting regarding school capacity and the adequate public facilities ordinance, a brief history of the adequate public facilities ordinance and its application is attached for your review.

#### **STORMWATER**

The stormwater/drainage improvements required as shown on the attached "PB 18-23 Fost PD-R Rezoning Stormwater Conditions dated 7/2019" prepared by engineering staff remain unchanged. On-site stormwater will be conveyed to on-site retention ponds through a combination of curbs with inlets, stormwater pipes, and open vegetated swales.

#### **TRANSPORTATION**

The main subdivision entrance will be connected directly to Survey Road (SR 1215). Proposed improvements to Survey Road include widening, adding a turning lane, and adding a left turn lane restriction at Caratoke Highway (NC 168). Connectivity will be provided to the Fost Development. According to the application, streets will be in accordance with the approved right-of-way and pavement

PB 18-23 Fost Tract, Phases 6-9 Preliminary Plat/Special Use Permit Page 3 of 10

<sup>&</sup>lt;sup>2</sup>Capacity percentages are based on 2021-2022 school year classroom standards and Sep 2021 ADM

<sup>&</sup>lt;sup>3</sup>Capacity percentages are based on the 2021-2022 school year classroom standards and Sep 2021 ADM

width characteristics, using the NCDOT Complete Streets Planning Manual as a guide and appropriate access has been made for emergency service vehicles in accordance with applicable standards including the N.C. Fire Code.

#### **WASTEWATER**

The developer proposes to connect to the Eagle Creek wastewater treatment plant (WWTP). Currituck Water and Sewer, LLC (CWS) Major Utility operates the WWTP and is the process of acquiring ownership of the entire wastewater system (collection systems, treatment plant, etc.) from Sandler Utilities Company. A force main is being installed to connect the collection system in Fost development to the Eagle Creek WWTP and the this development will tap into that line. A wastewater collection system will be constructed by the developer for the entire development and managed by Currituck Water and Sewer. The utility will be regulated by the North Carolina Utilities Commission and the North Carolina Department of Environmental Quality. CWS has begun the process with state regulatory agencies for this transference.

#### **STAFF REVIEW**

#### **TECHNICAL REVIEW COMMITTEE**

The Technical Review Committee (TRC) reviewed the application and provided the following comments:

- 1. The applicant has not demonstrated that the proposed use will meet the special use permit review standards of the UDO including adequate public facilities (LUP Policy PP2 Adequate Public Facilities) and public health and safety (LUP Purpose of the Plan).
- 2. Noted plan deficiencies:
  - a. The adjoining property owner information is missing from the most recent version of the plat.
  - b. Residual tract note and references must be removed from the plat since there is not a residual tract.

#### Other staff concerns:

- 1. Staff has concerns about allowing additional lots from other subdivisions to connect to the Eagle Creek WWTP with the known performance problems of the plant. On August 10, 2021, NCDEQ Division of Water Resources issued a Wastewater Collection Extension Permit for 60,000 gallons per day for 92 four-bedroom homes and 44 three-bedroom townhouses for Fost Phase 1A. On September 17, 2021, NCDEQ Division of Water Resources issued a letter stating "the WWTP must demonstrate consistent performance achieving permit treatment requirements, prior to submittal of a sewer extension permit application. WWTP performance must be satisfactory for approval of any potential sewer extension permits." At this time, County staff has not received notification that these permit requirements have been met and a sewer extension permit for additional lots will be issued. (LUP Purpose of the Plan public health, safety, and welfare)
- 2. Although the Flora collection system will not connect to the Eagle Creek collection system, there are concerns from staff about the impact of the Consent Judgement on the Major Utility. There continues to be issues with the collection system at Eagle Creek. Untreated wastewater system backups into the homes of customers and mandatory water conservation efforts placed on Eagle Creek homeowners because of problems with the collection system. Please see the Consent Judgement issued by the Superior Court on June 23, 2021 attached to this staff report for more detailed information (North Carolina Division of Environmental Quality v. Sandler

PB 18-23 Fost Tract, Phases 6-9 Preliminary Plat/Special Use Permit Page 4 of 10 Utilities at Mill Run, LLC). The Court ordered Sandler Utilities at Mill Run, LLC to complete a system upgrade plan, and then a timetable to implement the upgrade plan. The timetable to implement the upgrade plan has not been shared by NCDEQ DWR staff due to the legal proceedings. For the protection of the existing users of the system, staff recommends the system be fully repaired and functioning properly as certified by NCDEQ for the Eagle Creek residents before other subdivisions are allowed to connect. (LUP Purpose of the Plan – public health, safety, and welfare)

#### 2006 Land Use Plan

The 2006 Land Use Plan classifies the site as Full Service within the Moyock subarea. At its March 1, 2021, meeting, the BOC determined this development was compatible with the Land Use Plan Policies HN3 and CD1 and that the request is reasonable and in the public interest because the increase in the number of residential roof tops may bring additional commercial development to the Moyock area.

#### Moyock Small Area Plan

The Moyock Small Area Plan classifies the site as Limited Service and Full Service. The policy emphasis for Full Service in Moyock is to provide focal points in the community where high amounts of activity occur. Both residential and commercial components will be present in Full Service areas. Cluster or planned commercial and residential areas with diversity in housing types is preferred. The policy emphasis for Limited Service designations are less intensely developed than Full Service. Emphasis is more on residential development and densities. Limited Service designation has reduced public services such as fire protection, emergency service, recreation, and public water.

#### **SPECIAL USE PERMIT REVIEW STANDARDS**

Following an evidentiary hearing, the board shall decide if the application is in accordance with Section 2.3.10, Decision-Making Body Review and Decision, and Section 2.4.6.D, Special Use Permit Review Standards. It is the burden of the applicant to provide evident to address the Review Standards. The applicant's findings can be found in the attached application.

#### Special Use Permit Review Standards

A special use permit shall be approved on a finding that the applicant demonstrates the proposed use will:

- 1. Not endanger the public health or safety.
- 2. Not injure the value of adjoining or abutting lands and will be in harmony with the area in which it is located.
- 3. Be in conformity with the Land Use Plan or other officially adopted plan.
- 4. Not exceed the county's ability to provide adequate public facilities, including but not limited to, schools, fire and rescue, law enforcement, and other county facilities. Applicable state standards and guidelines shall be followed for determining when public facilities are adequate.

THE APPLICATION AND RELATED MATERIALS ARE AVAILABLE ON THE COUNTY'S WEBSITE Board of Commissioners: <a href="https://www.co.currituck.nc.us/board-of-commissioners-minutes-current.cfm">www.co.currituck.nc.us/board-of-commissioners-minutes-current.cfm</a>



PB 21-25 Flora Farms Preliminary Plat/SUP Context Map

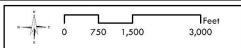


Currituck

Currituck County Planning and Community Development



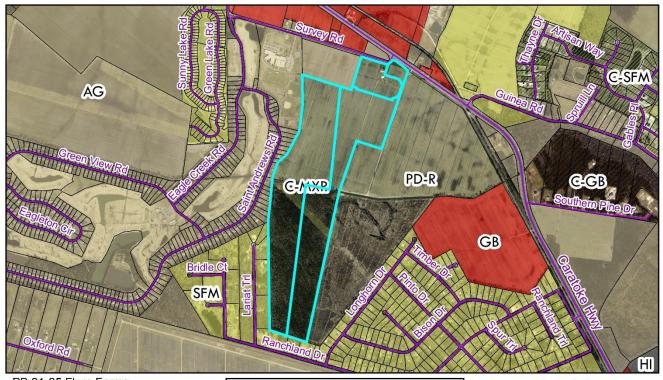
PB 21-25 Flora Farms Preliminary Plat/SUP 2020 Aerial Photography



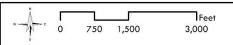
Currituck

Currituck County Planning and Community Development

PB 18-23 Fost Tract, Phases 6-9 Preliminary Plat/Special Use Permit Page 6 of 10



PB 21-25 Flora Farms Preliminary Plat/SUP Zoning

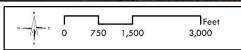




Currituck County Planning and Community Development



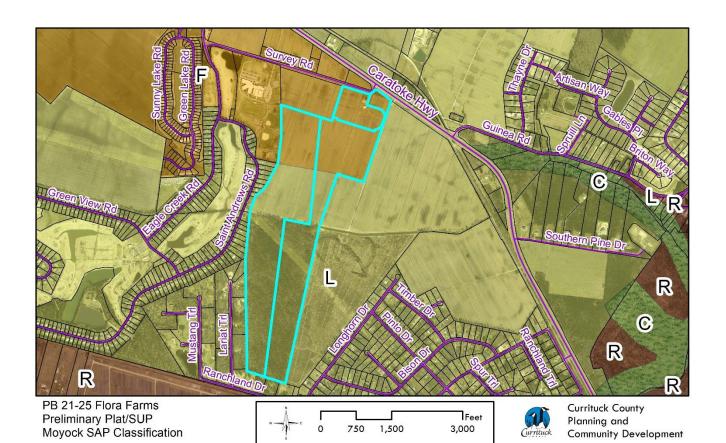
PB 21-25 Flora Farms Preliminary Plat/SUP 2006 LUP Classification

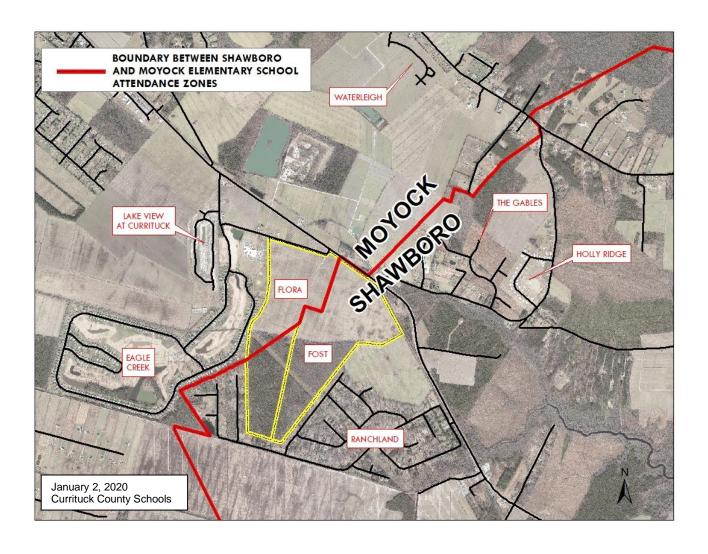


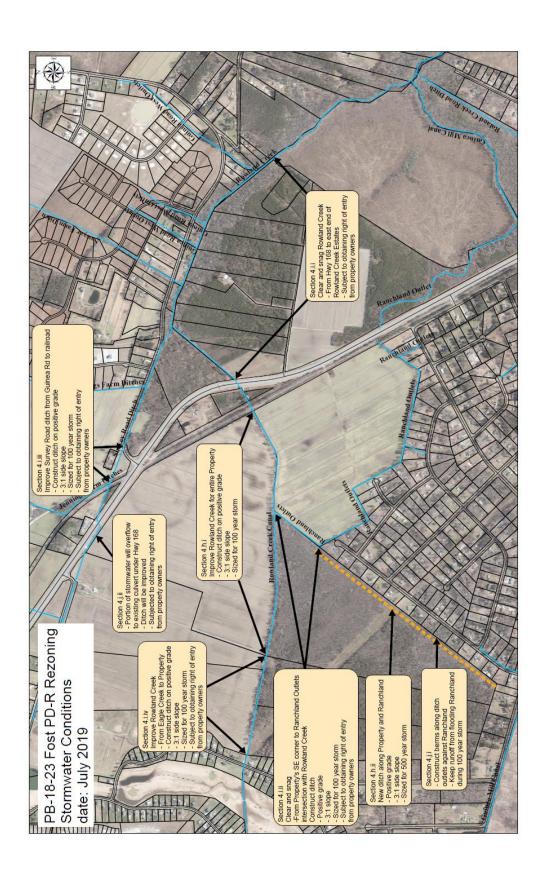
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Currituck County Planning and Community Development

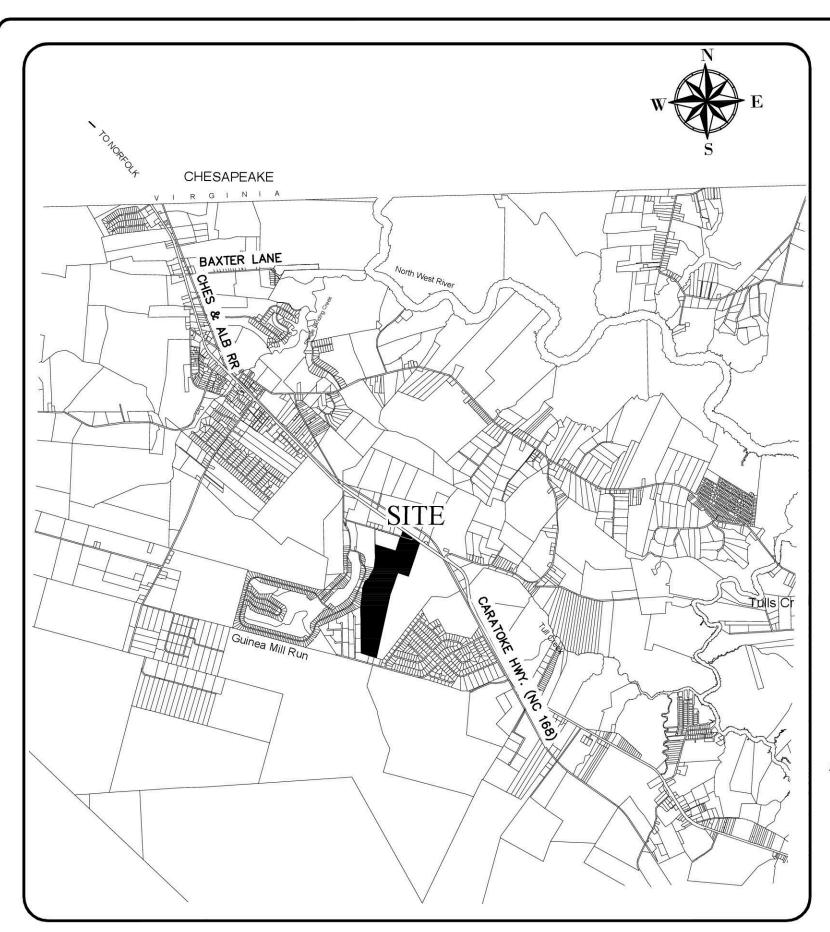
PB 18-23 Fost Tract, Phases 6-9 Preliminary Plat/Special Use Permit Page 7 of 10







PB 18-23 Fost Tract, Phases 6-9 Preliminary Plat/Special Use Permit Page 10 of 10



# VICINITY MAP SCALE: 1" = 5000'

OWNERSHIP AND DEDICATION CERTIFICATE

I HEREBY CERTIFY THAT I AM THE OWNER OF THE PROPERTY DESCRIBED HEREON,
WHICH PROPERTY IS LOCATED WITHIN THE SUBDIVISION REGULATION JURISDICTION
OF CURRITUCK COUNTY, THAT I HEREBY FREELY ADOPT THIS PLAT OF SUBDIVISION
AND DEDICATE TO PUBLIC USE ALL AREA SHOWN ON THIS PLAT AS STREETS,
UTILITIES, ALLEYS, WALKS, RECREATION AND PARKS, OPEN SPACE AND EASEMENTS,
EXCEPT THOSE SPECIFICALLY INDICATED AS PRIVATE AND THAT I WILL MAINTAIN
ALL SUCH AREAS UNTIL THE OFFER OF DEDICATION IS ACCEPTED BY THE
APPROPRIATE PUBLIC AUTHORITY OR HOME OWNERS' ASSOCIATION. ALL PROPERTY
SHOWN ON THIS PLAT AS DEDICATED FOR A PUBLIC USE SHALL BE DEEMED TO BE
DEDICATED FOR ANY OTHER PUBLIC USE AUTHORIZED BY LAW WHEN SUCH USE IS

DATE	OWNER
l	, A NOTARY PUBLIC
OF	COUNTY, NORTH CAROLINA, DO HEREBY CERTIFY THAT
APPEARED BEFORE MITTHE FOREGOING CERT	PERSONAL E THIS DATE AND ACKNOWLEDGED THE DUE EXECUTION OF IFICATE.
WITNESS MY HAND AN 20	ND OFFICIAL SEAL THIS DAY OF
	NOTARY PUBLIC

APPROVED BY THE APPROPRIATE PUBLIC AUTHORITY IN THE PUBLIC INTEREST.

#### PRIVATE STREETS OWNER CERTIFICATE

I HEREBY CERTIFY THAT THE PRIVATE STREETS SHOWN ON THIS PLAT ARE INTENDED FOR PRIVATE USE AND WILL REMAIN UNDER THE CONTROL, MAINTENANCE, AND RESPONSIBILITY OF THE DEVELOPER AND/OR A HOMEOWNER'S ASSOCIATION AND ACKNOWLEDGE THAT SOME PUBLIC SERVICES MAY NOT BE PROVIDED DUE TO THE PRIVATE NATURE OF THE ROAD(S).

ER

JRVEYOR'S CERTIFICATION

I, MICHAEL D. BARR, CERTIFY THAT THIS PLAT WAS DRAWN UNDER MY SUPERVISION FROM AN ACTUAL SURVEY MADE UNDER MY SUPERVISION (DEED AND DESCRIPTION RECORDED IN BOOKS REFERENCED); THAT THE BOUNDARIES NOT SURVEYED ARE CLEARLY INDICATED AS DRAWN FROM INFORMATION FOUND IN BOOKS REFERENCED; THAT THE RATIO OF PRECISION AS CALCULATED IS 1:10,000; THAT THIS PLAT WAS PREPARED IN ACCORDANCE WITH G.S. 47-30 AS AMENDED.

THIS IS TO CERTIFY THAT THIS SURVEY CREATES A SUBDIVISION OF LAND WITHIN THE AREA OF A COUNTY OR MUNICIPALITY THAT HAS AN ORDINANCE THAT REGULATES

WITNESS MY ORIGINAL SIGNATURE, REGISTRATION NUMBER AND SEAL THIS  $\underline{01}_{-}$ 

DAY OF DECEMBER A.D., 2021.

SIGNATURE L-1756

# FLORA FARM MXR DEVELOPMENT

# MOYOCK TOWNSHIP CURRITUCK COUNTY NORTH CAROLINA PRELIMINARY PLAT FOR PHASES 1-5

# GENERAL DEVELOPMENT NOTES PROPERTY OWNERS: JOHN J. FLORA III MARY NELL FLORA BRUMSEY P.O. BOX 369 117 PUDDIN RIDGE ROAD MOYOCK, NC 27958 MOYOCK, NC 27958

2. APPLICANT: NORTH SOUTH DEVELOPMENT GROUP, LLC 417-D CARATOKE HIGHWAY MOYOCK, NC 27958

PROPERTY DATA: ADDRESS: CARATOKE HIGHWAY, MOYOCK, NC 27958
PIN: 0015-000-085C-0000

D.B. 1230, PG. 402
P.C. "Q". SL. 149
14.80-ACRES (PER PLAT)

PIN: 0015-000-0085B-0000
D.B. 1230, PG. 398 & 402
P.C. "D", SL. 315
94.53-ACRES (PER GIS)

PIN: 0015-000-085A-0000

D.B. 1230, PG. 398 & 402 P.C. "D", SL. 315 111.78-ACRES (PER GIS)

SUBDIVISION TOTAL ACREAGE: 202.44 ACRES
(DOES NOT INCLUDE 1.47 ACRE R/W DEDICATION OF A 30' STRIP ALONG SURVEY ROAD)

4. ZONING: EXISTING: MIXED RESIDENTIAL (C-MXR)

I.R.M. DATA:

ZONE X PER F.I.R.M. MAP NOS. 3721803100 K & 3721803000 K, BOTHL HAVING AN EFFECTIVE
DATE DECEMBER 21, 2018. USE OF LAND WITHIN A FLOODWAY OR FLOOD PLAIN IS
SUBSTANTIALLY RESTRICTED BY CHAPTER 7 OF THE CURRITUCK COUNTY UNIFIED DEVELOPMENT
ORDINANCE

6. THIS PROPERTY CONTAINS ACOE "404' JURISDICTIONAL WETLANDS AS SHOWN AND CONFIRMED BY USACOE ACTION ID# SAW 2018-00838, DATED APRIL 30, 2018, AND MAY REQUIRE U.S. CORP OF ENGINEERS APPROVAL PRIOR TO DEVELOPMENT OF THE PROPERTY.

7. A 10' EASEMENT FOR UTILITIES AND DRAINAGE ALONG REAR AND SIDE PROPERTY LINES AND A 15'

EASEMENT FOR UTILITIES AND DRAINAGE ALONG FRONT PROPERTY LINE IS HEREBY ESTABLISHED.

AND STREET TREE EASEMENTS ARE HEREBY PROVIDED PER TYPICAL LOT DETAIL ON SHEET 19.

8. A 25' DRAINAGE EASEMENT SHALL BE ESTABLISHED ALONG ALL MAJOR DRAINAGE WAYS SERVING
MORE THAN 5 ACRES A BLANKET DRAINAGE UTILITY AND PEDESTRIAN ACCESS FASEMENT IS

MORE THAN 5 ACRES. A BLANKET DRAINAGE, UTILITY, AND PEDESTRIAN ACCESS EASEMENT IS HEREBY ESTABLISHED ACROSS ALL OPEN SPACE AREAS. A 25' DRAINAGE EASEMENT IS HEREBY ESTABLISHED ALONG ROWLAND CREEK CANAL (SEE SHEET 7 FOR DETAIL). ALL DRAINAGE EASEMENTS SHALL BE DEDICATED TO CURRITUCK COUNTY.

9. EXISTING CONDITION INFORMATION BASED ON A COMBINATION OF THE FOLLOWING: 2012 AERIAL IMAGERY OBTAINED FROM NCONEMAP.COM FIELD TOPOGRAPHIC SURVEY DATA BY BISSELL PROFESSIONAL GROUP. ELEVATIONS ARE REFERENCED TO NAVD 1988 VERTICAL DATUM.

10. SECTION 7.6.5 OF THE CURRITUCK U.D.O. SUBSTANTIALLY RESTRICTS DEVELOPMENT WITHIN A 30' RIPARIAN BUFFER.

11. ALL UTILITIES ARE TO BE UNDERGROUND.

12. NEAREST HYDRANT (MCH15) FLOW TESTED AT 1,146 GPM. DESIGN FIRE FLOW IS 1,000 GPM FOR SINGLE FAMILY RESIDENCES BASED ON STRUCTURES HAVING LESS THAN 4,800 SQ. FT. AND NOT OVER 2-STORIES HIGH. NFF FOR MULTI-FAMILY STRUCTURES IS BASED ON ISO STANDARDS.

13. THERE IS A 50' FARM BUFFER IN ALL AREAS ADJOINING ACTIVE FARM LANDS.

#### LOT DEVELOPMENT CONFIGURATION: LOT AREAS: VARY FROM 15,000 TO 23,167 SQUARE FEET.

RECREATIONAL AND PARK LAND FEE-IN-LIEU CALCULATION:

REQUIRED UPLAND RECREATION/PARKLAND DEDICATION ACREAGE = 7.06 AC. (277 LOTS \* 0.0255AC./LOT)

CURRENT PROPERTY ACCESSED VALUE (LAND ONLY) = \$1,392,100 OR \$6876.61/AC. (\$1,392,100/202.44 AC.)

FEE-IN-LIEU IS \$48,548.87 (7.06 AC. \* \$6876.61/AC.)

SEE SHEET 7 FOR PROPOSED RECREATION AND PARK LAND DEDICATION.

STREET CONNECTIVITY INDEX = 31 LINKS/22 NODES = 1.41 (REQUIRED = 1.40)

#### DEVELOPMENT NOTES: TRACT SUMMARY:

LINEAR FEET OF ONSITE ROADWAY:

TOTAL AREA OF TRACT:

DEVELOPMENT AREA SUMMARY:
PROPOSED LOTS AREA:
PROPOSED COMMERCIAL AREA:
PUBLIC PROPOSED R/W AREA:
REQUIRED RESIDENTIAL OPEN SPACE = 30% (0.3\*200.14):
RESIDENTIAL OPEN SPACE PROVIDED:
REQUIRED COMMERCIAL OPEN SPACE = 10% (0.1\*2.30):
COMMERCIAL OPEN SPACE PROVIDED:
1.07 AC. (46.3%)

21,463 L.F.±

# OF PROPOSED LOTS:

PROPOSED RIGHT-OF-WAY WIDTHS (FT.): SEE SHEETS 20
PROPOSED PAVED ROADWAY WIDTH: SEE SHEETS 20

0.14): 60.04 AC. 74.92 AC. (37.0%) ): 0.23 AC. 1.07 AC. (46.3%) 277 (1.37 LOTS/ACRE)

Leonard A Section &	RIGHT-OF-WAT
	PROPERTY BOUNDARY
	ADJOINING PROPERTY LINE
	— — EASEMENT LINE
	CENTERLINE OF ROADWAY
•	SET IRON ROD
0	EXISTING IRON ROD
0	EXISTING IRON PIPE
•	EXISTING CONCRETE MONUMENT
N/F	NOW OR FORMERLY
TWP.	TOWNSHIP
P.C.	PLAT CABINET
SL.	SLIDE
D.B.	DEED BOOK
M.B.	MAP BOOK
PG.	PAGE
SQ.FT. or S.F.	SQUARE FEET
M.B.L.	MAXIMUM BUILDING LINE
AC or AC.	ACRES
P/0	PART OF
TYP.	TYPICAL
N.T.S.	NOT TO SCALE
0.S.	OPEN SPACE
R/W	RIGHT-OF-WAY

SURVEY LEGEND

## Table of Contents

Sheet Title		
COVER SHEET, DEVELOPMENT NOTES & SITE LOCATION		
EXISTING CONDITIONS & SITE FEATURES PLAN		
PRELIMINARY PHASING PLAN		
DRAINAGE & UTILITY OVERVIEW-PRELIMINARY PLAN		
HARDSCAPING OVERVIEW PRELIMINARY PLAN		
LANDSCAPING, LIGHTING, BUFFERING & SIGNAGE PLAN		
SUBDIVISION PRELIMINARY PLAT OVERVIEW		
PLAT SHEET 8 PRELIMINARY PLAT		
PLAT SHEET 9 PRELIMINARY PLAT		
PLAT SHEET 10 PRELIMINARY PLAT		
PLAT SHEET 11 PRELIMINARY PLAT		
PLAT SHEET 12 PRELIMINARY PLAT		
PLAT SHEET 13 PRELIMINARY PLAT		
PLAT SHEET 14 PRELIMINARY PLAT		
PLAT SHEET 15 PRELIMINARY PLAT		
PLAT SHEET 16 PRELIMINARY PLAT		
PRELIMINARY PLAN - COMMERCIAL		
ROADWAY & DRAINAGE CONSTRUCTION DETAILS		
GENERAL DETAILS		

LOT DESCRIPTIONS CHARTS AND TABLES

LEGEND	
	EXISTING DITCH CENTERLINE
	EXISTING DITCH TOP OF BANK
	PROPOSED SWALE W/ FLOW ARROW
	PROPOSED SWALE HIGH POINT
FEMA-FEMA-	FEMA BOUNDARY LINE
	EXISTING 404 BOUNDARY
6	EXISTING GRADE CONTOUR
	30' UNDISTURBED BUFFER (COUNTY)
	50' WETLANDS BUFFER (COUNTY)
	EXISTING CULVERT
Q	EXISTING UTILITY POLE
OHE OHE	EXISTING OVERHEAD TRANSMISSION LINES
EWL	EXISTING WATER LINE
WL	PROPOSED WATER LINE (SIZE AS NOTED)
<b>&gt;</b>	PROPOSED FIRE HYDRANT ASSEMBLY
-	PROPOSED WATER SERVICE
<b>■→←</b>	PROPOSED BLOW-OFF ASSEMBLY
H	PROPOSED VALVE
◀	PROPOSED REDUCER
	PROPOSED SIDEWALK
	PROPOSED FORCE MAIN SANITARY SEWER (SIZE AS NOTED)
SS	PROPOSED GRAVITY SANITARY SEWER (SIZE AS NOTED)
<b>S</b>	PROPOSED SANITARY SEWER MANHOLE
BOC	BACK OF CURB
EOP	EDGE OF PAVEMENT
D	PROPOSED CATCH BASIN
<b>\rightarrow</b>	PROPOSED STREET LIGHT
	PROPOSED STORM SEWER PIPE

PROPOSED EDGE OF WATER

STORMWATER STATEMENT

NO MORE THAN 30% OF ANY LOT SHALL BE COVERED BY IMPERVIOUS STRUCTURES AND MATERIALS, INCLUDING ASPHALT, GRAVEL, CONCRETE, BRICK STONE, SLATE, OR SIMILAR MATERIAL, NOT INCLUDING WOOD DECKING OR THE WATER SURFACE OF SWIMMING POOLS. THIS COVENANT IS INTENDED TO ENSURE COMPLIANCE WITH THE STORMWATER PERMIT NUMBER \_\_\_\_\_\_\_ ISSUED BY THE STATE OF NORTH CAROLINA. THE COVENANT MAY NOT BE CHANGED OR DELETED WITHOUT

THE CONSENT OF THE STATE. FILLING IN OR PIPING OF ANY VEGETATIVE CONVEYANCES (DITCHES, SWALES, ETC.) ASSOCIATED WITH THIS DEVELOPMENT, EXCEPT FOR AVERAGE DRIVEWAY CROSSINGS, IS STRICTLY PROHIBITED BY ANY PERSON. THE LOT COVERAGE ALLOWANCE PROVIDED IN THE CURRITUCK COUNTY UNIFIED DEVELOPMENT ORDINANCE MAY BE DIFFERENT THAN THE NC STATE STORMWATER PERMIT. THE MOST RESTRICTIVE LOT COVERAGE SHALL APPLY.

#### REVIEW OFFICER CERTIFICATE STATE OF NORTH CAROLINA

COUNTY OF CURRITUCK

I,\_\_\_\_\_\_, REVIEW OFFICER OF CURRITUCK COUNTY, CERTIFY THAT THE MAP OR PLAT TO WHICH THIS CERTIFICATION IS AFFIXED MEETS ALL STATUTORY REQUIREMENTS FOR RECORDING.

DATE REVIEW OFFICER

PUBLIC STREETS DIVISION OF HIGHWAY DISTRICT ENGINEER CERTIFICATE

I HEREBY CERTIFY THAT THE PUBLIC STREETS SHOWN ON THIS PLAT ARE INTENDED FOR DEDICATION AND HAVE BEEN DESIGNED OR COMPLETED IN ACCORDANCE WITH AT LEAST THE MINIMUM SPECIFICATIONS AND STANDARDS OF THE NC DEPARTMENT OF TRANSPORTATION FOR ACCEPTANCE OF SUBDIVISION STREETS ON THE NC HIGHWAY SYSTEM FOR MAINTENANCE.

DATE DISTRICT ENGINEER

#### APPROVAL CERTIFICA

I HEREBY CERTIFY THAT THE SUBDIVISION SHOWN ON THIS PLAT IS IN ALL RESPECTS IN COMPLIANCE WITH THE CURRITUCK COUNTY UNIFIED DEVELOPMENT ORDINANCE AND, THEREFORE, THIS PLAT HAS BEEN APPROVED BY THE CURRITUCK COUNTY TECHNICAL REVIEW COMMITTEE AND SIGNED BY THE ADMINISTRATOR, SUBJECT TO ITS BEING RECORDED IN THE CURRITUCK COUNTY REGISTRY WITHIN 90 DAYS OF THE DATE BELOW.

DATE ADMINISTRATOR

# REVIEW OFFICER CERTIFICATE STATE OF NORTH CAROLINA COUNTY OF CURRITUCK

CURRITUCK COUNTY, CERTIFY THAT THE MAP OR PLAT TO WHICH THIS CERTIFICATION IS AFFIXED MEETS ALL STATUTORY REQUIREMENTS FOR RECORDING.

DATE REVIEW OFFICER

#### REQUIRED IMPROVEMENTS CERTIFICATE

I HEREBY CERTIFY THAT ALL IMPROVEMENTS REQUIRED BY THE CURRITUCK COUNTY UNIFIED DEVELOPMENT ORDINANCE HAVE BEEN INSTALLED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS PREPARED BY BISSELL PROFESSIONAL GROUP, AND SAID IMPROVEMENTS COMPLY WITH CURRITUCK COUNTY SPECIFICATIONS.

DATE

REGISTERED LAND SURVEYOR/ENGINEER

REGISTRATION NUMBER

ATION

BPG, INC. OF KITTY

PROFESSIONAL GROUP

Firm License # C-956
3512 North Croatan Highway
P.O. Box 1068
Kitty Hawk, North Carolina 27949
(252) 261-3266
FAX (252) 261-1760

THIS DOCUMENT IS THE SOLE PROPERTY OF BPG, HAWK, NORTH CAROLINA. THE REPRODUCTION, PART, OR THE MODIFICATION OF ANY DETAIL OR IT THE EXAMPLES WITHOUT THE EXAMPLES.

MINARY SUBDIVISION PLAN

PROJECT:

BY

FLORA

NOYOCK TOWNSHIP

SING

FW

MOYOCK TOWNSHIP

NO. DATE DESCRIPTION

10 /22 /21 Pre-App Conference Comments
10 /27 /21 ADD ADJOINERS & UPDATE PHASING
15 1. 30 /21 ADD ADJOINERS & UPDATE PHASING
15 1. 30 /21 ADD ADJOINERS & UPDATE PHASING
15 1. 30 /21 ADD ADJOINERS & UPDATE PHASING
16 1. 30 /21 ADD ADJOINERS & UPDATE PHASING
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10 /21 ADD ADJOINERS & UPDATE PHASING

SEAL

L-1756

SURVE

DATE:

06/09/21

N/A

DATE:

06/09/21

N/A

DESIGNED:

BPG

MSB

DRAWN:

KFW

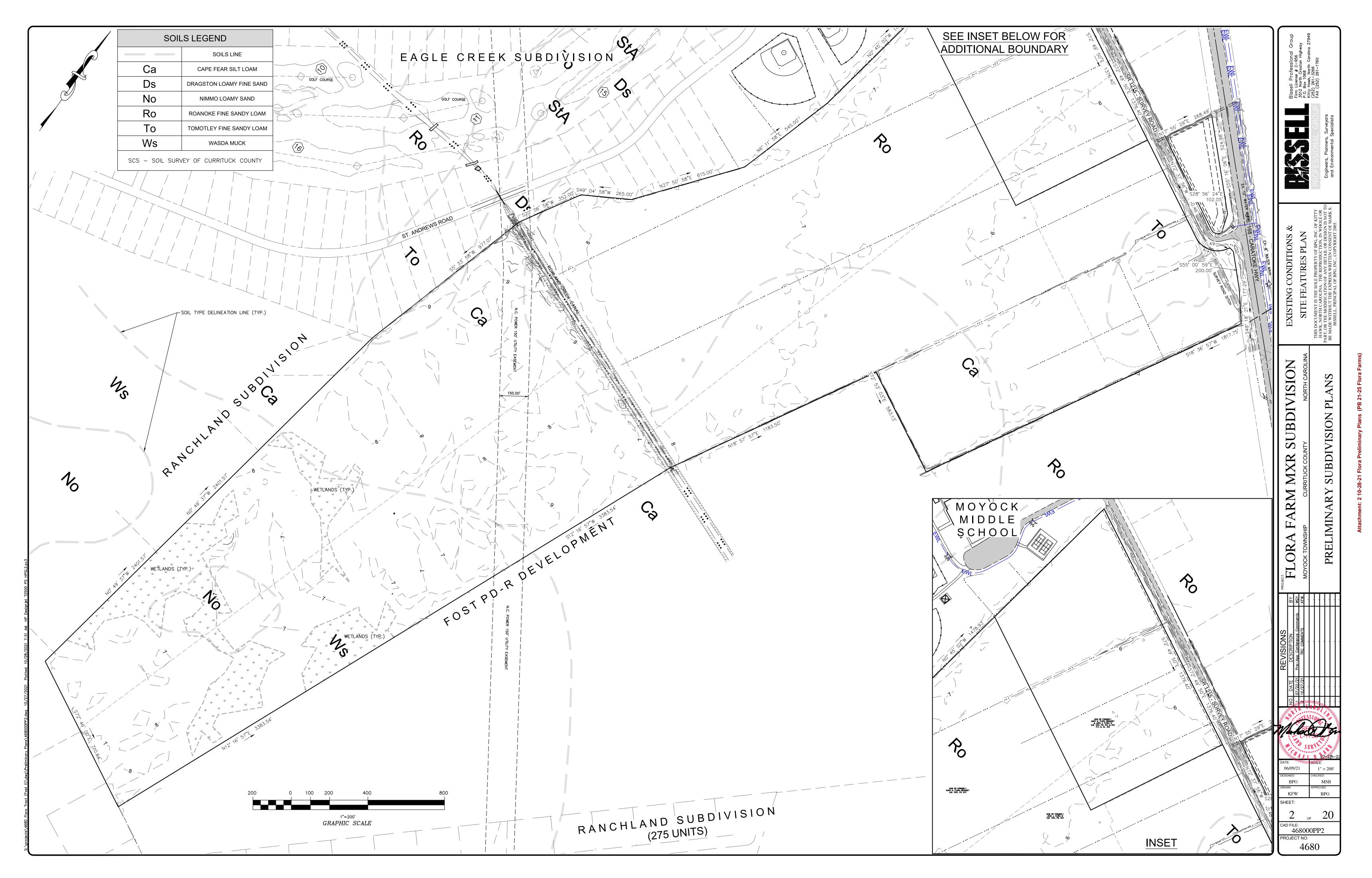
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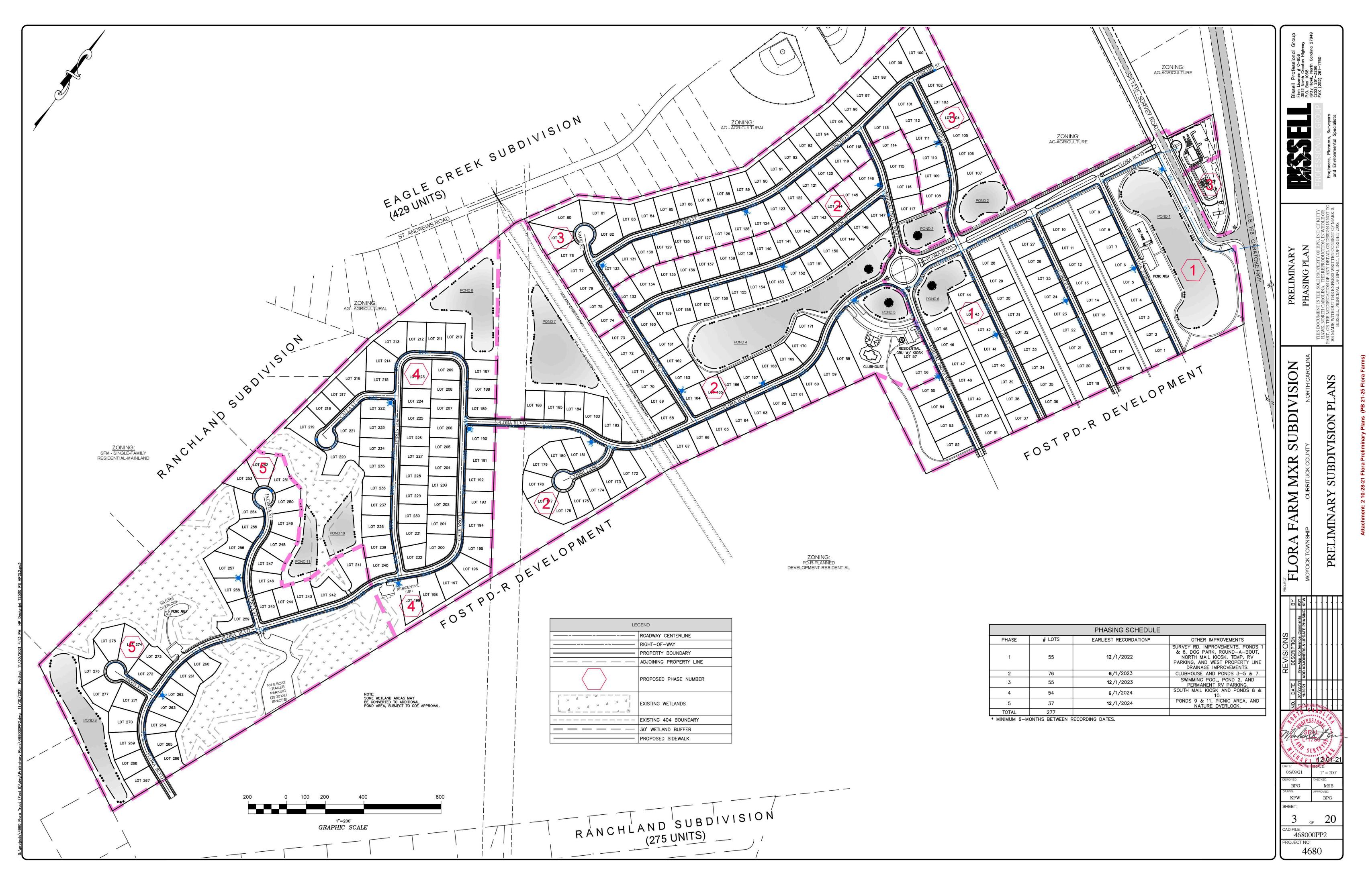
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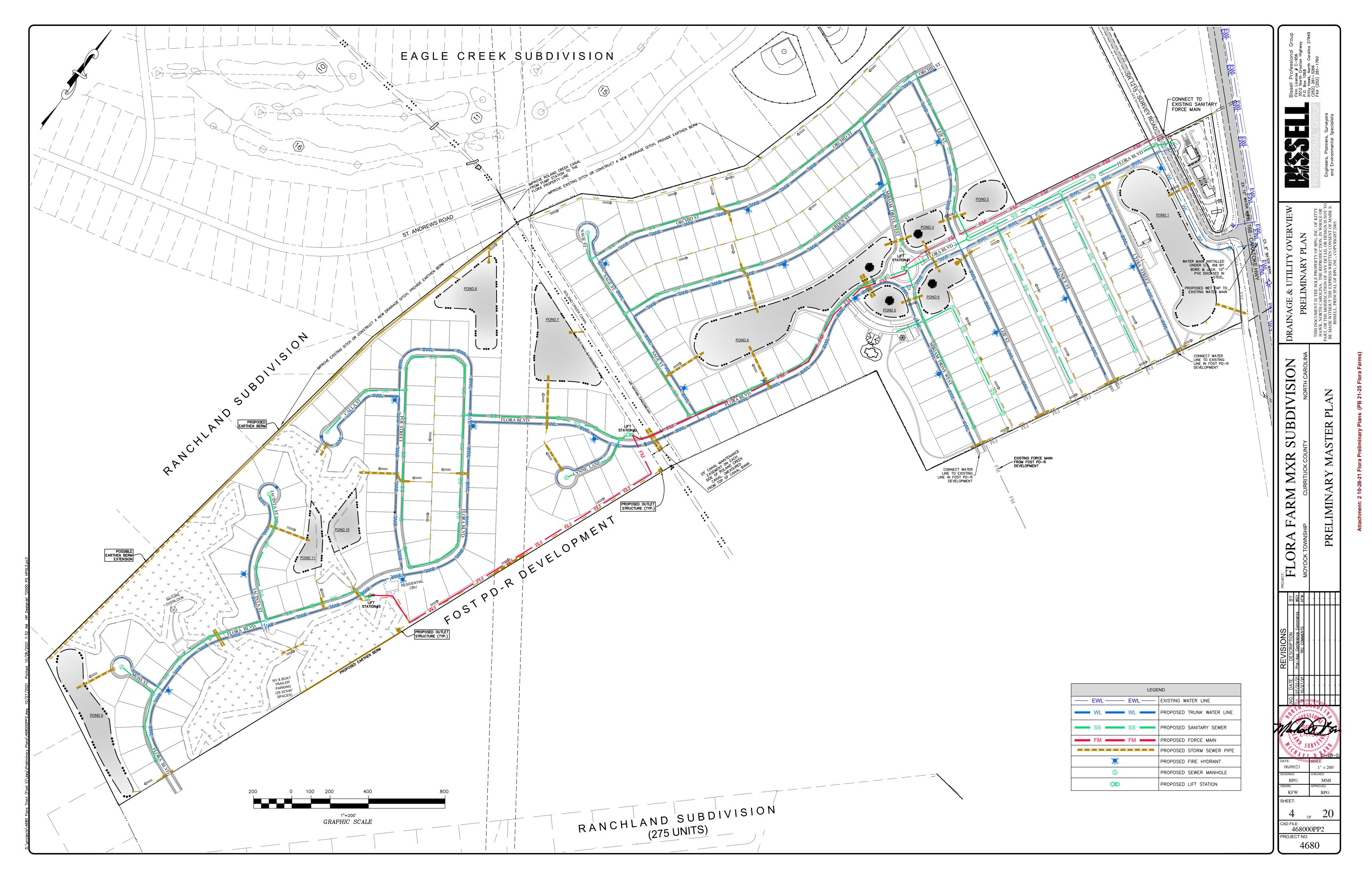
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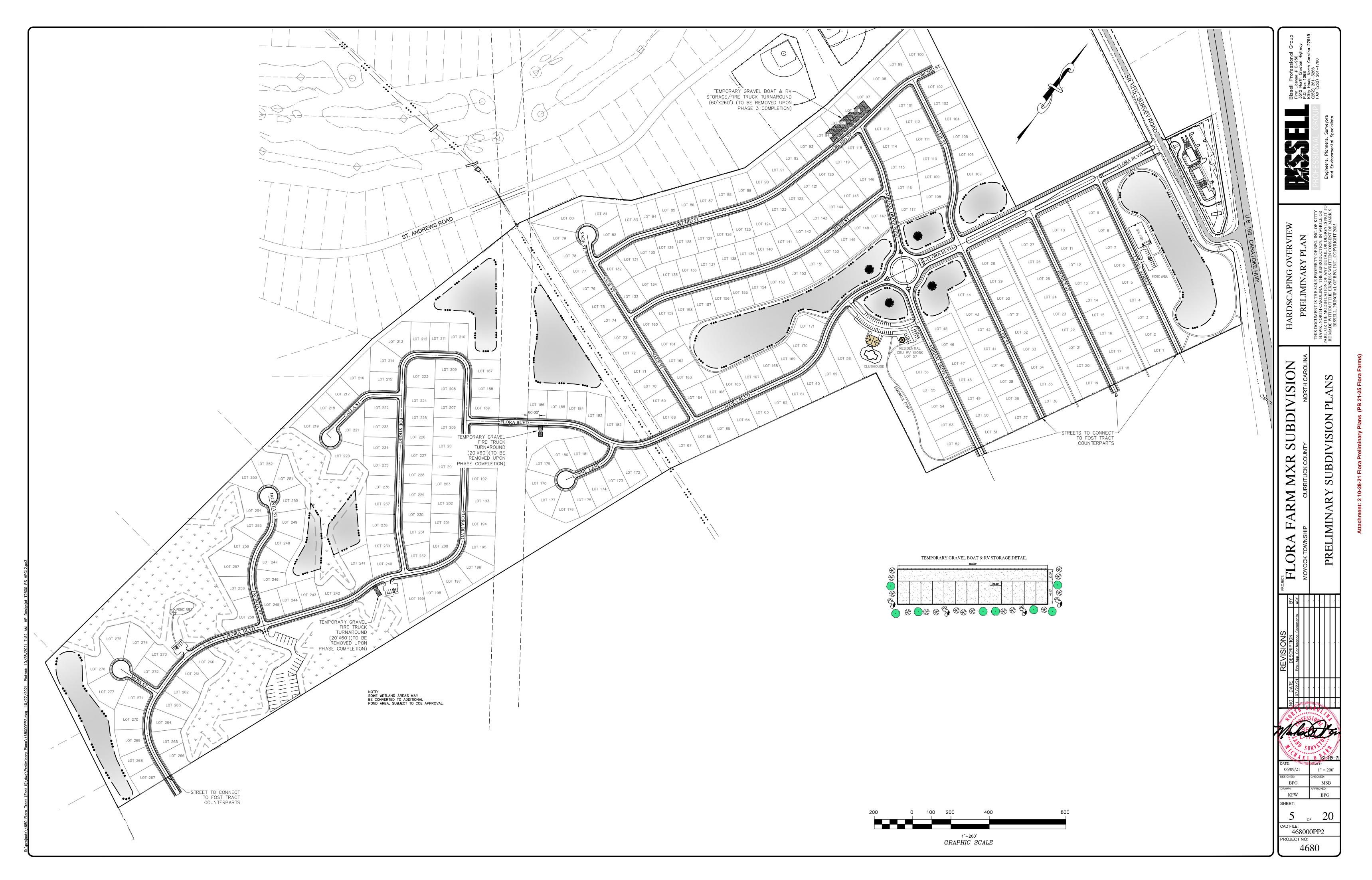
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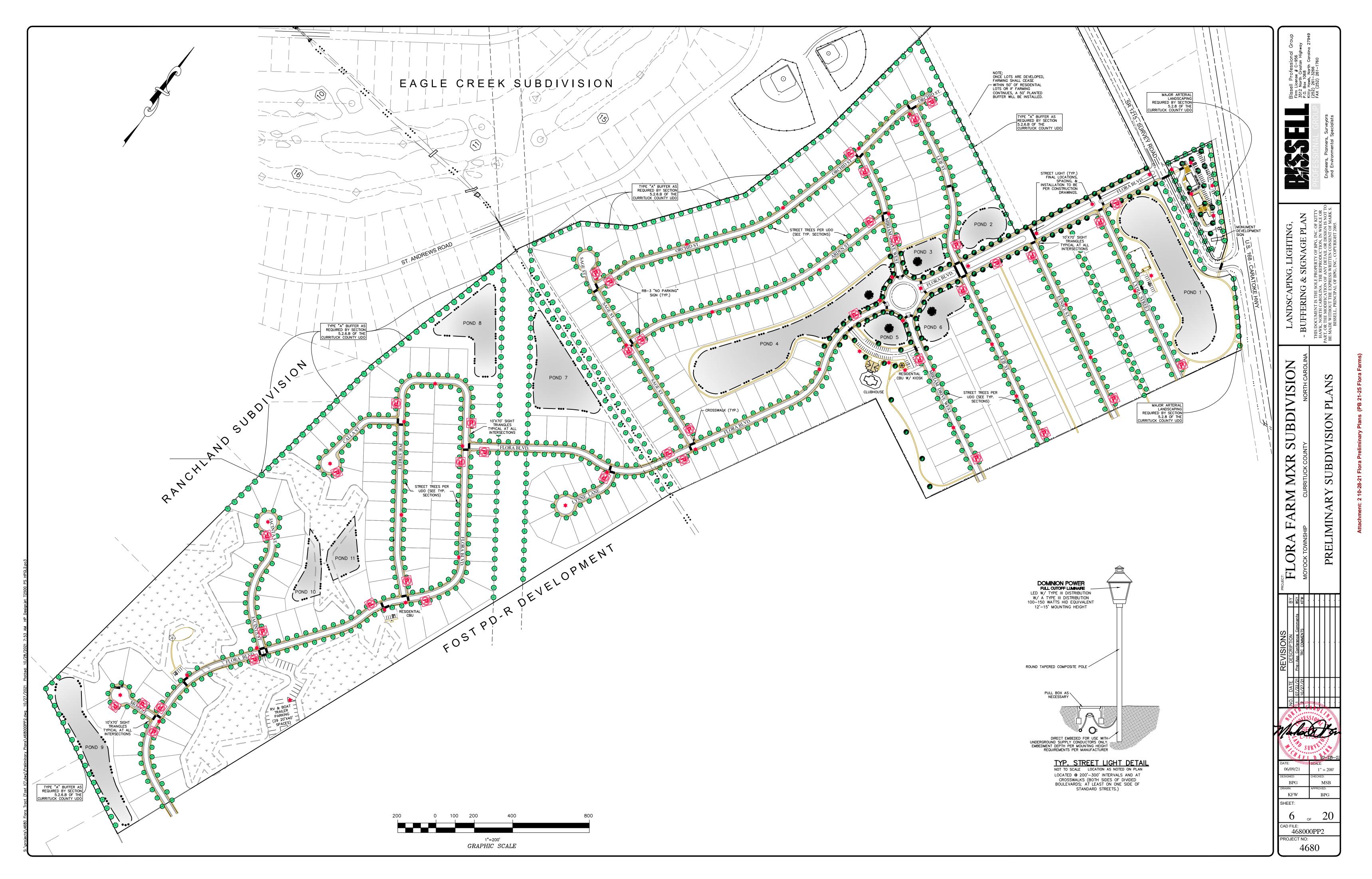
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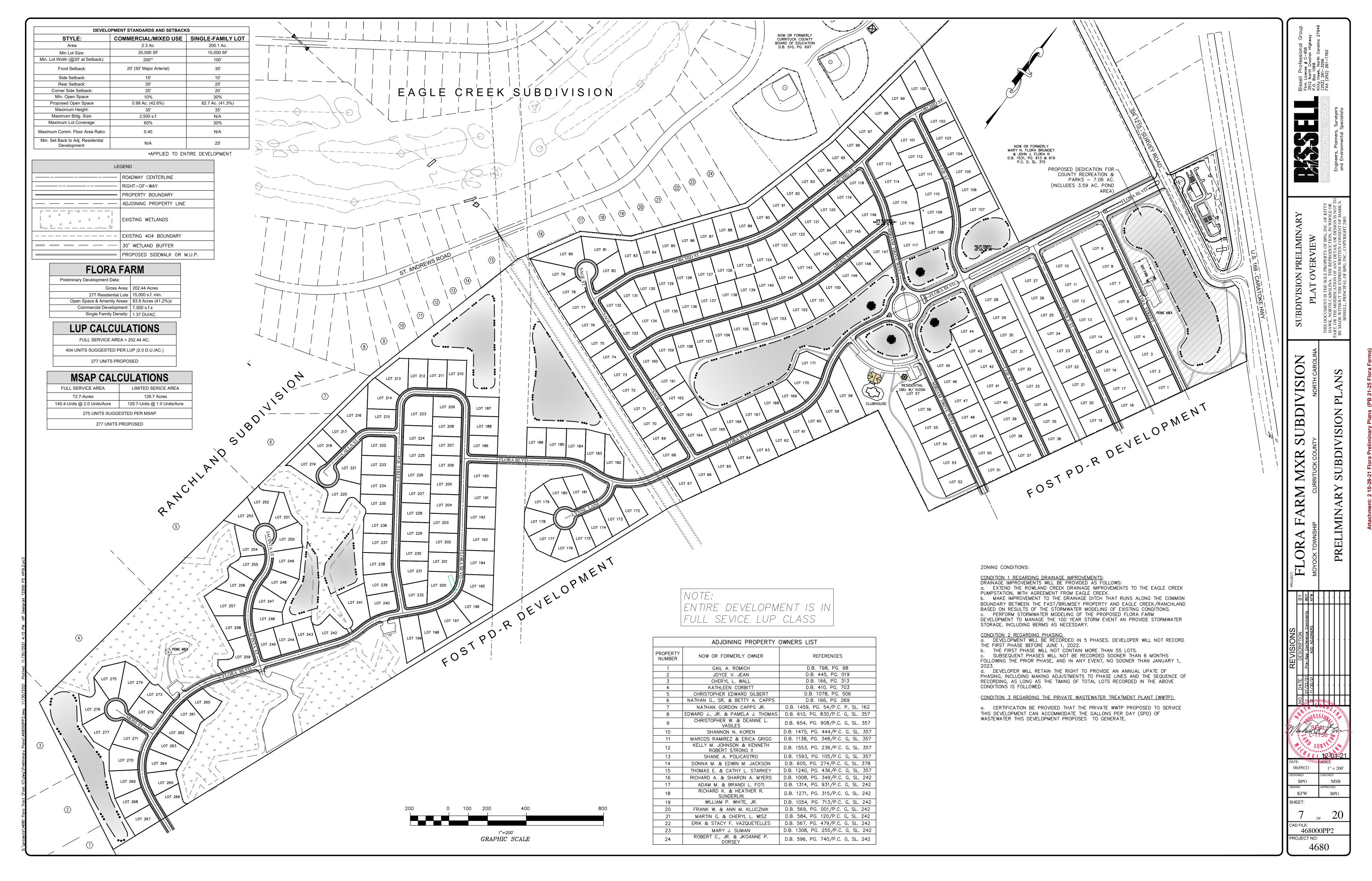


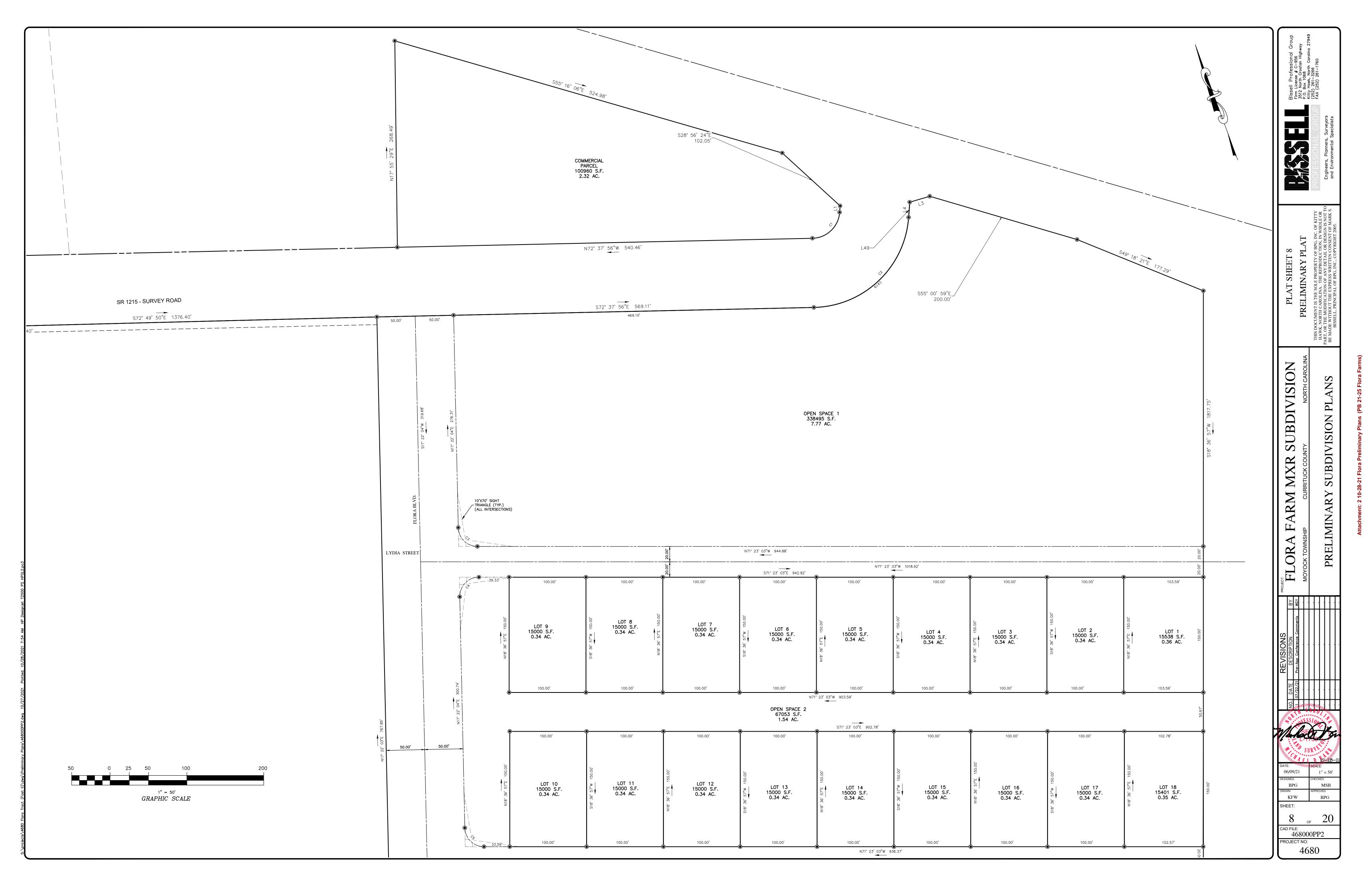


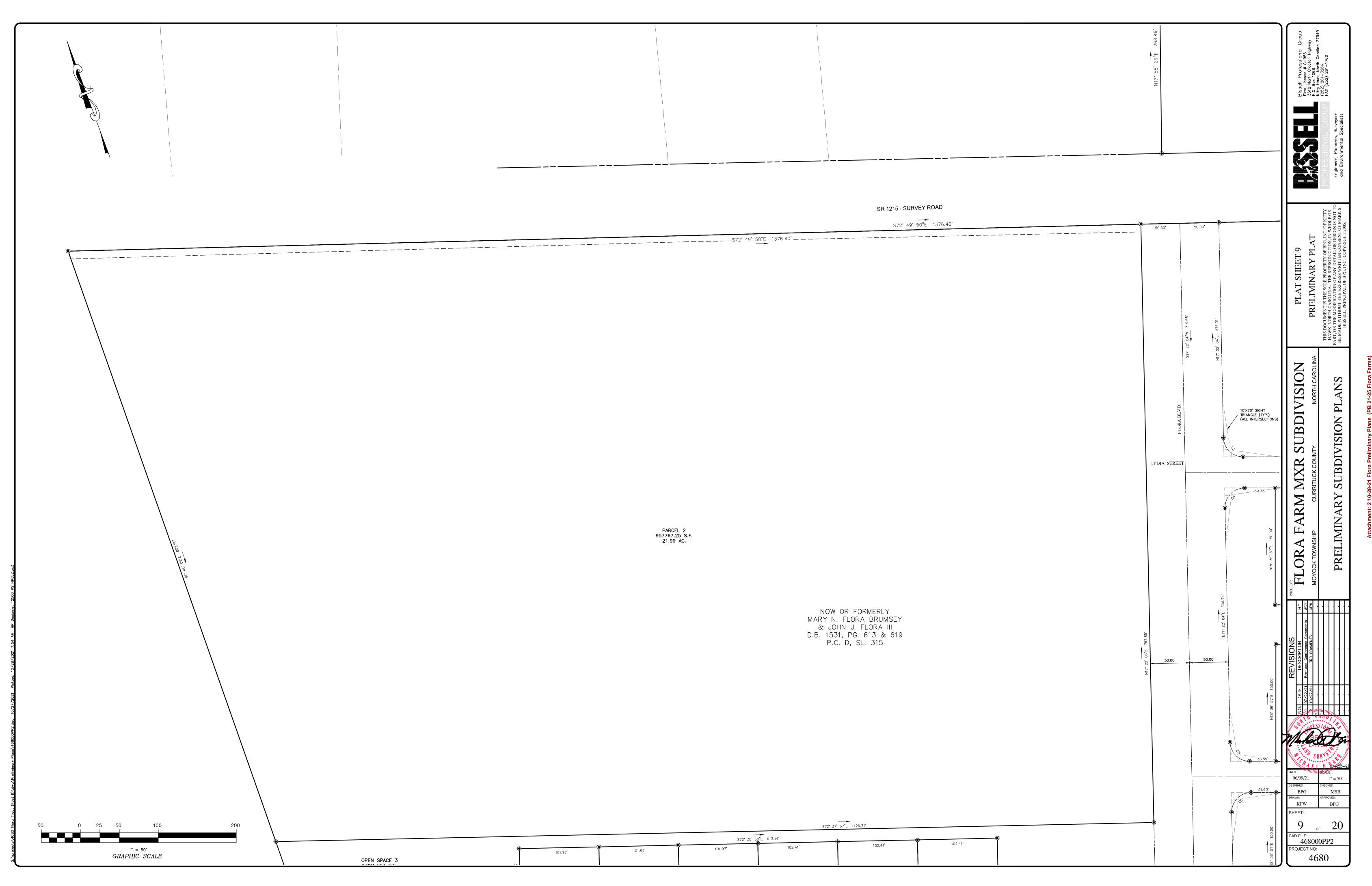




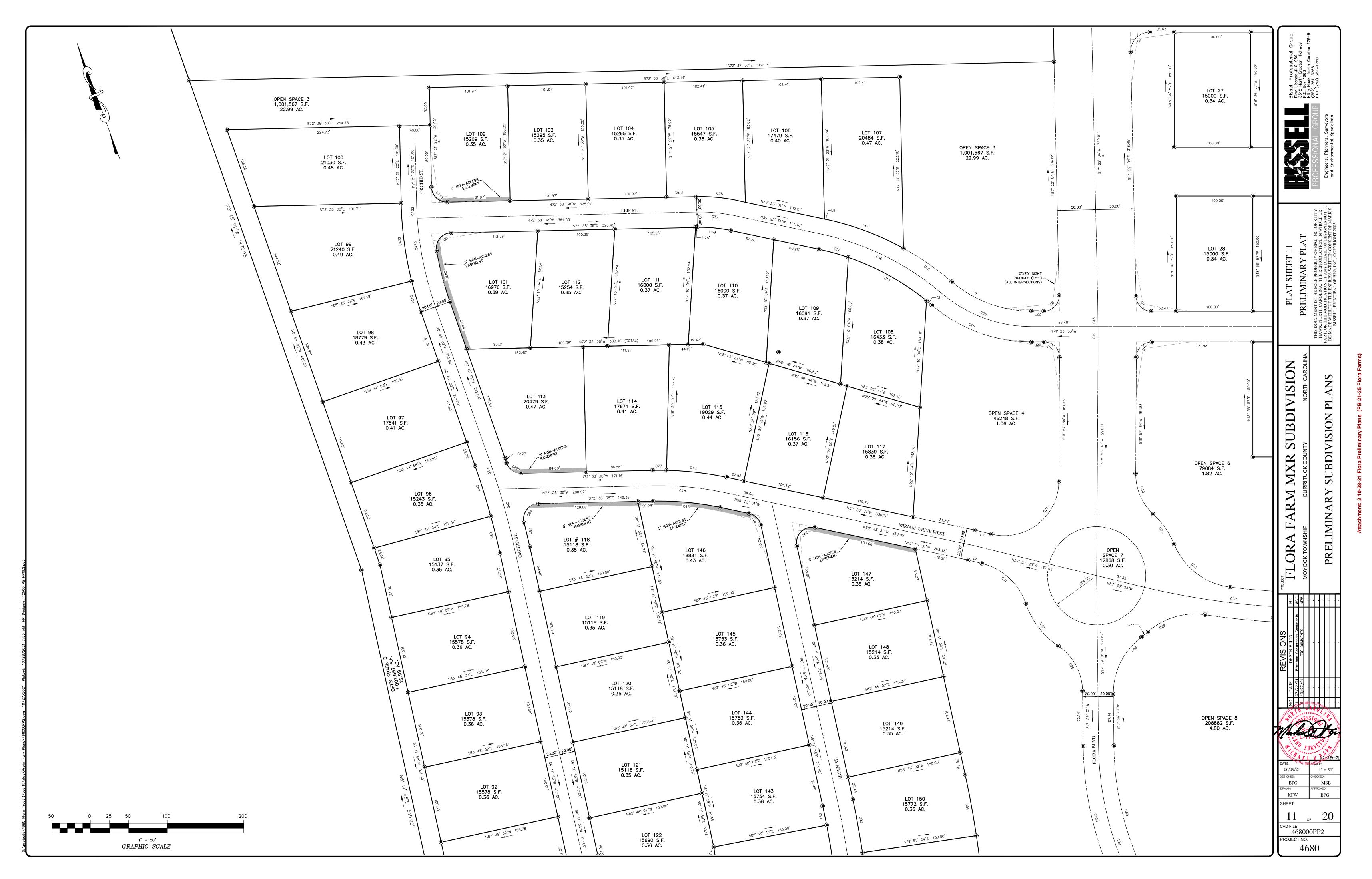


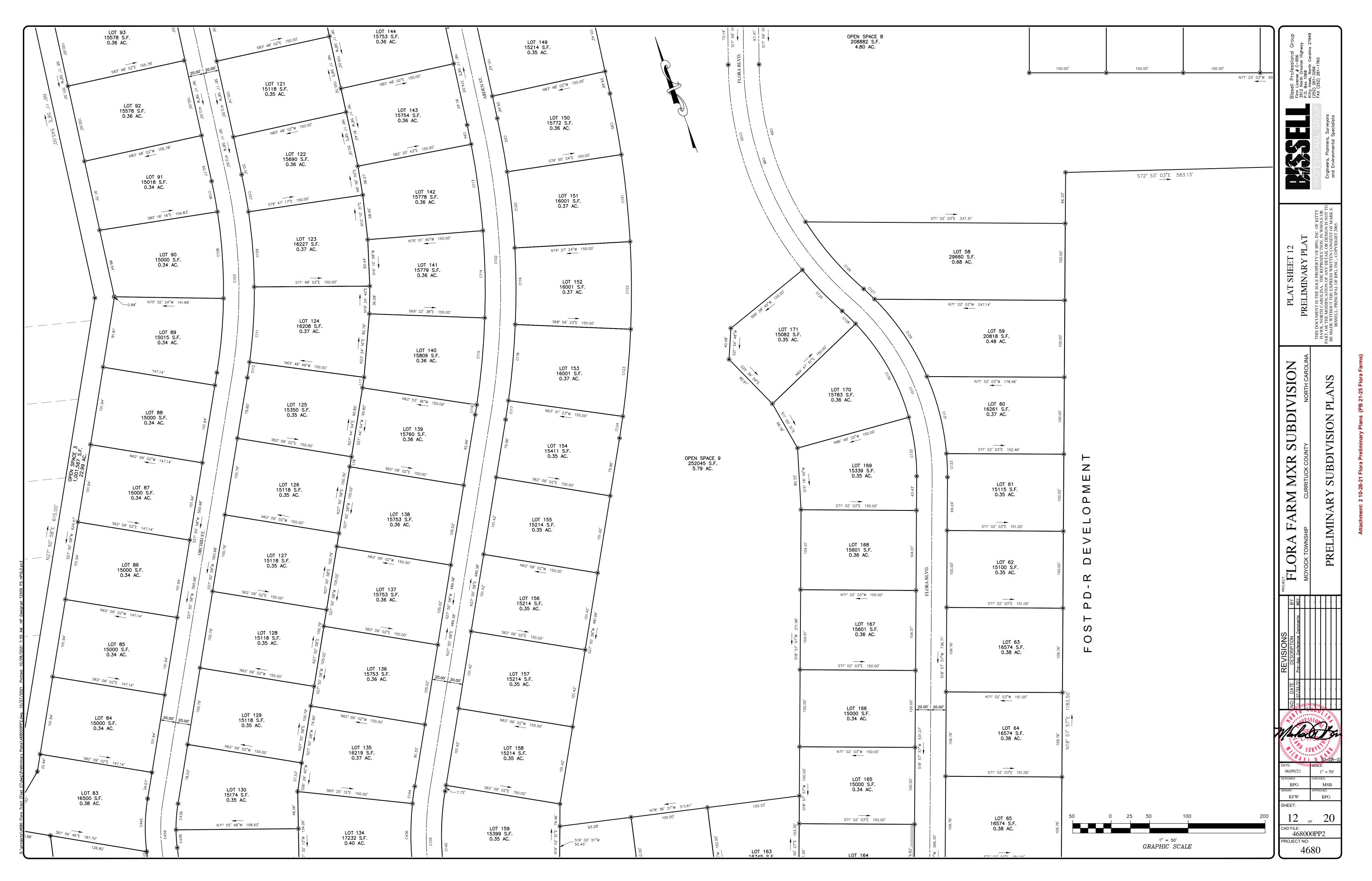


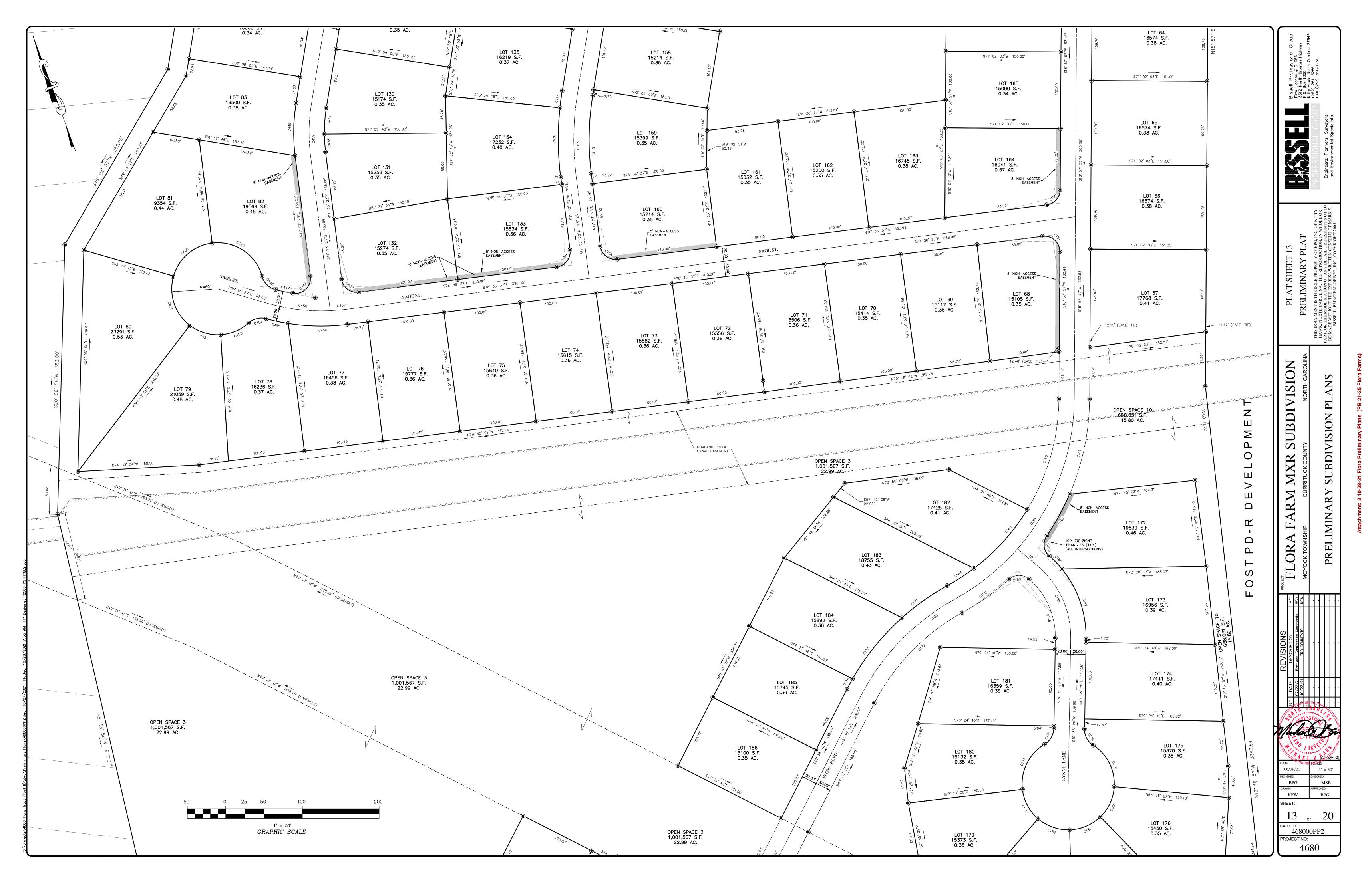






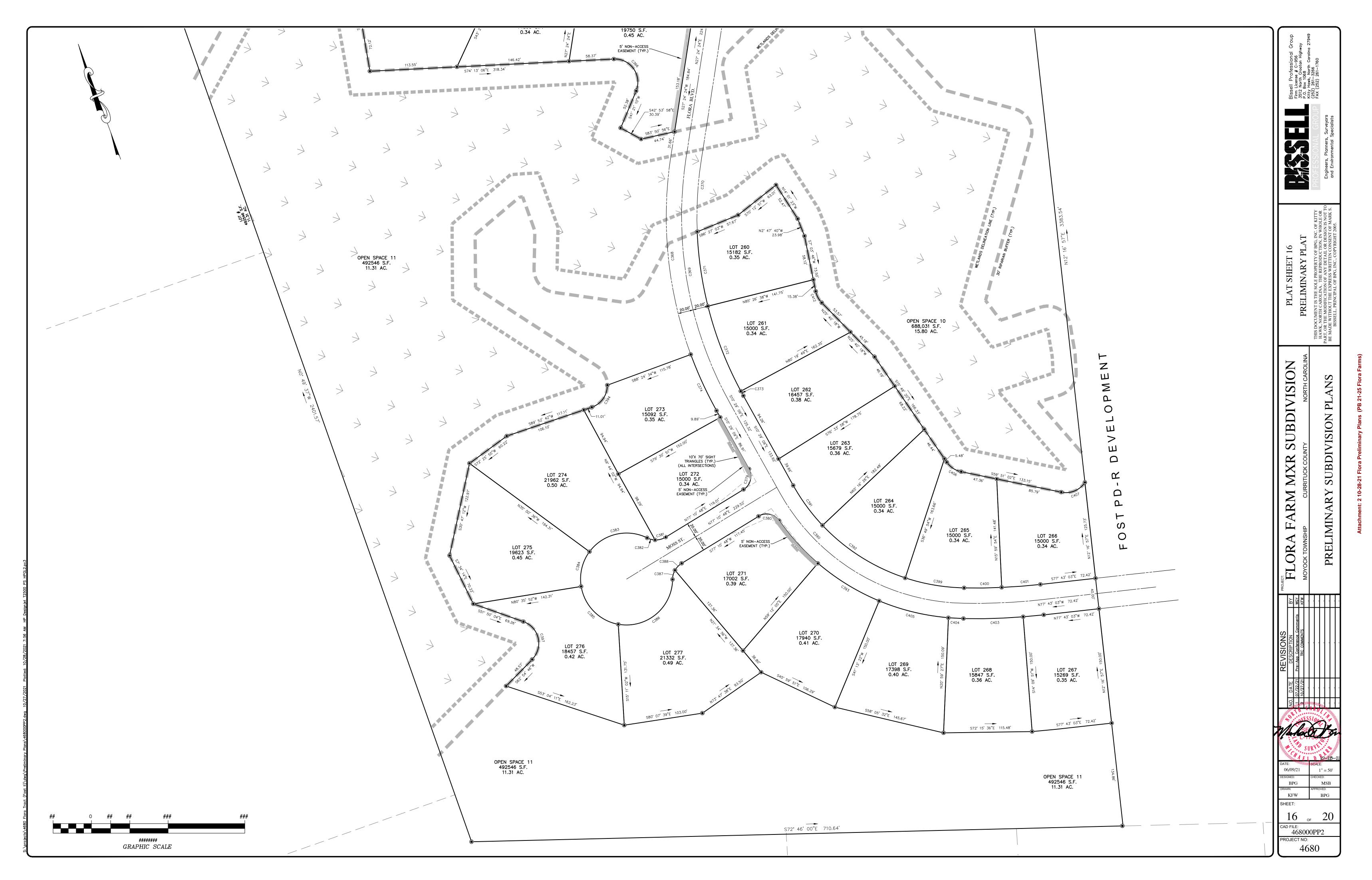


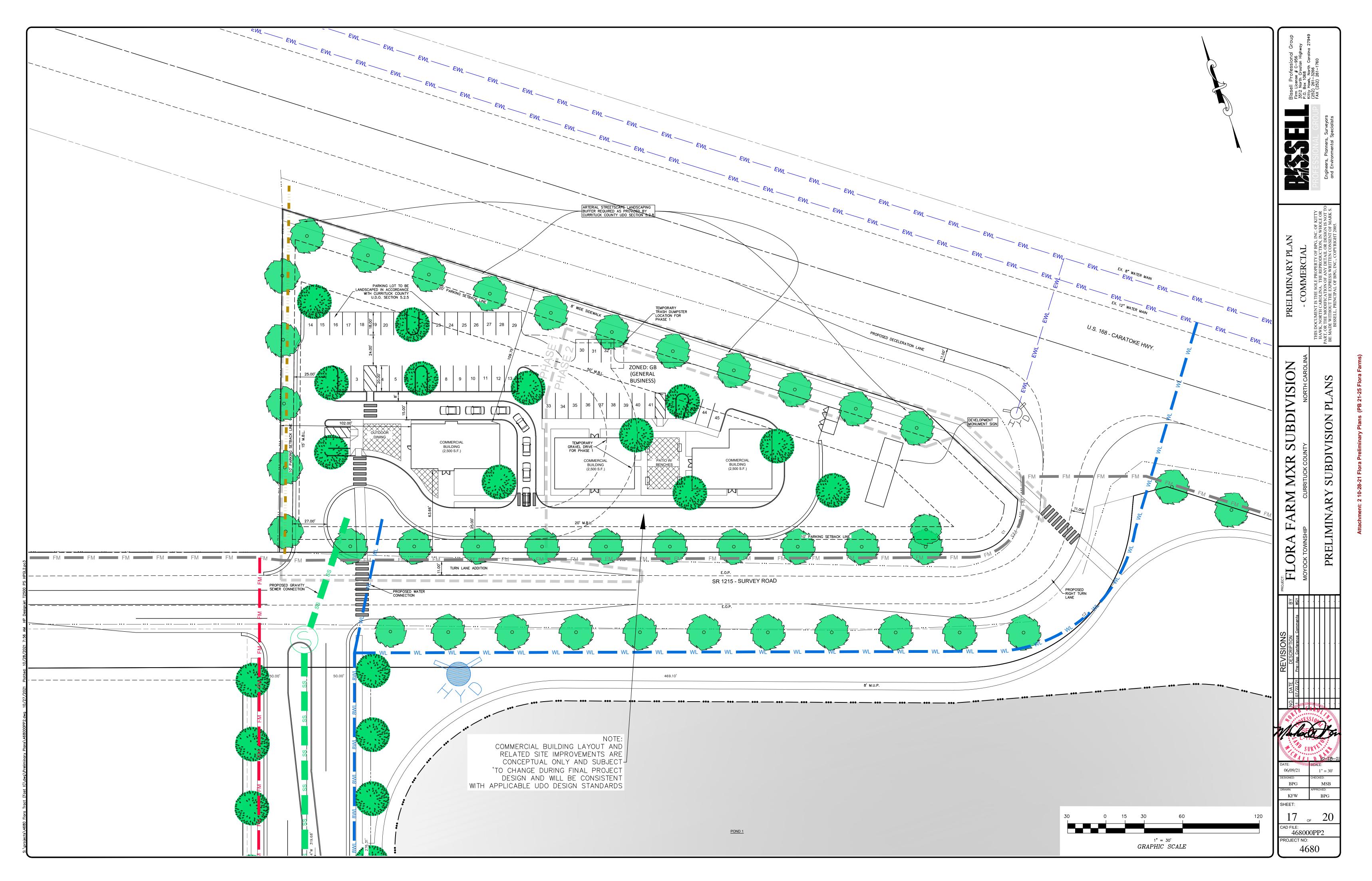


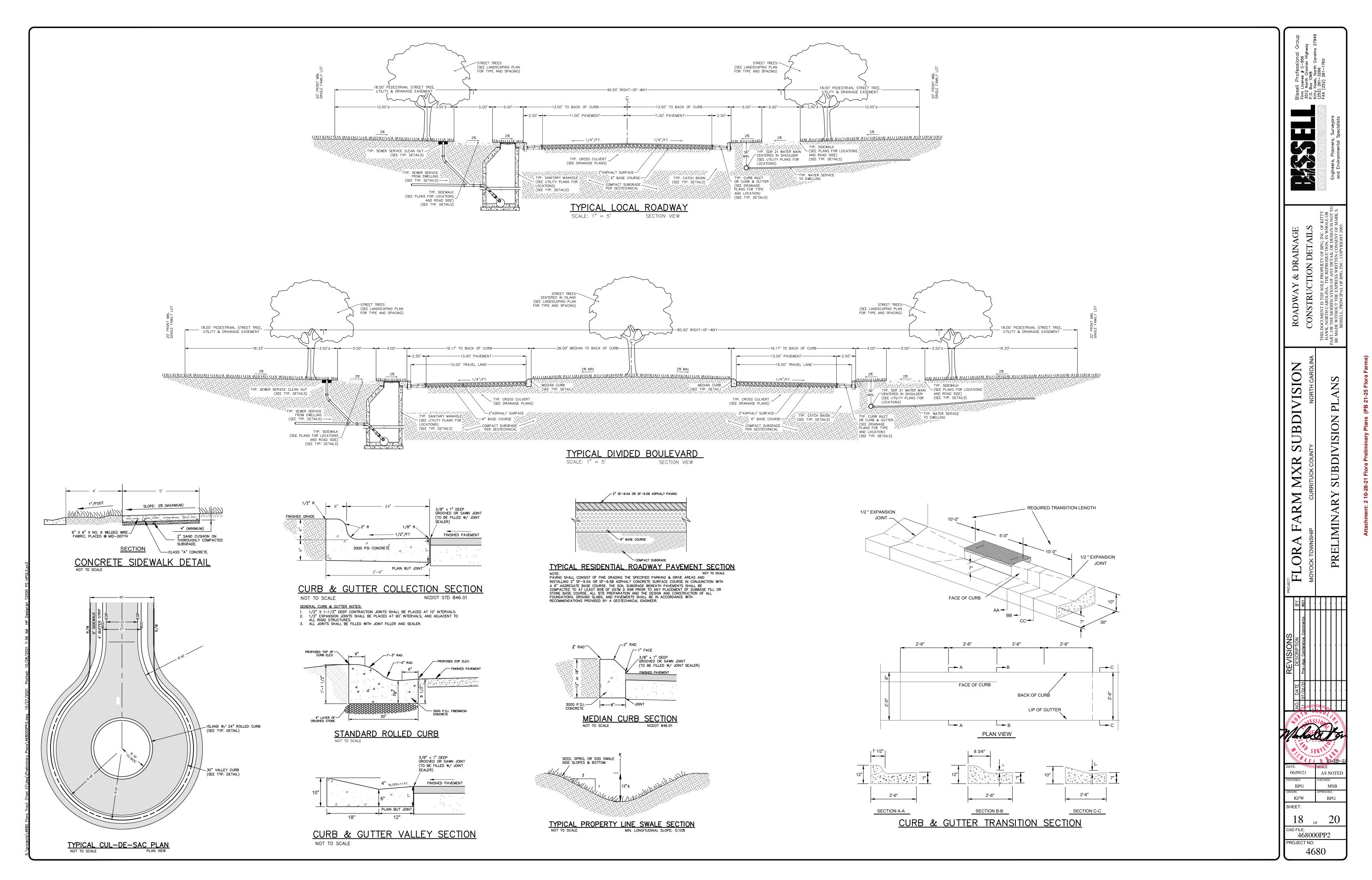










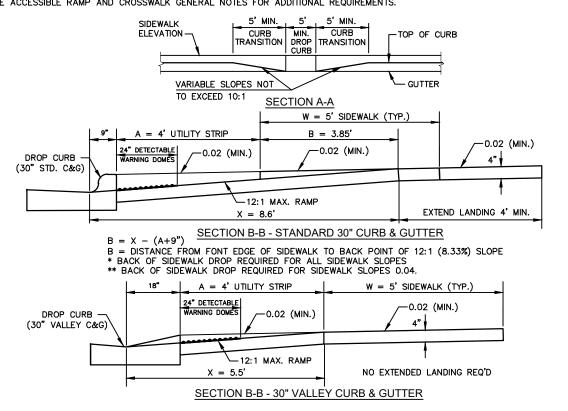


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PRELIMIN/

STANDARD CURB RAMP ISOMETRIC VIEW NOTES

1. PLACE DETECTABLE WARNING DOMES TO COVER 2'-0" LENGTH AND FILL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
2. OBTAIN VISIBLE CONTRACT WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.
3. SEE ACCESSIBLE RAMP AND CROSSWALK GENERAL NOTES FOR ADDITIONAL REQUIREMENTS.



### ACCESSIBLE RAMP DETAILS

ACCESSIBLE RAMP GENERAL NOTES: [NCDOT STD. DETAIL 848.05]

1. CONSTRUCT THE RAMP SURFACE TO BE STABLE, FIRM AND SLIP RESISTANT. CONSTRUCT THE CURB RAMP TYPE AS SHOWN IN THE PAVEMENT MARKING PLANS OR AS DIRECTED BY THE ENCINEER

- DIRECTED BY THE ENGINEER.

  2. LOCATE CURB RAMPS AND PLACE PEDESTRIAN CROSSWALK MARKINGS AS SHOWN ON THE PROJECT DRAWINGS OR AS DIRECTED BY THE ENGINEER. WHEN FIELD ADJUSTMENTS REQUIRE MOVING CURB RAMPS OR MARKINGS AS SHOWN, CONTACT AND LOCATE AS DIRECTED BY THE ENGINEER. 3. COORDINATE THE CURB RAMP AND THE PEDESTRIAN CROSSWALK MARKINGS SO A MIN. 4'X4' CLEAR SPACE AT THE BASE OF THE CURB RAMP WILL FALL WITHIN THE
- PEDESTRIAN CROSSWALK LINES.

  4. SETBACK DISTANCE FROM INSIDE CROSSWALK MARKING TO NEAREST EDGE OF TRAVEL LANE IS 4' MINIMUM.
- S. REFER TO PROJECT DRAWINGS FOR STOP BAR LOCATIONS, IF PROVIDED, OR AS DIRECTED BY THE ENGINEER.. S. TERMINATE PARKING A MINIMUM OF 20' BACK OF A PEDESTRIAN CROSSWALK.
- 7. CONSTRUCT CURB RAMPS A MINIMUM OF 4' WIDE, OR AS SPECIFIED ON PLANS.

  B. CONSTRUCT THE RUNNING SLOPE OF THE RAMP 8.33% (12:1 H:V) MAXIMUM.
- 9. ALLOWABLE CROSS SLOPE OF THE RAMP 8.33% (12:1 H: Y) MAXIMUM.
  10. WHERE REQUIRED, CONSTRUCT THE SIDE FLARE SLOPE A MAXIMUM OF 10% MEASURED ALONG THE CURB LINE.
  11. CONSTRUCT THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE BASE OF THE CURB RAMP A MAXIMUM OF 5% AND MAINTAIN A SMOOTH TRANSITION.
  12. CONSTRUCT LANDINGS FOR SIDEWALK A MIN. OF 4'X4' WITH A MAXIMUM SLOPE OF 2% IN ANY DIRECTION. CONSTRUCT LANDINGS FOR MEDIAN ISLANDS A MINIMUM OF
- 13. TO USE A MEDIAN ISLAND AS A PEDESTRIAN REFUGE AREA, MEDIAN ISLANDS WILL BE A MINIMUM OF 6' WIDE. CONSTRUCT MEDIAN ISLANDS TO PROVIDE PASSAGE OVER OR THROUGHOUT THE ISLAND
- 14. SMALL CHANNELIZATION ISLANDS THAT CAN NOT PROVIDE A 5'X5' LANDING AT THE TOP OF A RAMP, WILL BE CUT THROUGH LEVEL WITH THE SURFACE STREET.

  15. CURB RAMPS WITH RETURNED CURBS MAY BE USED ONLY WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP. THE ADJACENT SURFACE IS PLANTING OR OTHER NON-WALKING SURFACE OR THE SIDE APPROACH IS SUBSTANTIALLY OBSTRUCTED.

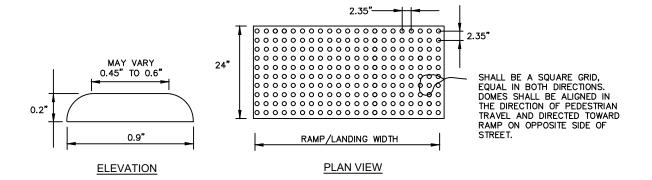
  16. PLACE A 1 EXPANSION JOINT WHERE THE CONCRETE CURB RAMP JOINS THE CURB AS SHOWN IN ROADWAY STANDARD DRAWING 848.01

  17. CURB RAMPS THROUGH MEDIAN ISLANDS, SINGLE RAMPS AT DUAL CROSSWALKS OR LIMITED R/W SITUATIONS, WILL BE HANDLED BY SPECIAL DETAILS.
- PEDESTRIAN CROSSWALK GENERAL NOTES: [NCDOT STD. DETAIL 1205.07]

  1. USE THE DETAILS ABOVE AND THE FOLLOWING NOTES FOR GUDIANCE IN PLACING CROSSWALK MARKINGS. REFER TO NCDOT ROADWAY STANDARD DRAWINGS, MUTCD AND ADA STANDARDS FOR ADDITIONAL GUIDANCE.

  2. THE LOCATION AND TYPE OF CROSSWALK MARKINGS SHOWN ON THE ABOVE DETAILS ARE FOR REFERENCE ONLY. LOCATE CROSSWALK MARKINGS AS SHOWN ON THE PROJECT DRAWINGS OR AS DIRECTED BY THE ENGINEER. THE CROSSWALK MARKING TYPE, STANDARD OR HI-VISIBILITY, SHALL BE INSTALLED AS SPECIFIED ON THE PROJECT DRAWINGS OR AS DIRECTED BY THE ENGINEER.
- 3. THE STANDARD CROSSWALK IS THE TWO WHITE 8" MIN. TRANSVERSE LINES WITH A 6' MIN. GAP BETWEEN THE LINES. THE HI-VISIBILITY CROSSWALK IS WHITE 24" MAX. WIDE LONGITUDINAL LINES WITH 24" MIN. GAPS BETWEEN LINES, SEE DETAIL, HI-VISIBILITY CROSSWALKS SHOULD BE A MINIMUM OF 6' WIDE OR AS SPECIFIED ON THE PROJECT DRAWINGS. CURB RAMPS SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS, EXCLUDING AN FLARES.
- 4. STOP BARS (IF PROVIDED) SHOULD BE PLACED A 4' MIN. IN ADVANCE OF NEAREST CROSSWALK LINE.
  5. SET BACK DISTANCE FROM INSIDE CROSSWALK MARKING TO NEAREST EDGE OF TRAVEL IS 4' MIN.
- BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE OF 4'X4' MIN. SHALL BE PROVIDED WITHIN THE MARKINGS.
- 7. SINGLE DIAGONAL CURB RAMPS WITH FLARED SIDES SHALL HAVE A SEGMENT OF CURB 2' MIN. LONG LOCATED ON EACH SIDE OF THE CURB RAMP AND WITHIN THE MARKED CROSSING, SEE DETAIL.

  8. CURB RAMPS SHALL BE CONSTRUCTED IN ACCORDANCE TO THE LATEST NCDOT ROADWAY STANDARD DRAWINGS. CURB RAMPS THROUGH MEDIAN ISLANDS, SINGLE RAMPS AT DUAL CROSSWALKS OR LIMITED R/W SITUATIONS, WILL BE HANDLED BY SPECIAL DETAILS.

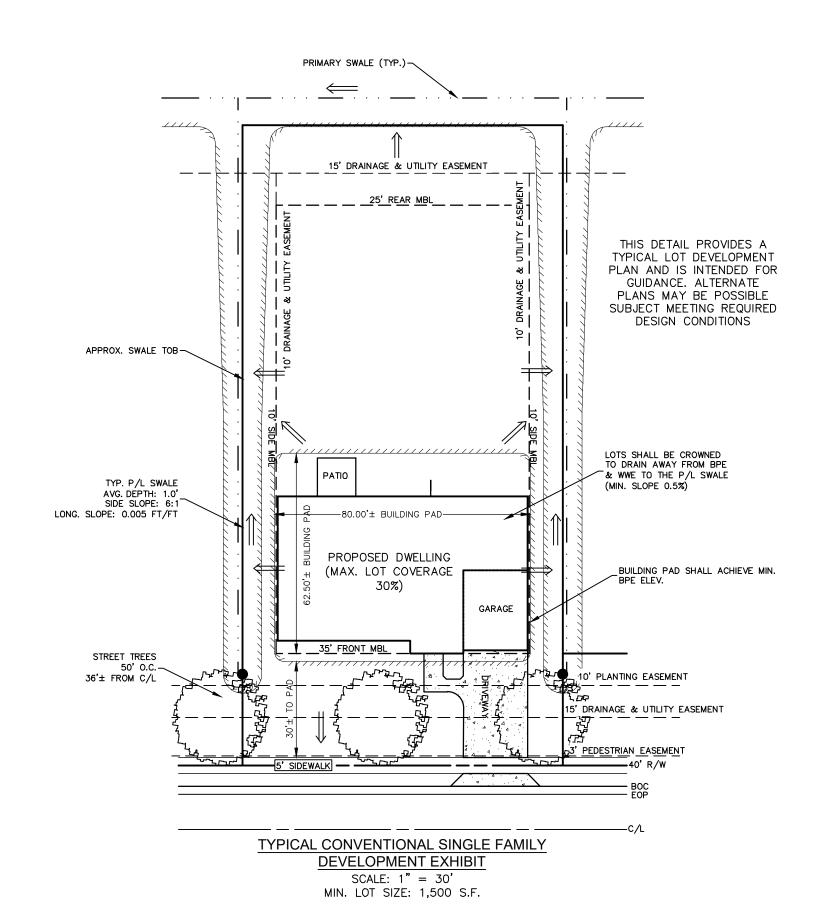


## TRUNCATED DOME DETECTABLE WARNING DETAIL

NOT TO SCALE

### DETECTABLE WARNING GENERAL NOTES

- 1. DETECTABLE WARNING SHALL CONSIST OF RAISED TRUNCATED DOMES MANUFACTURED BY "COTE-L INDUSTRIES, INC.". CALLED "SAFTI-TRAX", WITH POLYURETHANE COATING "DURABAK", OR APPROVED EQUAL. APPLIED ON SMOOTH (NON-GROOVED) CLEAN CONCRETE RAMP, AND SHALL CONFORM TO THE DETAILS IN THE PLANS AND IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND INSTALLATION INSTRUCTIONS.
- 2. ALL DETECTABLE WARNING AREAS SHALL START AT BACK OF CURB, BE 24 INCHES IN DEPTH AND COVER THE COMPLETE WIDTH OF THE RAMP AREA 48 INCHES MIN..
- 3. 70% VISUAL CONTRAST IS REQUIRED. THE COLOR SHALL BE AN INTEGRAL PART OF THE DETECTABLE WARNING MATERIAL, AS SPECIFIED ON THE PLANS. COLOR TO BE DETERMINED
- BY THE CITY STAFF, SAFETY YELLOW IS THE DEFAULT COLOR. 4. THE SMOOTH AND CLEAN CONCRETE UNDER DETECTABLE WARNING DEVICE AREA SHALL BE INCLUDED IN THE COST OF THE CONCRETE CURB RAMP. THE COST OF FURNISHING AND INSTALLING THE DETECTABLE WARNING DEVICE SHALL BE INCLUDED SEPARATELY AS "DETECTABLE WARNING DEVICE" PER SQUARE FOOT OR AS OUTLINED IN THE SPECIFICATIONS.
- 5. DETECTABLE WARNING SURFACE: APPLIED A COATING OF "DURABAK" SLIP-RESISTANT POLYURETHANE COATING TO THE SMOOTH, CLEAN CONCRETE SURFACE. ON TOP OF THE POLYURETHANE COATING APPLY TRUNCATED DOMES FROM A "SAFTY-TRAX" CONTACT SHEET. ON TOP OF THE TRUNCATED DOMES AND INITIAL POLYURETHANE COATING PLACE THREE ADDITIONAL COATS OF "DURABAK" POLYURETHANE COATING. COLOR TO BE DETERMINED BY CITY STAFF OR AS SPECIFIED ON THE PLANS. SAFETY YELLOW IS A DEFAULT COLOR.
- 6. ALL RAMPS AND DETECTABLE WARNING SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON THE OPPOSITE SIDE OF STREET.



Packet Pg. 43

AS NOTED

MSB

BPG

KFW

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ROJECT NO:

CURVE	LENGTH	RADIUS	CHORD	BEARING	DELTA
C1	53.70	36.37	48.95	N65° 04' 23"E	84°35'21"
C2	186.57	126.37	170.08	N65° 04' 23"E	84*35'21"
C3	38.73	25.00	34.97	S27' 00' 30"E	88'45'07"
C4	39.81	25.00	35.74	S62° 59′ 30″W	91'14'53"
C5 C6	38.73 39.81	25.00 25.00	34.97 35.74	S27' 00' 30"E	88°45'07" 91°14'53"
C7	30.98	20.00	27.97	S27° 00' 30"E	88'45'07"
C8	31.85	20.00	28.59	N62° 59' 30"E	91"14'53"
C9	113.23	160.00	110.88	S51° 06' 39"E	40*32'48"
C10	76.47	374.23	76.34	N36° 45' 44"W	11'42'29"
C11	108.18	372.43	107.80	S50° 55′ 33″E	16'38'35"
C12	39.60	332.39	39.58	S55° 58' 44"E	6°49'34"
C13	117.13	332.40	116.53	N42° 28′ 14″W	20°11'25"
C14	8.92	332.40	8.92	N31° 36′ 23″W	1°32'17"
C15	141.53	200.00	138.60	S51° 06' 39"E	40*32'48"
C16	31.54	20.00	28.37	N26° 12' 45"W	90°20′38″
C17	31.30	20.00	28.20	S63° 47' 15"W	89*39'22"
C18	25.36	2992.70	25.36	N17° 58' 30"E	0*29'08"
C19	33.50	2992.70	33.50	N18° 32' 18"E	0*38'29"
C20	59.18	71.00	57.49	S4° 55' 16"E	47°45'40"
C21	128.11	71.00	111.43	N70° 39' 06"E	103°23'02"
C22	48.02	109.00	47.63	N16° 10' 51"W	25°14'31"
C23	96.61	91.00	92.13	S33° 58′ 23″E	60°49'35"
C24	119.69	980.00	119.62	S67° 53' 07"E	6*59'53"
C25	152.81	1020.00	152.67	S67° 05' 33"E	8°35'01"
C26	80.47	91.00	77.87	N88° 08' 01"W	50*39'57"
C27	10.57	109.00	10.57	N69° 18' 42"E	5*33'23"
C28	85.93	91.00	82.78	S45° 02' 13"W	54*06'23"
C29	68.77	71.00	66.11	N9° 45′ 47″W	55*29'37"
C30	70.81	109.00	69.58	S18° 53' 53"E	37°13'25" 57°22'12"
C31	81.10	81.00	77.76	N28* 58' 17"W	
C32	239.60	180.00	239.02 124.74	S64° 31' 13"E S51° 06' 39"E	13'43'40"
C35 C36	127.38 175.63	352.40	173.81	N45° 06' 53"W	40°32'48" 28°33'16"
C37	48.57	210.00	48.46	N66° 01' 04"W	13"15'07"
C38	53.20	230.00	53.08	S66° 01' 04"E	1315'07"
C39	43.95	190.00	43.85	N66° 01' 04"W	13"15'07"
C40	78.97	420.00	78.85	S64° 46' 42"E	10'46'22"
C42	39.94	20.00	33.62	S63° 24' 14"W	114°24'31"
C43	87.89	380.00	87.69	N66° 01' 04"W	13"15'07"
C44	22.90	20.00	21.67	N26° 35′ 46″W	65*35'29"
C77	18.17	420.00	18.17	S71° 24' 16"E	2°28'45"
C78	92.52	400.00	92.31	N66° 01' 04"W	13°15'07"
C79	39.35	1000.00	39.35	N0° 22' 37"E	2°15'16"
C80	81.95	1000.00	81.93	N3° 51' 07"E	4°41′44″
C84	35.94	20.00	31.29	S55° 52' 43"W	102*57'18"
C85	32.02	1020.00	32.02	S5° 18' 01"W	1°47'55"
C86	49.78	980.00	49.78	N4° 44′ 39″E	2°54'38"
C87	69.09	980.00	69.08	N1° 16' 09"E	4°02'22"
C93	70.58	1043.00	70.56	S8° 08' 17"W	3*52'37"
C94	25.47	1003.00	25.47	N6° 55' 38"E	1°27'19"
C95	80.73	1193.00	80.71	N8° 08' 17"E	3*52'37"
C98	345.36	410.00	335.24	S6° 08' 51"E	48"15'44"
C99	216.09	390.00	213.34	S2° 06' 38"W	31°44'46"
C100	288.97	430.00	283.56	S1° 16' 07"E	38*30'16"
C101	262.14	305.00	254.15	N5° 39' 23"W	49"14'39"
C102	386.55	1023.00	384.26	N17° 01' 28"E	21'39'00"
C103	258.08	683.00	256.55	N17° 01' 28"E	21'39'00"
C105	107.73 40.45	375.00	107.36	S19° 37' 11"W	16'27'34"
C106 C107	49.23	663.00 703.00	40.45 49.22	N7° 56′ 51″E S8° 12′ 21″W	3°29'46" 4°00'45"
C107	112.99	663.00	112.85	N14* 34' 40"E	4*00'45" 9*45'51"
C108	98.00	703.00	97.92	S14° 12' 20"W	7*59'14"
C103	98.00	703.00	97.92	S22° 11' 34"W	7'59'14"
C112	20.41	703.00	20.40	S27° 01' 04"W	1'39'47"
0112			113.45	N10° 53' 48"E	6*29'02"
C113	113.51	1003.00		·	
	113.51 113.51	1003.00	113.45	N17° 22' 51"E	6°29'02"
C113			113.45 113.45	N17' 22' 51"E N23' 51' 53"E	6°29'02" 6°29'02"
C113	113.51	1003.00			
C113 C114 C115	113.51 113.51	1003.00 1003.00	113.45	N23° 51' 53"E	6*29'02"
C113 C114 C115 C116	113.51 113.51 13.00	1003.00 1003.00 1003.00	113.45 13.00	N23* 51' 53"E N27* 28' 41"E	6°29'02" 0°44'33"
C113 C114 C115 C116 C117	113.51 113.51 13.00 24.98	1003.00 1003.00 1003.00 1043.00	113.45 13.00 24.98	N23' 51' 53"E N27' 28' 41"E S27' 09' 48"W	6*29'02" 0*44'33" 1*22'20"
C113 C114 C115 C116 C117 C118	113.51 113.51 13.00 24.98 99.52	1003.00 1003.00 1003.00 1043.00	113.45 13.00 24.98 99.48	N23' 51' 53"E N27' 28' 41"E S27' 09' 48"W S23' 44' 37"W	6'29'02" 0'44'33" 1'22'20" 5'28'01"
C113 C114 C115 C116 C117 C118 C119	113.51 113.51 13.00 24.98 99.52 99.52	1003.00 1003.00 1003.00 1043.00 1043.00 1043.00	113.45 13.00 24.98 99.48	N23' 51' 53"E N27' 28' 41"E S27' 09' 48"W S23' 44' 37"W S18' 16' 37"W	6'29'02" 0'44'33" 1'22'20" 5'28'01"
C113 C114 C115 C116 C117 C118 C119 C120	113.51 113.51 13.00 24.98 99.52 99.52 99.52	1003.00 1003.00 1003.00 1043.00 1043.00 1043.00 1043.00	113.45 13.00 24.98 99.48 99.48	N23' 51' 53"E N27' 28' 41"E S27' 09' 48"W S23' 44' 37"W S18' 16' 37"W S12' 48' 36"W	6'29'02" 0'44'33" 1'22'20" 5'28'01" 5'28'01"
C113 C114 C115 C116 C117 C118 C119 C120 C121	113.51 113.51 13.00 24.98 99.52 99.52 99.52 113.83	1003.00 1003.00 1003.00 1043.00 1043.00 1043.00 1043.00 1193.00	113.45 13.00 24.98 99.48 99.48 99.48 113.79	N23' 51' 53"E N27' 28' 41"E S27' 09' 48"W S23' 44' 37"W S18' 16' 37"W S12' 48' 36"W N12' 48' 36"E	6'29'02" 0'44'33" 1'22'20" 5'28'01" 5'28'01" 5'28'01"
C113 C114 C115 C116 C117 C118 C119 C120 C121 C122	113.51 113.51 13.00 24.98 99.52 99.52 99.52 113.83 113.83	1003.00 1003.00 1003.00 1043.00 1043.00 1043.00 1043.00 1193.00	113.45 13.00 24.98 99.48 99.48 99.48 113.79	N23' 51' 53"E N27' 28' 41"E S27' 09' 48"W S23' 44' 37"W S18' 16' 37"W S12' 48' 36"W N12' 48' 36"E N18' 16' 37"E	6'29'02" 0'44'33" 1'22'20" 5'28'01" 5'28'01" 5'28'01" 5'28'01"
C113 C114 C115 C116 C117 C118 C119 C120 C121 C122 C123	113.51 113.51 13.00 24.98 99.52 99.52 99.52 113.83 113.83	1003.00 1003.00 1003.00 1043.00 1043.00 1043.00 1043.00 1193.00 1193.00	113.45 13.00 24.98 99.48 99.48 99.48 113.79 113.79	N23' 51' 53"E  N27' 28' 41"E  S27' 09' 48"W  S23' 44' 37"W  S18' 16' 37"W  S12' 48' 36"E  N12' 48' 36"E  N18' 16' 37"E  N23' 44' 37"E	6'29'02" 0'44'33" 1'22'20" 5'28'01" 5'28'01" 5'28'01" 5'28'01"

CURVE TABLE

C129	25.25	285.00	25.24	N27° 44' 26"W	5°04'34"		C303	12.25	25.00	12.13	N13° 12' 34"E	28'04'21"
	122.03	325.00	121.31	S15° 30' 52"E	21°30′46″		C304	143.12	340.00	142.07	N15° 20' 51"E	24*07'07'
C130	141.15	285.00	139.71	N11° 00' 51"W	28*22'35"		C305	151.54	360.00	150.42	N15° 20' 51"E	24.07,07,
C131	103.77	325.00	103.33	S4° 23' 19"W	18'17'36"		C306	74.88	364.00	74.75	N9* 10' 53"E	11°47'11"
		285.00										
C132	78.55		78.30	N11° 04' 12"E	15*47'30"		C307	39.44	320.00	39.41	N6° 49' 08"E	7'03'41"
C133	30.80	325.00	30.79	S16° 15' 02"W	5°25'49"		C308	75.74	320.00	75.56	N20° 37' 34"E	13°33'40"
C144	22.54	395.00	22.54	N26° 12′ 52″E	3°16'12"		C309	19.52	320.00	19.52	N12° 05' 51"E	3°29'45"
C145	101.98	355.00	101.63	S19° 37' 11"W	16°27'34"		C311	20.68	30.00	20.28	N57° 54' 09"E	39°30'13"
C156	28.77	20.00	26.35	N60° 10' 40"E	82*25'27"		C315	2.24	60.00	2.24	S0° 14' 34"W	2.08,522
C157	34.06	20.00	30.09	S29° 49' 20"E	97*34'33"		C317	45.68	30.00	41.40	S33° 16' 28"E	87'14'53"
C158	31.42	20.00	28.28	S33° 36′ 37″E	90,00,00,		C318	20.24	30.00	19.86	S68° 20' 20"W	38'39'34'
C159	31.42	20.00	28.28	N56° 23' 23"E	90'00'00"		C319	72.87	30.00	56.23	S67° 19' 53"W	13910'46
C160	151.97	280.00	150.11	N34° 30' 52"E	31'05'52"		C320	18.76	30.00	18.46	N60° 59' 39"W	35*49'50'
C161	127.59	320.00	126.75	N30° 23' 17"E	22°50'41"		C324	31.42	20.00	28.28	N72° 24' 24"E	90,00,00,
C162	62.12	331.26	62.03	S47° 22' 22"W	10°44'43"		C325	31.42	20.00	28.28	N17° 35' 36"W	90,00,00,
C163	104.17	280.00	103.57	N60° 43' 17"E	21°18'57"		C326	124.45	296.00	123.54	S50° 32' 54"E	24'05'25'
C164	41.38	280.00	41.35	N75° 36' 48"E	8°28'06"		C327	309.86	316.00	297.60	S34° 30' 06"E	56°10'59"
C165	29.01	20.00	26.54	S11° 22' 28"W	83'07'16"		C328	97.60	336.00	97.26	N54° 16' 18"W	16*38'36"
												16'08'52'
C166	23.52	140.00	23.49	S25° 22' 27"E	9*37'25"		C329	94.70	336.00	94.38	N37° 52' 35"W	
C167	98.11	140.00	96.11	S0° 29' 12"E	40°09'04"		C330	94.70	336.00	94.38	N21° 43′ 43″W	16'08'52'
C168	86.66	100.00	83.97	N5* 14' 15"W	49'39'08"		C331	131.10	296.00	130.03	S25° 48' 53"E	25'22'35'
C169	29.14	20.00	26.63	N71° 48′ 28″W	83'29'19"		C332	34.70	296.00	34.68	S9° 46' 06"E	6*42'59"
C170	74.84	320.00	74.67	N73° 08' 51"E	13°23'59"		C333	61.32	180.00	61.02	S16° 10' 09"E	19°31'04"
C171	72.73	320.00	72.57	N73° 20' 12"E	13.01,17,		C334	111.49	140.00	108.57	N29° 13′ 28″W	45*37'42'
C172	167.19	280.00	164.71	S62° 44′ 32″W	34'12'39"		C335	146.05	160.00	141.04	N32° 33′ 40″W	52'18'07'
							-					
C173	102.69	324.86	102.26	N57° 42' 06"E	18*06'39"	<u> </u>	C336	42.48	336.00	42.46	N10° 01' 57"W	7*14'40"
C174	15.67	621.98	15.67	N46° 35′ 23″E	1*26'36"		C342	90.96	180.00	90.00	S40° 24' 20"E	28*57'18
C175	25.32	25.00	24.25	N48° 36' 21"E	58*02'03"		C343	16.31	140.00	16.30	N55° 22' 31"W	6*40'26"
C176	25.32	25.00	24.25	S9° 25′ 42″E	58'02'03"		C344	12.03	180.00	12.03	S56° 47' 52"E	3°49'45"
C177	68.90	60.00	65.18	N44° 43' 27"E	65*47'52"		C345	25.32	25.00	24.25	S29° 41' 42"E	58'02'03
C178	63.36	60.00	60.46	S8° 11' 34"E	60'30'19"		C346	25.32	25.00	24.25	N87° 43' 46"W	58'02'03
C179	44.44	60.00	43.44	N9° 23' 44"W	42°26'29"		C347	13.56	30.00	13.45	N57° 46' 07"W	25*54'13'
			ļ	S43° 16' 50"W								
C180	44.44	60.00	43.44				C348	69.85	60.00	65.97	S34° 01' 42"E	
C181	44.44	60.00	43.44	S85° 43' 19"W	42*26'29"		C349	52.78	60.00	51.09	N87° 25' 21"E	50'23'51'
C182	44.44	60.00	43.44	N51° 50' 13"W	42*26'29"		C350	45.52	60.00	44.44	N40° 29' 16"E	43°28'21'
C184	318.78	300.00	303.99	N49° 24' 24"E	60°52'54"		C351	48.81	60.00	47.47	N4° 33' 09"W	46°36'29'
C185	179.13	300.00	176.48	S62° 44′ 32″W	34°12'39"		C352	93.09	60.00	84.03	N72* 18' 06"W	88'53'24'
C186	104.51	120.00	101.23	N5° 21' 36"W	49'53'51"		C353	15.43	30.00	15.26	S43° 58' 33"E	29*28'22'
C233	31.42	20.00	28.28	S89° 21' 48"E	90,00,00,		C354	22.17	30.00	21.67	S8° 03' 51"E	42'21'03'
-			1				-					
C234	31.42	20.00	28.28	S0° 38' 12"W	90'00'00"		C355	34.68	30.00	32.79	S24° 25' 33"E	66*14'35'
C238	125.66	80.00	113.14	N89° 21′ 48″W	90'00'00"		C357	12.91	30.00	12.81	N77° 21' 41"W	24'39'05
C239	69.90	100.00	68.49	S64° 23′ 20″E	40°03'04"		C358	36.30	30.00	34.12	S85° 57' 10"W	69"19'10"
C240	87.18	100.00	84.44	N70° 36' 40"E	49*56'56"		C366	60.51	30.00	50.76	S16° 25' 58"E	115*34*15
C241	94.25	60.00	84.85	N89° 21' 48"W	90'00'00"		C368	263.17	520.00	260.37	S12* 54' 29"W	28*59'50
C242	44.95	100.00	44.57	N32° 45′ 33″E	25°45'19"		C369	330.67	500.00	324.68	S8° 27' 37"W	37*53'33'
C243	52.43	100.00	51.83	N4° 51' 38"E	30°02'29"		C370	93.27	480.00	93.13	S21° 50' 24"W	11*08'01*
C244	125.66	80.00	113.14	S0° 38' 12"W	90'00'00"		C371	98.16	480.00	97.99	S10° 24' 53"W	11'43'01'
		55.55	113.14	30 36 12 W				30.10	+00.00	37.33	310 24 33 W	11 45 01
C245	94.25	60.00	04.05	!!				44040	400.00	440 07		
C246		60.00	84.85	S0° 38′ 12″W	90°00'00"		C372	119.18	480.00	118.87	S2° 33′ 24″E	
- 02.10	59.70	60.00 100.00	84.85 58.81	S0° 38' 12"W N27° 15' 42"W	90°00′00″ 34°12'12″		C373	119.18 6.84	480.00 480.00	118.87 6.84	S2° 33′ 24″E S10° 04′ 40″E	
C247	59.70 31.42						-					0°48'58"
		100.00	58.81	N27° 15′ 42″W	3412'12"		C373	6.84	480.00	6.84	S10° 04' 40"E	0°48'58" 8°53'43"
C247	31.42	100.00	58.81 28.28	N27* 15' 42"W N0* 38' 12"E	34°12'12" 90°00'00"	-	C373 C374	6.84 80.73	480.00 520.00	6.84 80.65	S10° 04' 40"E N6° 02' 17"W	0°48'58" 8°53'43" 87°39'57
C247 C248 C249	31.42 31.42	100.00 20.00 20.00	58.81 28.28 28.28	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W	34*12'12" 90*00'00" 90*00'00" 55*45'24"	_	C373 C374 C379	6.84 80.73 30.60	480.00 520.00 20.00	6.84 80.65 27.70	S10' 04' 40"E N6' 02' 17"W N33' 20' 49"E N59' 36' 42"W	0°48′58″ 8°53′43″ 87°39′57 86°25′00
C247 C248 C249 C250	31.42 31.42 136.24 67.81	100.00 20.00 20.00 140.00	58.81 28.28 28.28 130.93 67.30	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59"	_	C373 C374 C379 C380 C381	6.84 80.73 30.60 30.17 14.56	480.00 520.00 20.00 20.00 25.00	6.84 80.65 27.70 27.39 14.36	S10° 04′ 40″E N6° 02′ 17″W N33° 20′ 49″E N59° 36′ 42″W S86° 08′ 05″E	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15
C247 C248 C249 C250 C251	31.42 31.42 136.24 67.81 16.76	100.00 20.00 20.00 140.00 160.00	58.81 28.28 28.28 130.93 67.30 16.76	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12"		C373 C374 C379 C380 C381 C382	6.84 80.73 30.60 30.17 14.56 10.76	480.00 520.00 20.00 20.00 25.00 25.00	6.84 80.65 27.70 27.39 14.36	S10° 04′ 40″E N6° 02′ 17″W N33° 20′ 49″E N59° 36′ 42″W S86° 08′ 05″E S57° 07′ 03″E	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15' 24'39'48
C247 C248 C249 C250 C251 C252	31.42 31.42 136.24 67.81 16.76 157.89	100.00 20.00 20.00 140.00 160.00 400.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58"	-	C373 C374 C379 C380 C381 C382 C383	6.84 80.73 30.60 30.17 14.56 10.76 84.01	480.00 520.00 20.00 20.00 25.00 25.00 60.00	6.84 80.65 27.70 27.39 14.36 10.68	S10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  S86' 08' 05"E  S57' 07' 03"E  S84' 53' 52"E	14'13'33' 0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15' 24'39'48 80'13'27'
C247 C248 C249 C250 C251 C252 C253	31.42 31.42 136.24 67.81 16.76	100.00 20.00 20.00 140.00 160.00 400.00 120.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E N16' 29' 06"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12"	-	C373 C374 C379 C380 C381 C382	6.84 80.73 30.60 30.17 14.56 10.76	480.00 520.00 20.00 20.00 25.00 25.00	6.84 80.65 27.70 27.39 14.36	S10° 04′ 40″E N6° 02′ 17″W N33° 20′ 49″E N59° 36′ 42″W S86° 08′ 05″E S57° 07′ 03″E	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15' 24'39'48 80'13'27' 45'35'17'
C247 C248 C249 C250 C251 C252	31.42 31.42 136.24 67.81 16.76 157.89	100.00 20.00 20.00 140.00 160.00 400.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58"		C373 C374 C379 C380 C381 C382 C383	6.84 80.73 30.60 30.17 14.56 10.76 84.01	480.00 520.00 20.00 20.00 25.00 25.00 60.00	6.84 80.65 27.70 27.39 14.36 10.68	S10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  S86' 08' 05"E  S57' 07' 03"E  S84' 53' 52"E	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15' 24'39'48 80'13'27'
C247 C248 C249 C250 C251 C252 C253	31.42 31.42 136.24 67.81 16.76 157.89 116.78	100.00 20.00 20.00 140.00 160.00 400.00 120.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E N16' 29' 06"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24"		C373 C374 C379 C380 C381 C382 C383 C384	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74	480.00 520.00 20.00 25.00 25.00 60.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49	S10° 04′ 40″E N6° 02′ 17″W N33° 20′ 49″E N59° 36′ 42″W S86° 08′ 05″E S57° 07′ 03″E S84° 53′ 52″E N32° 11′ 46″E	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15' 24'39'48 80'13'27' 45'35'17'
C247 C248 C249 C250 C251 C252 C253 C254	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91	100.00 20.00 20.00 140.00 160.00 400.00 120.00 380.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E N16' 29' 06"W N19' 36' 15"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16"		C373 C374 C379 C380 C381 C382 C383 C384 C385	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75	480.00 520.00 20.00 25.00 25.00 60.00 60.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19	S10° 04′ 40″E  N6° 02′ 17″W  N33° 20′ 49″E  N59° 36′ 42″W  S86° 08′ 05″E  S57° 07′ 03″E  S84° 53′ 52″E  N32° 11′ 46″E  N25° 48′ 33″W	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15' 24'39'48 80'13'27' 45'35'17' 70'25'21'
C247 C248 C249 C250 C251 C252 C253 C254 C255	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09	100.00 20.00 20.00 140.00 160.00 400.00 120.00 380.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E N16' 29' 06"W N19' 36' 15"E N30' 54' 44"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81	S10° 04′ 40″E  N6° 02′ 17″W  N33° 20′ 49″E  N59° 36′ 42″W  S86° 08′ 05″E  S57° 07′ 03″E  S84° 53′ 52″E  N32° 11′ 46″E  N25° 48′ 33″W  S69° 03′ 46″W	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15' 24'39'48 80'13'27' 45'35'17' 70'25'21' 99'50'02
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34	100.00 20.00 20.00 140.00 160.00 400.00 120.00 380.00 420.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81 12.53 12.53	S10° 04′ 40″E  N6° 02′ 17″W  N33° 20′ 49″E  N59° 36′ 42″W  S86° 08′ 05″E  S57′ 07′ 03″E  S84° 53′ 52″E  N32° 11′ 46″E  N25° 48′ 33″W  S69° 03′ 46″W  S33° 39′ 15″W  S62° 40′ 17″W	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15' 24'39'48 80'13'27' 45'35'17' 70'25'21' 99'50'02 29'01'02'
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42	100.00 20.00 20.00 140.00 160.00 400.00 120.00 380.00 420.00 420.00 60.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C388	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 300.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81 12.53 12.53 75.18	S10° 04′ 40″E  N6° 02′ 17″W  N33° 20′ 49″E  N59° 36′ 42″W  S86° 08′ 05″E  S57° 07′ 03″E  S84° 53′ 52″E  N32° 11′ 46″E  N25° 48′ 33″W  S69° 03′ 46″W  S33° 39′ 15″W  S62° 40′ 17″W  N23° 36′ 04″W	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15 24'39'48 80'13'27 45'35'17 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80	100.00 20.00 20.00 140.00 160.00 400.00 120.00 380.00 420.00 420.00 60.00 100.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W  N39' 40' 42"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C388 C389	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 75.37 292.78	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00 300.00 280.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81 12.53 12.53 75.18 279.62	S10° 04′ 40″E  N6° 02′ 17″W  N33° 20′ 49″E  N59° 36′ 42″W  S86° 08′ 05″E  S57° 07′ 03″E  S84° 53′ 52″E  N32° 11′ 46″E  N25° 48′ 33″W  S69° 03′ 46″W  S33° 39′ 15″W  S62° 40′ 17″W  N23° 36′ 04″W  S40° 26′ 27″E	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15 24'39'48 80'13'27 45'35'17 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43 59'54'36
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30	100.00 20.00 20.00 140.00 160.00 160.00 400.00 380.00 420.00 420.00 100.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W  N39' 40' 42"E  N16' 26' 47"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 75.37 292.78 64.62	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00 300.00 280.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81 12.53 12.53 75.18 279.62 64.45	S10° 04′ 40″E  N6° 02′ 17″W  N33° 20′ 49″E  N59° 36′ 42″W  S86° 08′ 05″E  S57° 07′ 03″E  S84° 53′ 52″E  N32° 11′ 46″E  N25° 48′ 33″W  S69° 03′ 46″W  S33° 39′ 15″W  S62° 40′ 17″W  N23° 36′ 04″W  S40° 26′ 27″E  S17° 36′ 22″E	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15 24'39'48 80'13'27 45'35'17 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43 59'54'36 14'14'25'
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88	100.00 20.00 20.00 140.00 160.00 160.00 120.00 380.00 420.00 420.00 100.00 100.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W  N39' 40' 42"E  N16' 26' 47"E  S22' 24' 18"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391 C392	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37 292.78 64.62 129.07	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 260.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81 12.53 12.53 75.18 279.62 64.45 127.74	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15 24'39'48 80'13'27 45'35'17 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43 59'54'36 14'14'25' 28'26'31
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30	100.00 20.00 20.00 140.00 160.00 160.00 400.00 380.00 420.00 420.00 100.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W  N39' 40' 42"E  N16' 26' 47"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 75.37 292.78 64.62	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00 300.00 280.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81 12.53 12.53 75.18 279.62 64.45	S10° 04′ 40″E  N6° 02′ 17″W  N33° 20′ 49″E  N59° 36′ 42″W  S86° 08′ 05″E  S57° 07′ 03″E  S84° 53′ 52″E  N32° 11′ 46″E  N25° 48′ 33″W  S69° 03′ 46″W  S33° 39′ 15″W  S62° 40′ 17″W  N23° 36′ 04″W  S40° 26′ 27″E  S17° 36′ 22″E	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15 24'39'48 80'13'27 45'35'17 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43 59'54'36 14'14'25' 28'26'31
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88	100.00 20.00 20.00 140.00 160.00 160.00 120.00 380.00 420.00 420.00 100.00 100.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W  N39' 40' 42"E  N16' 26' 47"E  S22' 24' 18"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391 C392	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37 292.78 64.62 129.07	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 260.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81 12.53 12.53 75.18 279.62 64.45 127.74	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15 24'39'48 80'13'27 45'35'17 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43 59'54'36 14'14'25' 28'26'31 17'58'43
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94	100.00 20.00 20.00 140.00 160.00 160.00 120.00 380.00 420.00 60.00 100.00 80.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W  N39' 40' 42"E  N16' 26' 47"E  S22' 24' 18"W  S50' 31' 30"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391 C392 C393	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 75.37 292.78 64.62 129.07 94.14	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00 300.00 260.00 300.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81 12.53 75.18 279.62 64.45 127.74	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15 24'39'48 80'13'27 45'35'17 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43 59'54'36 14'14'25' 28'26'31 17'58'49
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35	100.00 20.00 20.00 140.00 160.00 160.00 120.00 380.00 420.00 420.00 100.00 100.00 440.00 440.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W  N39' 40' 42"E  N16' 26' 47"E  S22' 24' 18"W  S50' 31' 30"E  S50' 10' 36"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391 C392 C393 C394	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 75.37 292.78 64.62 129.07 94.14 37.69	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 260.00 260.00 300.00 300.00	6.84  80.65  27.70  27.39  14.36  10.68  77.31  46.49  69.19  91.81  12.53  75.18  279.62  64.45  127.74  93.75  35.26	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15 24'39'48 80'13'27 45'35'17 70'25'21 99'50'02 29'01'02
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32	100.00 20.00 20.00 140.00 160.00 160.00 400.00 380.00 380.00 420.00 60.00 100.00 80.00 440.00 460.00 480.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W  N39' 40' 42"E  N16' 26' 47"E  S22' 24' 18"W  S50' 31' 30"E  S50' 10' 36"E  N48' 14' 53"W  N54' 03' 41"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51" 11'37'37" 7'46'10" 3'51'27"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391 C392 C393 C394 C397 C399	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00 300.00 260.00 30.00 30.00 260.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81 12.53 75.18 279.62 64.45 127.74 93.75 35.26 50.53 77.88	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$51' 36' 55"E	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15 24'39'48 80'13'27 45'35'17 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43 59'54'36 14'14'25' 28'26'31 17'58'43 71'58'49 114'44'49
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282 C283	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32 31.42	100.00 20.00 20.00 140.00 160.00 160.00 120.00 380.00 420.00 420.00 100.00 100.00 440.00 460.00 480.00 480.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31 28.28	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W  N39' 40' 42"E  N16' 26' 47"E  S22' 24' 18"W  S50' 31' 30"E  N48' 14' 53"W  N54' 03' 41"W  N79' 00' 35"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51" 11'37'37" 7'46'10" 3'51'27" 90'00'00"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C390 C391 C392 C393 C394 C397 C399 C400	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18 49.09	480.00 520.00 20.00 20.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 300.00 260.00 30.00 30.00 30.00 780.00	6.84  80.65  27.70  27.39  14.36  10.68  77.31  46.49  69.19  91.81  12.53  75.18  279.62  64.45  127.74  93.75  35.26  50.53  77.88  49.08	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$51' 46' 55"E  \$72' 11' 55"E	0'48'58" 8'53'43" 87'39'57 86'25'00 33'22'15 24'39'48 80'13'27 45'35'17 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43 59'54'36 14'14'25' 28'26'31 17'58'49 114'44'49 17'13'39 3'36'21"
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282 C283 C284	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32 31.42	100.00 20.00 20.00 140.00 160.00 160.00 120.00 380.00 420.00 420.00 100.00 100.00 440.00 440.00 440.00 450.00 480.00 480.00 20.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31 28.28 28.28	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E N16' 29' 06"W N19' 36' 15"E N30' 54' 44"E S17' 01' 01"W S28' 19' 30"W S23' 28' 29"W N39' 40' 42"E N16' 26' 47"E S22' 24' 18"W S50' 31' 30"E S50' 10' 36"E N48' 14' 53"W N54' 03' 41"W N79' 00' 35"E N10' 59' 25"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51" 11'37'37" 7'46'10" 90'00'00"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391 C392 C393 C394 C397 C399 C400 C401	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18 49.09 50.59	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 300.00 260.00 300.00 30.00 30.00 780.00	6.84  80.65  27.70  27.39  14.36  10.68  77.31  46.49  69.19  91.81  12.53  12.53  75.18  279.62  64.45  127.74  93.75  35.26  50.53  77.88  49.08  50.58	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$51' 46' 55"E  \$72' 11' 55"E  \$75' 51' 35"E	0'48'58' 8'53'43' 87'39'57' 86'25'00 33'22'15 24'39'48 80'13'27' 45'35'17' 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43 59'54'36 14'14'25 28'26'31 17'58'43 71'58'49 114'44'49 17'13'39 3'36'21" 3'42'58'
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282 C283	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32 31.42	100.00 20.00 20.00 140.00 160.00 160.00 120.00 380.00 420.00 420.00 100.00 100.00 440.00 460.00 480.00 480.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31 28.28	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W  N39' 40' 42"E  N16' 26' 47"E  S22' 24' 18"W  S50' 31' 30"E  N48' 14' 53"W  N54' 03' 41"W  N79' 00' 35"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51" 11'37'37" 7'46'10" 3'51'27" 90'00'00"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C390 C391 C392 C393 C394 C397 C399 C400	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18 49.09	480.00 520.00 20.00 20.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 300.00 260.00 30.00 30.00 30.00 780.00	6.84  80.65  27.70  27.39  14.36  10.68  77.31  46.49  69.19  91.81  12.53  75.18  279.62  64.45  127.74  93.75  35.26  50.53  77.88  49.08	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$51' 46' 55"E  \$72' 11' 55"E	0'48'58' 8'53'43' 87'39'57' 86'25'00 33'22'15 24'39'48 80'13'27' 45'35'17' 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43 59'54'36 14'14'25 28'26'31 17'58'43 71'58'49 114'44'49 17'13'39 3'36'21" 3'42'58'
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282 C283 C284	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32 31.42	100.00 20.00 20.00 140.00 160.00 160.00 120.00 380.00 420.00 420.00 100.00 100.00 440.00 440.00 440.00 450.00 480.00 480.00 20.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31 28.28 28.28	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E N16' 29' 06"W N19' 36' 15"E N30' 54' 44"E S17' 01' 01"W S28' 19' 30"W S23' 28' 29"W N39' 40' 42"E N16' 26' 47"E S22' 24' 18"W S50' 31' 30"E S50' 10' 36"E N48' 14' 53"W N54' 03' 41"W N79' 00' 35"E N10' 59' 25"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51" 11'37'37" 7'46'10" 90'00'00"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391 C392 C393 C394 C397 C399 C400 C401	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18 49.09 50.59	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 300.00 260.00 300.00 30.00 30.00 780.00	6.84  80.65  27.70  27.39  14.36  10.68  77.31  46.49  69.19  91.81  12.53  12.53  75.18  279.62  64.45  127.74  93.75  35.26  50.53  77.88  49.08  50.58	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$51' 46' 55"E  \$72' 11' 55"E  \$75' 51' 35"E	0'48'58' 8'53'43' 87'39'57' 86'25'00' 33'22'15' 24'39'48' 80'13'27' 45'35'17' 70'25'21' 99'50'02' 29'01'02' 29'01'02' 14'23'43' 59'54'36' 14'14'25' 28'26'31' 17'58'49' 114'44'48' 17'13'39' 3'36'21' 3'42'58' 5'26'24'
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282 C283 C284 C285	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32 31.42 31.42 184.45	100.00 20.00 20.00 140.00 160.00 160.00 400.00 380.00 380.00 420.00 60.00 100.00 80.00 440.00 480.00 480.00 20.00 20.00 344.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31 28.28 28.28 182.25	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W  N39' 40' 42"E  N16' 26' 47"E  S22' 24' 18"W  S50' 31' 30"E  S50' 10' 36"E  N48' 14' 53"W  N54' 03' 41"W  N79' 00' 35"E  N10' 59' 25"W  S18' 38' 56"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51" 11'37'37" 7'46'10" 3'51'27" 90'00'00" 90'00'00"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391 C392 C393 C394 C397 C399 C400 C401 C403	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18 49.09 50.59 77.86	480.00 520.00 20.00 20.00 25.00 60.00 60.00 60.00 25.00 25.00 300.00 260.00 30.00 30.00 30.00 780.00 820.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81 12.53 12.53 75.18 279.62 64.45 127.74 93.75 35.26 50.53 77.88 49.08 50.58 77.83	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$55' 32' 21"W  \$61' 46' 55"E  \$72' 11' 55"E  \$75' 51' 35"E  N73' 06' 57"W	0'48'58' 8'53'43' 87'39'57 86'25'00 33'22'15 24'39'48 80'13'27 45'35'17 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43 59'54'36 14'14'25 28'26'31 17'58'49 114'44'49 17'13'39 3'36'21"
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282 C283 C284 C285	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32 31.42 184.45 173.73	100.00 20.00 20.00 140.00 160.00 160.00 160.00 380.00 420.00 420.00 400.00 100.00 100.00 460.00 480.00 480.00 20.00 344.00 324.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31 28.28 28.28 182.25 171.65	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E N16' 29' 06"W N19' 36' 15"E N30' 54' 44"E S17' 01' 01"W S28' 19' 30"W S23' 28' 29"W N39' 40' 42"E N16' 26' 47"E S22' 24' 18"W S50' 31' 30"E S50' 10' 36"E N48' 14' 53"W N54' 03' 41"W N79' 00' 35"E N10' 59' 25"W S18' 38' 56"W S18' 38' 56"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51" 11'37'37" 7'46'10" 35'1'27" 90'00'00" 30'43'18"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C390 C391 C392 C393 C394 C397 C399 C400 C401 C403 C404	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18 49.09 50.59 77.86 19.85	480.00 520.00 20.00 20.00 25.00 60.00 60.00 60.00 25.00 25.00 300.00 260.00 300.00 30.00 780.00 780.00 820.00 300.00	6.84  80.65  27.70  27.39  14.36  10.68  77.31  46.49  69.19  91.81  12.53  75.18  279.62  64.45  127.74  93.75  35.26  50.53  77.88  49.08  50.58  77.83  19.84	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$55' 32' 21"W  \$61' 46' 55"E  \$72' 11' 55"E  \$75' 51' 35"E  N73' 06' 57"W  N68' 30' 02"W	0'48'58' 8'53'43' 87'39'57' 86'25'00' 33'22'15' 24'39'48' 80'13'27' 45'35'17' 70'25'21' 99'50'02' 29'01'02' 29'01'02' 14'23'43' 59'54'36' 14'14'25' 28'26'31' 17'58'49' 114'44'49' 3'42'58' 5'26'24' 3'47'25' 17'49'42'
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282 C283 C284 C285 C286 C287 C296	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32 31.42 31.42 184.45 173.73 120.30 34.83	100.00 20.00 20.00 140.00 160.00 160.00 120.00 380.00 380.00 420.00 60.00 100.00 100.00 440.00 480.00 480.00 20.00 20.00 324.00 364.00 25.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31 28.28 28.28 182.25 171.65 119.75 32.08	N27' 15' 42"W  N0' 38' 12"E  N89' 21' 48"W  N16' 29' 06"W  S32' 13' 18"E  S8' 23' 31"W  N22' 42' 06"E  N16' 29' 06"W  N19' 36' 15"E  N30' 54' 44"E  S17' 01' 01"W  S28' 19' 30"W  S23' 28' 29"W  N39' 40' 42"E  N16' 26' 47"E  S22' 24' 18"W  S50' 31' 30"E  S50' 10' 36"E  N48' 14' 53"W  N54' 03' 41"W  N79' 00' 35"E  N10' 59' 25"W  S18' 38' 56"W  S18' 38' 56"W  N24' 32' 32"E  S40' 44' 41"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51" 11'37'37" 7'46'10" 3'51'27" 90'00'00" 30'43'18" 18'56'07" 79'50'09"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391 C392 C393 C394 C397 C399 C400 C401 C403 C404 C405 C406	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18 49.09 50.59 77.86 19.85 93.35 23.05	480.00 520.00 20.00 20.00 25.00 60.00 60.00 60.00 25.00 25.00 300.00 260.00 300.00 30.00 780.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81 12.53 12.53 75.18 279.62 64.45 127.74 93.75 35.26 50.53 77.88 49.08 50.58 77.83 19.84 92.97 22.49	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$53' 53' 17"W  \$51' 36' 55"E  \$72' 11' 55"E  \$75' 51' 35"E  N73' 06' 57"W  N68' 30' 02"W  N57' 41' 29"W  N37' 50' 11"W	0'48'58' 8'53'43' 87'39'57' 86'25'00' 33'22'15' 24'39'48' 80'13'27' 45'35'17' 70'25'21' 99'50'02' 29'01'02' 29'01'02' 14'23'43' 59'54'36' 14'14'25' 28'26'31' 17'58'49' 114'44'48' 17'13'39' 3'42'58'' 5'26'24' 3'47'25'' 17'49'42'
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282 C283 C284 C285 C286 C287 C296	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32 31.42 184.45 173.73 120.30 34.83 50.90	100.00 20.00 20.00 140.00 160.00 160.00 160.00 380.00 420.00 420.00 420.00 100.00 100.00 460.00 480.00 480.00 20.00 324.00 364.00 25.00 60.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31 28.28 28.28 182.25 171.65 119.75 32.08 49.39	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E N16' 29' 06"W N19' 36' 15"E N30' 54' 44"E S17' 01' 01"W S28' 19' 30"W S23' 28' 29"W N39' 40' 42"E N16' 26' 47"E S22' 24' 18"W S50' 31' 30"E S50' 10' 36"E N48' 14' 53"W N54' 03' 41"W N79' 00' 35"E N10' 59' 25"W S18' 38' 56"W S18' 38' 56"W N24' 32' 32"E S40' 44' 41"E S56' 21' 27"E	34'12'12" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51" 11'37'37" 7'46'10" 35'1'27" 90'00'00" 30'43'18" 18'56'07" 79'50'09" 48'36'38"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C390 C391 C392 C393 C394 C397 C399 C400 C401 C403 C404 C405 C406 C407	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18 49.09 50.59 77.86 19.85 93.35 23.05 35.47	480.00 520.00 20.00 20.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 300.00 260.00 30.00 30.00 780.00 780.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00	6.84  80.65  27.70  27.39  14.36  10.68  77.31  46.49  69.19  91.81  12.53  75.18  279.62  64.45  127.74  93.75  35.26  50.53  77.88  49.08  50.58  77.83  19.84  92.97  22.49  33.44	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$55' 32' 21"W  \$61' 46' 55"E  \$72' 11' 55"E  \$75' 51' 35"E  N73' 06' 57"W  N68' 30' 02"W  N57' 41' 29"W  N37' 50' 11"W  \$86' 16' 30"W	0'48'58' 8'53'43' 87'39'57 86'25'00' 33'22'15 24'39'48' 80'13'27' 45'35'17' 70'25'21' 99'50'02 29'01'02 29'01'02 29'01'02 14'23'43 59'54'36' 14'14'25 28'26'31 17'58'49 114'44'48' 17'13'39 3'36'21' 3'42'58' 5'26'24' 3'47'25' 17'49'42 44'01'42 67'44'56
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282 C283 C284 C285 C286 C287 C296 C297 C298	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32 31.42 184.45 173.73 120.30 34.83 50.90 97.03	100.00 20.00 20.00 140.00 160.00 160.00 120.00 380.00 380.00 420.00 420.00 100.00 100.00 480.00 480.00 20.00 20.00 344.00 324.00 354.00 25.00 60.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31 28.28 182.25 171.65 119.75 32.08 49.39 86.80	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E N16' 29' 06"W N19' 36' 15"E N30' 54' 44"E S17' 01' 01"W S28' 19' 30"W S23' 28' 29"W N39' 40' 42"E N16' 26' 47"E S22' 24' 18"W S50' 31' 30"E S50' 10' 36"E N48' 14' 53"W N54' 03' 41"W N79' 00' 35"E N10' 59' 25"W S18' 38' 56"W S18' 38' 56"W N24' 32' 32"E S40' 44' 41"E S56' 21' 27"E S14' 16' 41"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 6'00'12" 22'36'58" 6'55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51" 11'37'37" 7'46'10" 30'43'18" 30'43'18" 18'56'07" 79'50'09" 48'36'38" 92'39'37"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391 C392 C393 C394 C397 C399 C400 C401 C403 C404 C405 C406 C407 C412	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18 49.09 50.59 77.86 19.85 93.35 23.05 35.47 17.14	480.00 520.00 20.00 20.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 300.00 260.00 300.00 30.00 780.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00	6.84  80.65  27.70  27.39  14.36  10.68  77.31  46.49  69.19  91.81  12.53  12.53  75.18  279.62  64.45  127.74  93.75  35.26  50.53  77.88  49.08  50.58  77.83  19.84  92.97  22.49  33.44  16.91	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$53' 53' 17"W  \$51' 46' 55"E  \$72' 11' 55"E  \$75' 51' 35"E  N73' 06' 57"W  N68' 30' 02"W  N57' 41' 29"W  N37' 50' 11"W  \$86' 16' 30"W  N9' 18' 16"W	0'48'58' 8'53'43' 87'39'57' 86'25'00' 33'22'15' 24'39'48' 80'13'27' 45'35'17' 70'25'21' 99'50'02' 29'01'02' 29'01'02' 14'23'43' 59'54'36' 14'14'25' 28'26'31' 17'58'49' 114'44'49' 17'13'39' 3'36'21' 3'42'58' 5'26'24' 3'47'25' 17'49'42' 44'01'42' 67'44'56' 32'44'04'
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282 C283 C284 C285 C286 C287 C296	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32 31.42 184.45 173.73 120.30 34.83 50.90	100.00 20.00 20.00 140.00 160.00 160.00 160.00 380.00 420.00 420.00 420.00 100.00 100.00 460.00 480.00 480.00 20.00 324.00 364.00 25.00 60.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31 28.28 28.28 182.25 171.65 119.75 32.08 49.39	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E N16' 29' 06"W N19' 36' 15"E N30' 54' 44"E S17' 01' 01"W S28' 19' 30"W S23' 28' 29"W N39' 40' 42"E N16' 26' 47"E S22' 24' 18"W S50' 31' 30"E S50' 10' 36"E N48' 14' 53"W N54' 03' 41"W N79' 00' 35"E N10' 59' 25"W S18' 38' 56"W S18' 38' 56"W N24' 32' 32"E S40' 44' 41"E S56' 21' 27"E	34'12'12" 90'00'00" 55'45'24" 24'16'59" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51" 11'37'37" 7'46'10" 35'1'27" 90'00'00" 30'43'18" 18'56'07" 79'50'09" 48'36'38"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C390 C391 C392 C393 C394 C397 C399 C400 C401 C403 C404 C405 C406 C407	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18 49.09 50.59 77.86 19.85 93.35 23.05 35.47	480.00 520.00 20.00 20.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 300.00 260.00 30.00 30.00 780.00 780.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00	6.84  80.65  27.70  27.39  14.36  10.68  77.31  46.49  69.19  91.81  12.53  75.18  279.62  64.45  127.74  93.75  35.26  50.53  77.88  49.08  50.58  77.83  19.84  92.97  22.49  33.44	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$55' 32' 21"W  \$61' 46' 55"E  \$72' 11' 55"E  \$75' 51' 35"E  N73' 06' 57"W  N68' 30' 02"W  N57' 41' 29"W  N37' 50' 11"W  \$86' 16' 30"W	0'48'58' 8'53'43' 87'39'57' 86'25'00 33'22'15 24'39'48' 80'13'27' 45'35'17' 70'25'21' 99'50'02' 29'01'02' 14'23'43' 59'54'36' 14'14'25' 28'26'31' 17'58'49' 114'44'49' 17'13'39' 3'36'21'' 3'42'58' 5'26'24' 3'47'25' 17'49'42' 44'01'42' 67'44'56'
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282 C283 C284 C285 C286 C287 C296 C297 C298	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32 31.42 184.45 173.73 120.30 34.83 50.90 97.03	100.00 20.00 20.00 140.00 160.00 160.00 120.00 380.00 380.00 420.00 420.00 100.00 100.00 480.00 480.00 20.00 20.00 344.00 324.00 354.00 25.00 60.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31 28.28 182.25 171.65 119.75 32.08 49.39 86.80	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E N16' 29' 06"W N19' 36' 15"E N30' 54' 44"E S17' 01' 01"W S28' 19' 30"W S23' 28' 29"W N39' 40' 42"E N16' 26' 47"E S22' 24' 18"W S50' 31' 30"E S50' 10' 36"E N48' 14' 53"W N54' 03' 41"W N79' 00' 35"E N10' 59' 25"W S18' 38' 56"W S18' 38' 56"W N24' 32' 32"E S40' 44' 41"E S56' 21' 27"E S14' 16' 41"W	34'12'12" 90'00'00" 90'00'00" 55'45'24" 6'00'12" 22'36'58" 6'55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 46'27'49" 10'55'51" 11'37'37" 7'46'10" 30'43'18" 30'43'18" 18'56'07" 79'50'09" 48'36'38" 92'39'37"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391 C392 C393 C394 C397 C399 C400 C401 C403 C404 C405 C406 C407 C412	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18 49.09 50.59 77.86 19.85 93.35 23.05 35.47 17.14	480.00 520.00 20.00 20.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 300.00 260.00 300.00 30.00 780.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00	6.84  80.65  27.70  27.39  14.36  10.68  77.31  46.49  69.19  91.81  12.53  12.53  75.18  279.62  64.45  127.74  93.75  35.26  50.53  77.88  49.08  50.58  77.83  19.84  92.97  22.49  33.44  16.91	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$53' 53' 17"W  \$51' 46' 55"E  \$72' 11' 55"E  \$75' 51' 35"E  N73' 06' 57"W  N68' 30' 02"W  N57' 41' 29"W  N37' 50' 11"W  \$86' 16' 30"W  N9' 18' 16"W	0'48'58' 8'53'43' 87'39'57 86'25'00 33'22'15 24'39'48 80'13'27 45'35'17 70'25'21 99'50'02 29'01'02 29'01'02 14'23'43 59'54'36 14'14'25 28'26'31 17'58'49 114'44'49 17'13'39 3'36'21" 3'42'58' 5'26'24' 3'47'25'
C247 C248 C249 C250 C251 C252 C253 C254 C255 C256 C257 C258 C259 C260 C262 C272 C273 C274 C282 C283 C284 C285 C286 C287 C296 C297 C298	31.42 31.42 136.24 67.81 16.76 157.89 116.78 108.91 41.09 82.44 83.34 46.42 20.80 60.30 64.88 83.94 93.35 65.09 32.32 31.42 31.42 184.45 173.73 120.30 34.83 50.90 97.03 54.88	100.00 20.00 20.00 140.00 160.00 160.00 120.00 380.00 380.00 420.00 420.00 60.00 100.00 440.00 480.00 480.00 20.00 20.00 324.00 364.00 25.00 60.00 60.00 60.00	58.81 28.28 28.28 130.93 67.30 16.76 156.87 112.22 108.54 41.07 82.31 83.21 45.27 20.76 59.39 63.11 83.82 93.19 65.04 32.31 28.28 28.28 182.25 171.65 119.75 32.08 49.39 86.80 47.54	N27' 15' 42"W N0' 38' 12"E N89' 21' 48"W N16' 29' 06"W S32' 13' 18"E S8' 23' 31"W N22' 42' 06"E N16' 29' 06"W N19' 36' 15"E N30' 54' 44"E S17' 01' 01"W S28' 19' 30"W S23' 28' 29"W N39' 40' 42"E N16' 26' 47"E S22' 24' 18"W S50' 31' 30"E S50' 10' 36"E N48' 14' 53"W N54' 03' 41"W N79' 00' 35"E N10' 59' 25"W S18' 38' 56"W S18' 38' 56"W S18' 38' 56"W N24' 32' 32"E S40' 44' 41"E S56' 21' 27"E S14' 16' 41"W N37' 02' 52"E	34'12'12" 90'00'00" 90'00'00" 55'45'24" 6'00'12" 22'36'58" 55'45'24" 16'25'16" 6'11'42" 11'14'48" 11'22'10" 44'19'27" 11'55'01" 34'32'48" 10'35'51" 11'37'37" 7'46'10" 30'43'18" 18'56'07" 79'50'09" 48'36'38" 92'39'37" 104'48'31"		C373 C374 C379 C380 C381 C382 C383 C384 C385 C386 C387 C388 C389 C390 C391 C392 C393 C394 C397 C399 C400 C401 C403 C401 C403 C404 C405 C406 C407 C412 C419	6.84 80.73 30.60 30.17 14.56 10.76 84.01 47.74 73.75 104.55 12.66 12.66 75.37 292.78 64.62 129.07 94.14 37.69 60.08 78.18 49.09 50.59 77.86 19.85 93.35 23.05 35.47 17.14 186.57	480.00 520.00 20.00 25.00 25.00 60.00 60.00 60.00 25.00 25.00 25.00 300.00 260.00 300.00 30.00 780.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00 300.00	6.84 80.65 27.70 27.39 14.36 10.68 77.31 46.49 69.19 91.81 12.53 12.53 75.18 279.62 64.45 127.74 93.75 35.26 50.53 77.88 49.08 50.58 77.83 19.84 92.97 22.49 33.44 16.91 170.08	\$10' 04' 40"E  N6' 02' 17"W  N33' 20' 49"E  N59' 36' 42"W  \$86' 08' 05"E  \$57' 07' 03"E  \$84' 53' 52"E  N32' 11' 46"E  N25' 48' 33"W  \$69' 03' 46"W  \$33' 39' 15"W  \$62' 40' 17"W  N23' 36' 04"W  \$40' 26' 27"E  \$17' 36' 22"E  \$38' 56' 50"E  N39' 47' 17"W  \$53' 53' 17"W  \$53' 53' 17"W  \$55' 32' 21"W  \$61' 46' 55"E  \$72' 11' 55"E  \$72' 11' 55"E  \$72' 11' 55"E  \$75' 51' 35"E  N73' 06' 57"W  N68' 30' 02"W  N57' 41' 29"W  N37' 50' 11"W  \$86' 16' 30"W  N9' 18' 16"W  \$65' 04' 23"W	0'48'58' 8'53'43' 87'39'57' 86'25'00' 33'22'15' 24'39'48' 80'13'27' 45'35'17' 70'25'21' 99'50'02' 29'01'02' 29'01'02' 14'23'43' 59'54'36' 14'14'25' 28'26'31' 17'58'49' 114'44'48' 17'13'39' 3'42'58' 5'26'24' 3'47'25' 17'49'42' 44'01'42' 67'44'56' 32'44'04'

CURVE TABLE

 CURVE
 LENGTH
 RADIUS
 CHORD
 BEARING
 DELTA

 C128
 25.25
 285.00
 25.24
 N27' 44' 26"W
 5'04'34"

CURVE TABLE

 CURVE
 LENGTH
 RADIUS
 CHORD
 BEARING

 C303
 12.25
 25.00
 12.13
 N13\* 12' 34"E

						CU	RV	Έ	TAE	3LE	<u> </u>	
DELTA		CURV	Έ	LENGT	Н	RADII	JS	CH	IORD		BEA	
28°04'21"		C426		25.30		20.0	0	23	.65	s	36° 2	24' 2
24.07,07,		C427		10.37		1020.	00	10.	37	s	0° 27	7' 33
24.07,07,		C431		42.14		457.6	6	42	.13	N	1° 53	j' 15
11°47'11"		C432		102.49	,	457.6	6	10:	2.27	N	10° 5	6' 2
7'03'41"		C433		31.42		20.0	0	28	.28	S	27° 3	38' 3
13'33'40"		C435		118.30	)	437.6	6	117	7.94	S	6. 28	36
3°29'45"		C436		90.93		395.0	ю	90	.73	N	17° 5	9, 0
39*30'13"		C437		31.42		20.0	0	28	.28	s	33. 3	36' 3
2.08,522		C438		23.32		200.0	ю	23	.30	S	14° 4	-3'4
87*14'53"		C439		34.14		200.0	ю	34	.10	S	22° 5	57' 3
38'39'34"		C445		68.95		240.0	00	68	.71	N	19° 3	57' 1
139'10'46"		C446		37.13		20.0	0	32	.02	N	64° 3	34'1
35*49'50"		C447		22.76		232.9	97	22	.75	s	59° 2	26' 5
90,00,00		C448		25.91		25.0	0	24	.76	S	26° 5	57' 4
90,00,00		C449		85.08		60.0	0	78	.13	s	37° 5	53' 4
24'05'25"	Ī	C450		69.87		60.0	0	65	.99	N	68° C	)7' 2
56*10'59"	Ī	C451		71.95		60.0	0	67	.72	N	0° 24	1 25
16*38'36"	Ī	C452	_	42.87	_	60.0	0	41.	96	N	54° 2	25' (
16'08'52"		C453		40.26		60.0	0	39	.51	s	85° 5	53' 3
16'08'52"	Ī	C454		24.91		25.0	0	23	.89	N	84° 4	<del>1</del> 7' (
25'22'35"		C455		29.11		272.9	97	29	.10	N	59° 1	7' 4
6'42'59"		C456		77.46		272.9	97	77	.20	N	70° 2	28' 5
19'31'04"		C457		20.02		252.9	97	20	.02	S	76° 2	20' 3
45*37'42"		C458		83.09		252.9	97	82	.71	S	64° 4	10, C
52°18'07"		C459		63.20		220.0	00	62	.98	S	19° 3	57 <b>'</b> 1
714'40"												
28*57'18"	Г							_				
6'40'26"			L	INE T	Αŀ	3LE						
3°49'45"		LINE	LE	ENGTH		BEARI	NG					
58'02'03"		L1		8.40	N:	22° 46′	43"	=				
58'02'03"		L3		27.12	N	37° 56'	27"\	N				
25*54'13"		L4		19.75	N:	22° 46′	43"	Ξ				
66*42'02"		L5		15.86	N.	71° 23'	03 <b>"</b> \	v				
50°23'51"		L6		16.38	N.	71° 23'	03"v	v				
43*28'21"		L7		22.47	N!	57° 40'	47"\	N				
46°36'29"		L8		18.83	s	57° 39'	23"					
88*53'24"		L9		2.32	s	59° 23'	31"[					
29*28'22"		L17		13.70	N	27° 01'	04"					
42°21'03"		L18		13.19	s	27° 50'	58"\	v				
66°14'35"		L19		37.30	s	30° 18′	31"E					
24°39'05"	<u> </u>	L20		25.26	s	30° 35'	18"					
69"19'10"	<u> </u>	L22		15.86	N.	71° 23'	03"v	v				
115'34'15"	<u> </u>	L23		16.38	N.	71° 23'	03 <b>"</b> \	v				
28*59'50"	<u> </u>	L26		11.88	N.	34° 00'	35"					
37°53'33"	<u> </u>	L27		9.14	N.	38, 09,	02"					
11°08'01"		L28		6.03	N:	29, 53,	31 <b>"</b> \	<u>v</u>				
14'13'33"		L29		21.70	⊢	79° 39'		$\dashv$				
0°48'58"		L30		27.69	$\vdash$	78° 03'		$\dashv$				
8°53'43"		L34		15.22	⊢	13° 04'		$\dashv$				
87*39'57"		L35		20.56	⊢	15' 00'		-				
86*25'00"		L36		7.34	$\vdash$	13' 55'		$\dashv$				
33°22'15"		L37		20.73	$\vdash$	44° 43'		$\dashv$				
24*39'48"		L38		9.37	$\vdash$	58' 42'		$\dashv$				
80°13'27"		L39		9.37	$\vdash$	58' 42'		-				
45*35'17"		L41		16.73	N:	55° 15'	50"\	٧				
70°25'21"												
99*50'02"												
29'01'02"												

		CURV	Æ TAB	LE	
	LENGTH	RADIUS	CHORD	BEARING	DELTA
	25.30	20.00	23.65	S36° 24' 22"E	72*28'33"
	10.37	1020.00	10.37	S0° 27′ 33″E	0°34'56"
	42.14	457.66	42.13	N1° 53' 15"E	516'33"
	102.49	457.66	102.27	N10° 56′ 27″E	12'49'51"
	31.42	20.00	28.28	S27° 38' 38"E	90'00'00"
	118.30	437.66	117.94	S6° 59′ 36″W	15'29'14"
_	90.93	395.00	90.73	N17° 59' 04"E	13'11'22"
_	31.42	20.00	28.28	S33° 36′ 37″E	90'00'00"
_	23.32	200.00	23.30	S14° 43' 48"W	6*40'48"
	34.14	200.00	34.10	S22° 57' 35"W	9*46'46"
	68.95	240.00	68.71	N19° 37' 11"E	16'27'34"
_	37.13	20.00	32.02	N64° 34' 17"E	106'21'47"
	22.76	232.97	22.75	S59° 26' 54"E	5'35'50"
	25.91	25.00	24.76	S26° 57' 40"E	59°22'40"
	85.08	60.00	78.13	S37° 53′ 42″E	81*14'45"
_	69.87	60.00	65.99	N68° 07' 21"E	66°43'10"
	71.95	60.00	67.72	NO° 24' 25"E	68'42'40"
_	42.87	60.00	41.96	N54° 25' 01"W	40*56'12"
_	40.26	60.00	39.51	S85° 53' 38"W	38'26'30"
_	24.91	25.00	23.89	N84° 47' 04"W	57*05'06"
	29.11	272.97	29.10	N59° 17' 49"W	6*06'35"
_	77.46	272.97	77.20	N70° 28' 51"W	16"15'30"
	20.02	252.97	20.02	S76° 20' 34"E	4°32'04"
	83.09	252.97	82.71	S64° 40' 00"E	18*49'05"
	63.20	220.00	62.98	S19° 37' 11"W	16*27'34"

SUBDIVISION

FARM MX

FLORA MOYOCK TOWNSHI



### **Currituck County**

Department of Planning and Community Development 153 Courthouse Road, Suite 110 Currituck, North Carolina 27929 252-232-3055 FAX 252-232-3026

### **MEMORANDUM**

To: Mark Bissell, Bissell Professional Group

Justin Old, North-South Development LLC

From: Tammy D. Glave, CZO, Senior Planner

Date: October 14, 2021 10-28-21 reply to TRC comments

November 10, 2021 TRC Responses

December 1, 2021 reply to November 10, 2021 comments

Subject: PB 21-25 Flora Farm, Preliminary Plat/Special Use Permit

The following comments have been received for the October 13, 2021 TRC meeting for Flora Farm, Preliminary Plat/Special Use Permit. In order to be scheduled for the November 10, 2021 TRC Revision Review meeting, please address all comments and resubmit a revised plan by 3:00 p.m. on October 28, 2021. TRC comments are valid for six months from the date of the TRC meeting.

### Planning (Tammy Glave. 252-232-6025)

Denial:

- 1. While permitted by the NCDEQ Division of Water Resources, staff notes its concern about connecting dwelling units to the WWTP at Eagle Creek until the system is compliant with State standards. DWR has agreed that flow from Flora can be allocated to the facility, and funds are being set aside for system upgrades, which are expected to be completed prior to new homes being ready for occupancy at Flora.
- 2. This development is split between the Moyock and Shawboro Elementary School districts. Staff notes the following concerns regarding school capacity:
  - a. Per the plat, Phase 1 (55 Lots/13 Students) has an earliest recording date of June 1, 2022. The substantial completion date of expansion of Moyock Elementary is not until August 2023. Homes in the development could be constructed within that 14-month period without capacity available for the elementary school students those homes are projected to generate. Phase 2 (76 Lots/19 Students), with an earliest recording date of January 1, 2023, would face a similar issue.

The schedule for new connections is approximately as follows: SUP approval - assumed December, 2021; construction drawing approval: approximately April, 2022; construction completion – first phases: approx. October, 2022; final plat recordation: approx. Dec. 1, 2022; Completion of first homes for occupancy: summer, 2023. New students are therefore not

expected to enroll in the school system until the Moyock school expansions are ready for occupancy.

Please amend the Phasing Schedule on Sheet 3 to match this response. Earliest recording date for Phase 1, December 1, 2022 and amended the subsequent recording dates for phases accordingly to match the six month between recording phases.

The phasing schedule on sheet 3 has been updated to show 12-1-22 as the earliest recording date.

2021-2022 Moyock Elementary School Capacity	529
10-4-2021 School Enrollment	676
Current Available Capacity	0
After Expansion Capacity (Substantial	750
Completion Date of August 2023)	(Per County Manager 1/15/2021)
Available Capacity	74
Project Capacity Changes for this Development	+42
*Not factored into this count are students enrolling	g from homes under

<sup>\*</sup>Not factored into this count are students enrolling from homes under construction or homes to be constructed in subdivisions that have received final plat approvals

- b. The above then sets back the dates in Condition of C-MXR approval 2.C for subsequent recording of phases six months apart.
- c. There is zero capacity left at Shawboro Elementary School and no plans for expansion. There is not capacity for the 26 students this development is project to generate.

2021-202 Capacity	22 Shawbord	622			
10-4-202	1 School En	rollment			622
Available	e Capacity				0
Project Developr	Capacity ment	Changes	for	this	+26
+11 ( (	1 ' ( (1 '		1 1	1111	, ,

<sup>\*</sup>Not factored into this count are students enrolling from homes under construction or homes to be constructed in subdivisions that have received final plat approvals

With the recording of phase 1 pushed back nearly a year from the phasing projection that was included in the zoning approval, and the first 3 phases being substantially in the Moyock school district, it appears that the earliest recordation of the phases that are in the Shawboro school district will be June, 2024; with new home construction now taking 8-10 months to complete, it is projected that the first homes that are located in the Shawboro school district will be ready for occupancy between February and April, 2025. Substantial occupancy would likely not take place until after the end of the 2024-25

school year, and we understand that a new elementary school is planned to be open in August, 2025.

UDO Section 6.6 Adequate Public Facilities mandates that all required public facilities be in place or programmed to be in place within two years after the initial approval of the special use permit. It does not appear that a finding can be made that adequate school capacity will be available within two years of the special use permit approval of each of these phases.

- 3. The redesign of the road/lot layout on norther property line east of Roland Creek Canal has been determined to be a minor amendment since the change has no material effect on the character of the approved development. An amended C- MXR approval is not required for a minor amendment. Thank you.
- 4. The redesign for the portions of the 50' canal easement on individual lots has been determined to be a minor amendment since the change has no material effect on the character of the approved development; however, staff request that those buyers be made well aware that development is not allowed in that drainage easement area (i.e. plat notation, HOA docs, deeds) The BOC amended the UDO so that this required easement width is now 25'. A request is being submitted for minor modification of the zoning conditions to make this easement with consistent with the UDO, with the intent that no part of the easement be located within the lot boundaries.
- 5. Remove reference to "residual tract" from plan sheets for 0015-000-085D-0000 since it has been subdivided from the subject property. Identify ownership. The reference has been removed and ownership identified. The adjoining property owner information is now missing from all of the other adjoining lots. See Sheet 2 of the 10/18/2021 Resubmittal for comparison to this 10/28/2021 resubmittal for reference. Please correct. (G.S.)
  - Adjoining property owner information has been added as requested (sheet 7).
- 6. It does not appear that FIRM Map 3721804000K applies to this development. If correct, please remove reference to that map from Sheet 1, Note 5. The reference has been removed. COMPLETE
- 7. Please verify that the 5' non-access easement on all corner lots is not along the street that generates less traffic. For example, it is staff's opinion that Lots 271 and 272 should have the non-access easement on Flora Blvd since that will get substantially more traffic than the Moss Street cul-de-sac that has the non-access easement. (UDO Section 10.3.3) The non-access easement locations have been reviewed and modified. COMPLETE
- 8. Please verify that the heavy black lines at street intersections on Sheet 6 represent crosswalk stripping. (UDO Section 5.6.10) The markings do represent crosswalk stripping; a note to that effect has been added. COMPLETE
- 9. Call out easements for future connections of infrastructure (i.e. streets, sidewalks) not extending to the property line. (i.e. Orchid Street's northern terminus). If the ultimate plan is to connect Orchid Street to the exempt lot, that portion of right-of-way cannot be counted as open space. There is no intent to extend Orchid Street, so it appears that no future roadway easements are needed. COMPLETE
- 10. Call out the street tree planting easement on the Typical Lot Detail as described on Sheet 1, Note 7. The street tree planting easement has been added to the Typical Lot Detail. COMPLETE
- 11. Note 7 mentions pedestrian easements. Are there locations where the sidewalk is not within the ROW? If so, please identify. No pedestrian easements appear to be needed, so the notation has been removed. COMPLETE
- 12. Type A buffer required (not B) for property lines that adjoin AG zoning district (Eagle

- Creek) and the Exempt Division for Mary Nell Flora Brumsey and John J Flora III (if not remaining in cultivation). (UDO Sections 6.2.1, 5.2.6) The buffer has been changed to Type A. COMPLETE
- 13. Call out the major arterial buffer along the residential portion of the development where it abuts Caratoke Highway. (UDO Section 5.2.8) The major arterial buffer along the residential portion of the development has been added to landscaping plan. COMPLETE
- 14. As noted on the plan, if the exempt division continues to be farmed, a farmland buffer is required along that property line. COMPLETE
  - Correct "Fast" to "Fost" on Sheet 7, Condition 2.b. The spelling has been corrected. COMPLETE
  - 15. The red dashed lines on Sheet 7 appear to be a phasing line. Please add phase numbers or remove the red line to clarify. Phase lines have been removed from sheet 7. COMPLETE
  - 16. There is a meeting scheduled for later this month to discuss the availability of all County services (i.e. water, fire and rescue, law enforcement, etc.) because of the increased demand for development. Staff will not be able to speak to the availability of county services until the results of that meeting are shared. We have not received any updated information. The BOC will share information as it available.
  - 17. Please indicate that drainage easements are dedicated to Currituck County. (UDO Section 7.3.4) A note that drainage easements are dedicated to Currituck County has been added on sheet 1. COMPLETE
  - 18. Prior to Construction Drawing submittal, verify photometric plan of streetlights closest to Caratoke Highway and property lines do not exceed the footcandle limits listed in Table 5.4.6.D of the UDO. Photometrics will be verified at the construction drawing stage as requested.
  - 19. The commercial portion of the development will be reviewed using the non-residential standards in the UDO upon site plan submittal.

### Currituck County Chief Building Inspector/Fire Official (Bill Newns. 252-232-6023)

Reviewed with comment:

1. No street parking signage on streets. Parking not permitted. "No parking" sign locations have been added to the landscaping and signage plan. COMPLETE

### Currituck County Economic Development (Larry Lombardi, 252-232-6015)

Reviewed without comment.

### Currituck County Engineer (Eric Weatherly, 252-232-6035)

Reviewed with comments:

- 1. Please provide an explanation of the sewer flows.
  - a. Acceptance letter from Sandler is 45,000 gpd
  - b. The CWS Willingness to Serve letter is 62,847 gpd (277 lots and 7500 sf commercial)
  - c. The major subdivision application if for 277 lots

The 45,000 gpd allocation from Sandler is based on 160 gpd per lot, which is closer to, but still above, what is actually expected o be generated from this development. The CWS letter is based on a peak flow condition, but is actually superfluous to the application requirements. A separate site plan approval application will be filed for the commercial development when end users are knows for this part of the development.

- 2. It is preferred to keep stormwater ponds out of the Rowland Creek easement Since the easement width is being changed to 25', the ponds will be outside of the easement.
- 3. Include improvements to Rowland Creek (see attached) Limits of excavation have been added to sheet 4. Clarify that Rowland Creek through Flora has been or will be excavated to 3:1 slopes and 100-year storm carrying capacity.

The top of bank and grades shown on the preliminary plan set reflect these conditions.

### Currituck County GIS (Harry Lee. 252-232-4039)

Reviewed with comment:

1. THE STREET LABELED AS LYNNE LN NEEDS A DIFFERENT NAME. LYNNE LN IS ALREADY APPROVED IN THE FOST DEVELOPMENT. PLEASE CONTACT GIS AT 252.232.2034 TO DISCUSS PROPOSED STREET NAMES. (Not corrected with 10/28/2021 resubmittal)

### Currituck County Parks and Recreation (Jason Weeks, 252-232-3007)

Reviewed without comment.

### Currituck County Soil and Stormwater (Dylan Lloyd, 252-232-3360)

Reviewed with comment:

- 1. Are the public spaces / paths between Lydia Eunice Leif and Myriam Streets going to be used for drainage? If so, show flow arrows. Flow arrows have been added to sheet 4.
- 2. Berm along southwest portion of property will this impede flow across wetland delineated portions? It is intended that the berm be located between the new development and an improved property line ditch to improve existing and future drainage. Do not to construct berm through wetlands. Berm shall not impede flow of water between separated wetlands. Dylan will meet on site to discuss if desired.
  - Based on our recent discussion, the Fost and Flora wetlands are separated by an upland strip, and the berm is proposed to run along the edge of this strip, but its need and final location will be determined following computer modeling of the stormwater management system.
- 3. Align Pond 7 with Rowland Creek Canal Easement The edge of the pond has been aligned with the proposed 25' easement.

## <u>Currituck County Utilities Director (Will Rumsey, 252-232-2769)/Water Department – Distribution Supervisor (Dave Spence, 252-232-2769)</u>

Reviewed with comment: Approved as corrected.

1. We ask that all 2" waterlines are changed to a minimum of 4". The 2" line has been changed to 8" (it was mislabeled).

### Currituck County Public Utilities/Backflow (Chas Sawyer, 252-232-6060 ext. 4221)

Reviewed without comment.

### Albemarle Regional Health Services (Joe Hobbs, 252-232-6603)

Reviewed with comment:

1. OWNER/DEVELOPER NEEDS TO CONSULT WITH THE NC DEPT. OF

PB 18-23 Fost, Phases 6-9, Preliminary Plat/Use Permit 10/13/2021 TRC Comments Page 5 of 11 ENVIRONMENTAL QUALITY-DIVISION OF WATER RESOURCES (WASHINGTON REGIONAL OFFICE) CONCERNING WASTEWATER TREATMENT AND DISPOSAL APPROVAL FOR LOTS AND BUSINESSES THAT WILL MAKE UP THIS PROPOSED DEVELOPMENT. NCDWR has been consulted about wastewater service.

NC Department of Transportation – District Engineer (David Otts. 252-331-4860)
Reviewed without comment.

### NC Division of Coastal Management (Charlan Owens, 252-264-3901)

Reviewed without comment.

### US Army Corps of Engineers (Anthony Scarbraugh, 910-251-4619)

Approved with comment:

1. Any impacts to jurisdictional waters or wetlands of the US require prior approval from the US Army Corps of Engineers.



## Major Subdivision Application

OFFICIAL USE ONLY	
Case Numbers	
Date Made	
Guille Kareport	
Areason Feld	

Centact Information	
North-South Development, LLC	PROPERTY OWNERS  Name: John J. Flora III/Mary Neil Flora Br
Address 417 Caratoke Hwy Unit D Moyock, NC 27958	Address PO Box 369/117 Puddin Ridge Box Moyock, NC 27958
Telephone: 252-435-2718	Telephones 252-232-3095
-Mail Address Jold@qhoc.com	5-Mail Address
LEGAL RELATIONSHIP OF APPLICANT TO PROPERTY	OWNER Contract Purchaser
Request	
Physical Street Address: US Flwy, 168 and Su	
Prince   Manifeldmen Palenger   No.	0000, 0015-000-085B-0000, 0015-000-085C-0000
Subdivision Nomes Flora Farms	
Number of Lots or Units: 277	
TYPE OF SUBANITAL	TYPE OF SUBDIVISION
Conservation and Development Plan	Conservation Subdivision
O Amended Skerch Plan/Use Permit	Conservation Subdivision  Conservation Subdivision
CType I OR OType II	ci Planned Development
21 Construction Drawings (or amended)	
Final Flat (or amended)	
I hereby authorize county efficials to enter my proper applicable idendaryls: All information submitted and record.  Property Owner(1)/Applicable  ***HOTE: Note must be algored by the summe(s) of record managing of property informatic. If there are multiple property	6/23/202/
Community Meeting, if applicable	

Page II al 12 Page II al 12 Manage M NORTH

ho P	ermit Renigw Standards, if applicable media Statch Plan/Use Permit, Type II Protesting Plan
apo	ne of Use Permit and Project Norrative (please provide on additional paper if needed).
	Sec attached
	applicant shall provide a response to the each one of the following listers. The Board of resioners must provide specific findings of fact based on the evidence submitted. All findings shall be in the affirmative for the Board of Commissioners to bese the use permit.
L	the via will not endanger the public height or eafely.
	See attached.
i.	The use will not injure the value of adjoining or obutting fonds and will be in harmony with the area in which is is located.
	See attached
د	The use will be in conformity with the Lond Use Plan or other elficially adopted plan.  See attached
Ď.	The use will not exceed the county's obstry to provide adequate public facilities, including, but no limited to, schools, fire and rescue, law enforcement, and other county facilities. Applicable state standards and guidelines shall be followed for determining when public facilities are adequate.
	See attached
of r	e undersigned, do certify that all of the information presented in this application is occurate to the best by knowledge, information, and belief. Further, I hereby authorize county officials to enter my perty for purposes of determining zoning compliance. All information submitted and required as partitle application process shall become public record.
Jan 1	10/1 Xlaren 6/23/2
740 140	porty Carrier[1]/Apppicture  The Forest bush by digned by the numer(a) of recent, contract purchaser(s), as other partners, burning a consentral legislature. If there are multiple property numers/applicants a algorithm is required for each.
	Page 6 of
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# Major Subdivision Application

OFFICIAL USE ON	ILY:
Case Numbers	
Date filled:	
Gate Keepen	
Amount Paid:	

Contact Information	
APPUCANT: North-South Development, LLC  Address:  417 Caratoke Hwy., Unit D  Moyock, NC 27958	Address: PO Box 369/117 Puddin Ridge Road  Moyock, NC 27958
Telephone: 252-435-2718  E-Mail Address: jold@qhoc.com  LEGAL RELATIONSHIP OF APPLICANT TO PROPE	Telephone: 252-232-3005  E-Mail Address:  RTY OWNER: Contract Purchaser
Request	
Subdivision Name: Flora Farms	5A-0000, 0015-000-085B-0000, 0015-000-085C-0000  Phase:
Number of Lots or Units: 277	TYPE OF SUBDIVISION
TYPE OF SUBMITTAL  Conservation and Development Plan  Amended Sketch Plan/Use Permit  Preliminary Plat (or amended)  Type 1 OR Type II  Construction Drawings (or amended)  Final Plat (or amended)	Traditional Development Conservation Subdivision Planned Unit Development Planned Development
record.  Mary Well Flora Brusse  Property Officer(s)/Applicant*	roperty for purposes of determining compliance with all and required as part of this process shall become public  June 24 202    Date  Determining compliance with all and required, contract purchaser(s), or other person(s) having a property owners/applicants a signature is required for each.
Community Meeting, if applicable	Meeting Location:
Date Meeting Held:	

lse Pa	ermit Review Standards, if applicable
JD Am	ended Sketch Plan/Use Permit, Type II Preliminary Plat
IPOO	se of Use Permit and Project Narrative (please provide on additional paper if needed):
ai pos	See attached
	Obb Milhorida
	applicant shall provide a response to the each one of the following issues. The Board of issioners must provide specific findings of fact based on the evidence submitted. All findings shall be in the affirmative for the Board of Commissioners to issue the use permit.
١.	The use will not endanger the public health or safety.
	See attached
B.	The use will not injure the value of adjoining or abutting lands and will be in harmony with the area in which it is located.
	See attached
	oce attached
C.	The use will be in conformity with the Land Use Plan or other officially adopted plan.  See attached
D.	The use will not exceed the county's ability to provide adequate public facilities, including, but not limited to, schools, fire and rescue, law enforcement, and other county facilities. Applicable state standards and guidelines shall be followed for determining when public facilities are adequate.
	See attached
of n	o undersigned, do certify that all of the information presented in this application is accurate to the best my knowledge, information, and belief. Further, I hereby authorize county officials to enter my perty for purposes of determining zoning compliance. All information submitted and required as par- uis application process shall become public record.
A	June 24 2021
N9	Form must be signed by the owner(s) of record, contract purchaser(s), or other person(s) having a recognized orly interest. If there are multiple property owners/applicants a signature is required for each.
Lab	Mejor Subdivision Applicatio
	Revised 7/1/201

### ATTACHMENT TO APPLICATION FOR PRELIMINARY PLAT/USE PERMIT APPROVAL for Flora Farm

### Purpose of the Use Permit and Project Narrative

The goal is to build a community that has a creative design, with single family lots in an attractive arrangement, with an efficient use of open space that promotes an active lifestyle and strong sense of community. Limited, neighborhood commercial uses are also proposed on a 2.3 acre portion of the property.

The ways in which these goals and objectives will be achieved is shown graphically on the approved Conditional Zoning Plan, and is further described in the following paragraphs:

- Housing type has been revised to be all conventional frontloaded single family development lots.
- The Community will be pedestrian friendly, with open spaces and greenways strategically located throughout the community, and with a combination of sidewalks, multi-use paths, a dog park and a clubhouse with attractive outdoor recreation areas.
- Lower areas that are more flood prone are being set aside as open space, most of which will be
  used for the construction of lakes and ponds for the management of stormwater.

#### **USE PERMIT CRITERIA:**

### 1. The use will not endanger the public health or safety.

Transportation, portable water, wastewater and stormwater management will be addressed in accordance with the following paragraphs:

a. <u>Transportation</u>: The main subdivision entrance will be connected directly to Survey Road. Deceleration and acceleration lanes will be provided in accordance with NCDOT Standards and have been approved by NCDOT. Connectivity will be provided to the Fost development to the east.

Roadways have been laid out generally as shown on the Master Plan and will be in accordance with the approved right-of-way and pavement width characteristics, using the NCDOT standards.

Appropriate access has been made for emergency service vehicles in accordance with applicable standards including the N.C. Fire Code.

- <u>b.</u> <u>Potable Water</u>: Water will be supplied by Currituck County via an existing 12" main located on Caratoke Highway, which will be tapped and looped through the site. Fire protection will be provided in accordance with UDO Standards and the applicable ISO standards. The water system will be modeled to demonstrate adequate flow and pressure for fighting fires while meeting the maximum day domestic demand.
- <u>c.</u> <u>Wastewater</u>: A centralized off-site wastewater treatment and disposal facility will be utilized (Currituck Water & Sewer Major Utility), and connected to a force main running from the Fost Tract to the treatment facility at Eagle Creek. A wastewater collection system will be constructed by the Developer and managed by the same wastewater utility. The utility will be regulated by the North Carolina Utilities Commission.
- d. Stormwater Management: A major drainage way known as Rowland Creek Canal runs through the development property. Due to the importance of this drainage feature not only to the development but to other properties located both upstream and downstream of this property, the Developer has committed to cleaning and laying back the slopes and stabilizing the portion of Rowland Creek that runs through this property to the Eagle Creek pup station. The Developer has also committed to improving the common property line ditch along Ranchland and the Eagle Creek property to the west.

On-site stormwater will be managed by constructing a series of stormwater management ponds that will be interconnected and will retain and slow-release stormwater to existing outlets. In addition to modeling and retaining stormwater to the UDO and Stormwater Manual standard of the difference between runoff from the 10-year developed condition and runoff from a 2-year wooded condition site, stormwater will be modeled for the 100-year storm without adversely impacting neighboring properties. Implementation of this plan should improve drainage for neighboring properties compared to existing conditions.

Stormwater will be conveyed to on-site retention ponds through a combination of curbs with inlets, stormwater pipes, and open vegetated swales.

2. The use will not injure the value of adjoining or abutting lands and will be in harmony with the area in which it is located.

Land use to the west is residential. Land use to the north is commercial, land to the east is residential lots, and land to the south is residential lots. Perimeter compatibility is being addressed as follows:

- a. To the west: A 25' vegetated buffer is being provided to existing development.
- b. To the north No compatibility issues are envisioned
- c. To the east: A PD-r with higher density is being developed ahead of this community.

- d. To the south: Compatible residential development is being proposed and a minimum 50 foot open space buffer is shown to the property line.
- e. Limited commercial development is proposed along the highway where apporpriate.

Lot size ratios and density ratios are comparable to other developments that have been approved in the immediate area and in other parts of Moyock.

The applicant is not aware of any adverse impacts on land value in the surrounding area. The applicant anticipates that its proposed stormwater improvements may benefit the property values of the nearby Ranchland community. The Development is anticipated to **increase** the values of nearby properties by improving existing drainage problems.

The site is adjacent to the Eagle Creek neighborhood. The proposed use is compatible with these adjacent uses and furthers the County's goal of concentrating new residential development in the vicinity of existing residential development. Nearby communities include existing planned communities of 588 lots to the west, a 479 unit PD-R to the east and 426 lots under development to the north of Caratoke Highway, all in the vicinity of the Flora tract. The development is appropriate for the location as it continues existing development patterns in this part of Moyock. Lot area ratios and density ratios are consistent with other developments that have been approved recently nearby and in other parts of Moyock.

- 3. The use is consistent with the goals, objectives, and policies of the Land Use Plan or other applicable county-adopted plans:
  - a. The use is consistent with the following policies of the Currituck County Land Use Plan ("LUP"): The project is consistent with the Land Use Plan policies addressing density, recreational and open space, transportation infrastructure, and utilities:
  - *i.* **Density:** The project is located within the Full Service Area of the Land Use Plan, and proposes a density of 1.37 units per acre. The proposed use will include residential development in village like-clusters and will include open spaces surrounding the clusters, helping to prevent traditional suburban sprawl. The applicant is proposing centralized sewer, and the Full Service Area designation recognizes higher density (above 2 units per acre) is appropriate here. The proposed use will not interfere with nearby resource production activities. This is consistent with the County's density designations because development is occurring along Caratoke Highway, a major transportation corridor, and is an infill site between two existing residential subdivisions. This is consistent with the following Land Use Plan policies:

POLICY AG6: For areas experiencing intense development pressure, new residential development may be allowed to locate in COMPACT, VILLAGE-LIKE CLUSTERS, PREFERABLY NEAR EXISTING, NON-AGRICULTURAL ACTIVITIES AND SERVICES, or in other locations that will not interfere with resource production activities. Overall density shall remain very low, with permanent open space, dedicated during the development review process, surrounding such clusters of homes. LUP, p. 9-7.

POLICY HN1: Currituck County shall encourage development to occur at densities appropriate for the location. LOCATION AND DENSITY FACTORS shall include whether the development is within an environmentally suitable area, the type and capacity of sewage treatment available to the site, the

adequacy of transportation facilities providing access to the site, and the proximity of the site to existing and planned urban services. For example, projects falling within the Full Services areas of the Future Land Use Map would be permitted a higher density because of the availability of infrastructure as well as similarity to the existing development pattern. Such projects could be developed at a density of two (2) or more dwelling units per acre.... LUP, p. 9-7.

<u>POLICY HN3</u>: Currituck County shall especially encourage two forms of residential development, each <u>with the objective of avoiding traditional suburban sprawl</u>...COMPACT, MIXED USE DEVELOPMENTS or DEVELOPMENTS NEAR A MIXTURE OF USES that promote a return to balanced, self-supporting community centers generally served by centralized water and sewer. The types of development are contemplated for the Full Service Areas identified on the Future Land Use Map. LUP, p. 9-7.

#### ii. Utilities:

Stormwater: The applicant is aware of stormwater drainage issues in the surrounding properties and has conducted a stormwater study to investigate causes in Rowland Creek, upstream and downstream from the property. That study has shown significant restrictions to flow on both on- and off-site properties, which the applicant is willing to remove and restore with the off-site landowners' consent. The applicant is also placing significant drainage facilities on site to collect and control its own water. The applicant plans to model the 100-year storm event and to manage stormwater runoff from that event. These commitments will provide a public benefit to the County by improving existing drainage problems with private funds, and providing a responsible mechanism for the site and surrounding areas to drain.

<u>Wastewater</u>: The applicant is proposing to connect to an off-site wastewater treatment and disposal facility. This will give the existing system new funds for upgrades, repair, and maintenance. Thus, the development will have a positive impact on sewer facilities and will not strain the County's infrastructure, consistent with the Full Service Area designation which anticipates additional density on this site.

This is consistent with the following Land Use Plan policies:

POLICY WS7: Currituck County allows for the appropriate use of PACKAGE SEWAGE TREATMENT PLANTS as a means of achieving more efficient land use, while properly disposing of waste. Such systems shall have a permanent organizational ownership to guarantee their proper management, including operation, maintenance and replacement needs. Depending on their location in the county, such systems may be required to have a design that allows for assimilation into a centralized system at a future date. LUP, p. 9-13.

<u>POLICY WQ3</u>: Currituck County supports policies, plans and actions that help protect the water quality of the county's estuarine system by preventing SOIL EROSION AND SEDIMENTATION, and by controlling the quantity and quality of STORMWATER RUNOFF entering the estuary. LUP, p. 9-17.

<u>POLICY WQ4</u>: RUNOFF AND DRAINAGE from development, forestry and agricultural activities shall be of a quality and quantity as near to natural conditions as possible. Post-development runoff shall not exceed pre-development volumes. LUP, p. 9-17.

*iii.* Recreational and Open Space: The project will provide for sidewalks, greenways, and walking trails to encourage mobility by pedestrians and bicyclists. It also

preserves a significant amount of open space and natural features throughout to promote recreation and preserve natural areas. This is consistent with the following Land Use Plan policies:

<u>POLICY TR9</u>: BIKEWAY FACILITIES shall be encouraged as energy-efficient, healthful, and environmentally sound alternatives to the automobile. The inclusion of bikeways, sidewalks, trails, and other alternatives to the automobile shall be encouraged in both public and private developments. LUP, p. 9-11.

POLICY PR4: The County shall seek to identify, plan for and develop a system of OPEN SPACE GREENWAYS, HIKING and BIKING TRAILS as opportunities may allow. The use of (1) natural corridors such as streams and floodplains, and (2) man-made corridors such as utility and transportation rights-of-way and easements, shall be emphasized. LUP, p. 9-14.

POLICY PR6: All new residential development shall provide for ADEQUATE OPEN SPACE AND RECREATION IMPROVEMENTS including, as may be appropriate, funding in proportion to the demand created by the development. The amount of open space and improvements may be determined according to the number of dwelling units in the development and/or by a percentage of the total acreage in the development. Fees in lieu of land dedication shall be based on the inflation adjusted assessed value of the development or subdivision for property tax purposes. LUP, p. 9-14.

*iv. Transportation infrastructure:* As set forth in the master plan, the project will provide for paved public roadways and drainage infrastructure, and will include convenient circulation within the proposed neighborhood. The project will have direct access to Survey Road and connectivity to the Fost development, which has direct access to NC 168, and will not include access to higher intensity development through an area of lower intensity development. This is consistent with the following Land Use Plan policies:

<u>POLICY TR12</u>: New residential developments shall provide for the installation of PAVED PUBLIC ROADWAY AND DRAINAGE INFRASTRUCTURE at the time of development. This policy is intended to prevent the creation of substandard developments that must later correct for infrastructure problems that could have been avoided, had they been installed properly from the beginning.... LUP, p. 9-11.

<u>POLICY CA1</u>: The important economic, tourism, and community image benefits of attractive, functional MAJOR HIGHWAY CORRIDORS through Currituck County shall be recognized. Such highway corridors, beginning with US 158 and NC 168, shall receive priority attention for improved appearance and development standards, including driveway access, landscaping, buffering, signage, lighting and tree preservation. LUP, p. 9-19.

<u>POLICY TR11</u>: ACCESS TO HIGHER INTENSITY DEVELOPMENT shall generally not be permitted through an area of lower intensity development. For example, access to a multi-family development, a major park facility or other large traffic generator shall not be permitted through a local street serving a single-family residential neighborhood. LUP, p. 9-11.

<u>POLICY TR8</u>: Local streets shall be designed and built to allow for convenient CIRCULATION WITHIN AND BETWEEN NEIGHBORHOODS and to encourage mobility by pedestrians and bicyclists. Care shall be taken to encourage local street "connectivity" without creating opportunities for cut-through traffic from outside the connected areas. LUP, p. 9-11.

## b. The proposed use is consistent with the following provisions of the Moyock Area Policy Emphasis of the Land Use Plan:

Moyock Area Policy Emphasis: The policy emphasis of this plan is on properly managing the increased urban level of growth that this area is sure to experience over the next decade and beyond. Residential development densities should be medium to high depending upon available services. In areas where on-site wastewater is proposed and other County services are limited, development density should be limited to 1-2 units per acre. However in areas where central sewer is proposed or existing, additional services are available and the character of the surrounding areas supports it, higher density ranging from 3-4 units per acre could be considered through the use of overlay zones.

Clustered housing developments, with open space required by ordinance, will need to be encouraged. If sewage treatment collection systems are built, whether publicly or privately, all such systems should be designed so as to allow them to be tied together into a single system at some point in the future. LUP, p. 11-4.

The proposed use will include clustered housing accompanied by open spaces. The applicant is proposing connection to a privately developed sewer to avoid strain on County infrastructure. Because the wastewater system will be a central sewer system, and the 1.37 units/acre is below the 3-4 units per acre allowed where central sewer is available, the proposed use is consistent with the Moyock Area Policy Emphasis.

### c. The proposed use is consistent with the Future Land Use Map.

The Currituck County Future Land Use Map designates the site as a Full Service area. In Full Service areas, residential density can be increased to 3-4 units per acre depending on the available services. A greater diversity in housing types is also appropriate in Full Service Areas. The proposed use will have 1.37 units per acre, which is within the contemplated development densities from Section 3.7.3 of the UDO for MXR districts.

## d. The proposed use is consistent with the following policies and action items of the Moyock Area Small Area Plan ("SAP"):

The proposed use will include open spaces, sidewalks, greenways, and walking trails to for recreational purposes, and a clubhouse for community gathering. The plan will provide for connections to Ranchland, as appropriate. The applicant is studying the existing stormwater drainage issues and will ensure that the project improves these issues.

- <u>POLICY TR 2</u>: Ensure that development is designed with an interconnected, multi-modal transportation network between neighborhoods, activity centers, and other destinations to improve mobility and emergency access. Development of an interconnected road network east and west of Highway 16 that allows north-south movement for local residential traffic is strongly encouraged. SAP, p. 16.
- <u>POLICY IS 1</u>: The costs of infrastructure, facilities, and services related to new growth and development should be borne primarily by those creating the demand. SAP, p. 17.
- **POLICY IS 4**: Ensure that stormwater runoff, soil erosion, and sedimentation is properly managed to reduce nuisance flooding and pollution of sensitive environmental areas. SAP, p. 17.

**POLICY IS 5**: Encourage retrofitting of NCDOT drainage infrastructure to manage nuisance flooding. SAP, p. 18.

ACTION IS 5A: Coordinate with NCDOT to develop a plan of action for addressing ... Ranchland subdivision flooding.... SAP, p. 18.

<u>POLICY R-1</u>: Expand and develop recreational opportunities for all ages and users including access to the water and natural environment, walking trails, multi-purpose fields, multi-purpose community building, and other non-traditional types of recreational opportunities that are consistent with the Currituck County Parks and Recreation Master Plan. SUP, p. 21.

The use will not exceed the county's ability to provide adequate public facilities, including, but not limited to, schools, fire and rescue, law enforcement, and other county facilities.

The site has adequate access to Survey Road and NC 168 and will be adequately served by schools (it is our understanding that additional school capacity will be available within 24 months), police, fire, and emergency services. The applicant has completed a stormwater study and improvements plan to ensure that stormwater management will be enhanced not only for the development but also for the surrounding community. Sewer service will be provided off-site and will not burden the county's wastewater system.

### **The Flora Tract**

Community Meeting for a Request for Preliminary Plat and Special Use Permit

June 22, 2021

Scheduled Time/Place: 5:30pm, Eagle Creek Event Pavilion, Moyock, NC Meeting Began at 5:35PM/ Meeting Ended: at approximately 6:25PM

Attendees from community(See attached sign-in sheet)

The meeting began at 5:35 PM and ended at approximately 6:25 PM. Several attendees stayed and talked informally for about another half hour.

An overview was first provided of the review process, with this being the second major step in the approval process after the conditional rezoning that was approved earlier this year.

Since the plan has not changed since the previous community meeting, the community comments and responses that were documented at that meeting were reviewed first.

New comments and responses we're received as follows:

Comments from the Community	How Addressed
Where will the ponds drain?	Some of them will be interconnected. The front ponds will drain to Survey Road and the back ponds will drain to Rowland Creek canal.
What about the ponds in the back? Will any drain to the new ditch on the property line?	One or two of the ponds in the back part of the development may drain to the new ditch along the property line which will then drain to Rowland Creek. It is uncertain at this point where the outlet for pond 10 will be.
Who will maintain the stormwater systems?	A stormwater association is being formed that will be responsible for the stormwater systems for Fost, Flora and Moyock Farms. This will be its sole function. It will operate under the authority of the HOA's that are being set up for the three developments.

What about wind tide influence?	Wind tides have been studied along Rowland						
	Creek, and these is no real influence on Rowland creek on this side of 168.						
How will sewer be handled?	It will be pumped to Eagle Creek via a new force man that will be installed along Survey Road						
When will the ditch be dug?	With property owner permission, it will be dug from the top of the existing ditch bank on the Ranchland and Eagle Creek side and widened on the Flora side.						
Will trees be taken down?	No trees will be taken beyond the top of the existing ditch bank.  Comment: : It's OK with me if you take the trees on my side						
What can be done on the 22 acres?	Right now it has agricultural zoning and there are a few things that could be done but there probably will need to be a rezoning in the future for some business uses.						
Will there be ranch homes as well as two-story?	Probably yes.						
How will the houses be priced?	Home construction is too far in the future to be able to price them right now.						
When will you know how big the ditch will be along the property line?	It will be sized when the construction drawings are done for the first phases of the development which will probably be this fall, following stormwater modeling.						
Will we be able to see the results of the model?	We will share the results of the modeling and the plans.						
Will there be a traffic light at the Fost entrance?	Most likely there will be, following a signal warrant study when there is enough traffic to justify one.						
Where will the entrance be to Flora?	(It was pointed out on the map.)						
Will there be a signal at survey Road at 168?	No light is planned there but there will be improvements to survey road such as widening and a turning lane and then a left turn restriction at 168.						
Is there any way to connect Moyock Farms to Ranchland?	No, because any road would need to cross private property.  Comment: Good.						
Will there be natural gas?	Yes						
Will there be recreational amenities?	Yes, there will be a clubhouse and pool, dog park, picnic and gathering areas, and multi-use paths.						

Community Meeting Sign-In Sheet Flora Farm Preliminary Plat/Use Permit June 22, 2021

					1										6.A.
E-MAIL			mar L@ bisellpretssrana   group. com	Korreen corbitta USOnabay	Na Carlo										
5:30PM TELEPHONE	252-435-1018		(22) 201-3266	252 540.0258	252.207.3328		757 328-5678		2830 630 136		757 404-5873				
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NAME	Nathan Capps, SR	BeHy Capps	MARK Assier		David Roberson	V		Paller Bussend		CANDA CHECOTAL ACAD	Mis Pailbert & Rose		P	acket	Pa. 6

Attachment: 7 Comm. Mtg. Minutes (PB 21-25 Flora Farms)

ROY COOPER Governor ELIZABETH S. BISER Secretary S. DANIEL SMITH Director



September 17, 2021

Ms. Laurie B. LoCicero Currituck County Planning and Community Development 153 Courthouse Road, Suite 110 Currituck, North Carolina 27929

RE: Wastewater Service – Flora Development and Fost Development

Eagle Creek WWTP

Moyock - Currituck County Permit No. WQ0014306

Dear Ms. LoCicero:

Sandler Utilities at Mill Run, LLC holds permit Number WQ0014306 allowing for the continued operation of the Eagle Creek wastewater treatment and disposal system. The subject treatment system permit is in effect through June 30, 2027 and can be renewed and reissued. Based on a cursory review of the August 4, 2021 permit (attached), the following is offered in regards to available flow allocation for the Eagle Creek WWTP:

- Permit WQ0014306 was renewed and updated on August 4, 2021. The current permit specifies and/or covers:
  - Operation of a 175,000 gallon per day reclaimed water utilization and high-rate infiltration facility
  - o Continued operation of a 5.5 million gallon high rate infiltration basin
  - Ocontinued operation of a dedicated reclaimed water utilization system with 262,625 gpd of capacity. Note, Condition I.6 of the Permit (attached) specifies, in part, "Prior to the expansion of the facility above the currently permitted 175,000 GPD, the Permittee shall provide a new Soil Evaluation and Hydrogeologic Report to determine the total disposal capacity of the facility."
  - Construction only of the WWTP expansion to its original permitted capacity of 350,000 gpd
  - Expansion of the non-conjunctive reclaimed water utilization facilities to provide a disposal capacity of 321,000 gpd (with satisfaction of criteria specified in Condition I.6.).
- Pathways forward to increase the operational capacity of facility include:



- Reassessment of current disposal capacities for the golf course documenting its ability to accommodate higher disposal volumes
- Obtaining concurrence from DWR regarding recommendations presented in a Soil Evaluation and Hydrogeologic Report
- Seeking a permit amendment/modification to support higher disposal volumes (based on conclusions made from a recent Soil and Hydrogeologic evaluation)
- The WWTP must demonstrate consistent performance achieving permit treatment requirements, prior to submittal of a sewer extension permit application. WWTP performance must be satisfactory for approval of any potential sewer extension permits.

Please note that the above position is based on a preliminary review of Eagle Creek's operational status. A final determination regarding the feasibility of directing flow from potential new development projects to the Eagle Creek WWTP would not be made until review of a sewer extension permit application package is performed. All conditions and requirements for issuance of a sewer system extension permit must be satisfied to enable approval of such permit. No part of a sewer collection system shall be constructed without first obtaining required permits.

This letter does not commit or obligate Sandler Utilities at Mill Run, LLC or any future facility owner to provide sewer service for other development projects or prospective customers, nor does it obligate the Division of Water Resources to issue related sewer system permits for the project.

Should you have any questions concerning this matter, please contact me at 252-948-3939 or at *david.may@ncdenr.gov*.

Sincerely,

David May

David May, L.G., Supervisor Water Quality Regional Operations Section Division of Water Resources Washington Regional Office

Attachment: Eagle Creek WWTP Permit (No. WQ0014306)

cc: EnviroLink – electronic copy via e-mail Sandler Utilities at Mill Run, LLC Laserfiche



### SANDLER UTILITIES AT MILL RUN, LLC

Michael Myers

Currituck Water & Sewer, LLC

4700 Homewood Ct., Suite 108

Raleigh, NC 27609

Re: Acceptance of Additional Flow, Fost Tract

Dear Mr. Myers,

Sandler Utilities at Mill Run, LLC ("Sandler Utilities") provides this written confirmation that the wastewater agreement permits the acceptance of the flow from the Fost project.

The current capacity of our plant is 175,000 gpd with permitted disposal capacity of 350,000 gpd. Our actual average inflow is 52,695 gpd, leaving remaining capacity of 122,305 gpd prior to expansion of the facility. As such, we have the ability to accept 104,805 gpd of flow prior to expansion to 350,000 gpd. The Fost Tract is only expected to generate 57,480 gpd of flow.

As you are aware, Sandler Utility and Currituck Water & Sewer are in the process of transferring the Eagle Creek wastewater system, so responsibility for expansion will be on Currituck Water & Sewer.

Sincerely,

Brittney M. Willis – Project Manager

Sandler Utilities at Mill Run, LLC

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## STATE OF NORTH CAROLINA COUNTY OF CURRITUCK

### IN THE GENERAL COURT OF JUSTICE SUPERIOR COURT DIVISION 21 CVS 78

STATE OF NORTH CAROLINA, ex rel., NORTH CAROLINA DEPARTMENT OF ENVIRONMENTAL QUALITY, Plaintiff,	CONSENT JUDGMENT
v.	) .v .v
SANDLER UTILITIES AT MILL RUN, LLC,	) ) ;
Defendant.	j ,
	)

Plaintiff, the State of North Carolina, by and through the North Carolina Department of Environmental Quality ("Plaintiff" or "DEQ"), and Defendant Sandler Utilities at Mill Run, LLC ("Defendant") hereby agree to the entry of this Consent Judgment in order to resolve the matters in controversy between Plaintiff and Defendant.

The Court makes, and Plaintiff and Defendant (collectively "the Parties") hereby stipulate to, the following findings of fact and conclusions of law:

### FINDINGS OF FACT

1. Plaintiff is the sovereign State of North Carolina. This action was brought on the relation of the North Carolina Department of Environmental Quality ("DEQ"), the State agency established pursuant to N.C. Gen. Stat. § 143B-279.1 et seq., and vested with the statutory authority to enforce the State's environmental pollution laws, including laws enacted to protect the water quality of the State. The Division of Water Resources ("DWR") is a division within DEQ and all actions taken by DWR are necessarily actions of the Plaintiff.

- 2. Defendant is a limited liability corporation formed in North Carolina with its principal office in Virginia and doing business in North Carolina. Defendant is the owner and operator of the wastewater collection system serving the Eagle Creek Subdivision, Eagle Creek Golf Club, and Moyock Middle School in Moyock, Currituck County, North Carolina ("Collection System").
- 3. The Eagle Creek Subdivision is a development in Moyock, Currituck County, North Carolina. The development includes approximately 420 single-family homes and is generally situated between Roland Creek Canal to the north and Guinea Mill Run Canal to the south. Stormwater swales run beside the roadways in the development and between homes. Those swales drain to the Roland Creek Canal to the north and Guinea Mill Run Canal to the south of the development.
- On May 2, 2013, DWR issued non-discharge permit number WQCS00290
   ("Permit") to Defendant for operation of the Collection System.
- 5. The Collection System consists of 4.8 miles of vacuum sewer and utilizes vacuum pumps to maintain a constant negative pressure within the sewer pipes. Domestic waste from individual homes connected to the system collects in containment vessels, commonly referred to as "pits," with each pit serving two homes. When the level of waste within the pit reaches a determined level, pneumatic pressure triggers the opening of a valve to the piping connected to the sewer line. The vacuum withdraws waste and wastewater from the pit into the sewer line. When the waste level within the pit drops, the valve connecting the pit to the sewer line is closed, allowing waste to again collect within the pit and maintaining the vacuum in the sewer line.
- 6. Wastewater from the Collection System is conveyed to the Sandler Utilities Wastewater Treatment Plant for treatment and disposal as authorized by a separate permit,

WQ0014306.

- 7. A release of wastewater from a wastewater collection system such as Defendant's is referred to as a sanitary sewer overflow or "SSO."
- 8. Condition I.2 of the Permit requires that the Collection System "shall be effectively managed, maintained and operated at all times so that there is no SSO to land or surface waters, nor any contamination of groundwater." In the event of a system failure, the permittee is required to "take immediate corrective actions, including actions that may be required by [DWR] such as the construction of additional or replacement sewer lines and/or equipment."
- 9. Condition II.5 of the Permit provides that "for each pump station without pump reliability...at least one fully operational spare pump capable of pumping peak flow shall be maintained on hand."
- 10. Condition II.11 of the Permit provides that, if an SSO occurs, Defendant "shall restore the system operation, remove visible solids and paper, sanitize any ground area and restore the surroundings."
- 11. Condition IV.2 of the Permit provides that SSOs of over 1,000 gallons and any SSO that reaches surface waters, must be verbally reported to DWR as soon as possible, but no later than "24 hours following the occurrence or first knowledge of the occurrence."
- 12. On September 29, 2020, DWR's Washington Regional Office began receiving complaints from Eagle Creek residents indicating that the Collection System was not operating properly at homes throughout Eagle Creek.
- 13. On September 30, 2020, DWR inspectors Sarah Toppen and Victoria Herdt from the Washington Regional Office conducted a site inspection to investigate the complaints. DWR's investigation confirmed that the system had not been operating properly since September 27, 2020

due to failure of one of the two vacuum pumps. The vacuum pump failure caused SSOs that drained into stormwater swales in front of and between homes.

- 14. Upon learning of the incident, Defendant called in technical support from Airvac, the initial pump supplier, to assist in bringing the Collection System back on line. Due to insufficient replacement parts available on site, Defendant purchased replacement parts for the vacuum pumps and mechanical equipment located in the pits to address the reported issues.
- 15. Defendant provided cleanup and lime application at any location showing evidence of overflow and in all roadside ditches around October 9, 2021. Performance issues in the Collection System were resolved by October 11, 2020.
- 16. On October 26, 2020, the Washington Regional Office again began receiving complaints indicating that the Collection System was experiencing further performance issues resulting in SSOs.
- 17. On October 27, 2020, Ms. Toppen and another DWR inspector, Allen Stewart, conducted a site inspection to investigate the complaints and learned that a high water alarm within the Collection System had failed, causing the water to overflow and flood both of the system's vacuum pumps, taking them offline. The vacuum pumps' failure resulted in SSOs, and a water sample from the stormwater swale at 125 Eagleton Circle contained fecal coliform (bacteria from fecal matter) that were too numerous to count.
- 18. In response to this incident, Defendant requested assistance from technicians from Airvac and FloVac (another vacuum system supplier), and the vacuum pumps were restored and fully operational by October 29, 2020. Among other equipment and parts, Defendant ordered new parts for the malfunctioning high water alarm, which were installed on November 5, 2020.
  - 19. On November 13, 2020, the Washington Regional Office began receiving new

complaints stating that, beginning on or around November 11, 2020, the Collection System was not operating properly causing SSOs.

- 20. On November 14, 2020, David May, DWR Regional Supervisor for the Washington Regional Office, conducted a site visit to investigate the complaints. DWR's investigation confirmed the Collection System was experiencing performance issues resulting in SSOs. A substantial number of houses and customers remained without functional sewer service through at least November 16, 2020.
- 21. Complaints to the Washington Regional Office recommenced on November 20, 2020. Residents indicated that the Collection System was not functioning properly, that residents were once again without sewer service due to vacuum leaks disrupting operation of the Collection System, and that some pits were overflowing. A number of houses remained without functional sewer service or sporadic sewer service through about November 25, 2020.
- 22. On December 16, 2020, the Washington Regional Office once again received complaints from Eagle Creek residents regarding disruption of sewer service and SSOs. DWR's investigation suggested that the problem was limited to one section of the Eagle Creek development due to a disconnected line in a pit and sustained rainfall. Residences affected by the disconnected line remained without functional sewer service or sporadic sewer service through approximately December 18, 2020.
- 23. Additional complaints to the Washington Regional Office were received on January 26, 2021, with residents complaining that they lacked sewer service and describing other performance issues including SSOs.
- 24. Defendant has been subject to various enforcement actions by the DWR related to the Permit as a result of the incidents described above.

- 25. On October 7, 2020, DWR issued a Notice of Violation and Notice of Intent to Enforce ("First NOV") to Defendant pertaining to SSOs and system failures occurring from September 27, 2020 and ongoing as of the date the NOV was issued. The First NOV alleged the following violations:
  - Violation of Permit Condition I.2 for failure to effectively manage, maintain and operate the Collection System at all times so there are no SSOs to land or surface waters;
  - Violation of Permit Condition II.11 for failure to restore the system operation,
     remove visible solids and paper, sanitize any ground area and restore
     surroundings after an SSO;
  - c. Violation of Permit Condition IV.2 for failure to properly report SSOs.
- 26. In a letter dated October 27, 2020, Defendant responded to the First NOV. Defendant stated that replacement parts for the failed vacuum pumps were not readily available, causing a delay in fixing the Collection System. Once the vacuum pump was replaced, Defendant noted that two sewage pumps malfunctioned, forcing the entire system offline again for cleaning. Defendant acknowledged that its operator's personnel resources were stretched during the event limiting the ability to address the Collection System problems in a timely manner and that additional support from a vacuum sewer specialist was called in. Defendant stated that cleanup of any discharges was performed and the System was ultimately repaired.
- 27. DWR assessed a civil penalty of \$62,517.96 against Defendant arising out of the allegations in the First NOV on December 10, 2020 ("Civil Penalty") for fifteen violations of Permit Condition I.2 between September 27, 2020 and October 11, 2020.
  - 28. On November 23, 2020, DWR issued a Second Notice of Violation and Notice of

Intent to Enforce ("Second NOV") to Defendant pertaining to SSOs and system failures occurring during the period from October 26, 2020 through November 5, 2020. In addition to alleging violations of Permit Conditions I.2, II.11 and IV.2, the Second NOV alleged violations of Permit Condition II.5<sup>1</sup> for failure to maintain operational replacement pumps for stations without pump reliability and N.C. Gen. Stat. § 143-215.1(a)(1) for making an outlet into waters of the State without a permit.

- 29. In a letter dated December 15, 2020, Defendant responded to the Second NOV. Defendant stated that a "very large investment has been made into new equipment and parts" and that new equipment and parts were ordered. Defendant also stated that spills were addressed with lime application and solid waste overflow was physically removed. In response to the incident, Defendant installed a new vacuum pump and motor, acquired a spare vacuum pump and motor, purchased a new sewer pump, and replaced multiple controllers and valves within individual pits. Defendant further replaced parts needed to ensure functionality of the high-level alarm (which was the precipitating cause of this incident) to alleviate water intake into the vacuum sewer pumps causing failure.
- 30. On December 16, 2020, DWR issued a Third Notice of Violation and Notice of Intent to Enforce ("Third NOV") to Defendant pertaining to SSOs and system failures occurring during the period from November 11, 2020 through November 16, 2020. The Third NOV alleged violations of Conditions I.2, II.11, and IV.2 as well as N.C. Gen. Stat. § 143-215.1(a)(1).
- 31. On January 14, 2021, DWR issued a Fourth Notice of Violation and Notice of Intent to Enforce ("Fourth NOV") to Defendant pertaining to SSOs and system failures occurring during the period from November 20 through November 25, 2020. The Fourth NOV alleged violations

<sup>&</sup>lt;sup>1</sup> A typographical error in the Second NOV identifies a violation of Permit Condition II.6, not II.5. The narrative description of the violation refers to Permit Condition II.5, however.

of Conditions I.2, II.11, and IV.2 as well as N.C. Gen. Stat. § 143-215.1(a)(1).

- 32. DEQ filed the instant action on March 4, 2021, seeking injunctive relief for existing or threatened violations of various laws and rules governing the protection of water quality pursuant to N.C. Gen. Stat. § 143-215.6C.
- 33. The Parties have reached a mutually agreeable and reasonable resolution of the injunctive relief sought by DEQ through this suit which they seek to memorialize in this Consent Judgment.
- 34. The Court has reviewed the pleadings and supporting materials in this matter. Counsel for the Parties have represented to the Court that their respective clients have reviewed and approved the substance of the proposed Consent Judgment and that the Parties supported a request that this Court approve a Consent Judgment embodying their agreement.

## **CONCLUSIONS OF LAW**

- 1. This matter is properly before this Court, which has jurisdiction over the Parties and subject matter of this action pursuant to N.C. Gen. Stat. § 143-215.6C and other provisions of law.
  - Venue is proper pursuant to under N.C. Gen. Stat. §§ 1-79 and 143-215.6C.
- 3. Defendant is subject to non-discharge collection system permit number WQCS00290. Pursuant to that permit, as well as North Carolina laws and regulations, Defendant is expressly prohibited from discharging collected sewage and wastewater to land and waters of the State. See 15A NCAC subchapter 2T ("Waste Not Discharged to Surface Waters"); N.C. Gen. Stat. § 143-215.1(a)(1) (disallowing any person from making "any outlets into waters of the State" without receiving a permit to do so).
  - 4. Whenever DEQ has reasonable cause to believe that any person has violated or is

threatening to violate any of the provisions of the State's environmental laws or administrative rules, including State water quality laws and rules, DEQ is authorized to "request the Attorney General to institute a civil action in the name of the State upon the relation of [DEQ] for injunctive relief to restrain the violation or threatened violation." N.C. Gen. Stat. § 143-215.6C. That section further provides that "[u]pon a determination by the court that the alleged violation of the provisions of this Part or the regulations of the Commission has occurred or is threatened, the court shall grant the relief necessary to prevent or abate the violation or threatened violation." N.C. Gen. Stat. § 143-215.6C.

- 5. As of the date of entry of this Consent Judgment, Defendant has repeatedly violated the conditions of its Permit and North Carolina's water quality laws, including, but not limited to, failing to properly manage, maintain and operate the Collection System to prevent SSOs, failing to maintain replacement equipment to prevent SSOs, and failure to restore consistent service to Eagle Creek residents.
- 6. The current state of the Collection System presents an ongoing threat that the Eagle Creek Development will continue to experience performance issues and SSOs into nearby surface water in violation of the Collection System Permit, and in violation of N.C. Gen. Stat. § 143-215.1(a)(1).
- 7. The State is entitled to permanent injunctive relief against Defendant to abate the repeated previous violations and prevent the threatened violations set forth in the Complaint pursuant to N.C. Gen. Stat. § 143-215.6C.
- 8. Based on the Court's review of the pleadings and materials submitted, the Court has concluded that the relief reflected in the Consent Judgment represents a lawful, fair, and reasonable resolution of this matter, consistent with the purposes of N.C. Gen. Stat. § 143-215.1,

and this Court further concludes that it is fully authorized and justified in entering this Consent Judgment.

9. The Parties expressly waive any argument that the recitation of the above Findings of Fact and Conclusions of Law is insufficient to support the injunctive relief ordered below.

Based upon the foregoing Findings of Fact and Conclusions of Law, IT IS THEREFORE ORDERED, ADJUDGED, AND DECREED as follows:

Defendant is ordered to do the following:

- Fully comply with all terms and conditions of the Permit and State water quality laws, including by maintaining the Collection System in a manner that prevents the discharge of waste onto to land or into surface waters;
- 2. Report to a DWR staff member as soon as possible but in no case more than 24 hours following knowledge of the occurrence of any material equipment failure, any material system failure, and any SSO regardless of volume and file a written report within five days outlining actions taken or proposed to address the equipment failure, system failure, and SSO and prevent recurrence. An equipment or system failure shall be considered material if there is any SSO associated with the failure or the failure affects four or more residences in the Eagle Creek Development. The reports referenced in this subparagraph shall be submitted to the following email address:

David May, DWR david.may@ncdenr.gov

- 3. Within 30 calendar days of entry of this Consent Judgment:
  - a. Secure all pits to ensure only Defendant and its operators have pit access;

- b. Submit to DWR for approval (which may include conditions) a plan for Defendant's operators to receive any necessary training in operating and maintaining the Collection System, including specialized training in vacuum system operation, or provide documentation that such training has been received ("Operator Training Plan");
- c. Submit to DWR for review and approval (which may include conditions) a plan to prevent future SSOs, restore and sanitize areas impacted by prior SSOs, and expeditiously restore and maintain service to homeowners in the event of any system failures pending an engineering evaluation of the Collection System ("Interim Service and Restoration Plan");
- d. Provide to DWR a complete engineering evaluation detailing actions necessary to prevent future SSOs and system failures, including but not limited to necessary upgrades to the design and physical infrastructure of the Collection System ("Engineering Evaluation").
- 4. Within 60 calendar days of entry of this Consent Judgment:
  - a. Submit to DWR for review and approval (which may include conditions) a plan with actions to be taken to address necessary and proposed upgrades included in the Engineering Evaluation ("System Upgrade Plan"). The System Upgrade Plan must include a proposed schedule with dates by which each activity will be completed and, as appropriate, the frequency with which those activities will be repeated.
- If DWR requires plan revisions in order for any of the plans listed above to be approved,
   Defendant shall resubmit the plan incorporating said revisions within 15 days of written

notification by DWR that such revisions are required. DWR's discretion to require plan revisions shall be limited to revisions necessary to ensure compliance with North Carolina's water quality laws and regulations including provisions applicable to wastewater collection systems.

- 6. Once each submission is approved (the Operator Training Plan, the Interim Service and Restoration Plan, and the System Upgrade Plan) including any conditions added by DWR to each, Defendant shall execute each plan in accordance with the dates included therein.
- 7. Once the work set forth in the approved System Upgrade Plan, including any conditions added by DWR, is completed, Defendant shall submit a final report documenting the results of the activities set forth in each respective plan. The final report shall include a certification from a licensed professional engineer.
- 8. Unless otherwise indicated, Defendant shall submit all plans and reports referenced above to:

David May, DWR david.may@ncdenr.gov

- 9. It is further ordered that this Consent Judgment shall take effect immediately and shall remain in effect until the Permittee can adequately demonstrate that the collection system can operate in a reliable manner and maintain compliance with North Carolina water quality laws and regulations on a consistent basis. The Defendant may request termination of this Consent Judgment by the Court at no time sooner than one year following submittal of the final report described in Paragraph 7 above and subsequent approval by DWR. The Parties shall comply with all terms of this Consent Judgment.
- 10. This Consent Judgment shall be binding upon Defendant's successors and assigns.

Defendant shall not transfer any of the assets that are the subject of the Complaint, including the Permit or the Collection System, unless and until Defendant moves to join the transferee as a defendant in this case such that this Consent Judgment shall be binding upon the transferee and the Court issues an order granting such motion. DEQ agrees that it will not oppose dismissal of Defendant in the event the assets that are the subject of the Complaint have been transferred and the transferee has been added as a defendant in this case and is bound by this Consent Judgment. Nothing in this Consent Judgment relieves Defendant of its duty to abide by the terms of the Permit and State water quality law. DEQ retains its authority, in accordance with applicable law, to initiate any and all enforcement actions that would otherwise be available to it in the absence of this Consent Judgment.

- 11. The Court shall retain continuing jurisdiction in this case to enforce the terms and conditions of this Consent Order, to modify this Consent Order, and to resolve disputes arising under this Consent Order until all parties have complied with all provisions of this Consent Judgment.
- 12. The contempt provisions of Article 2, Chapter 5A of North Carolina General Statutes shall be available to enforce this Consent Judgment.
- 13. This Consent Judgment may be signed out-of-court, out-of-term, out-of-county, and may be signed in multiple counterpart originals, all of which, taken together, shall be considered one and the same document. Facsimile or scanned signatures will be sufficient to render this Consent Judgment effective. Original signatures will be substituted at a later date.
- 14. Each undersigned representative of a party to this Consent Judgment certifies that the

representative is authorized to enter into the terms and conditions of this Consent Judgment, and to execute and legally bind such party to this Consent Judgment.

day of

. 2021

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[ADDITIONAL SIGNATURES ON FOLLOWING PAGES]

CONSENTED TO BY:

NORTH CAROLINA DEPARTMENT OF ENVIRONMENTAL QUALITY

By:

Regional Supervisor, Washington Regional Office, Division of Water Resources

CONSENTED TO BY:

SANDLER UTILITIES AT MILL RUN, LLC

Ву:

Raymond Gottlieb Manager

# SANDLER UTILITIES AT MILL RUN, LLC

April 1, 2021

Mr. William Grantmyre Staff Attorney North Carolina Public Staff Utilities Commission 4326 Mail Service Center Raleigh, NC 27699-4300

> Re: Docket No. W-1130, Sub 8 Eagle Creek Subdivision

Dear Mr. Grantmyre:

Sandler Utilities at Mill Run, LLC ("Sandler Utilities") sincerely regrets the recent wastewater system backups into the homes of customers caused by controller and valve failures due to water intrusion into the value pit packages of the Eagle Creek Vacuum System ("Vacuum System"). As discussed in our January 22, 2021 letter, Sandler Utilities performed substantial capital improvements to the Vacuum System since the Commission's Recommended Order Granting Rate Increase, Requiring Refund, and Customer Notice was issued on December 11, 2015. As discussed in detail on our January 22, 20121 letter, more recently, Sandler Utilities (i) retained the services of FloVac to provide expert recommendations about how to address and correct the problems with the Vacuum System; (ii) enlisted FloVac's assistance in training the operator of the system, Envirolink, Inc. ("Envirolink"), as to how to prevent water ingestion in the system; (iii) installed 120 new controllers from October 12, 2020 through November 23, 2020 that are capable of processing small amounts of water and being submerged up to five feet with no impact to performance; (iv) performed proper installation of in-sump breather hoses on several valve pits; and (v) installed one new valve pit in December 2020. Also, Sandler Utilities has requested recommendations from FloVac about whether to install a monitoring system at each valve pit so that the operator of the system can timely respond to any issues with the system.

In addition to those actions and capital improvements, Sandler has performed, or will be performing, further remedial actions and improvements to the system to reduce rainwater intrusion and minimize flooding of the valve pits. Below is a list of the actions items and completion date for the additional improvements to reduce rainwater intrusion and minimize flooding of the valve pits.

Action Item	Recommended Response	Completion Schedule for Remedial Action
Rainwater intrusion. Reduce rainwater intrusion and minimize flooding in valve pits. Most rainwater intrusion is believed to be caused by I&I from the homeowners" laterals causing vacuum systems to struggle with inundation during heavy rain.	Install a monitoring system to detect in real time issues, such as missing clean out caps that the homeowners may have removed to relieve wastewater system back-ups. It will also alleviate time spent searching for leaks in the system, and response time is critical to keep problems from escalating.	FloVac has submitted proposals for the monitoring system to both Sandler Utilities and Envirolink, the potential purchaser of the Vacuum System. Sandler Utilities and Envirolink are reviewing FloVac's proposal, and determining the most effective method of addressing the problems with the system.

448 Viking Drive, Suite 220, Virginia Beach, Virginia 23452
Mailing Address: Post Office Box 8790, Virginia Beach, Virginia 23450
Telephone 757-463-5000 / Telefax 757-463-3358

Continuous operation of vacuum pump. The vacuum pump. The vacuum pump frequently operates continuously during periods of high water levels within the vacuum holding tank, and the continuous operation of the vacuum pump creates the potential for motor failure.	Repair high-level lock-out valve.	This repair was completed in December 2020.
System monitoring. Appropriate and effective system monitoring during off-hours has been accomplished.	Sandler Utilities is committed to ensuring that Envirolink is appropriately staffed so that any required service to the system will be performed timely. Sandler Utilities has instructed Envirolink to have a 24/7 presence at the plant. Envirolink's 24/7 presence at the plant will address previous issues of response time, such as when issues occurred during weekends when Envirolink was not physically on-site.	At the instruction of Sandler Utilities, effective on 3/19/2021, Envirolink has a physical presence at the site for 7-day, 20-hour shifts. Also, Envirolink will have at least two technicians on call during the remaining 4 hours of the day in which the plant is not physically staffed by an operator. During projected wet weather events, Sandler Utilities will be proactive and request that Envirolink ensure that 24-7 physical coverage is maintained until the wet weather event has subsided.
Outsource controllers. Outsource controllers and valves to be rebuilt so fully functional ones are available when needed.	37 controllers that had been waiting for technician repair were delivered to FloVac for repair and rebuilding by their expert technicians. FloVac repaired the controllers and delivered them to the Eagle Creek plant.	The repairs to the controllers were completed in December 2021. In the event that Envirolink has a back-log for repair of the controllers in the future, Sandler Utilities has informed Envirolink to deliver the controllers to FloVac for repair.
Controller failure. Controller failure from water ingestion was the result of new controllers being properly fitted to the valves within the collection network.	Envirolink technicians were trained about the proper procedure for removing water from the valve upper before fitting a new controller to prevent water ingestion.	FloVac provided training to Envirolink during the week beginning November 20, 2020.
Operator's technical experience.	Envirolink has been gaining experience and expertise in operating a vacuum wastewater system since the EnviroTech team who had previously operated the system left in	In November 2002, Sandler Utilities hired a former EnviroTech employee with experience in operating vacuum systems. This former EnviroTech employees is leading operations at the facility, and he has been

April 1, 2021 Page 3

	September 2020. Training in vacuum systems for all technicians who work at or will potentially work at this plant is critical.	instrumental in leading his team to detect and address potential issues. Also, Sandler Utilities has requested that Envirolink continue a training regimen with its operators to ensure that the operators will be proficient with the vacuum system. Mike Myers with Envirolink has that training has occurred and that additional training by a technician who leads training efforts with both AirVac and FloVac will be scheduled.
Extended outages. A delay in receipt of the initial vacuum pump replacement caused an extended outage that resulted serious problems to the system. The ongoing disassembly and reassembly of the valve pit by pump truck operators to clear the pits left the valve pits vulnerable to failures.	Once the system was stabilized, technicians analyzed each valve pit, and tested, repaired, reconnected, and replaced items as necessary. During this analysis, a technician discovered that a valve pit had collapsed. Therefore, a new valve pit was ordered and installed to replace the failed valve pit.	The analysis of each valve pit was conducted and completed during December 2020. As a result of the analysis, most of the valve pits that had sustained damage during the September and October outages were repaired. For example, each valve was fired multiple times by hand and then analyzed for hose orientation and correct hose connection and timing.
Problem identification. Identify problems early that cannot be readily observed.	Envirolink operators are required to record daily run times during their daily station "walk-through."	An example of a log and recording document was provided to Envirolink in November 2020 to use as an example. This document is now utilized as part of Envirolink's daily protocol.
Pump station alarm. The excessive pump run alarm is the most effective warning to keep the system from collapsing and causing a complete failure. Therefore, the pump alarm must be operating properly at all times.	Envirolink is required to check the pump alarm on a weekly basis, and check the control panel and sensors associated with all alarm systems at the plant on a weekly basis.	Since January 2021, Envirolink has been inspecting these systems to make sure they are operating properly. By the end of 2021, Sandler Utilities plans to provide and overhaul the control panel.
Couplings wearing prematurely to the motor of the sewer pumps.  Nonoperational check valves on the vacuum main.	Alignment of the motor on the pumps has been checked.  Replace or repair as needed.	Pearson pumps completed this work during their repair and replacement efforts in November 2020.  Valves are exercised weekly and are all currently in working order.

April 1, 2021 Page 4

Inadequate stock levels.	Ensure multiple spare parts are available at all times.	From October 2020 through January 2021, numerous controllers, valves, pumps, motors, and valve pits have been ordered and kept on-site for Envirolink to use when needed.
Excessive activations caused by inflow and infiltration from the homeowners' gravity laterals.	Install a monitoring system to identify locations of excessive activations.	In December 2020, FloVac submitted proposals for a monitoring system to Sandler Utilities and Envirolink. Sandler Utilities and Envirolink are considering the proposals.
The length of time it takes Envirolink to identify the location of a valve that is stuck open.	Install a monitoring system to eliminate the need for this search.	In December 2020, FloVac submitted proposals for a monitoring system to Sandler Utilities and Envirolink. Sandler Utilities and Envirolink are considering the proposals.
Short cycling of sewage pumps likely caused by debris in the vacuum collection tanks.	Clean and inspect the collection tank thoroughly.	The vacuum tank was cleaned and thoroughly inspected in October 2020.

Sandler Utilities is committed to ensuring that the wastewater system for the Eagle Creek Subdivision is both safe and reliable. We are continuing our efforts to ensuring that the problems with the Vacuum System are satisfactorily addressed.

If you have any question about our plan to address the problems with the system, please let me know.

Sincerely,

Brittney M. Willis – Project Manager Sandler Utilities at Mill Run, LLC

Currituck Water & Sewer, LLC 4700 Homewood Ct., Suite 108 Raleigh, North Carolina 27609

August 26, 2021

Mr. Justin Olds Allied Properties, LLC 417 Caratoke Hwy, Unit D Moyock, NC 27958

Re: Flora Farm Phases 1-5 Sanitary Sewer Utility, Willingness to Serve

To Whom it may Concern,

Currituck Water & Sewer (CWS) is currently working through the regulatory approvals to acquire Eagle Creek Wastewater Treatment Facility serviing the Eagle Creek community. CWS intends to provide wastewater service to the Fost Development. We understand the project to include 277 residential dwelling units and approximately 7,500 square feet of commercial for an estimated 62,847 gpd.

Eagle Creek WWTP has a NC DEQ approved 321,000 gpd of permitted capacity with actual plant capacity of 175,000 gpd and annual average flow of 53,917 gpd. In accordance with NC DEQ regulation, CWS will expand the capacity from 175,000 gpd on or before the annual average flow exceeds 157,500 gpd.

The following summarizes the capacity commitments for the Eagle Creek Wastewater Treatment System.

Description	Allocation	Remaining Permitted	Plant Capacity
7		Capacity	
Permitted Capacity		321,000 gpd	175,000 gpd
Eagle Creek Community	53,917 gpd	267,083 gpd	121,083 gpd
Golf Club	1,462 gpd	265,621 gpd	119,621 gpd
Moyock Middle School	3,546.91 gpd	262,074 gpd	116, 047 gpd
Fost, phase 1-5	63,511 gpd	198,563 gpd	52,563 gpd
Fost, phase 6-9	37,758 gpd	160,805 gpd	14,805 gpd
Additional 175,000 gpd capacity			189,805 gpd
Flora	62,847 gpd	97 958 gpd	316,763 gpd
Total	223, 042 gpd		

Prior to submitting for an NC DEQ collection system permit, CWS anticipates completion of the expansion from 175,000 gpd to 350,000 gpd.

As such upon approval by Currituck County, Currituck Water & Sewer, LLC intends to provide sanitary sewer service to Fost phases 6-9 and Flora.

Should you have any questions, please do not hesitate to contact me directly at 252-235-4900 or email at <a href="mailto:mmyers@envirolinkinc.com">mmyers@envirolinkinc.com</a>

Sincerely

Michael Myers



KIMLEY-HORN AND ASSOCIATES, INC NC License #F-0102

8/24/2021

## **MEMORANDUM**

To: Mark S. Bissell, P.E. – Bissell Professional Group

From: Lyle Overcash, P.E. – Kimley-Horn and Associates, Inc.

Date: August 24, 2021

Subject: Flora Farms Development – Moyock, NC – Traffic Assessment

The Flora Farms Subdivision TIA (VHB, May 2020) analyzed the traffic impacts of constructing 285 single-family homes, 125 apartments, and 100,000 square feet (SF) of general retail space south of Caratoke Highway (NC 168) and Survey Road in Moyock, North Carolina. As stated in the Flora Farms Trip Generation Update Memorandum (VHB, November 2020), the project's site plan (shown in **Figure 1**) has been updated and now includes up to 277 single-family homes and 7,500 SF of general retail space. This memorandum analyzes the traffic impacts of the reduced development on the local roadway network.

## Study Area and Volume Development

The study intersections, existing roadway laneage, existing and background traffic volumes were obtained from the *Flora Farms Subdivision TIA* and were not modified as part of this analysis.

#### Trip Generation and Distribution

The traffic generation potential of the proposed project was determined using the data published in the 10<sup>th</sup> Edition of the ITE Trip Generation Manual. This data, as well as a comparison with the trip generation from the original TIA, is shown below in Table 1.

			Table 1				
		<u>ΓΕ Traffic G</u>	eneration (Ve	ehicles)			
Land Use Code	Land Use	Intensity	Daily (Weekday)	AM Pea	ak Hour	PM Pea	ık Hour
Code		_	Total	ln	Out	In	Out
210	Single Family Detatched Housing	277 d.u.	277	50	151	170	100
820	Shopping Center	7,500 s.f.	7,500	138	84	38	42
	Subtotal		3,690	188	235	208	142
	Internal Capture Total		346	3	3	3	3
	Pass-By Total		293	0	0	12	14
Total Net No	ew External Trips: 2021 F	lan Update	3,051	185	232	193	125
Total Ne	t New External Trips: Oriç (2020)	jinal TIA	8,380	189	274	316	246
Remainir	ng Trip Bank: Original T August 2021 Update	A minus	5,329	4	42	123	121



Page 2

As shown in Table 1, compared to the uses assumed in the original TIA, the updated development plan is expected to generate 5,329 fewer trips daily, a 64% trip reduction. Also, there were 46 fewer trips during the AM peak hour and 244 fewer trips in the PM peak hour on a typical weekday, 10% and 43% reductions respectively.

The generated trips were assigned to the external roadway network according to the overall distribution and percent assignment shown in **Figure 2**. Directional distribution and assignment were based on existing travel patterns, land uses in the area, and a review of origins and destinations.

## Capacity Analysis

The assigned site traffic volumes were added to projected (2026) background volumes to obtain the projected (2026) build-out traffic volumes, which are shown in **Figure 3.** Peak hour capacity analyses for the build-out condition were performed at the study intersections using Synchro version 10 software. <u>Table 2</u> below summarizes the results of these capacity analyses and compares them to the original TIA.

Table	_	
Level-of-Servic		DM D. J. III.
Condition	AM Peak Hour LOS (Delay)	PM Peak Hour LOS (Delay)
Caratoke Hwy (NC 168) at S		
Build-Out (2026) Traffic – May 2020 TIA	Overall – B (15.7) EB – D (41.5) NB – A (9.2) SB – B (12.0)	Overall – B (18.0) EB – E (61.2) NB – A (4.8) SB – B (16.2)
Build-Out (2026) Traffic – August 2021 Update	Overall – B (15.2) EB – D (41.2) NB – A (9.0) SB – B (11.8)	Overall – B (15.3) EB – D (49.9) NB – A (5.1) SB – B (15.2)
Caratoke Hwy (NC 168) at Su	rvey Road (unsignal	ized)
Build-Out (2026) Traffic with Improvements – May 2020 TIA	EB – B (11.4)	EB – E (37.9)
Build-Out (2026) Traffic with Improvements – August 2021 Update	EB – B (11.3)	EB – D (25.7)
Caratoke Hwy (NC 168) at Gu	inea Road (unsigna	lized)
Build-Out (2026) Traffic – May 2020 TIA	WB – C (22.6)	WB – C (23.7)
Build-Out (2026) Traffic – August 2021 Update	WB – C (22.5)	WB – C (22.4)
Eagle Creek Road at Surve	ey Road (unsignalize	ed)
Build-Out (2026) Traffic – May 2020 TIA	WB – B (11.2)	WB – B (12.1)
Build-Out (2026) Traffic – August 2021 Update	WB – B (11.2)	WB – B (11.0)



Page 3

Table 2 (d Level-of-Servic		
Condition	AM Peak Hour LOS (Delay)	PM Peak Hour LOS (Delay)
Caratoke Hwy (NC 168) at Fost E	Boulevard (future sig	nalized)
Build-Out (2026) Traffic with Improvements – May 2020 TIA	Overall – B (13.9) EB – C (30.2) NB – B (11.6) SB – A (9.4)	Overall – B (14.1) EB – D (43.7) NB – B (13.3) SB – A (9.9)
Build-Out (2026) Traffic with Improvements – August 2021 Update	Overall – B (13.6) EB – C (30.3) NB – B (11.3) SB – A (9.2)	Overall – B (13.0) EB – D (38.4) NB – B (12.9) SB – A (9.4)
Future Access #1/#2 at Surv	vey Road (unsignaliz	ed)
Build-Out (2026) Traffic with Improvements – May 2020 TIA	NB – B (11.7) SB – B (11.7)	NB – C (15.4) SB – C (16.2)
Build-Out (2026) Traffic with Improvements – August 2021 Update	NB – B (10.5) SB – B (10.1)	NB – B (10.2) SB – B (10.1)

#### **Summary & Recommendations**

The following improvements were committed to be performed by other projects in the area and were assumed to be in place in the build-out condition:

#### Caratoke Highway (NC 168) at Fost Boulevard

- Construct a southbound right-turn lane with 150 feet of storage and appropriate taper on NC 168.
- Stripe out 200 feet of storage within the existing two-way left turn lane to provide a northbound left-turn lane on NC 168.
- Construct Fost Boulevard with one ingress lane and two egress lanes. Provide a continuous right-turn lane and a left-turn lane with 250 feet of storage and appropriate taper.
- Install a traffic signal.

The following improvements were recommended to be performed as part of the Flora Development project and were assumed to be in place in the build-out condition:

#### Caratoke Highway (NC 168) at Survey Road

- Construct a southbound right-turn lane with 100 feet of storage and appropriate taper on NC 168.
- Stripe out 200 feet of storage within the existing two-way left turn lane to provide a northbound left-turn lane on NC 168.
- Restrict access to not allow left turns from Survey Road.



Page 4

#### Future Access #1/#2 at Survey Road

- Construct Future Access #1 with one ingress lane and two egress lanes. Provide an exclusive northbound left-turn lane with 100 feet of storage and appropriate taper and a shared through/right-turn lane.
- Construct Future Access #2 with one ingress lane and one egress lane.
- Provide eastbound left- and right-turn lanes on Survey Road, both with 100 feet of storage and appropriate taper.
- Construct a westbound left-turn lane on Survey Road with 100 feet of storage and appropriate taper.

Based on the analysis presented in Table 2, the committed and recommended laneage is anticipated to provide acceptable levels of service under projected build-out traffic demands. **Figure 4** shows the committed and recommended laneage.

Should you have any questions or comments, please do not hesitate to contact me at (919) 678-4131 or <a href="mailto:lyle.overcash@kimley-horn.com">lyle.overcash@kimley-horn.com</a>.

Attachments: Figures 1-4, Trip Generation Calculations, Synchro LOS Reports

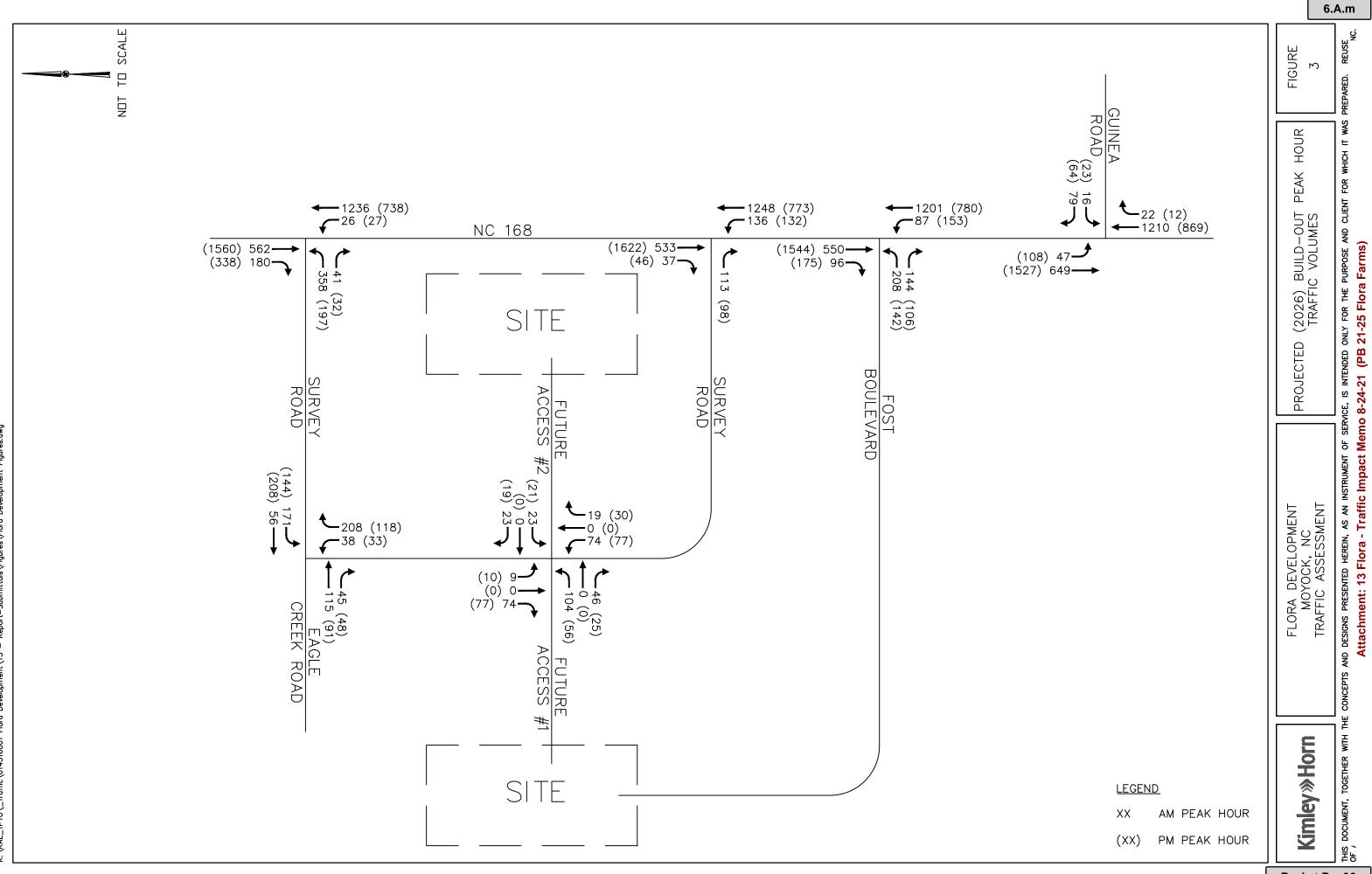
AND DESIGNS PRESENTED HEREIN, AS AN INSTRUMENT OF SERVICE, IS INTENDED ONLY FOR THE PURPOSE AND Attachment: 13 Flora - Traffic Impact Memo 8-24-21 (PB 21-25 Flora Farms) CONCEPTS , THIS DOCUMENT, TOGETHER WITH THE OF / **Kimley** » Horn

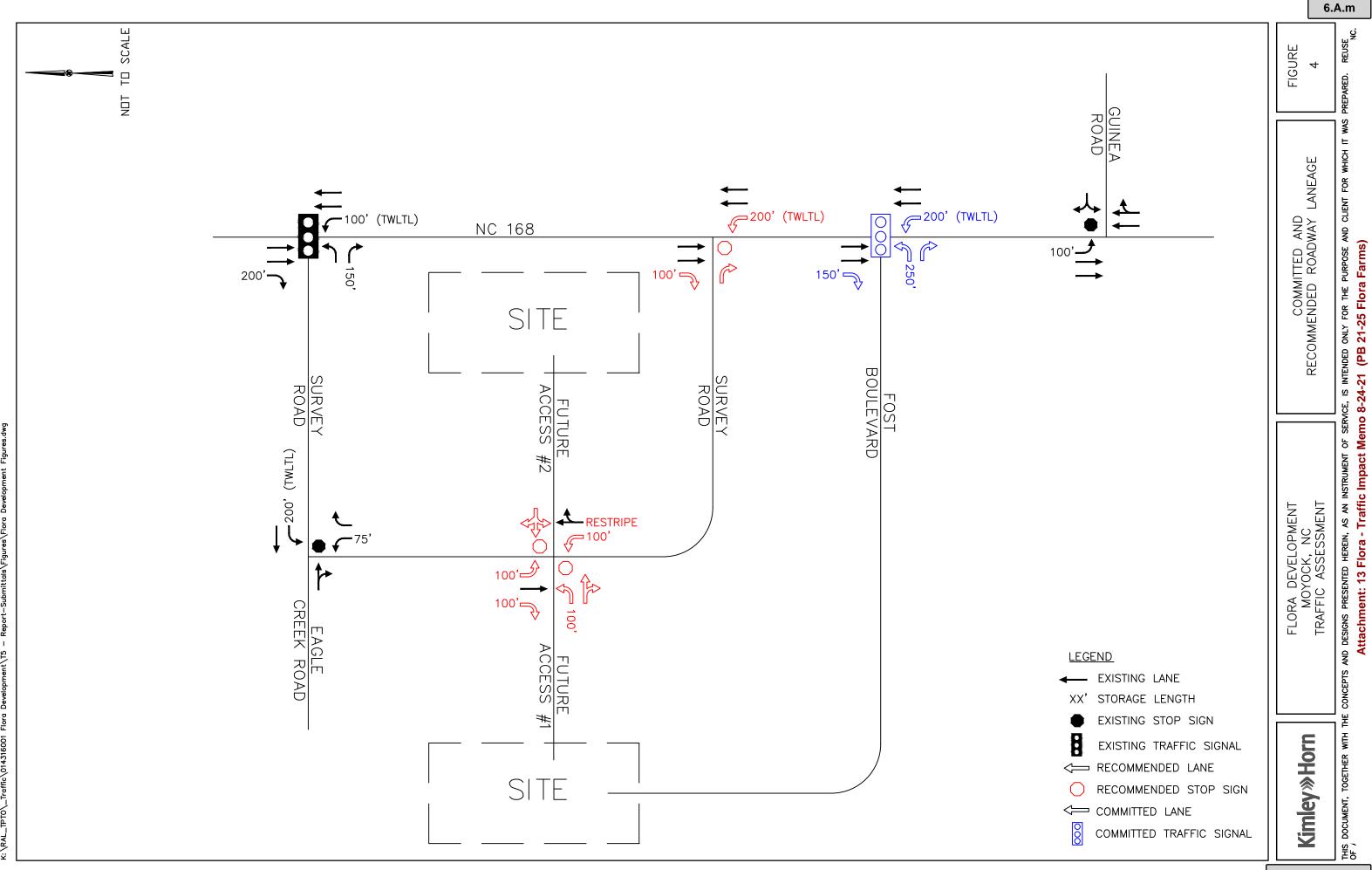
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# Flora Development

## **Table 1 - Trip Generation**

Land Use	Inter	nsity	Da	ily (Weeko	lay)	Al	/I Peak Ho	ur	PI	/I Peak Ho	ur
Land OSE		ilioity	Total	In	Out	Total	In	Out	Total	In	Out
210 Single Family Detached Housing	277	d.u.	2,656	1,328	1,328	201	50	151	270	170	100
820 Shopping Center	7,500	s.f.	1,034	517	517	222	138	84	80	38	42
Subtotal			3,690	1,845	1,845	423	188	235	350	208	142
Internal Capture											
210 Single Family Detached Housing			173	103	70	3	1	2	3	1	2
820 Shopping Center			173	70	103	3	2	1	3	2	1
Internal Capture Total	8.5	57%	346	173	173	6	3	3	6	3	3
Total External Trips			3,344	1,672	1,672	417	185	232	344	205	139
Pass-By Traffic (ITE)	<u>AM</u>	<u>PM</u>									
820 Shopping Center	0%	34%	293	147	146	0	0	0	26	12	14
Pass-By Total:	6.5	7%	293	147	146	0	0	0	26	12	14
Total Net New External Trips			3,051	1,525	1,526	417	185	232	318	193	125

## 1: Caratoke Hwy (NC 168) & Survey Road (West)

	•	•	4	<b>†</b>	ţ	4
Lane Group	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	ሻ	7	ሻ	<b>^</b>	<b>^</b>	7
Traffic Volume (vph)	359	41	26	1236	562	180
Future Volume (vph)	359	41	26	1236	562	180
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
	1900	150	200	1900	1900	200
Storage Length (ft)	1		200			
Storage Lanes	•	1	•			1
Taper Length (ft)	100	1500	100	2525	00.40	4500
Satd. Flow (prot)	1770	1583	1770	3505	3343	1583
Flt Permitted	0.950	A = -	0.370	C=	00:-	
Satd. Flow (perm)	1770	1583	689	3505	3343	1583
Right Turn on Red		No				No
Satd. Flow (RTOR)						
Link Speed (mph)	35			55	55	
Link Distance (ft)	1728			4412	2769	
Travel Time (s)	33.7			54.7	34.3	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	2%	3%	8%	2%
Shared Lane Traffic (%)	270	270	2,0	0,0	070	270
Lane Group Flow (vph)	399	46	29	1373	624	200
Enter Blocked Intersection	No	No	No	No	No	No
Lane Alignment	Left	Right	Left	Left	Left	Right
	24	Rigiii	Len	12	12	Rigiii
Median Width(ft)						
Link Offset(ft)	0			0	0	
Crosswalk Width(ft)	16			16	16	
Two way Left Turn Lane				Yes	Yes	
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15	9	15			9
Number of Detectors	1	1	1	2	2	1
Detector Template	Left	Right	Left	Thru	Thru	Right
Leading Detector (ft)	20	20	20	100	100	20
Trailing Detector (ft)	0	0	0	0	0	0
Detector 1 Position(ft)	0	0	0	0	0	0
Detector 1 Size(ft)	20	20	20	6	6	20
Detector 1 Type	CI+Ex	CI+Ex	CI+Ex	CI+Ex	CI+Ex	CI+Ex
Detector 1 Channel	OITEX	OHLX	OHLA	OHLA	OHLA	OHLA
Detector 1 Extend (s)	0.0	0.0	0.0	0.0	0.0	0.0
Detector 1 Queue (s)	0.0	0.0	0.0	0.0	0.0	0.0
Detector 1 Delay (s)	0.0	0.0	0.0	0.0	0.0	0.0
Detector 2 Position(ft)				94	94	
Detector 2 Size(ft)				6	6	
Detector 2 Type				CI+Ex	CI+Ex	
Detector 2 Channel						
Detector 2 Extend (s)				0.0	0.0	
Turn Type	Prot	Perm	D.P+P	NA	NA	pm+ov
Protected Phases	4		5	2	6	4
Permitted Phases		4	6			6
Detector Phase	4	4	5	2	6	4
Switch Phase				_		
Minimum Initial (s)	7.0	7.0	7.0	14.0	14.0	7.0
iviii iiiiiiiiiiiiiiiiiiiiiiiiiiiiiiii	7.0	1.0	1.0	14.0	14.0	7.0

1: Caratoke Hwy (NC 168) & Survey Road (West)

	۶	•	4	<b>†</b>	ļ	4
Lane Group	EBL	EBR	NBL	NBT	SBT	SBR
Minimum Split (s)	12.9	12.9	11.9	20.4	20.4	12.9
Total Split (s)	38.0	38.0	12.0	52.0	40.0	38.0
Total Split (%)	42.2%	42.2%	13.3%	57.8%	44.4%	42.2%
Yellow Time (s)	3.0	3.0	3.0	5.4	5.4	3.0
All-Red Time (s)	2.9	2.9	1.9	1.0	1.0	2.9
Lost Time Adjust (s)	-0.9	-0.9	0.1	-1.4	-1.4	-0.9
Total Lost Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lead/Lag			Lag		Lead	
Lead-Lag Optimize?			Yes		Yes	
Recall Mode	None	None	None	C-Min	C-Min	None
Act Effct Green (s)	25.0	25.0	53.0	55.0	48.9	81.9
Actuated g/C Ratio	0.28	0.28	0.59	0.61	0.54	0.91
v/c Ratio	0.81	0.10	0.06	0.64	0.34	0.14
Control Delay	43.4	22.2	7.4	9.1	15.0	1.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	43.4	22.2	7.4	9.1	15.0	1.7
LOS	D	С	Α	Α	В	Α
Approach Delay	41.2			9.0	11.8	
Approach LOS	D			Α	В	
Queue Length 50th (ft)	210	19	5	121	81	0
Queue Length 95th (ft)	281	40	m10	192	188	32
Internal Link Dist (ft)	1648			4332	2689	
Turn Bay Length (ft)		150	200			200
Base Capacity (vph)	649	580	505	2143	1829	1422
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.61	0.08	0.06	0.64	0.34	0.14
Intersection Summary						

#### Intersection Summary

Area Type: Other

Cycle Length: 90 Actuated Cycle Length: 90

Offset: 13 (14%), Referenced to phase 2:NBT and 6:NBSB, Start of Green

Natural Cycle: 55

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.81

Intersection Signal Delay: 15.2 Intersection LOS: B Intersection Capacity Utilization 62.4% ICU Level of Service B

Analysis Period (min) 15

m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 1: Caratoke Hwy (NC 168) & Survey Road (West) **₹** Ø2 (R)

## 2: Caratoke Hwy (NC 168) & Survey Road (East)

	۶	•	4	<b>†</b>	ļ	4
Lane Group	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations		7	ሻ	<b>^</b>	<b>^</b>	7
Traffic Volume (vph)	0	113	136	1248	533	37
Future Volume (vph)	0	113	136	1248	533	37
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0	0	200			100
Storage Lanes	0	1	1			1
Taper Length (ft)	100		100			
Satd. Flow (prot)	0	1596	1612	3505	3343	1583
Flt Permitted			0.950			
Satd. Flow (perm)	0	1596	1612	3505	3343	1583
Link Speed (mph)	35			55	55	
Link Distance (ft)	328			1116	4412	
Travel Time (s)	6.4			13.8	54.7	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	3%	12%	3%	8%	2%
Shared Lane Traffic (%)						
Lane Group Flow (vph)	0	126	151	1387	592	41
Enter Blocked Intersection	No	No	No	No	No	No
Lane Alignment	Left	Right	Left	Left	Left	Right
Median Width(ft)	0			12	12	
Link Offset(ft)	0			0	0	
Crosswalk Width(ft)	16			16	16	
Two way Left Turn Lane				Yes	Yes	
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15	9	15			9
Sign Control	Stop			Free	Free	
Intersection Summary						

Area Type: Other Control Type: Unsignalized

Intersection Capacity Utilization 37.8%

ICU Level of Service A

Analysis Period (min) 15

Intersection						
Int Delay, s/veh	1.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	LDL	LDK.	NDL			JDK 7
Traffic Vol, veh/h	0	113	136	<b>↑↑</b> 1248	<b>↑</b> ↑ 533	37
Future Vol, veh/h	0	113	136	1248	533	37
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	Siup -	None	-		-	None
Storage Length	-	0	200	None -	-	100
		-	200	0	0	
Veh in Median Storage						-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	3	12	3	8	2
Mvmt Flow	0	126	151	1387	592	41
Major/Minor N	/linor2	N	/lajor1	N	/lajor2	
Conflicting Flow All	_	296	633	0		0
Stage 1	-	-	_	-	-	-
Stage 2		_	_	_	_	_
Critical Hdwy	_	6.96	4.34	-	_	_
Critical Hdwy Stg 1	_	-	-	_	_	_
Critical Hdwy Stg 2	_	_	-	_	_	_
Follow-up Hdwy	_	3.33	2.32	_	_	_
Pot Cap-1 Maneuver	0	697	881		_	-
Stage 1	0	- 071			-	-
Stage 2	0	-	-	-	-	-
Platoon blocked, %	U	•	-	-	-	
		407	001	-		-
Mov Cap-1 Maneuver	-	697	881	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Approach	EB		NB		SB	
	11.3		1		0	
HCM LOS	В					
, <u></u>	_					
N. 1 /N. 1 N. 1		NDI	NDT	EDL 4	CDT	CDD
Minor Lane/Major Mvm	t	NBL	MBI	EBLn1	SBT	SBR
Capacity (veh/h)		881	-	697	-	-
HCM Lane V/C Ratio		0.172	-	0.18	-	-
HCM Control Delay (s)		9.9	-	11.3	-	-
HCM Lane LOS		Α	-	В	-	-
HCM 95th %tile Q(veh)		0.6	-	0.7	-	-

## 3: Caratoke Hwy (NC 168) & Guinea Road

	•	•	<b>†</b>	<b>/</b>	-	ļ
Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		<b>∱</b> ∱		*	<b>^</b>
Traffic Volume (vph)	16	79	1210	22	47	649
Future Volume (vph)	16	79	1210	22	47	649
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0	0		0	100	
Storage Lanes	1	0		0	1	
Taper Length (ft)	100				100	
Satd. Flow (prot)	1615	0	3457	0	1770	3343
Flt Permitted	0.992				0.950	
Satd. Flow (perm)	1615	0	3457	0	1770	3343
Link Speed (mph)	55		55			55
Link Distance (ft)	1144		980			859
Travel Time (s)	14.2		12.1			10.6
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	4%	4%	11%	2%	8%
Shared Lane Traffic (%)						
Lane Group Flow (vph)	106	0	1368	0	52	721
Enter Blocked Intersection	No	No	No	No	No	No
Lane Alignment	Left	Right	Left	Right	Left	Left
Median Width(ft)	12		12			12
Link Offset(ft)	0		0			0
Crosswalk Width(ft)	16		16			16
Two way Left Turn Lane			Yes			Yes
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15	9		9	15	
Sign Control	Stop		Free			Free
Intersection Summary						

Area Type: Other Control Type: Unsignalized

Intersection Capacity Utilization 51.5%

ICU Level of Service A

Analysis Period (min) 15

Intersection						
Int Delay, s/veh	1.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
	WDL	WDK		NDK	JDL Š	
Lane Configurations		70	<b>†</b>	าา		<b>^</b>
Traffic Vol, veh/h	16	79	1210	22	47	649
Future Vol, veh/h	16	79	1210	22	47	649
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	100	-
Veh in Median Storage	e,# 0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	4	4	11	2	8
Mvmt Flow	18	88	1344	24	52	721
Major/Minor	Minor1	N	/lajor1	ı	Major2	
		684				0
Conflicting Flow All	1821		0	U	1368	0
Stage 1	1356	-	-	-	-	-
Stage 2	465	-	-	-	-	-
Critical Hdwy	6.84	6.98	-	-	4.14	-
Critical Hdwy Stg 1	5.84	-	-	-	-	-
Critical Hdwy Stg 2	5.84	-	-	-	-	-
Follow-up Hdwy	3.52	3.34	-	-	2.22	-
Pot Cap-1 Maneuver	69	386	-	-	498	-
Stage 1	205	-	-	-	-	-
Stage 2	599	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	62	386	-	-	498	-
Mov Cap-2 Maneuver	157	-	-	-	-	-
Stage 1	205	-	-	-	-	-
Stage 2	537	-	_	_	-	-
g · -	50.					
A	MD		ND		CD	
Approach	WB		NB		SB	
HCM Control Delay, s			0		0.9	
HCM LOS	С					
Minor Lane/Major Mvn	nt	NBT	NBRV	VBLn1	SBL	SBT
Capacity (veh/h)	TC .	NDT	אוטויי	310	498	301
HCM Lane V/C Ratio		-		0.341		-
	\	-				-
HCM Long LOS	)	-	-	22.5	13.1	-
HCM Lane LOS		-	-	C	В	-
HCM 95th %tile Q(veh	1)	-	-	1.5	0.3	-

## Flora Farms TIA

Build (2026) AM with Improvements (East) 08/20/2021

4: Eagle Creek Road/Survey	/ Road	(West)	& Survey	/ Road (F	East)
T. Lagie Oreek Noad/Julive	/ INDAU	(	a Julye	, itoau (L	_ası,

	•	•	<b>†</b>	/	-	ļ
Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	*	7	1>		*	<b>†</b>
Traffic Volume (vph)	38	208	115	45	171	56
Future Volume (vph)	38	208	115	45	171	56
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Storage Length (ft)	75	0		0	200	
Storage Lanes	1	1		0	1	
Taper Length (ft)	45				100	
Satd. Flow (prot)	1641	1538	1753	0	1703	1845
Flt Permitted	0.950				0.950	
Satd. Flow (perm)	1641	1538	1753	0	1703	1845
Link Speed (mph)	35		25			35
Link Distance (ft)	198		1362			1728
Travel Time (s)	3.9		37.1			33.7
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	10%	5%	2%	10%	6%	3%
Shared Lane Traffic (%)						
Lane Group Flow (vph)	42	231	178	0	190	62
Enter Blocked Intersection	No	No	No	No	No	No
Lane Alignment	Left	Right	Left	Right	Left	Left
Median Width(ft)	12		12			12
Link Offset(ft)	0		0			0
Crosswalk Width(ft)	16		16			16
Two way Left Turn Lane						
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15	9		9	15	
Sign Control	Stop		Free			Free
Intersection Summary						
Area Type:	Other					

Area Type: Otl Control Type: Unsignalized

Intersection Capacity Utilization 31.6%

ICU Level of Service A

Analysis Period (min) 15

HCM 95th %tile Q(veh)

## 4: Eagle Creek Road/Survey Road (West) & Survey Road (East)

Intersection						
Int Delay, s/veh	6.5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	ሻ	7	₽		ኘ	<b>↑</b>
Traffic Vol, veh/h	38	208	115	45	171	56
Future Vol, veh/h	38	208	115	45	171	56
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	- -	None	-		-	None
Storage Length	75	0	_	-	200	-
Veh in Median Storage		-	0	_	-	0
Grade, %	0	_	0	_	_	0
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	10	5	2	10	6	3
Mymt Flow	42	231	128	50	190	62
IVIVIIIL I IOVV	42	231	120	50	170	02
Major/Minor 1	Minor1	N	/lajor1		Major2	
Conflicting Flow All	595	153	0	0	178	0
Stage 1	153	-	-	-	-	-
Stage 2	442	-	-	-	-	-
Critical Hdwy	6.5	6.25	-	-	4.16	-
Critical Hdwy Stg 1	5.5	-	-	-	-	-
Critical Hdwy Stg 2	5.5	-	-	-	-	-
Follow-up Hdwy	3.59	3.345	-	-	2.254	-
Pot Cap-1 Maneuver	454	885	-	-	1374	-
Stage 1	856	-	_	-	-	-
Stage 2	631	-	-	_	-	_
Platoon blocked, %			_	_		_
Mov Cap-1 Maneuver	391	885	-	-	1374	-
Mov Cap-2 Maneuver	391	-	_	_	-	_
Stage 1	856	_	_		_	_
Stage 2	544	_	_	_	<u>-</u>	_
Stage 2	544	-	-	-	-	-
Approach	WB		NB		SB	
HCM Control Delay, s	11.2		0		6.1	
HCM LOS	В					
Minor Long/Major May	\.	NDT	MDDV	V/DI ~1V	VDI ~2	CDI
Minor Lane/Major Mvm	Il	NBT	MRKA	VBLn1V		SBL
Capacity (veh/h)		-	-	391	885	1374
HCM Lane V/C Ratio		-		0.108		0.138
HCM Control Delay (s)		-	-		10.5	8
HCM Lane LOS		-	-	С	В	Α

0.5

0.4

## 5: Caratoke Hwy (NC 168) & Fost Boulevard

Lane Group
Lane Configurations
Traffic Volume (vph)         208         144         87         1201         550         96           Future Volume (vph)         208         144         87         1201         550         96           Ideal Flow (vphpl)         1900         1900         1900         1900         1900         1900           Storage Length (ft)         0         250         200         150         150           Storage Lanes         1         1         1         1         1         1           Fil Permitted         0.950         0.950         0.950         3539         3539         1583           Right Turn on Red         No         No         3539         3539         1583           Right Turn on Red         No         No         859         1116         No           Satd, Flow (pcrm)         1770         1583         1770         3539         3539         1583           Right Turn on Red         No         No         3559         155         55         55         55         151         153         160         13.8         160         13.8         160         13.8         160         13.8         160         160         160         160
Future Volume (vphp)         208         144         87         1201         550         96           Ideal Flow (vphpl)         1900         1900         1900         1900         1900         1900           Storage Length (ft)         0         250         200         150           Storage Lanes         1         1         1         1         1           Taper Length (ft)         100         100         100         Sadd. Flow (prot)         1770         1583         1770         3539         3539         1583           FIL Permitted         0.950         0.950         0.950         Sadd. Flow (prot)         1770         1583         1770         3539         3539         1583           File Pemitted         0.950         0.950         Sadd. Flow (prot)         170         1583         1770         3539         3539         1583           Fill Pemitted         0.950         0.950         0.950         0.950         1583         1583         1583         1583         1583         1583         1583         1583         1583         1583         1583         1583         1583         1583         1583         1583         1583         1583         1583 <t< td=""></t<>
Ideal Flow (vphpl)
Storage Length (ft)
Storage Lanes
Taper Length (ft)         100         100           Satd. Flow (prot)         1770         1583         1770         3539         3539         1583           FIt Permitted         0.950         0.950         0.950         0.950         0.950           Satd. Flow (perm)         1770         1583         1770         3539         3539         1583           Right Turn on Red         No         No         No         No         No           Satd. Flow (RTOR)         Link Speed (mph)         25         55         55         55           Link Distance (ft)         557         859         1116         116         116         118         116         116         116         116         116         116         116         116         116         116         116         116         116         116         116         116         116         116         117         117         118         118         118         118         118         118         118         118         118         118         118         118         118         118         118         118         118         118         119         119         119         119         119         11
Said. Flow (prot)         1770         1583         1770         3539         3539         1583           Fit Permitted         0.950         0.950         0.950         0.950         0.950         0.950         0.950         0.950         0.950         0.950         0.950         0.950         0.950         0.950         0.950         0.950         0.950         0.950         0.90         1583         1770         3539         3539         1583         1583         1770         3539         3539         1583         1583         1770         3539         3539         1583         1583         1770         3539         3539         1583         1583         1770         3539         3539         1583         1583         1770         3539         3539         1583         1583         1583         1770         3539         3539         1583         1584         1584         1584         1584         1484         1484         1584         1584 </td
Satd. Flow (perm)   1770   1583   1770   3539   3539   1583   Right Turn on Red   No   Satd. Flow (RTOR)
Satd. Flow (perm)         1770         1583         1770         3539         3539         1583           Right Turn on Red         No         No         No         No           Satd. Flow (RTOR)         Link Speed (mph)         25         55         55           Link Distance (ft)         557         859         1116           Travel Time (s)         15.2         10.6         13.8           Peak Hour Factor         0.90         0.90         0.90         0.90         0.90           Shared Lane Traffic (%)         15.2         10.6         13.8         10.0         10.0         0.90
No
Satd. Flow (RTOR)
Link Speed (mph)         25         55         55           Link Distance (ft)         557         859         1116           Travel Time (s)         15.2         10.6         13.8           Peak Hour Factor         0.90         0.90         0.90         0.90         0.90           Shared Lane Traffic (%)         231         160         97         1334         611         107           Enter Blocked Intersection Lane Alignment         Left         Right         Left         Left         Left         Right           Median Width(ft)         12         12         12         12         12         Left         Right           Link Offset(ft)         0
Link Distance (ft)   557
Travel Time (s)
Peak Hour Factor         0.90
Shared Lane Traffic (%)   Lane Group Flow (vph)   231   160   97   1334   611   107
Lane Group Flow (vph)         231         160         97         1334         611         107           Enter Blocked Intersection Lane Alignment         Left         Right         Left         Left         Left         Left         Right           Median Width(ft)         12         12         12         12         12           Link Offset(ft)         0         0         0         0         0         0           Crosswalk Width(ft)         16         16         16         16         16         16         16         16         16         16         16         16         16         100         1.00
Enter Blocked Intersection         No         Left         Right         Left         Left         Left         Right         Left         Left         Left         Right         Left         Left         Right         Left         Left         Right         Left         Right         Left         Thru         Thru         Right         Left         Left         Right         Left         Left         Le
Lene Alignment   Left Right   Left   Left   Left   Right
Median Width(ft)         12         12         12         12           Link Offset(ft)         0         0         0         0           Crosswalk Width(ft)         16         16         16         16           Two way Left Turn Lane         Yes         Yes         Yes           Headway Factor         1.00         1.00         1.00         1.00         1.00           Turning Speed (mph)         15         9         15         9           Number of Detectors         1         1         1         2         2         1           Detector Template         Left         Right         Left         Thru         Thru         Right           Leading Detector (ft)         20         20         20         100         100         20           Trailing Detector (ft)         0
Link Offset(ft)         0         0         0           Crosswalk Width(ft)         16         16         16           Two way Left Turn Lane         Yes         Yes           Headway Factor         1.00         1.00         1.00         1.00           Turning Speed (mph)         15         9         15         9           Number of Detectors         1         1         1         2         2         1           Detector Template         Left         Right         Left         Thru         Thru         Right           Leading Detector (ft)         20         20         20         100         100         20           Trailing Detector (ft)         0
Crosswalk Width(ft)         16         16         16         16           Two way Left Turn Lane         Yes         Yes         Yes           Headway Factor         1.00         1.00         1.00         1.00         1.00           Turning Speed (mph)         15         9         15         9           Number of Detectors         1         1         1         2         2         1           Detector Template         Left         Right         Left         Thru         Thru         Right           Leading Detector (ft)         20         20         20         100         100         20           Trailing Detector (ft)         0 <td< td=""></td<>
Two way Left Turn Lane         Yes         Yes           Headway Factor         1.00         1.00         1.00         1.00         1.00         1.00           Turning Speed (mph)         15         9         15         9         9           Number of Detectors         1         1         1         2         2         1           Detector Template         Left         Right         Left         Thru         Thru         Right           Leading Detector (ft)         20         20         20         100         100         20           Trailing Detector (ft)         0
Headway Factor
Turning Speed (mph)         15         9         15         9           Number of Detectors         1         1         1         2         2         1           Detector Template         Left         Right         Left         Thru         Thru         Right           Leading Detector (ft)         20         20         20         100         100         20           Trailing Detector (ft)         0
Turning Speed (mph)         15         9         15         9           Number of Detectors         1         1         1         2         2         1           Detector Template         Left         Right         Left         Thru         Thru         Right           Leading Detector (ft)         20         20         20         100         100         20           Trailing Detector (ft)         0         0         0         0         0         0         0         0           Detector 1 Position(ft)         0         <
Detector Template         Left         Right         Left         Thru         Thru         Right           Leading Detector (ft)         20         20         20         100         100         20           Trailing Detector (ft)         0         0         0         0         0         0         0           Detector 1 Position(ft)         0         0         0         0         0         0         0           Detector 1 Position(ft)         20         20         20         6         6         20           Detector 1 Size(ft)         20         20         20         6         6         20           Detector 1 Type         Cl+Ex         Cl+Ex         Cl+Ex         Cl+Ex         Cl+Ex         Cl+Ex           Detector 1 Channel         Detector 1 Queue (s)         0.0         0.0         0.0         0.0         0.0         0.0           Detector 2 Position(ft)         94
Detector Template         Left         Right         Left         Thru         Thru         Right           Leading Detector (ft)         20         20         20         100         100         20           Trailing Detector (ft)         0         0         0         0         0         0         0           Detector 1 Position(ft)         0         0         0         0         0         0         0           Detector 1 Position(ft)         20         20         20         6         6         20           Detector 1 Size(ft)         20         20         20         6         6         20           Detector 1 Type         Cl+Ex         Cl+Ex         Cl+Ex         Cl+Ex         Cl+Ex         Cl+Ex           Detector 1 Channel         Detector 1 Queue (s)         0.0         0.0         0.0         0.0         0.0         0.0           Detector 2 Position(ft)         94
Leading Detector (ft)         20         20         20         100         100         20           Trailing Detector (ft)         0 <t< td=""></t<>
Trailing Detector (ft)         0         0         0         0         0         0         0           Detector 1 Position(ft)         0
Detector 1 Position(ft)         0
Detector 1 Size(ft)         20         20         20         6         6         20           Detector 1 Type         Cl+Ex         Cl+Ex         Cl+Ex         Cl+Ex         Cl+Ex         Cl+Ex           Detector 1 Channel         0.0         0.0         0.0         0.0         0.0         0.0         0.0           Detector 1 Extend (s)         0.0         0.0         0.0         0.0         0.0         0.0         0.0         0.0           Detector 1 Queue (s)         0.0
Detector 1 Type         Cl+Ex         Cl-Ex         Cl-Ex
Detector 1 Channel         Detector 1 Extend (s)         0.0
Detector 1 Extend (s)         0.0         Turn Type         Prot         pm+ov         Prot         NA         NA         pm+ov
Detector 1 Queue (s)         0.0         Turn Type         Prot         Prot         Prot         NA         NA         pm+ov
Detector 1 Delay (s)         0.0         Turn Type         Prot         Prot         Prot         Prot         Prot         NA         NA         pm+ov
Detector 2 Position(ft)         94         94           Detector 2 Size(ft)         6         6           Detector 2 Type         CI+Ex         CI+Ex           Detector 2 Channel         0.0         0.0           Detector 2 Extend (s)         0.0         NA           Turn Type         Prot pm+ov         Prot NA         NA pm+ov
Detector 2 Size(ft)         6         6           Detector 2 Type         CI+Ex         CI+Ex           Detector 2 Channel         0.0         0.0           Detector 2 Extend (s)         Prot pm+ov         Prot NA         NA pm+ov
Detector 2 Type         CI+Ex         CI+Ex           Detector 2 Channel         0.0         0.0           Detector 2 Extend (s)         0.0         0.0           Turn Type         Prot pm+ov         Prot NA         NA pm+ov
Detector 2 Channel  Detector 2 Extend (s)  Turn Type  Prot pm+ov  Prot NA NA pm+ov
Detector 2 Extend (s)  Turn Type  Prot pm+ov  Prot NA  NA pm+ov
Turn Type Prot pm+ov Prot NA NA pm+ov
Direct and Diseases A F F 7 / A
Protected Phases 4 5 5 2 6 4
Permitted Phases 4 6
Detector Phase 4 5 5 2 6 4
Switch Phase
Minimum Initial (s) 7.0 7.0 7.0 14.0 14.0 7.0
Minimum Split (s) 14.0 14.0 21.0 21.0 14.0

# 5: Caratoke Hwy (NC 168) & Fost Boulevard

	۶	•	1	<b>†</b>	ļ	4
Lane Group	EBL	EBR	NBL	NBT	SBT	SBR
Total Split (s)	30.0	17.0	17.0	60.0	43.0	30.0
Total Split (%)	33.3%	18.9%	18.9%	66.7%	47.8%	33.3%
Yellow Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	-2.0	-2.0	-2.0	-2.0	-2.0	-2.0
Total Lost Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lead/Lag		Lead	Lead		Lag	
Lead-Lag Optimize?		Yes	Yes		Yes	
Recall Mode	None	None	None	C-Min	C-Min	None
Act Effct Green (s)	18.9	36.2	12.3	61.1	43.8	67.7
Actuated g/C Ratio	0.21	0.40	0.14	0.68	0.49	0.75
v/c Ratio	0.62	0.25	0.40	0.56	0.36	0.09
Control Delay	39.2	17.4	39.7	9.2	10.5	1.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	39.2	17.4	39.7	9.2	10.5	1.8
LOS	D	В	D	Α	В	Α
Approach Delay	30.3			11.3	9.2	
Approach LOS	С			В	Α	
Queue Length 50th (ft)	120	59	51	179	86	11
Queue Length 95th (ft)	179	83	94	288	64	9
Internal Link Dist (ft)	477			779	1036	
Turn Bay Length (ft)		250	200			150
Base Capacity (vph)	491	653	260	2402	1753	1297
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.47	0.25	0.37	0.56	0.35	0.08
Intersection Summary						

## Intersection Summary

Area Type: Other

Cycle Length: 90 Actuated Cycle Length: 90

Offset: 73 (81%), Referenced to phase 2:NBT and 6:SBT, Start of Green

Natural Cycle: 50

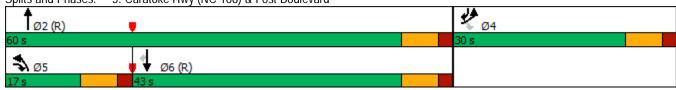
Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.62

Intersection Signal Delay: 13.6 Intersection LOS: B
Intersection Capacity Utilization 53.1% ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 5: Caratoke Hwy (NC 168) & Fost Boulevard



## Flora Farms TIA

Build (2026) AM with Improvements st) 08/20/2021

6: Future Access #1/Future Access #2 & Survey Road (East)

	۶	<b>→</b>	$\rightarrow$	•	<b>←</b>	•	•	<b>†</b>	/	<b>&gt;</b>	ţ	4
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	¥	<b>†</b>	7	¥	ĵ.		*	ĵ.			4	
Traffic Volume (vph)	9	4	74	74	4	19	104	4	46	23	4	23
Future Volume (vph)	9	4	74	74	4	19	104	4	46	23	4	23
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	100		100	100		0	100		0	0		0
Storage Lanes	1		1	1		0	1		0	0		0
Taper Length (ft)	100			100			100			100		
Satd. Flow (prot)	1770	1863	1583	1770	1628	0	1770	1604	0	0	1705	0
Flt Permitted	0.950			0.950			0.950				0.977	
Satd. Flow (perm)	1770	1863	1583	1770	1628	0	1770	1604	0	0	1705	0
Link Speed (mph)		35			35			25			25	
Link Distance (ft)		2903			390			327			235	
Travel Time (s)		56.6			7.6			8.9			6.4	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Shared Lane Traffic (%)												
Lane Group Flow (vph)	10	4	82	82	25	0	116	55	0	0	56	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		12			12			12			12	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Sign Control		Free			Free			Stop			Stop	

Intorco	ction	Summan	,
merse		Summan	v

Area Type: Other

Control Type: Unsignalized

Intersection Capacity Utilization 28.4%

Analysis Period (min) 15

ICU Level of Service A

HCM 95th %tile Q(veh)

0.6

0.2

# 6: Future Access #1/Future Access #2 & Survey Road (East)

Intersection												
Int Delay, s/veh	7.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	ች	<b></b>	7	ች	f)		ች	f)			44	
Traffic Vol, veh/h	9	4	74	74	4	19	104	4	46	23	4	23
Future Vol, veh/h	9	4	74	74	4	19	104	4	46	23	4	23
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	100	-	100	100	-	-	100	-	-	-	-	-
Veh in Median Storage	e,# -	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	90	90	90	90	90	90	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	10	4	82	82	4	21	116	4	51	26	4	26
Major/Minor I	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	25	0	0	86	0	0	218	213	4	272	285	15
Stage 1	-	-	-	-	-	-	24	24	-	179	179	-
Stage 2	_	_	_	_	_	_	194	189	_	93	106	_
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1		-	_		-	_	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	_	2.218	-	-	3.518	4.018	3.318		4.018	3.318
Pot Cap-1 Maneuver	1589	-	-	1510	-	-	738	684	1080	680	624	1065
Stage 1	-	-	-	-	-	-	994	875	-	823	751	-
Stage 2	-	-	-	-	-	-	808	744	-	914	807	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	1589	-	-	1510	-	-	683	643	1080	615	587	1065
Mov Cap-2 Maneuver	-	-	-	-	-	-	683	643	-	615	587	-
Stage 1	-	-	-	-	-	-	988	870	-	818	710	-
Stage 2	-	-	-	-	-	-	741	704	-	861	802	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.8			5.7			10.5			10.1		
HCM LOS	0.0			5.1			В			В		
TIOW LOO							U			U		
Minor Lane/Major Mvm	nt I	NBLn1	MRI n2	EBL	EBT	EBR	WBL	WBT	WBR	SRI n1		
Capacity (veh/h)	it l		1024	1589	LDI	LDK -	1510	VVDI	WDK -	760		
HCM Lane V/C Ratio			0.054				0.054	-		0.073		
HCM Control Delay (s)		11.3	8.7	7.3	-	-	7.5		-			
HCM Lane LOS		11.3 B	Α	7.3 A	-	-	7.5 A			В		
LICINI FUILE FOS		Ď	А	А	-	-	A	-	-	Ď		

0.2

0.2

# 1: Caratoke Hwy (NC 168) & Survey Road (West)

	•				ı	,
		*	7	T	¥	*
Lane Group	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	ሻ	7	ኻ	<b>^</b>	<b>^</b>	7
Traffic Volume (vph)	197	32	27	738	1560	338
Future Volume (vph)	197	32	27	738	1560	338
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0	150	200			200
Storage Lanes	1	1	1			1
Taper Length (ft)	100		100			
Satd. Flow (prot)	1752	1509	1770	3438	3505	1583
Flt Permitted	0.950		0.073			
Satd. Flow (perm)	1752	1509	136	3438	3505	1583
Right Turn on Red		No				No
Satd. Flow (RTOR)						
Link Speed (mph)	35			55	55	
Link Distance (ft)	1728			4412	2769	
Travel Time (s)	33.7			54.7	34.3	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	3%	7%	2%	5%	3%	2%
Shared Lane Traffic (%)			2.3	2.3	2.3	
Lane Group Flow (vph)	219	36	30	820	1733	376
Enter Blocked Intersection	No	No	No	No	No	No
Lane Alignment	Left	Right	Left	Left	Left	Right
Median Width(ft)	24	·g	20.0	12	12	
Link Offset(ft)	0			0	0	
Crosswalk Width(ft)	16			16	16	
Two way Left Turn Lane				Yes	Yes	
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15	9	15			9
Number of Detectors	1	1	1	2	2	1
Detector Template	Left	Right	Left	Thru	Thru	Right
Leading Detector (ft)	20	20	20	100	100	20
Trailing Detector (ft)	0	0	0	0	0	0
Detector 1 Position(ft)	0	0	0	0	0	0
Detector 1 Size(ft)	20	20	20	6	6	20
Detector 1 Type	CI+Ex	CI+Ex	CI+Ex	CI+Ex	CI+Ex	CI+Ex
Detector 1 Channel	OHEX	OHEX	OHEX	OHLK	OHLA	OI LX
Detector 1 Extend (s)	0.0	0.0	0.0	0.0	0.0	0.0
Detector 1 Queue (s)	0.0	0.0	0.0	0.0	0.0	0.0
Detector 1 Delay (s)	0.0	0.0	0.0	0.0	0.0	0.0
Detector 2 Position(ft)	0.0	0.0	0.0	94	94	0.0
Detector 2 Size(ft)				6	6	
Detector 2 Type				CI+Ex	CI+Ex	
Detector 2 Channel				CITLA	CITLX	
Detector 2 Extend (s)				0.0	0.0	
Turn Type	Prot	Perm	D.P+P	NA	NA	pm+ov
Protected Phases	4	I CIIII	D.P+P	2	6	piii+0v 4
Permitted Phases	4	4	6		U	6
Detector Phase	4	4	5	2	6	4
Switch Phase	4	4	J		U	4
Minimum Initial (s)	7.0	7.0	7.0	14.0	14.0	7.0
iviii iiiiiiiiiiiiiiiiiiiiiiiiiiiiiiii	7.0	7.0	7.0	14.0	14.0	7.0

1: Caratoke Hwy (NC 168) & Survey Road (West)

	۶	•	4	<b>†</b>	ļ	1
Lane Group	EBL	EBR	NBL	NBT	SBT	SBR
Minimum Split (s)	12.9	12.9	11.9	20.4	20.4	12.9
Total Split (s)	23.0	23.0	11.9	67.0	55.1	23.0
Total Split (%)	25.6%	25.6%	13.2%	74.4%	61.2%	25.6%
Yellow Time (s)	3.0	3.0	3.0	5.4	5.4	3.0
All-Red Time (s)	2.9	2.9	1.9	1.0	1.0	2.9
Lost Time Adjust (s)	-0.9	-0.9	0.1	-1.4	0.0	-0.9
Total Lost Time (s)	5.0	5.0	5.0	5.0	6.4	5.0
Lead/Lag			Lead		Lag	
Lead-Lag Optimize?			Yes		Yes	
Recall Mode	None	None	None	C-Min	C-Min	None
Act Effct Green (s)	14.8	14.8	62.2	65.2	56.7	79.9
Actuated g/C Ratio	0.16	0.16	0.69	0.72	0.63	0.89
v/c Ratio	0.76	0.15	0.14	0.33	0.79	0.27
Control Delay	52.8	32.0	7.3	5.0	18.1	2.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	52.8	32.0	7.3	5.0	18.1	2.1
LOS	D	С	Α	Α	В	Α
Approach Delay	49.9			5.1	15.2	
Approach LOS	D			Α	В	
Queue Length 50th (ft)	119	18	3	62	411	37
Queue Length 95th (ft)	191	43	m16	116	#629	59
Internal Link Dist (ft)	1648			4332	2689	
Turn Bay Length (ft)		150	200			200
Base Capacity (vph)	350	301	219	2491	2207	1413
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.63	0.12	0.14	0.33	0.79	0.27

#### **Intersection Summary**

Area Type: Other

Cycle Length: 90

Actuated Cycle Length: 90

Offset: 34 (38%), Referenced to phase 2:NBT and 6:NBSB, Start of Green

Natural Cycle: 80

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.79

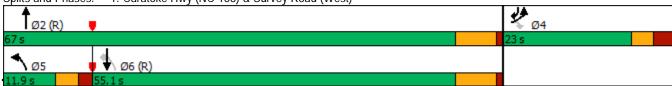
Intersection Signal Delay: 15.3 Intersection LOS: B Intersection Capacity Utilization 63.5% ICU Level of Service B

Analysis Period (min) 15

95th percentile volume exceeds capacity, queue may be longer. Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 1: Caratoke Hwy (NC 168) & Survey Road (West)



K:\RAL\_TPTO\\_Traffic\014316001 Flora Development\T4 - Analysis\Synchro\Build (2026) PM - Original Pcts Improved.syn Kimley-Horn

Page 2

# 2: Caratoke Hwy (NC 168) & Survey Road (East)

	•	•	•	<b>†</b>	<b>↓</b>	4
Lane Group	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations		7	ሻ	<b>^</b>	<b>^</b>	7
Traffic Volume (vph)	0	98	132	773	1622	46
Future Volume (vph)	0	98	132	773	1622	46
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0	0	200			100
Storage Lanes	0	1	1			1
Taper Length (ft)	100		100			
Satd. Flow (prot)	0	1611	1719	3505	3539	1583
Flt Permitted			0.950			
Satd. Flow (perm)	0	1611	1719	3505	3539	1583
Link Speed (mph)	35			55	55	
Link Distance (ft)	328			1116	4412	
Travel Time (s)	6.4			13.8	54.7	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	5%	3%	2%	2%
Shared Lane Traffic (%)						
Lane Group Flow (vph)	0	109	147	859	1802	51
<b>Enter Blocked Intersection</b>	n No	No	No	No	No	No
Lane Alignment	Left	Right	Left	Left	Left	Right
Median Width(ft)	0			12	12	
Link Offset(ft)	0			0	0	
Crosswalk Width(ft)	16			16	16	
Two way Left Turn Lane				Yes	Yes	
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15	9	15			9
Sign Control	Stop			Free	Free	
Intersection Summary						
Area Type:	Other					

Area Type: Othe Control Type: Unsignalized

Intersection Capacity Utilization 58.8%

ICU Level of Service B

Analysis Period (min) 15

Intersection						
Int Delay, s/veh	2.3					
		ED.	ND	NDT	CDT	CDD
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations		7		<b>^</b>	<b>^</b>	7
Traffic Vol, veh/h	0	98	132	773	1622	46
Future Vol, veh/h	0	98	132	773	1622	46
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	200	-	-	100
Veh in Median Storage,	, # 0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	5	3	2	2
Mvmt Flow	0	109	147	859	1802	51
	_					
				_		
	1inor2		Najor1		/lajor2	
Conflicting Flow All	-	901	1853	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.94	4.2	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.32	2.25	-	-	-
Pot Cap-1 Maneuver	0	281	311	-	-	-
Stage 1	0	-	_	-	-	-
Stage 2	0	-	-	-	-	-
Platoon blocked, %				_	_	_
Mov Cap-1 Maneuver	_	281	311	_	_	_
Mov Cap-1 Maneuver	-	201	J11 -	_	_	-
Stage 1	-		-	-	-	-
Ğ	-	-	-			-
Stage 2	-	-	-	-	-	-
Approach	EB		NB		SB	
HCM Control Delay, s	25.7		3.9		0	
HCM LOS	D					
N 40 1 /N 4 - 1 - N 4		NDI	NET	CDL 4	CDT	CDD
Minor Lane/Major Mvm	l	NBL	MRI	EBLn1	SBT	SBR
Capacity (veh/h)		311	-	_~.	-	-
HCM Lane V/C Ratio		0.472	-	0.388	-	-
HCM Control Delay (s)		26.5	-	_0	-	-
HCM Lane LOS		D	-	D	-	-
HCM 95th %tile Q(veh)		2.4	-	1.8	-	-

# 3: Caratoke Hwy (NC 168) & Guinea Road

	•	•	<b>†</b>	/	-	ļ	
Lane Group	WBL	WBR	NBT	NBR	SBL	SBT	
Lane Configurations	¥		<b>↑</b> ↑		ሻ	<b>^</b>	
Traffic Volume (vph)	23	64	869	12	108	1528	
Future Volume (vph)	23	64	869	12	108	1528	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	
Storage Length (ft)	0	0		0	100		
Storage Lanes	1	0		0	1		
Taper Length (ft)	100				100		
Satd. Flow (prot)	1632	0	3465	0	1770	3539	
Flt Permitted	0.987				0.950		
Satd. Flow (perm)	1632	0	3465	0	1770	3539	
Link Speed (mph)	55		55			55	
Link Distance (ft)	1144		980			859	
Travel Time (s)	14.2		12.1			10.6	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	
Heavy Vehicles (%)	5%	3%	4%	2%	2%	2%	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	97	0	979	0	120	1698	
Enter Blocked Intersection	No	No	No	No	No	No	
Lane Alignment	Left	Right	Left	Right	Left	Left	
Median Width(ft)	12		12			12	
Link Offset(ft)	0		0			0	
Crosswalk Width(ft)	16		16			16	
Two way Left Turn Lane			Yes			Yes	
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	
Turning Speed (mph)	15	9		9	15		
Sign Control	Stop		Free			Free	
Intersection Summary							

Area Type: Other Control Type: Unsignalized

Intersection Capacity Utilization 54.1%

ICU Level of Service A

Analysis Period (min) 15

Intersection						
Int Delay, s/veh	1.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
	WDL	WDK	<b>↑</b> ↑	NDR	3DL N	<u>361</u>
Lane Configurations	23	<i>L</i> 1		12		
Traffic Vol. veh/h	23	64	869	12	108	1528 1528
Future Vol, veh/h		64	869		108	
Conflicting Peds, #/hr		O Ctop	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	100	None
Storage Length	0	-	-	-	100	-
Veh in Median Storag		-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	5	3	4	2	2	2
Mvmt Flow	26	71	966	13	120	1698
Major/Minor	Minor1	N	/lajor1	ı	Major2	
Conflicting Flow All	2062	490	0	0	979	0
	973	490		U	919	-
Stage 1			-	-		
Stage 2	1089	-	-	-	-	-
Critical Hdwy	6.9	6.96	-	-	4.14	-
Critical Hdwy Stg 1	5.9	-	-	-	-	-
Critical Hdwy Stg 2	5.9	-	-	-	-	-
Follow-up Hdwy	3.55	3.33	-	-	2.22	-
Pot Cap-1 Maneuver	46	521	-	-	701	-
Stage 1	320	-	-	-	-	-
Stage 2	278	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver		521	-	-	701	-
Mov Cap-2 Maneuver	139	-	-	-	-	-
Stage 1	320	-	-	-	-	-
Stage 2	230	-	-	-	-	-
Annroach	\A/D		NB		CD	
Approach	WB				SB	
HCM Control Delay, s			0		0.7	
HCM LOS	С					
Minor Lane/Major Mvr	nt	NBT	NBRV	VBLn1	SBL	SBT
Capacity (veh/h)				302	701	
HCM Lane V/C Ratio		_	-		0.171	-
HCM Control Delay (s	.)	-	-	22.4	11.2	-
HCM Lane LOS	7)	-				
	<b>6</b> )	-	-	C	B	-
HCM 95th %tile Q(vel	1)	-	-	1.3	0.6	-

## Flora Farms TIA

Build (2026) PM with Improvements (East) 08/20/2021

4: Eagle Creek Road/Surve	v Road (	West) &	Surve	v Road (East)

	•	•	1	~	-	¥
Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	*	7	ĵ.		*	<b>†</b>
Traffic Volume (vph)	33	118	91	48	144	208
Future Volume (vph)	33	118	91	48	144	208
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Storage Length (ft)	75	0		0	200	
Storage Lanes	1	1		0	1	
Taper Length (ft)	45				100	
Satd. Flow (prot)	1719	1583	1771	0	1687	1863
Flt Permitted	0.950				0.950	
Satd. Flow (perm)	1719	1583	1771	0	1687	1863
Link Speed (mph)	35		25			35
Link Distance (ft)	198		1362			1728
Travel Time (s)	3.9		37.1			33.7
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	5%	2%	2%	3%	7%	2%
Shared Lane Traffic (%)						
Lane Group Flow (vph)	37	131	154	0	160	231
Enter Blocked Intersection	No	No	No	No	No	No
Lane Alignment	Left	Right	Left	Right	Left	Left
Median Width(ft)	12		12			12
Link Offset(ft)	0		0			0
Crosswalk Width(ft)	16		16			16
Two way Left Turn Lane						
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15	9		9	15	
Sign Control	Stop		Free			Free
Int	•					

**Intersection Summary** 

Area Type: Other Control Type: Unsignalized

Intersection Capacity Utilization 29.0%

ICU Level of Service A

Analysis Period (min) 15

# 4: Eagle Creek Road/Survey Road (West) & Survey Road (East)

`	,	
ast)		08/20/2021

Intersection						
Int Delay, s/veh	4.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
				INRK		
Lane Configurations	<u>ች</u>	110	<b>♣</b>	40	144	200
Traffic Vol, veh/h	33	118	91	48	144	208
Future Vol, veh/h	33	118	91	48	144	208
Conflicting Peds, #/hr		0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	75	0	-	-	200	-
Veh in Median Storag		-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	5	2	2	3	7	2
Mvmt Flow	37	131	101	53	160	231
Major/Minor	Minor1	N	Najor1		Major2	
						0
Conflicting Flow All	679	128	0	0	154	0
Stage 1	128	-	-	-	-	-
Stage 2	551	-	-	-	-	-
Critical Hdwy	6.45	6.22	-	-	4.17	-
Critical Hdwy Stg 1	5.45	-	-	-	-	-
Critical Hdwy Stg 2	5.45	-	-	-	-	-
Follow-up Hdwy	3.545		-	-	2.263	-
Pot Cap-1 Maneuver	413	922	-	-	1396	-
Stage 1	890	-	-	-	-	-
Stage 2	571	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	366	922	-	-	1396	-
Mov Cap-2 Maneuver	366	-	-	-	-	-
Stage 1	890	-	-	-	-	-
Stage 2	505	-	-	-	-	-
J						
Annraach	WD		ND		CD	
Approach	WB		NB		SB	
HCM Control Delay, s			0		3.2	
HCM LOS	В					
Minor Lane/Major Mvr	mt	NBT	NBRV	VBLn1V	VBLn2	SBL
Capacity (veh/h)		-		366	922	1396
HCM Lane V/C Ratio		_	_		0.142	
HCM Control Delay (s	:)	-	_	15.9	9.6	7.9
HCM Lane LOS	9)	-	-	15.9 C	9.0 A	7.9 A
HCM 95th %tile Q(vel	h)			0.3	0.5	0.4
HOW YOU WINE Q(VE	11)	-	-	0.3	0.5	0.4

# 5: Caratoke Hwy (NC 168) & Fost Boulevard

	۶	•	•	<b>†</b>	ļ	4
Lane Group	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	<u> </u>	ZDK.	<u> </u>	<b>†</b>	<b>†</b> †	7
Traffic Volume (vph)	142	106	153	780	1544	175
Future Volume (vph)	142	106	153	780	1544	175
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0	250	200	1700	1700	150
Storage Lanes	1	250	1			130
	100	I	100			ı
Taper Length (ft)		1502		2520	2520	1502
Satd. Flow (prot)	1770	1583	1770	3539	3539	1583
Flt Permitted	0.950	1502	0.950	2520	2520	1502
Satd. Flow (perm)	1770	1583	1770	3539	3539	1583
Right Turn on Red		No				No
Satd. Flow (RTOR)	0.5					
Link Speed (mph)	25			55	55	
Link Distance (ft)	586			859	1116	
Travel Time (s)	16.0			10.6	13.8	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Shared Lane Traffic (%)						
Lane Group Flow (vph)	158	118	170	867	1716	194
Enter Blocked Intersection	No	No	No	No	No	No
Lane Alignment	Left	Right	Left	Left	Left	Right
Median Width(ft)	12	J		12	12	
Link Offset(ft)	0			0	0	
Crosswalk Width(ft)	16			16	16	
Two way Left Turn Lane				Yes	Yes	
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	1.00	9	1.00	1.00	1.00	9
Number of Detectors	13	1	13	2	2	1
Detector Template	Left	-	Left	Thru	Thru	
•		Right				Right
Leading Detector (ft)	20	20	20	100	100	20
Trailing Detector (ft)	0	0	0	0	0	0
Detector 1 Position(ft)	0	0	0	0	0	0
Detector 1 Size(ft)	20	20	20	6	6	20
Detector 1 Type	CI+Ex	CI+Ex	CI+Ex	CI+Ex	CI+Ex	CI+Ex
Detector 1 Channel						
Detector 1 Extend (s)	0.0	0.0	0.0	0.0	0.0	0.0
Detector 1 Queue (s)	0.0	0.0	0.0	0.0	0.0	0.0
Detector 1 Delay (s)	0.0	0.0	0.0	0.0	0.0	0.0
Detector 2 Position(ft)				94	94	
Detector 2 Size(ft)				6	6	
Detector 2 Type				CI+Ex	CI+Ex	
Detector 2 Type  Detector 2 Channel				OI? EX	OHEA	
Detector 2 Extend (s)				0.0	0.0	
Turn Type	Prot	pm+ov	Prot	NA	NA	pm+ov
Protected Phases	4	piii+0v 5	5	2	1NA 6	piii+0v 4
	4		5	2	0	
Permitted Phases		4	-	2	,	6
Detector Phase	4	5	5	2	6	4
Switch Phase	7.0		7.0	1.0	4.5	7.0
Minimum Initial (s)	7.0	7.0	7.0	14.0	14.0	7.0
Minimum Split (s)	14.0	14.0	14.0	21.0	21.0	14.0

# 5: Caratoke Hwy (NC 168) & Fost Boulevard

	•	•	4	<b>†</b>	ţ	4
Lane Group	EBL	EBR	NBL	NBT	SBT	SBR
Total Split (s)	18.0	17.0	17.0	72.0	55.0	18.0
Total Split (%)	20.0%	18.9%	18.9%	80.0%	61.1%	20.0%
Yellow Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	-2.0	-2.0	-2.0	-2.0	-2.0	-2.0
Total Lost Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lead/Lag		Lag	Lag		Lead	
Lead-Lag Optimize?		Yes	Yes		Yes	
Recall Mode	None	None	None	C-Min	C-Min	None
Act Effct Green (s)	12.5	29.3	11.8	67.5	50.7	68.2
Actuated g/C Ratio	0.14	0.33	0.13	0.75	0.56	0.76
v/c Ratio	0.65	0.23	0.73	0.33	0.86	0.16
Control Delay	49.7	23.2	57.2	4.2	10.3	1.6
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	49.7	23.2	57.2	4.2	10.3	1.6
LOS	D	С	Е	Α	В	Α
Approach Delay	38.4			12.9	9.4	
Approach LOS	D			В	Α	
Queue Length 50th (ft)	86	48	94	71	52	8
Queue Length 95th (ft)	#153	90	#188	93	76	m15
Internal Link Dist (ft)	506			779	1036	
Turn Bay Length (ft)		250	200			150
Base Capacity (vph)	255	500	236	2655	1994	1191
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.62	0.24	0.72	0.33	0.86	0.16

#### **Intersection Summary**

Area Type: Other

Cycle Length: 90 Actuated Cycle Length: 90

Offset: 8 (9%), Referenced to phase 2:NBT and 6:SBT, Start of Green

Natural Cycle: 65

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.86

Intersection Signal Delay: 13.0 Intersection LOS: B
Intersection Capacity Utilization 71.5% ICU Level of Service C

Analysis Period (min) 15

# 95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5: Caratoke Hwy (NC 168) & Fost Boulevard



## Flora Farms TIA

Build (2026) PM with Improvements st) 08/20/2021

6: Future Access #1/Future Access #2 & Survey Road (East)

	۶	<b>→</b>	$\rightarrow$	•	<b>←</b>	•	<b>1</b>	<b>†</b>	/	-	ļ	1
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	7	<b>↑</b>	7	ሻ	1>		ሻ	₽			4	
Traffic Volume (vph)	10	4	77	77	4	30	56	4	25	21	4	19
Future Volume (vph)	10	4	77	77	4	30	56	4	25	21	4	19
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	100		100	100		0	100		0	0		0
Storage Lanes	1		1	1		0	1		0	0		0
Taper Length (ft)	100			100			100			100		
Satd. Flow (prot)	1770	1863	1583	1770	1613	0	1770	1619	0	0	1713	0
Flt Permitted	0.950			0.950			0.950				0.977	
Satd. Flow (perm)	1770	1863	1583	1770	1613	0	1770	1619	0	0	1713	0
Link Speed (mph)		35			35			25			25	
Link Distance (ft)		2916			377			351			255	
Travel Time (s)		56.8			7.3			9.6			7.0	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Shared Lane Traffic (%)												
Lane Group Flow (vph)	11	4	86	86	37	0	62	32	0	0	48	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		12			12			12			12	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Sign Control		Free			Free			Stop			Stop	

Intersection Summary

Area Type: Other

Control Type: Unsignalized

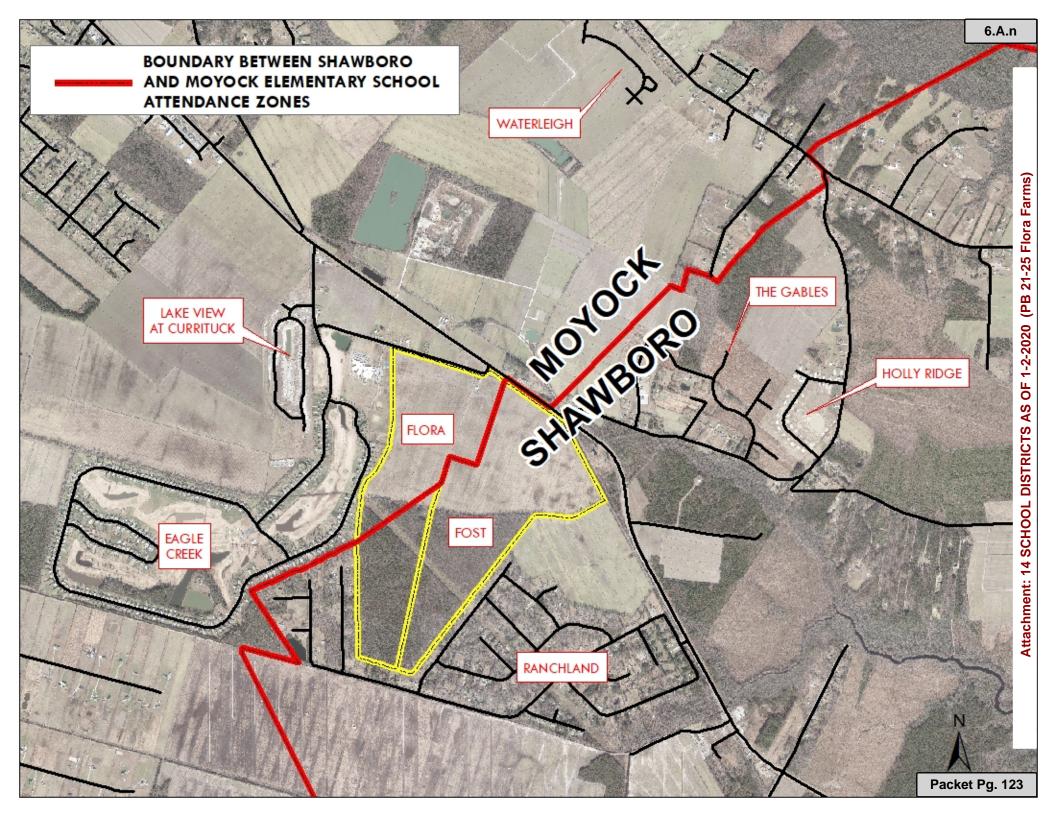
Intersection Capacity Utilization 26.8%

Analysis Period (min) 15

ICU Level of Service A

# 6: Future Access #1/Future Access #2 & Survey Road (East)

Intersection												
Int Delay, s/veh	5.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	ሻ	<b>↑</b>	7	ሻ	ĵ.		*	ĵ.			44	
Traffic Vol, veh/h	10	4	77	77	4	30	56	4	25	21	4	19
Future Vol, veh/h	10	4	77	77	4	30	56	4	25	21	4	19
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	100	-	100	100	-	-	100	-	-	-	-	-
Veh in Median Storage	e,# -	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	90	90	90	90	90	90	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	11	4	86	86	4	33	62	4	28	23	4	21
Major/Minor	Major1			Major2		<u> </u>	Vinor1			Minor2		
Conflicting Flow All	37	0	0	90	0	0	231	235	4	278	305	21
Stage 1	-	-	-	-	-	-	26	26	-	193	193	-
Stage 2	-	-	-	-	-	-	205	209	-	85	112	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1574	-	-	1505	-	-	724	666	1080	674	608	1056
Stage 1	-	-	-	-	-	-	992	874	-	809	741	-
Stage 2	-	-	-	-	-	-	797	729	-	923	803	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	1574	-	-	1505	-	-	671	623	1080	621	569	1056
Mov Cap-2 Maneuver	-	-	-	-	-	-	671	623	-	621	569	-
Stage 1	-	-	-	-	-	-	985	868	-	803	699	-
Stage 2	-	-	-	-	-	-	732	687	-	888	797	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.8			5.2			10.2			10.1		
HCM LOS							В			В		
Minor Lane/Major Mvn	nt	NBLn11	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1		
Capacity (veh/h)		671	981	1574	-	-	1505		-	748		
HCM Lane V/C Ratio		0.093		0.007	-		0.057	-	-	0.065		
HCM Control Delay (s)	)	10.9	8.8	7.3	-	-	7.5	-	-			
HCM Lane LOS		В	A	A	-	-	Α	-	-	В		
HCM 95th %tile Q(veh	1)	0.3	0.1	0	-	-	0.2	-	-	0.2		
	•											



## U.S. ARMY CORPS OF ENGINEERS

WILMINGTON DISTRICT

Action Id. SAW-2019-00619 County: Currituck U.S.G.S. Quad: Lambs Corner NE

## NOTIFICATION OF JURISDICTIONAL DETERMINATION

Property Owner/Applicant: Mary Flora Brumsey

Address: 196 Ed Brumsey Lane

Currituck, North Carolina 27929

Telephone Number: 252-232-3005

Size (acres)

111.7- acres

River Basin

Nearest Town Snowden

Nearest Waterway **USGS HUC** 

Roland Creek 03010205

Coordinates

Albemarle-Chowan Latitude: 36.490988 N Longitude: -76.156546 W

Location description: Property is an approximate 111.7-acre tract located off US Highway 168 and Survey Road, adjacent to Roland Creek and the Northwest River, near Snowden, in Currituck County, North Carolina. The project area consists of an approximate 42-acre wooded area immediately adjacent to the existing agricultural fields on the property. Currituck County Parcel No. 8031-61-2088. Deed Book 1230. Page 398.

## **Indicate Which of the Following Apply:**

## A. Preliminary Determination



X There are waters, including wetlands, on the above described property that may be subject to Section 404 of the Clean Water Act (CWA)(33 USC § 1344) and/or Section 10 of the Rivers and Harbors Act (RHA) (33 USC § 403). The waters, including wetlands, have been delineated, and the delineation has been verified by the Corps to be sufficiently accurate and reliable. Therefore this preliminary jurisdiction determination may be used in the permit evaluation process, including determining compensatory mitigation. For purposes of computation of impacts, compensatory mitigation requirements, and other resource protection measures, a permit decision made on the basis of a preliminary JD will treat all waters and wetlands that would be affected in any way by the permitted activity on the site as if they are jurisdictional waters of the U.S. This preliminary determination is not an appealable action under the Regulatory Program Administrative Appeal Process (Reference 33 CFR Part 331). However, you may request an approved JD, which is an appealable action, by contacting the Corps district for further instruction.

There are wetlands on the above described property, that may be subject to Section 404 of the Clean Water Act (CWA)(33 USC § 1344) and/or Section 10 of the Rivers and Harbors Act (RHA) (33 USC § 403). However, since the waters, including wetlands, have not been properly delineated, this preliminary jurisdiction determination may not be used in the permit evaluation process. Without a verified wetland delineation, this preliminary determination is merely an effective presumption of CWA/RHA jurisdiction over all of the waters, including wetlands, at the project area, which is not sufficiently accurate and reliable to support an enforceable permit decision. We recommend that you have the waters of the U.S. on your property delineated. As the Corps may not be able to accomplish this wetland delineation in a timely manner, you may wish to obtain a consultant to conduct a delineation that can be verified by the Corps.

# **B.** Approved Determination

There are Navigable Waters of the United States within the above described property subject to the permit requirements of Section 10 of the Rivers and Harbors Act (RHA) (33 USC § 403) and Section 404 of the Clean Water Act (CWA)(33 USC § 1344). Unless there is a change in law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.

There are waters of the U.S., including wetlands, on the above described property subject to the permit requirements of Section 404 of the Clean Water Act (CWA) (33 USC § 1344). Unless there is a change in the law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.

We recommend you have the waters of the U.S. on your property delineated. As the Corps may not be able to accomplish this wetland delineation in a timely manner, you may wish to obtain a consultant to conduct a delineation that can be verified by the Corps.

SAW-2019-00619 Brumsey Corps Regulatory Official: RAPORN, BENJ, P.N.S

Date: March 8, 2019 Expiration Date: N/A for PJD

The Wilmington District is committed to providing the highest level of support to the public. To help us ensure we continue to do so, please complete our Customer Satisfaction Survey, located online at <a href="http://corpsmapu.usace.army.mil/cm\_apex/f?p=136:4:0">http://corpsmapu.usace.army.mil/cm\_apex/f?p=136:4:0</a>.

Copy Furnished: CESAW/RG-W/Bland

SAW-2019-00619 Brumsey

E: PRELIMINARY JURISDICTIONAL DETERMINATION: You do not need to respond to the Corps regarding the preliminary JD. The Preliminary JD is not appealable. If you wish, you may request an approved JD (which may be appealed), by contacting the Corps district for further instruction. Also you may provide new information for further consideration by the Corps to reevaluate the JD.

## SECTION II - REQUEST FOR APPEAL or OBJECTIONS TO AN INITIAL PROFFERED PERMIT

REASONS FOR APPEAL OR OBJECTIONS: (Describe your reasons for appealing the decision or your objections to an initial proffered permit in clear concise statements. You may attach additional information to this form to clarify where your reasons or objections are addressed in the administrative record.)

ADDITIONAL INFORMATION: The appeal is limited to a review of the administrative record, the Corps memorandum for the record of the appeal conference or meeting, and any supplemental information that the review officer has determined is needed to clarify the administrative record. Neither the appellant nor the Corps may add new information or analyses to the record. However, you may provide additional information to clarify the location of information that is already in the administrative record.

#### POINT OF CONTACT FOR QUESTIONS OR INFORMATION:

If you have questions regarding this decision and/or the appeal process you may contact:

District Engineer, Wilmington Regulatory Division,

Attn: Raleigh W. Bland, PWS

2407 West 5th Street

Washington, North Carolina 27889

910-252-4558

If you only have questions regarding the appeal process you may also contact:

Mr. Jason Steele, Administrative Appeal Review Officer

CESAD-PDO

U.S. Army Corps of Engineers, South Atlantic Division

60 Forsyth Street, Room 10M15 Atlanta, Georgia 30303-8801

Phone: (404) 562-5137

RIGHT OF ENTRY: Your signature below grants the right of entry to Corps of Engineers personnel, and any government consultants, to conduct investigations of the project site during the course of the appeal process. You will be provided a 15 day notice of any site investigation, and will have the opportunity to participate in all site investigations.

Date:

Telephone number:

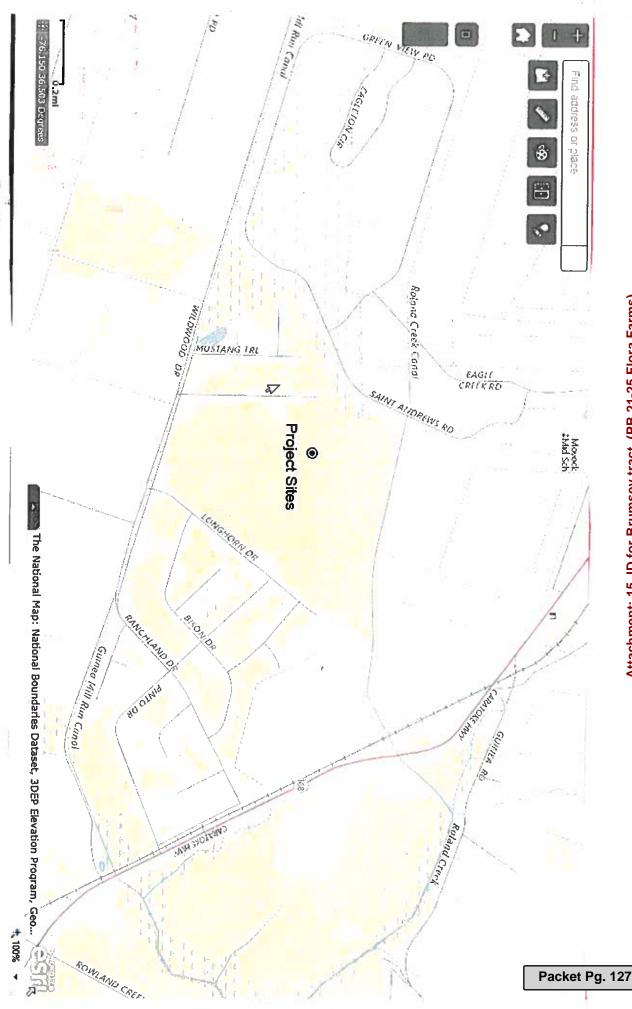
Signature of appellant or agent.

For appeals on Initial Proffered Permits send this form to:

District Engineer, Wilmington Regulatory Division, Raleigh W. Bland, PWS, 2407 West 5th Street Washington, North Carolina 27889

For Permit denials, Proffered Permits and approved Jurisdictional Determinations send this form to:

Division Engineer, Commander, U.S. Army Engineer Division, South Atlantic, Attn: Mr. Jason Steele, Administrative Appeal Officer, CESAD-PDO, 60 Forsyth Street, Room 10M15, Atlanta, Georgia 30303-8801 Phone: (404) 562-5137



Attachment: 15 JD for Brumsey tract (PB 21-25 Flora Farms)

## WETLAND DETERMINATION DATA FORM - Atlantic and Gulf Coastal Plain Region

Project/Site: Brumsey Tract City/County: Moyock / Currituck Sampling Date: March 1, 2019
Investigator(s): R. Bland / USACE RG-W Section, Township, Range: Snowden – Lambs Comer NE
Landform (hillslope, terrace, etc.): Flat Local relief (concave, convex, none): None Stope (%): 0-2 %
Subregion (LRR or MLRA):         USDA LRR-T         Lat:         36.490988 N         Long:         -76.156546 W         Datum:         NAD 83
Soil Map Unit Name: Mapped as Ws – Wasda muck NWI classification: None
Are climatic / hydrologic conditions on the site typical for this time of year? YesNo(If no, explain in Remarks.)
Are Vegetation, Soil, or Hydrology significantly disturbed? Are "Normal Circumstances" present? Yes 🖾 No
Are Vegetation, Soil, or Hydrology naturally problematic? (If needed, explain any answers in Remarks.)
SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.
Hydrophytic Vegetation Present? Yes No D
Hydric Soil Present? Wetland  Yes No Is the Sampled Area within a Wetland?  Yes No I
Hydrology Present? Yes No
Remarks: All 3 required parameters are satisfied per the Corps of Engineers 1987 Manual and the Atlantic and Gulf Coastal Plain Regional Supplement.
w
HYDROLOGY
Wetland Hydrology Indicators: Secondary Indicators (minimum of two required)
Primary Indicators (minimum of one is required; check all that apply)
Surface Water (A1) Aquatic Fauna (B13) Sparsely Vegetated Concave Surface (B8)
High Water Table (A2)  Marl Deposits (B15) (LRR U)  Drainage Patterns (B10)
Saturation (A3)
☐ Water Marks (B1) ☐ Oxidized Rhizospheres on Living Roots (C3) ☐ Dry-Season Water Table (C2)
Sediment Deposits (B2)  Presence of Reduced Iron (C4)  Crayfish Burrows (C8)
Drift Deposits (B3) Recent Iron Reduction in Tilled Soils (C6) Saturation Visible on Aerial Imagery (C9)
Algal Mat or Crust (B4)  Thin Muck Surface (C7)  Geomorphic Position (D2)
☐ Iron Deposits (B5) ☐ Other (Explain in Remarks) ☐ Shallow Aquitard (D3)
☐ Inundation Visible on Aerial Imagery (B7) ☐ FAC-Neutral Test (D5) ☐ Sphagnum moss (D8) (LRR T. U)
Water-Stained Leaves (B9)
Surface Water Present? Yes No Depth (inches):
Water Table Present? Yes No Depth (inches): < 14 inch
Saturation Present? Yes No Depth (inches): < 8 Inch Wetland Hydrology Present? Yes No D
(includes capillary fringe)
Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available: Aerial photography reveals field ditches on both ends of woods area and subdivision bordering other sides.
Remarks:
Drainage ditches evident throughout the immediate geographic area.
Dramage ditches evident infoughout the inimediate geographic area.
Vi.

	Absolute	Dominant	Indicator	Dominance Test worksheet:	
Free Stratum (Plot size: 30 ft )	% Cover	Species?	Status	Politica last Antequest:	
Pinus taeda	60		FAC	Number of Dominant Species	
Acer rubrum	10		FAC	That Are OBL, FACW, or FAC:3	(A)
Liquidambar styraciflua	10	<del></del>	FAC	Total Number of Dominant	
Quercus alba	5	<del></del>	FACU	Species Across All Strata: 33	(B)
··· <del></del>		<del>- H</del>	1700	Percent of Dominant Species	
š		<del>- H</del>		That Are OBL, FACW, or FAC:100	(A/B
5		<del></del>			
7				Prevalence Index worksheet:	
3				Total % Cover of: Multiply by	<u>r:</u>
		: Total Cover		OBL species x 1 =	
50% of total cover:42.5_	_ 20% of tota	al cover:1	<b>7</b>	FACW species x 2 =	
Sapling/Shrub Stratum (Plot size: 30 ft )				FAC species x 3 =	
lilex opaca	40	M	FACW	EACH angeing	
Cornus florida	10	<del>- #</del>	FACU	FACU species x 4 =	
Ilex coricea	5	<del></del>	FAC	UPL species x 5 =	
	5			Column Totals: (A)	(
Phytolaca americana	5		FACU	- 25	
j				Prevalence Index = B/A =	
3.				Hydrophytic Vegetation Indicators:	
7		$\overline{}$		1 - Rapid Test for Hydrophytic Vegetation	n
8.				2 - Dominance Test is >50%	
	60 % =	Total Cover		1/2	
50% of total cover:30				3 - Prevalence Index is ≤3.0¹	
3	_		<del></del>	Problematic Hydrophytic Vegetation¹ (Expl.	ain)
lerb Stratum (Plot size:30 ft)		_			
Arundinaria gigantea	20	$\underline{\hspace{1cm}}$	FAC	<sup>1</sup> Indicators of hydric soil and wetland hydrology m	ust be
Euthamia spp	5		FAC	present, unless disturbed or problematic.	
3	·		-		
•		<del></del>		Definitions of Four Vegetation Strata:	
·		<del>- =</del>		<u> </u>	
S		<del>-    </del>		Tree – Woody plants, excluding vines, 3 in. (7.	
ò		<del>- =</del>		larger in diameter at breast height (DBH), rega height.	idless of
		<del></del>			
3				Sapling/Shrub – Woody plants, excluding vine than 3 in. DBH and greater than 3.28 ft. (1m) to	es, iess ali
)				than 5 in. Dorrand greater than 5.20 it. (1111) to	all.
).				Herb - All herbaceous (non-woody) plants, reg	gardless
I.	-			of size, and woody plants less than 3.28 ft tall.	
2.	-	— <u> </u>		Woody vine - All woody vines, greater than 3.	20 8 14
	25 % =	Total Cover		height.	.20 IL III
50% of total cover:12.5_				noight.	
	_		_		
Voody Vine Stratum (Plot size: 30 ft )					
Smilax glauca	5		FAC		
Smilax rotundifolia	5	<del></del>	FAC		
	<del>-</del>	<del></del>	170		
b					
i. <u> </u>				Hydrophytic	
52		Total Cover		Vegetation	_
50% of total cover:5%_	_ 20% of total	cover:2	_	Present? Yes No No	
Remarks: (If observed, list morphological adaptations below	v)			<u> </u>	
terraine, (ii observed, not therprological adaptations below	7.			<b>X</b>	
Aajority of project consists of mature timber with little evide	nce of timber	activities.			

Depth			btu needed to di	cument th	e indicator o	r confirm	the absence of indicator	s.)		
	Matrix			Redox Fea	tures					
nches)	Color (moist)	%	Color (moist)	%	Type <sup>1</sup>	Loc <sup>2</sup>	Texture	Remarks		
0-6 inch	10 YR 2/1	100		245			muck			
3-14 Inch	5 YR 2/2	100								
4-20 inch	10 YR 4/2	100					loam			
:0-35 inch	10 YR 5/2	90	10 YR 4/1	10	С	M		mottles		
	<del></del>									
Type: C=C	oncentration, D=Dep	oletion, RM	/=Reduced Matrix	, CS=Cove	red or Coated	Sand Gra	ains <sup>2</sup> Location: F	L=Pore Lining, M=Matrix.		
ydric Soil li	ndicators:						Indicators for Problem	natic Hydric Soils³:		
Histosol (	(A1)		Polyvalue	Below Sur	face (S8) (LR	R S, T, U)	1 cm Muck (A9) (LR	(R O)		
Histic Epi	ipedon (A2)		Thin Dark	Surface (S	9) (LRR S, T,	, U)	2 cm Muck (A10) (L	RR S)		
Black His			Loamy M	ucky Minera	al (F1) (LRR C	<b>)</b> )	Reduced Vertic (F1)	B) (outside MLRA 150A,I	3)	
	n Sulfide (A4)			eyed Matrix	•			n Soils (F19) <b>(LRR P, S, 1</b>	•	
_	Layers (A5)			Matrix (F3)				pamy Soils (F20) (MLRA	153B)	
_	Bodies (A6) (LRR P			irk Surface			Red Parent Material			
7	cky Mineral (A7) (LF	`		Dark Surfac			Very Shallow Dark S			
_	esence (A8) (LRR U	)		pressions (	(F8)		Cther (Explain in Re	marks)		
_	ck (A9) (LRR P, T) Below Dark Surface	n (A11)	Mari (F10)		I) (MLRA 151	,				
7	rk Surface (A12)	c (A11)		•	ses (F12) (LF	•	") 3ladicators of hydroni	udic upgatation and watta	nd budea	logu
=	airie Redox (A16) (N	ALRA 150		-	) (LRR P, T, L			rytic vegetation and wetla , unless disturbed or prob		iogy
_	ucky Mineral (S1) (L			nric (F17) (N		<i>.</i> ,				
<b>-</b>	leyed Matrix (S4)	,			) (MLRA 150A	4, 150B)				
_	edox (S5)				Soils (F19) (N		A)			
_	Matrix (S6)			•			149A, 153C, 153D)			
_	face (S7) (LRR P, S	, T, U)			,					
Dark Qui		:		• •				• • • • • • • • • • • • • • • • • • • •		
	_ayer (if observed)									
	_ayer (if observed)									
Restrictive L							Hydric Soil Present?	YesX	No .	
Restrictive L Type: Depth (in							Hydric Soil Present?	Yes	No.	
Type: Depth (in Remarks:	iches):	Network L	ink Profile Mappi	ag for Wasd	la muck		Hydric Soil Present?	YesX	No.	
Type: Depth (in Remarks:		Network L	.ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No.	
Type: Depth (in Remarks:	iches):	Network L	ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	YesX	No.	
Type: Depth (in Remarks:	iches):	Network L	ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	YesX	No .	
Type: Depth (in Remarks:	iches):	Network L	.ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No .	1
Type: Depth (in Remarks:	iches):	Network L	.ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No.	
Type: Depth (in Remarks:	iches):	Network L	ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No .	
Type: Depth (in Remarks:	iches):	Network L	ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No .	
Type: Depth (in Remarks:	iches):	Network L	.ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No.	
Type: Depth (in Remarks:	iches):	Network L	ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No _	
Type: Depth (in Remarks:	iches):	Network L	ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No .	
Type: Depth (in Remarks:	iches):	Network L	ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No.	
Type: Depth (in Remarks:	iches):	Network L	.ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No .	
Type: Depth (in Remarks:	iches):	Network L	.ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No.	
Type: Depth (in Remarks:	iches):	Network L	ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No	
Type: Depth (in Remarks:	iches):	Network L	ink Profile Mappi	ng for Wasd	la muck.		Hydric Soil Present?	Yes	No.	

# WETLAND DETERMINATION DATA FORM – Atlantic and Gulf Coastal Plain Region

Project/Site: Brumsey Tract City/County: Moyock / Currituck Sampling Date: March 1, 2019
Applicant/Owner: Mary Brumsey 196 Ed Brumsey Road Currituck NC 27929 State: NC Sampling Point: CF-1
Investigator(s): R. Bland / USACE RG-W Section, Township, Range: Snowden – Lambs Corner NE
Landform (hillslope, terrace, etc.): Flat Local relief (concave, convex, none): None Slope (%): 0-2 %
Subregion (LRR or MLRA):         USDA LRR-T         Lat:         36.490988 N         Long:         -76.156546 W         Datum:         NAD 83
Soil Map Unit Name: Mapped as Ca - Cape Fear loam NWI classification: None
Are climatic / hydrologic conditions on the site typical for this time of year? Yes No (If no, explain in Remarks.)
Are Vegetation, Soil, or Hydrology significantly disturbed?
Are Vegetation, Soil, or Hydrology naturally problematic? (If needed, explain any answers in Remarks.)
SUMMARY OF FINDINGS - Attach site map showing sampling point locations, transects, important features, etc.
Hydrophytic Vegetation Present? Yes No
Hydric Soil Present? Yes No Is the Sampled Area within a Wetland? Yes No X
Hydrology Present? Yes No No
Remarks: All 3 required parameters are NOT satisfied per the Corps of Engineers 1987 Manual and the Atlantic and Gulf Coastal Plain Regional Supplement.
, , , , , , , , , , , , , , , , , , ,
N N
HYDROLOGY
Wetland Hydrology Indicators:  Secondary Indicators (minimum of two required)
Primary Indicators (minimum of one is required; check all that apply)  Secondary molcators (minimum of wo required)  Surface Soil Cracks (B6)
Surface Water (A1) Aquatic Fauna (B13) Sparsely Vegetated Concave Surface (B8)
High Water Table (A2) Marl Deposits (B15) (LRR U) Drainage Patterns (B10)
Saturation (A3) Hydrogen Sulfide Odor (C1) Moss Trim Lines (B16)
☐ Water Marks (B1) ☐ Oxidized Rhizospheres on Living Roots (C3) ☐ Dry-Season Water Table (C2)
Sediment Deposits (B2)
☐ Drift Deposits (B3) ☐ Recent Iron Reduction in Tilled Soils (C6) ☐ Saturation Visible on Aerial Imagery (C9)
Algal Mat or Crust (B4)  Thin Muck Surface (C7)  Geomorphic Position (D2)
☐ Iron Deposits (B5) ☐ Other (Explain in Remarks) ☐ Shallow Aquitard (D3)
Inundation Visible on Aerial Imagery (B7)  FAC-Neutral Test (D5)
Water-Stained Leaves (B9) Sphagnum moss (D8) (LRR T, U) Field Observations:
Surface Water Present? Yes No Depth (inches):
Water Table Present? Yes No Depth (inches): >36 inch
Saturation Present? Yes No Depth (inches): >25 inch Wetland Hydrology Present? Yes No No
(includes capillary fringe)  Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available: Aerial photography reveals field ditches on both ends
of woods area and subdivision bordering other sides.
Remarks:
Drainage ditches evident throughout the immediate geographic area and some areas on this property are subject to those effects.

## VEGETATION (Four Strata) - Use scientific names of plants.

Sampling Point: CF-1

	Absolute	Dominant	Indicator	Dominance Test worksheet:
Tree Stratum (Plot size: 30 ft )	% Cover 60	Species?	Status FAC	Number of Dominant Species
2. Acer rubrum	10	<u> </u>	FAC	That Are OBL, FACW, or FAC:3 (A)
3. Liquidambar styraciflua	10	<del></del>	FAC	Total Number of Dominant
	-	<del></del>		Species Across All Strata: 3 (B)
4. Quercus alba	<del></del>	<del>-</del>	FACU	Percent of Dominant Species
5				That Are OBL, FACW, or FAC:100 (A/B)
6		<u></u>		
7		— <u></u>		Prevalence Index worksheet:
8	05.0/	7.4.10.		Total % Cover of: Multiply by:
50% of total cover:42.5_		= Total Cover	,	OBL speciesx 1 =
	_ 20%0110	(ai covoi1	_	FACW species
Sapling/Shrub Stratum (Plot size: 30 ft )	40		FACIAL	FAC species x 3 =
1. Ilex opaca	10	<u>₩</u>	FACU	FACU species x 4 =
2. Cornus florida	5	— <u></u> ⊢	FACU	UPL species x 5 =
3. Îlex coricea	5	<del></del>	FAC	Column Totals: (A) (B)
4. Phytolaca americana	5	<del>_</del> _	FACU	
5		<b>—</b> ⊟		Prevalence Index = B/A =
6				Hydrophytic Vegetation Indicators:
7				1 - Rapid Test for Hydrophytic Vegetation
8				2 - Dominance Test is >50%
50% of total cover:30		= Total Cover		3 - Prevalence Index is ≤3.01
30 % of total cover30	_ 20 % 01 1012	11 COVE112		Problematic Hydrophytic Vegetation¹ (Explain)
Herb Stratum (Plot size: 30 ft )				
1. Arundinaria gigantea	20	$\underline{\hspace{1cm}}$	FAC	1Indicators of hydric soil and wetland hydrology must be
2. Euthamia spp	5		FAC	present, unless disturbed or problematic.
3				
4		W		Definitions of Four Vegetation Strata:
5				Tree - Woody plants, excluding vines, 3 in. (7.6 cm) or
6				larger in diameter at breast height (DBH), regardless of
7				height.
8			_	Sapling/Shrub – Woody plants, excluding vines, less
9				than 3 in. DBH and greater than 3.28 ft. (1m) tall.
10				Herb – All herbaceous (non-woody) plants, regardless
11				of size, and woody plants less than 3.28 ft tall.
12				Woody vine - All woody vines, greater than 3.28 ft. in
30		<ul> <li>Total Cover</li> </ul>		height.
50% of total cover:12.5_	_ 20% of tota	il cover:5	_	
Missish Vine Charles (Disk size)				
Woody Vine Stratum (Plot size: 30 ft )  1 Smilax glauca	5		FAC	
2 Smilax rotundifolia	5	<del>-      </del>	FAC	1
		<del>-  - </del>	170	-
3		<del></del>		1
4		<del></del>		1
5	10 %	= Total Cover		Hydrophytic
50% of total cover:5%_			_	Vegetation   Present?   Yes   No
Remarks: (If observed, list morphological adaptations below	n).			
	•			12
Majority of project consists of mature timber with little evider very clean and sparse.	nce of timber	activities. Unde	rstory is	
vory album and openso.				
				21

Depth	ription: (Describe Matrix			Redox Fea								
(inches)	Color (moist)	%	Color (moist)	%	Type <sup>1</sup>	Loc2	Texture		Rema	rke		
0-15 inch	10 YR 2/1	100			Турс		loam		TCIIIa	110		
15-22	10 YR 4/1	90	10 YR 3/2	10	С	M	Clay loam					
22-36+	10 YR 5/1	90	10 YR 3/2	10		M						
22-30+	10 11 3/1		10 17 3/2				clay					—
										_	-	-
												-
									_			-
<del></del> -		. —										
	oncentration, D=De	pletion, RN	M=Reduced Matrix	, CS=Cove	red or Coated	Sand Gr		cation: PL=Pore				
Hydric Soil I			□ Oakarakia	Balan Our	(non (00) (1 B		Indicators for I		dric Soils	3;		
Histosol					face (S8) (LR		-	(A9) (LRR O)				
_ `	ipedon (A2)				9) (LRR S, T,			(A10) (LRR S)	ida MI DA	4504 D		
Black His	n Sulfide (A4)			icky Minera Byed Matrix	il (F1) (LRR (	رر		rtic (F18) <b>(out</b> t				
	Layers (A5)			Matrix (F3)	(FZ)			oodplain Soils : Bright Loamy S		•		
	Bodies (A6) (LRR P	T 10		rk Surface	(FR)			Material (TF2)	iolis (1 <sup>2</sup> 0)	(microx i	93B)	
	cky Mineral (A7) (Li		=	Dark Surface	• •		_	w Dark Surface	(TF12)			
	esence (A8) (LRR U			pressions (	• •			in in Remarks)	()			
=	ck (A9) (LRR P, T)	•	Marl (F10)				Other (Expla	iii iii ( toinaino)				
_	Below Dark Surfac	e (A11)		302	) (MLRA 151	13						
	rk Surface (A12)	, ,			ses (F12) (Lf		T) <sup>3</sup> Indicators of	hydrophytic ve	getation an	nd wetlan	d hvdro	logy
_	airie Redox (A16) (I	MLRA 150	_		) (LRR P, T, I		•	present, unless				
_	ucky Mineral (S1) (I			ric (F17) (N		·						
oanuy M												
_	leyed Matrix (S4)		Reduced \	Vertic (F18)	(MLRA 150	A, 150B)						
Sandy G					) (MLRA 150/ Soils (F19) (N		9A)					
Sandy G	leyed Matrix (S4)		Piedmont	Floodplain	Soils (F19) (F	MLRA 149	9A) A 149A, 153C, 153I	<b>)</b> )				
Sandy G Sandy R Stripped Dark Sur	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S		Piedmont	Floodplain	Soils (F19) (F	MLRA 149		<b>)</b>				
Sandy G Sandy R Stripped Dark Sur	leyed Matrix (S4) edox (S5) Matrix (S6)		Piedmont	Floodplain	Soils (F19) (F	MLRA 149		<b>)</b>				
Sandy G Sandy R Stripped Dark Sur	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S	):	Piedmont	Floodplain	Soils (F19) (F	MLRA 149		<b>D)</b>				
Sandy G Sandy R Stripped Dark Sur Restrictive	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont	Floodplain	Soils (F19) (F	MLRA 149			Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type:	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont	Floodplain	Soils (F19) (F	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (in	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (in	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (in	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _	<u> </u>	No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (ir Remarks:	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _	⊠	No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (in	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (ir Remarks:	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (ir Remarks:	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (ir Remarks:	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (ir Remarks:	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (ir Remarks:	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (in	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (in	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (ir Remarks:	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (in	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	
Sandy G Sandy R Stripped Dark Sur Restrictive Type: Depth (ir Remarks:	leyed Matrix (S4) edox (S5) Matrix (S6) face (S7) (LRR P, S Layer (if observed)	):	Piedmont Anomalou	Floodplain s Bright Lo	Soils (F19) (Famy Soils (F2	MLRA 149	A 149A, 153C, 1538		Yes _		No _	

## WETLAND DETERMINATION DATA FORM - Atlantic and Gulf Coastal Plain Region

Project/Site: Brumsey Tract City/County: Moyock / Currituck Sampling Date: March 1, 2019
Applicant/Owner: Mary Brumsey 196 Ed Brumsey Road Currituck NC 27929 State: NC Sampling Point: WM-2U
Investigator(s): R. Bland / USACE RG-W Section, Township, Range: Snowden – Lambs Corner NE
Landform (hillslope, terrace, etc.): Flat Local relief (concave, convex, none): None Slope (%): 0-2 %
Subregion (LRR or MLRA): USDA LRR-T Lat: 36.490988 N Long: -76.156546 W Datum: NAD 83
Soil Map Unit Name: Mapped as Ws Wasda muck NWI classification: None
Are climatic / hydrologic conditions on the site typical for this time of year? Yes No (If no, explain in Remarks.)
Are Vegetation, Soil, or Hydrology significantly disturbed? Are "Normal Circumstances" present? Yes No
Are Vegetation, Soil, or Hydrology naturally problematic? (If needed, explain any answers in Remarks.)
SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.
Hydrophytic Vegetation Present?  Yes No D
Is the Sampled Area
Hydric Soil Present? Wetland Yes No Wetland? Yes No Wetland? Yes No Wetland? Yes No Wetland?
Remarks:
All 3 required parameters are NOT satisfied per the Corps of Engineers 1987 Manual and the Atlantic and Gulf Coastal Plain Regional Supplement.
HYDROLOGY
Wetland Hydrology Indicators:  Secondary Indicators (minimum of two required)
Primary Indicators (minimum of one is required; check all that apply)  Surface Soil Cracks (B6)
☐ Surface Water (A1) ☐ Aquatic Fauna (B13) ☐ Sparsely Vegetated Concave Surface (B8)
High Water Table (A2)  Marl Deposits (B15) (LRR U)  Drainage Patterns (B10)
Saturation (A3)  Hydrogen Sulfide Odor (C1)  Moss Trim Lines (B16)  Outday Affect (B1)
☐ Water Marks (B1) ☐ Oxidized Rhizospheres on Living Roots (C3) ☐ Dry-Season Water Table (C2)
Sediment Deposits (B2)       ☐ Presence of Reduced Iron (C4)       ☐ Crayfish Burrows (C8)         ☐ Drift Deposits (B3)       ☐ Recent Iron Reduction in Tilled Soils (C6)       ☐ Saturation Visible on Aerial Imagery (C9)
☐ Algal Mat or Crust (B4)       ☐ Thin Muck Surface (C7)       ☐ Geomorphic Position (D2)         ☐ Iron Deposits (B5)       ☐ Other (Explain in Remarks)       ☐ Shallow Aquitard (D3)
Inundation Visible on Aerial Imagery (B7)  Inundation Visible on Aerial Imagery (B7)  FAC-Neutral Test (D5)
Water-Stained Leaves (B9)  Sphagnum moss (D8) (LRR T, U)
Field Observations:
Surface Water Present? Yes No Depth (inches):
Water Table Present? Yes No Depth (inches): >20 inch
Saturation Present? Yes No Depth (inches): >16 Inch Wetland Hydrology Present? Yes No
(includes capillary fringe)  Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available: Aerial photography reveals field ditches on both ends
of woods area and subdivision bordering other sides.
Remarks:
Drainage ditches evident throughout the immediate geographic area and some areas on this property are subject to those effects.
ordinage and not evident anoughout the immediate geographic area and some areas on this property are subject to inose enecus.
20

## VEGETATION (Four Strata) - Use scientific names of plants.

Sampling Point: WM-2U

•	Absolute	Dominant	Indicator	Dominance Test worksheet:	
Tree Stratum (Plot size: 30 ft )	% Cover	Species?	Status	Dominance rest worksheet.	
1. Pinus taeda	60		FAC	Number of Dominant Species That Are OBL, FACW, or FAC:	_3 (A)
2. Acer rubrum	10		FAC		_3 (^/
3. Liquidambar styraciflua	10		FAC	Total Number of Dominant Species Across All Strata:	3 (B)
4. Quercus alba	5		FACU		<u> </u>
5				Percent of Dominant Species That Are OBL, FACW, or FAC:	100 (A/B
6		<u>_</u>			
7		<u> </u>		Prevalence Index worksheet:	
8					lultiply by:
50% of total cover:4		= Total Cover	,	OBL species x 1 =	
_				FACW species x 2 = FAC species x 3 =	
Sapling/Shrub Stratum (Plot size: 30 ft )	40	$\nabla$	FACW		
1. Ilex opaca 2. Cornus florida	- <del>10</del>	<del></del>	FACU	FACU speciesx 4 =	
3. Ilex coricea	$-\frac{10}{5}$	<del></del>	FAC	UPL speciesx 5 =	
Phytolaca americana	- =	<del></del>	FACU	Column Totals: (A)	(
T		<del></del>	1700	. Bravelence Index - B/A -	
5	_	<del></del>		Prevalence Index = 8/A = Hydrophytic Vegetation Indicators:	
6		<del>-  -</del>		1 - Rapid Test for Hydrophytic	√egetation
7	_	<del>-         </del>		√ <del></del>	regetation
8	60 %	= Total Cover		1 🚔	
50% of total cover:3			_	3 - Prevalence Index is ≤3.0¹	
				Problematic Hydrophytic Vegetation	n¹ (Explain)
Herb Stratum (Plot size:30 ft)  1. Arundinaria gigantea	20		FAC		
Futhamia snn	- 6	<del></del>	FAC	¹Indicators of hydric soil and wetland hyd present, unless disturbed or problematic.	
		<del>- H</del>		present, unless disturved of problematic.	
3		<del>- H</del>		Definitions of Four Vegetation Strate	 a:
4		<del></del>		-	
5		<del></del>		Tree – Woody plants, excluding vines, larger in diameter at breast height (DB	
6		<del></del>		height.	· //, / oga: a.ooo o
7		<del>- H</del>		Sapling/Shrub - Woody plants, exclu-	ding vines, less
8		<del></del>		than 3 in. DBH and greater than 3.28 f	t. (1m) tall.
9n		<del> </del>	-	Herb - All herbaceous (non-woody) pl	ants, regardless
0		<del> </del>	-	of size, and woody plants less than 3.2	
1		<del>   </del>		Woody vine - All woody vines, greate	rthan 3 28 ft in
2	25 %	= Total Cover		height.	1 (11a)1 0.20 it. 111
50% of total cover:1			_		
Woody Vine Stratum (Plot size: 30 ft )	– <u> </u>		EAC		
1. Smilax glauca	- <del>5</del>	<del></del>	FAC	-	
2. Smilax rotundifolia	5	<del></del>	FAC	-	
3				- 2	
4		—— <del> </del>		<u>u</u> .	
5	40.0/	= Total Cover		Hydrophytic	
50% of total cover:				Vegetation   YesX	No $\square$
Remarks: (If observed, list morphological adaptations b				1.10001101	
Remarks. (II observed, list morphological adaptations of	elow).				
Majority of project consists of mature timber with little e	vidence of timber	activities. Unde	rstory is		
very clean and sparse.					

Dankh		to ale de				Commi	the absence of indic	ators./		
Depth	Matrix			edox Fea				*	00	
(inches)	Color (moist)	<u>%</u>	Color (moist)	<u> </u>	Type <sup>1</sup>	Loc <sup>2</sup>	Texture		Remarks	
0-6 inch	10 YR 2/1	100					muck			
6-14 inch	5 YR 2/2	100								
14-20 Inch	10 YR 4/2	100					loam			
20-35 Inch	10 YR 5/2	90	10 YR 4/1	10	C	M			mottles	
<u> </u>										
¹Type: C=C	oncentration, D=De	pletion, RM	=Reduced Matrix,	CS=Cove	red or Coated	Sand Gra	ains. <sup>2</sup> Locati	on: PL=Pore L	ining, M=Matrix.	
Hydric Soil In	ndicators:						Indicators for Pro	blematic Hyd	ric Soils³:	
Histosol (	(A1)		Polyvalue B	Below Surf	face (S8) (LR	R S, T, U)	1 cm Muck (A9	) (LRR 0)		
Histic Ep	ipedon (A2)		Thin Dark S	Surface (S	9) (LRR S, T,	, U)	2 cm Muck (A1	0) (LRR S)		
Black His	tic (A3)		Loamy Muc	ky Minera	al (F1) (LRR C	<b>)</b>	Reduced Vertic	c (F18) (outsid	e MLRA 150A,	3)
Hydroger	n Sulfide (A4)		Loamy Gle	yed Matrix	(F2)		Piedmont Floor	dplain Soils (F	19) <b>(LRR P, S, T</b>	")
	Layers (A5)		Depleted M	atrix (F3)				- 55	ls (F20) (MLRA	153B)
	Bodies (A6) (LRR P	•	Redox Dari	Surface	(F6)		Red Parent Ma	iterial (TF2)		
	cky Mineral (A7) (LI		=				Very Shallow [		F12)	
	esence (A8) (LRR L	1)	Redox Dep		(F8)		Other (Explain i	n Remarks)		
	ck (A9) (LRR P, T)		☐ Marl (F10) (	•						
	Below Dark Surface	æ (A11)			) (MLRA 151					
	rk Surface (A12)				ses (F12) (Lf				tation and wetla listurbed or prob	
	airie Redox (A16) (I				) (LRR P, T, l	J)	must be pre	saent, umeas u	starbed or prob	icinatio.
	ucky Mineral (S1) (I	LRR O, S)	Delta Ochri							
	leyed Matrix (S4)				(MLRA 150/					
	edox (S5)		<del></del>		Soils (F19) (A		•			
	Matrix (S6) face (S7) (LRR P, \$	2 T IN	Anomalous	DIĞIR CO	arriy Solls (F2	O) (MILICA	149A, 153C, 153D)			
	Layer (if observed		*				1			
Type:										
			<del></del>				Under Call Dance	.40	Yes 🖂	Na I
Depth (in Remarks:	icnes):					_	Hydric Soil Presen	it?	YesX	No
Matches USI	DA-NCSS Soil Web	Network L	ink Profile Mapping	for Wasd	la muck.					
			2.							

## U.S. ARMY CORPS OF ENGINEERS

WILMINGTON DISTRICT

Action Id. SAW-2019-00618 County: Currituck U.S.G.S. Quad: Lambs Corner NE

#### NOTIFICATION OF JURISDICTIONAL DETERMINATION

Property Owner/Applicant: John J. Flora III

Address: Post Office Box 369

Moyock, North Carolina 27958

Telephone Number: 252-232-3005

Size (acres)
Nearest Waterway

**USGS HUC** 

108.9 acres Roland Creek

03010205

River Basin

Nearest Town Snowden

Coordinates

Albemarle-Chowan
Latitude: 36.489537 N
Longitude: -76.154810 W

Location description: Property is an approximate 108.9-acre tract located off US Highway 168 and Survey Road, adjacent to Roland Creek and the Northwest River, near Snowden, in Currituck County, North Carolina. The project area consists of an approximate 48-acre wooded area immediately adjacent to the existing agricultural fields on the property. Currituck County Parcel No. 8031-71-1141. Deed Book 1230. Page 402.

## **Indicate Which of the Following Apply:**

## A. Preliminary Determination

**SUBX** 

There are waters, including wetlands, on the above described property that may be subject to Section 404 of the Clean Water Act (CWA)(33 USC § 1344) and/or Section 10 of the Rivers and Harbors Act (RHA) (33 USC § 403). The waters, including wetlands, have been delineated, and the delineation has been verified by the Corps to be sufficiently accurate and reliable. Therefore this preliminary jurisdiction determination may be used in the permit evaluation process, including determining compensatory mitigation. For purposes of computation of impacts, compensatory mitigation requirements, and other resource protection measures, a permit decision made on the basis of a preliminary JD will treat all waters and wetlands that would be affected in any way by the permitted activity on the site as if they are jurisdictional waters of the U.S. This preliminary determination is not an appealable action under the Regulatory Program Administrative Appeal Process (Reference 33 CFR Part 331). However, you may request an approved JD, which is an appealable action, by contacting the Corps district for further instruction.

There are wetlands on the above described property, that may be subject to Section 404 of the Clean Water Act (CWA)(33 USC § 1344) and/or Section 10 of the Rivers and Harbors Act (RHA) (33 USC § 403). However, since the waters, including wetlands, have not been properly delineated, this preliminary jurisdiction determination may not be used in the permit evaluation process. Without a verified wetland delineation, this preliminary determination is merely an effective presumption of CWA/RHA jurisdiction over all of the waters, including wetlands, at the project area, which is not sufficiently accurate and reliable to support an enforceable permit decision. We recommend that you have the waters of the U.S. on your property delineated. As the Corps may not be able to accomplish this wetland delineation in a timely manner, you may wish to obtain a consultant to conduct a delineation that can be verified by the Corps.

## **B.** Approved Determination

There are Navigable Waters of the United States within the above described property subject to the permit requirements of Section 10 of the Rivers and Harbors Act (RHA) (33 USC § 403) and Section 404 of the Clean Water Act (CWA)(33 USC § 1344). Unless there is a change in law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.

There are waters of the U.S., including wetlands, on the above described property subject to the permit requirements of Section 404 of the Clean Water Act (CWA) (33 USC § 1344). Unless there is a change in the law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.

We recommend you have the waters of the U.S. on your property delineated. As the Corps may not be able to accomplish this wetland delineation in a timely manner, you may wish to obtain a consultant to conduct a delineation that can be verified by the Corps.

SAW-2019-00618 Flora Corps Regulatory Official: RAMIN. BLM, P.W. S

Date: March 8, 2019

Expiration Date: N/A for PJD

The Wilmington District is committed to providing the highest level of support to the public. To help us ensure we continue to do so, please complete our Customer Satisfaction Survey, located online at <a href="http://corpsmapu.usace.army.mil/cm\_apex/f?p=136:4:0">http://corpsmapu.usace.army.mil/cm\_apex/f?p=136:4:0</a>.

Copy Furnished: CESAW/RG-W/Bland

#### SAW-2019-00618 Flora

E: PRELIMINARY JURISDICTIONAL DETERMINATION: You do not need to respond to the Corps regarding the preliminary JD. The Preliminary JD is not appealable. If you wish, you may request an approved JD (which may be appealed), by contacting the Corps district for further instruction. Also you may provide new information for further consideration by the Corps to reevaluate the JD.

#### SECTION II - REQUEST FOR APPEAL or OBJECTIONS TO AN INITIAL PROFFERED PERMIT

REASONS FOR APPEAL OR OBJECTIONS: (Describe your reasons for appealing the decision or your objections to an initial proffered permit in clear concise statements. You may attach additional information to this form to clarify where your reasons or objections are addressed in the administrative record.)

ADDITIONAL INFORMATION: The appeal is limited to a review of the administrative record, the Corps memorandum for the record of the appeal conference or meeting, and any supplemental information that the review officer has determined is needed to clarify the administrative record. Neither the appellant nor the Corps may add new information or analyses to the record. However, you may provide additional information to clarify the location of information that is already in the administrative record.

## POINT OF CONTACT FOR QUESTIONS OR INFORMATION:

If you have questions regarding this decision and/or the appeal process you may contact:

District Engineer, Wilmington Regulatory Division,

Attn: Raleigh W. Bland, PWS

2407 West 5th Street

Washington, North Carolina 27889

910-252-4558

If you only have questions regarding the appeal process you may also contact:

Mr. Jason Steele, Administrative Appeal Review Officer

CESAD-PDO

U.S. Army Corps of Engineers, South Atlantic Division

60 Forsyth Street, Room 10M15 Atlanta, Georgia 30303-8801

Phone: (404) 562-5137

RIGHT OF ENTRY: Your signature below grants the right of entry to Corps of Engineers personnel, and any government consultants, to conduct investigations of the project site during the course of the appeal process. You will be provided a 15 day notice of any site investigation, and will have the opportunity to participate in all site investigations.

Date:

Telephone number:

Signature of appellant or agent.

For appeals on Initial Proffered Permits send this form to:

District Engineer, Wilmington Regulatory Division, Raleigh W. Bland, PWS, 2407 West 5th Street Washington, North Carolina 27889

For Permit denials, Proffered Permits and approved Jurisdictional Determinations send this form to:

Division Engineer, Commander, U.S. Army Engineer Division, South Atlantic, Attn: Mr. Jason Steele, Administrative Appeal Officer, CESAD-PDO, 60 Forsyth Street, Room 10M15, Atlanta, Georgia 30303-8801 Phone: (404) 562-5137

# WETLAND DETERMINATION DATA FORM – Atlantic and Gulf Coastal Plain Region

Project/Site: Flora Tract City/County: Moyock / Currituck Sampling Date: March 1, 2019
Applicant/Owner: John J. Flora POB 369 Moyock NC 27958 State: NC Sampling Point: WM-1
Investigator(s): R. Bland / USACE RG-W Section, Township, Range: Snowden - Lambs Corner NE
Landform (hillslope, terrace, etc.): Flat Local relief (concave, convex, none): None Slope (%): 0-2 %
Subregion (LRR or MLRA):         USDA LRR-T         Lat:         36.489537 N         Long:         -76.154810 W         Datum:         NAD 83
Soil Map Unit Name: Mapped as Ws – Wasda muck NWI classification: None
Are climatic / hydrologic conditions on the site typical for this time of year? Yes No (If no, explain in Remarks.)
Are Vegetation, Soil, or Hydrology significantly disturbed?
Are Vegetation, Soil, or Hydrology naturally problematic? (If needed, explain any answers in Remarks.)
SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.
Hydrophytic Vegetation Present? Yes No П
Is the Sampled Area
Hydric Soil Present? Wetland Yes No Within a Wetland? Yes No Hydrology Present? Yes No
Remarks:
All 3 required parameters are satisfied per the Corps of Engineers 1987 Manual and the Atlantic and Gulf Coastal Plain Regional Supplement.
HYDROLOGY
Wetland Hydrology Indicators:  Secondary Indicators (minimum of two required)
Primary Indicators (minimum of one is required; check all that apply)
☐ Surface Water (A1) ☐ Aquatic Fauna (B13) ☐ Sparsely Vegetated Concave Surface (B8)
High Water Table (A2)  Marl Deposits (B15) (LRR U)  Drainage Patterns (B10)
Saturation (A3) Hydrogen Sulfide Odor (C1) Moss Trim Lines (B16)
☐ Water Marks (B1) ☐ Oxidized Rhizospheres on Living Roots (C3) ☐ Dry-Season Water Table (C2)
☐ Sediment Deposits (B2) ☐ Presence of Reduced Iron (C4) ☐ Crayfish Burrows (C8)
Drift Deposits (B3)  Recent Iron Reduction in Tilled Soils (C6)  Saturation Visible on Aerial Imagery (C9)
Algal Mat or Crust (B4)  Thin Muck Surface (C7)  Geomorphic Position (D2)
☐ tron Deposits (B5) ☐ Other (Explain in Remarks) ☐ Shallow Aquitard (D3)
☐ Inundation Visible on Aerial Imagery (B7) ☐ FAC-Neutral Test (D5) ☐ Water-Stained Leaves (B9) ☐ Sphagnum moss (D8) (LRR T, U)
Water-Stained Leaves (B9)     Sphagnum moss (D8) (LRR T, U)  Field Observations:
Surface Water Present? Yes No Depth (inches):
Water Table Present? Yes No Depth (inches): < 14 inch
Saturation Present? Yes No Depth (inches): <8 Inch Wetland Hydrology Present? Yes No D
(includes capillary fringe)  Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available: Aerial photography reveals field ditches on both ends
of woods area and subdivision bordering other sides.
Remarks:
Drainage ditches evident throughout the immediate geographic area.
Prainting districts evident unoughout the minimate geographic area.

	Absolute	Dominant	Indicator	Dominance Test worksheet:	
ree Stratum (Plot size: 30 ft )	% Cover	Species?	Status	Dominance real worksheet.	
Pinus taeda	60	$\overline{\boxtimes}$	FAC	Number of Dominant Species	
Acer rubrum	10	<del></del>	FAC	That Are OBL, FACW, or FAC:	_3 (A)
Liquidambar styraciflua	10	<del>-      </del>	FAC	Total Number of Dominant	
Quercus alba	5	<del></del>	FACU	Species Across All Strata:	3 (B)
	<del></del>	<del></del>	FACU	Bassant of Daminant Sassian	
				Percent of Dominant Species That Are OBL, FACW, or FAC:	100 (A/B
					(* * * * * * * * * * * * * * * * * *
				Prevalence Index worksheet:	
				Total % Cover of:	ultiply by:
	85 % =	= Total Cover	-	OBL species x 1 =	
50% of total cover:42.5			7	FACW species x 2 =	
				FAC species x 3 =	
apling/Shrub Stratum (Plot size: 30 ft )	40	<b>5</b> 7	E4014		
llex opaca	40	<u> </u>	FACW	FACU species x 4 =	
Cornus florida	10		FACU	UPL species x 5 =	
llex coricea	5		FAC	· ——	
Phytolaca americana	5	<del></del>	FACU	Column Totals: (A)	(
<del></del>		<del>                                  </del>		Prevalence Index = B/A =	
		<del>-  - </del>		Hydrophytic Vegetation Indicators:	
		<u>_</u>			
				1 - Rapid Test for Hydrophytic \	/egetation
				2 - Dominance Test is >50%	
		<ul> <li>Total Cover</li> </ul>		3 - Prevalence Index is ≤3.01	
50% of total cover:30_	20% of tota	l cover:12_			
				Problematic Hydrophytic Vegetation	ın' (Explain)
erb Stratum (Plot size: 30 ft )	20		540		
Arundinaria gigantea	20	<u> </u>	FAC	<sup>1</sup> Indicators of hydric soil and wetland hyd	
Euthamia spp	5		FAC	present, unless disturbed or problematic.	
				Definitions of Four Vegetation Strata	a:
			-	Too. Mandy plants avaluating visus	2:- (7.6)
		<del></del>		Tree – Woody plants, excluding vines, larger in diameter at breast height (DB	3 in. (7.6 cm) or
<del></del>			-	height.	i), regardless of
		<del></del>		*	dia
,, <u>, , , , , , , , , , , , , , , , , , </u>				Sapling/Shrub - Woody plants, exclude than 3 in. DBH and greater than 3.28 ft	
				litari o in. Dori ana greator triali o.zo i	. (IIII) tall.
				Herb - All herbaceous (non-woody) pla	ants, regardless
				of size, and woody plants less than 3.2	28 ft tall.
W)				Woody vine - All woody vines, greate	
	25 %	Total Cover		height.	r man 3.20 it. in
50% of total cover:12.5				neight.	
00% of total 00%112.0			_		
(and Winn Stratum / Plat size: 20.6					
/oody Vine Stratum (Plot size: 30 ft ) Smilax glauca	5		FAC		
	5				
Smilax rotundifolia	<u> </u>		FAC		
				Δ.	
<del>-</del>					
	10 %	= Total Cover		Hydrophytic	
50% of total cover:5%				Vegetation Present? Yes	No
_				i idadiiti	
emarks: (If observed, list morphological adaptations belo	w).				
ajority of project consists of mature timber with little evid	ance of timbor	activities			
ajority or project consists or matere timber with little evid	nedmill to some	activities.			

SOIL Profile Desc	rintion: (Describe	to the de	oth needed to doc	umant th	e indicator o	r confirm	the absence of indicate		Point: WM-1
		to the de		edox Fea		Commi	the absence of mulcate	и в. ј	
Depth (in the c)	Matrix					1 2	<b>T</b> . 1		
(inches)	Color (moist)	<u>%</u>	Color (moist)	<u> </u>	Type <sup>1</sup>	Loc <sup>2</sup>	Texture	Remarks	
0-6 Inch	10 YR 2/1	100					muck		
6-14 inch	5 YR 2/2	100							
14-20 inch	10 YR 4/2	100					loam		
20-35 inch	10 YR 5/2	90	10 YR 4/1		C	M		mottles	
<del></del> _	2 <u></u>		<del></del>						
	oncentration, D=Dep	oletion, RM	=Reduced Matrix,	CS=Cove	red or Coated	Sand Gra		PL=Pore Lining, M=Ma	itrix.
Hydric Soil Ir							Indicators for Proble		
Histosol (	• •		= '		face (S8) (LR		= '''	•	
Black His	ipedon (A2)				9) (LRR S, T,	-	2 cm Muck (A10) (	(LRR S) 18) (outside MLRA 15	0.4 B)
	n Sulfide (A4)		Loamy Gle	-	al (F1) (LRR C ( (F2)	"	=	ain Soils (F19) (LRR P,	
	Layers (A5)		Depleted M	•	1 3 2			Loamy Soils (F20) (ML	
	Bodies (A6) (LRR P,	. T. U)	Redox Dark	• •			Red Parent Materi		
_	cky Mineral (A7) (LR		=				Very Shallow Dark		
	esence (A8) (LRR U		Redox Dep				Other (Explain in R	* *	
	ck (A9) (LRR P, T)		Marl (F10) (	LRR U)					
Depleted	Below Dark Surface	e (A11)	Depleted O	chric (F11	) (MLRA 151	)			
Thick Da	rk Surface (A12)		Iron-Manga	nese Mas	ses (F12) <b>(LF</b>	RR O, P, T		phytic vegetation and w	
Coast Pra	airie Redox (A16) (N	/ILRA 150	A) 🔲 Umbric Sur	face (F13	) (LRR P, T, U	J)	must be prese	nt, unless disturbed or p	problematic.
	ucky Mineral (S1) (L	.RR O, S)	Delta Ochri	c (F17) (N	MLRA 151)				
	leyed Matrix (S4)		L		(MLRA 150/				
	edox (S5)				Soils (F19) (N		-		
	Matrix (S6)	T 115	Anomalous	Bright Lo	amy Soils (F2	(I) (MLRA	149A, 163C, 153D)		
	face (S7) (LRR P, S _ayer (if observed):						<u> </u>		<del> </del>
Type:									
Depth (in	ichae):						Utudaja Cali Danaanta	Van F	No □
Remarks:	iciles).		<del></del> -				Hydric Soil Present?	Yes	No
	0.4. M0000 0-31147-1-	61-4	i di Barisia Manada	<b>6</b> 141					
Matches USI	DA-NCSS Soil Web	Network L	ink Profile Mapping	for Waso	la muck.				

### WETLAND DETERMINATION DATA FORM - Atlantic and Gulf Coastal Plain Region

Project/Site Flora Tract City/County: Moyock / Currituck Sampling Date: March 1, 201
Applicant/Owner: John J. Flora POB 369 Moyock NC 27958 State: NC Sampling Point: WM-2U
Investigator(s): R. Bland / USACE RG-W Section, Township, Range: Snowden - Lambs Corner NE
Landform (hillslope, terrace, etc.): Flat Local relief (concave, convex, none): None Slope (%): 0-2 %
Subregion (LRR or MLRA):         USDA LRR-T         Lat:         36.489537 N         Long:         -76.154810 W         Datum:         NAD 83
Soil Map Unit Name: Mapped as Ws – Wasda muck NWI classification: None
Are climatic / hydrologic conditions on the site typical for this time of year? Yes No (If no, explain in Remarks.)
Are Vegetation, Soil, or Hydrology significantly disturbed? Are "Normal Circumstances" present? Yes 🛛 No
Are Vegetation, Soil, or Hydrology naturally problematic? (If needed, explain any answers in Remarks.)  SUMMARY OF FINDINGS - Attach site map showing sampling point locations, transects, important features, et
Hydrophytic Vegetation Present? Yes No I
Hydric Soil Present? Wetland Yes No I Is the Sampled Area within a Wetland? Yes No X
Hydrology Present? Yes No No
Remarks: All 3 required parameters are NOT satisfied per the Corps of Engineers 1987 Manual and the Atlantic and Gulf Coastal Plain Regional Supplement.
HYDROLOGY
Wetland Hydrology Indicators:  Secondary Indicators (minimum of two required)
Primary Indicators (minimum of one is required; check all that apply)
Surface Water (A1)  Aquatic Fauna (B13)  Sparsely Vegetated Concave Surface (B8)
High Water Table (A2)  Marl Deposits (B15) (LRR U)  Drainage Patterns (B10)
Saturation (A3) Hydrogen Sulfide Odor (C1) Moss Trim Lines (B16)
Water Marks (B1) ☐ Oxidized Rhizospheres on Living Roots (C3) ☐ Dry-Season Water Table (C2)
Sediment Deposits (B2)  Presence of Reduced Iron (C4)  Crayfish Burrows (C8)
☐ Drift Deposits (B3) ☐ Recent Iron Reduction in Tilled Soils (C6) ☐ Saturation Visible on Aerial Imagery (C9)
Algal Mat or Crust (B4)  Thin Muck Surface (C7)  Geomorphic Position (D2)
☐ Iron Deposits (B5) ☐ Other (Explain in Remarks) ☐ Shallow Aquitard (D3)
Inundation Visible on Aerial Imagery (B7)
Water-Stained Leaves (B9)  Field Observations:  Sphagnum moss (D8) (LRR T, U)
Surface Water Present? Yes No Depth (inches):
Water Table Present? Yes No Depth (inches): >20 inch
Saturation Present? Yes No Depth (inches): >16 inch Wetland Hydrology Present? Yes No No
(includes capillary fringe)  Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available: Aerial photography reveals field ditches on both end
of woods area and subdivision bordering other sides.
Remarks:
Drainage ditches evident throughout the immediate geographic area and some areas on this property are subject to those effects.
Standage and the standard of the standard georgiaphic and and come areas on this property and subject to allose effects.

Sampling Point: WM-2U

1	Absolute	Dominant	Indicator	Dominance Test worksheet:
Tree Stratum (Plot size: 30 ft )	% Cover	Species?	Status	
1. Pinus taeda	60	<u> </u>	FAC	Number of Dominant Species That Are OBL, FACW, or FAC:3 (A)
2. Acer rubrum	10		FAC	
3. Liquidambar styraciflua	10		FAC	Total Number of Dominant Species Across All Strata: 3 (B)
4. Quercus alba	5		FACU	
5				Percent of Dominant Species That Are OBL, FACW, or FAC:100 (A/B)
6.				THAT ALE OBE, FACW, OF FAC
7				Prevalence Index worksheet:
8				Total % Cover of: Multiply by:
		Total Cover		OBL speciesx 1 =
50% of total cover:42.5_	_ 20% of tota	al cover:17	_	FACW species x 2 =
Sapling/Shrub Stratum (Plot size: 30 ft )				FAC species x 3 =
1. Îlex opaca	40	$\square$	FACW	FACU species x 4 =
2. Cornus florida	10		FACU	UPL species x 5 =
3. Îlex coricea	5		FAC	Column Totals: (A) (B)
4. Phytolaca americana	5		FACU	(A) (B)
5				Prevalence Index = B/A =
6				Hydrophytic Vegetation Indicators:
7				1 - Rapid Test for Hydrophytic Vegetation
8				2 - Dominance Test is >50%
	60 % =	Total Cover		☐ 3 - Prevalence Index is ≤3.01
50% of total cover:30	_20% of total	cover:12	_	<del>'</del> =
Llock Shortum (Diet sine) 20 ft				Problematic Hydrophytic Vegetation¹ (Explain)
Herb Stratum (Plot size: 30 ft )  Arundinaria gigantea	20	$\nabla$	FAC	
2. Euthamia spp	5	<del>-      </del>	FAC	Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic.
	<del>-</del>	<del></del>		present, unless disturbed of problematic.
3				Definitions of Four Vegetation Strata:
4		<del></del>		
5		<del></del>		Tree – Woody plants, excluding vines, 3 in. (7.6 cm) or
6		<del></del>		larger in diameter at breast height (DBH), regardless of height.
7		<del></del>		Sapling/Shrub – Woody plants, excluding vines, less
8		<del></del>		than 3 in. DBH and greater than 3.28 ft. (1m) tall.
9				
10				Herb – All herbaceous (non-woody) plants, regardless of size, and woody plants less than 3.28 ft tall.
11				of size, and woody plants loss than 5.25 it tall.
12				Woody vine - All woody vines, greater than 3.28 ft. in
50% of total cover:12.5_		Total Cover		height.
30 % of total cover12.3_	_ 20 /0 01 (0(8)	COVET	_	
Woody Vine Stratum (Plot size: 30 ft )				
1. Smilax glauca	5		FAC	
2. Smilax rotundifolia	5		FAC	
3.				8
4				
5				
o	10 % =	Total Cover		Hydrophytic
50% of total cover:5%_			_	Vegetation   Present?   Yes
Remarks: (If observed, list morphological adaptations below	n			
	•			
Majority of project consists of mature timber with little evider	nce of timber	activities. Under	story is	1
very clean and sparse.				

SOIL									Sampli	ng Poin	. WM-	<u>2U</u>
Profile Desc	ription: (Describe	to the de	pth needed to doc	ument th	e indicator o	r confirm	the absence of indic	ators.)				
Depth	Matrix		W R	edox Fea	tures							
(inches)	Color (moist)	%	Color (moist)	%	Type¹	Loc <sup>2</sup>	Texture		Remai	rks		
0-6 inch	10 YR 2/1	100					muck					
6-14 Inch	5 YR 2/2	100		95								_
14-20 inch	10 YR 4/2	100					loam				-	
20-35 inch	10 YR 5/2	90	10 YR 4/1	10	С	M			mottle	98	•	
					55		100					
		-										
											P. C. 23 Z.Z.	
¹Type: C=Co	oncentration, D=Dep	pletion, RM	1=Reduced Matrix,	CS=Cove	red or Coated	Sand Gra	ains. <sup>2</sup> Locati	on: PL=Pore	Lining, Ma	=Matrix.		
Hydric Soil Ir	ndicators:		_				Indicators for Pro	blematic Hy	dric Soils	3:		
Histosol (	A1)		Polyvalue E	Below Surf	face (S8) (LR	R S, T, U)	1 cm Muck (A9	) (LRR 0)				
	pedon (A2)				9) (LRR S, T,		2 cm Muck (A1	0) (LRR S)				
Black His	* *			-	il (F1) (LRR C	<b>)</b> )	Reduced Vertic	. , ,			•	
	Sulfide (A4)		Loamy Gle		(F2)		Piedmont Floor					
_	Layers (A5) Bodies (A6) (LRR P	T 10	Depleted M		(E6) = 0		Anomalous Brig		oiis (F20) (	MLRA 1	53B)	
	odies (A6) (LRR P ky Mineral (A7) (LF		Redox Dari  Depleted D		. ,		Red Parent Ma  Very Shallow D		(TE12)			
6.7	sence (A8) (LRR U		Redox Dep				Other (Explain i		(1712)			
=	ck (A9) (LRR P, T)	•	☐ Marl (F10) (	•	, 0,		Other (Explain)					
	Below Dark Surfac	e (A11)			) (MLRA 151	)						
Thick Da	rk Surface (A12)		Iron-Manga	nese Mas	ses (F12) (LF	RR O, P, T	) 3Indicators of hyd	drophytic veg	etation an	d wetlan	d hydro	logy
Coast Pra	airie Redox (A16) (II	MLRA 150.	A) 🔲 Umbric Sur	face (F13)	) (LRR P, T, U	J)	must be pre	sent, unless	disturbed	or proble	ematic.	
Sandy Mi	ucky Minerał (S1) (I	LRR O, S)	Delta Ochri	c (F17) (N	ILRA 151)							
	eyed Matrix (S4)				(MLRA 150A	•						
Sandy Re					Soils (F19) (N		•					
	Matrix (S6)	. T (I)	Anomaious	Bright Lo	amy Soils (F2	(0) (MLRA	149A, 153C, 153D)					
	face (S7) (LRR P, S ayer (if observed)						<u> </u>					
Type:												
Depth (in	ches):		·				Hydric Soll Presen	•2	Yes_	$\boxtimes$	No_	
Remarks:	<u> </u>		<del></del>				Trydric Son Fresen		103_			
	DA-NCSS Soil Web	Mohuark I	ink Drofilo Manaine	for Mond	la munde			+				
Maiches Ooi	DA-NOSS SUII VVED	INGEWORK	ink Frome wapping	i ioi vvasu	a muck.							
								53				
	·		<del> </del>									

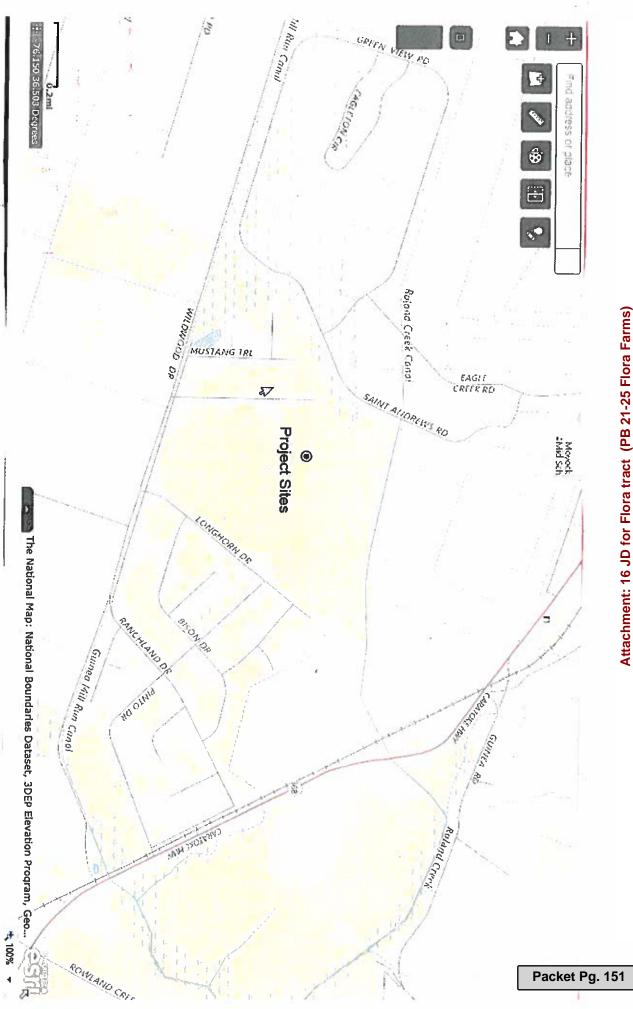
### WETLAND DETERMINATION DATA FORM – Atlantic and Gulf Coastal Plain Region

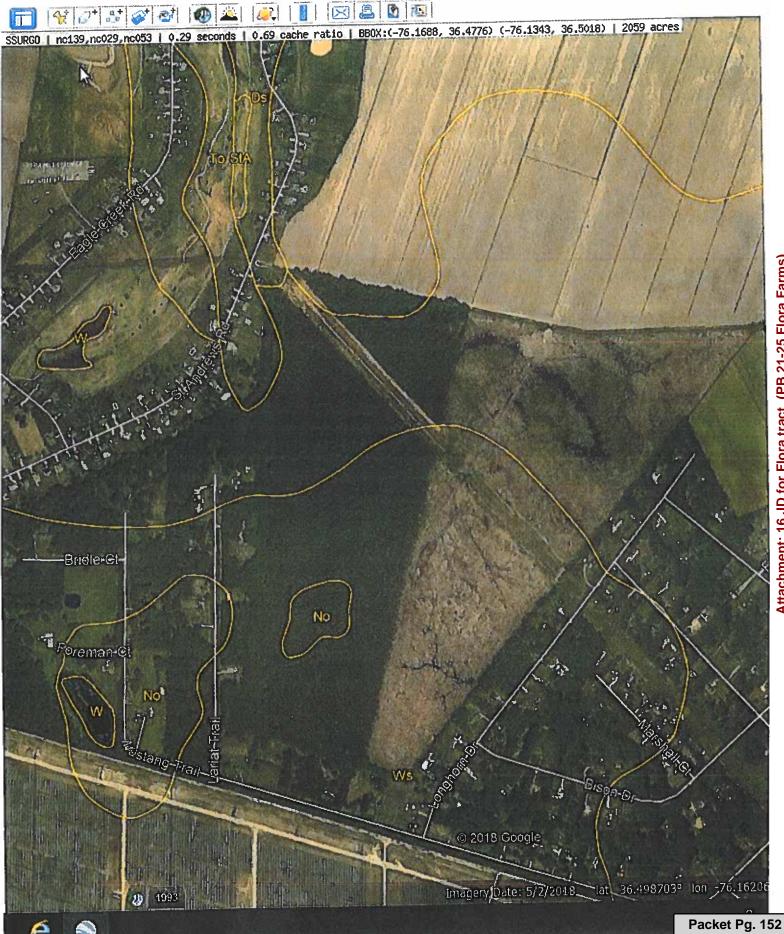
Project/Site: Flora Tract City/County: Moyock / Currituck Sampling Date: March 1, 2019
Applicant/Owner: John J. Flora POB 369 Moyock NC 27958 State: NC Sampling Point: CF-1
Investigator(s): R. Bland / USACE RG-W Section, Township, Range: Snowden - Lambs Corner NE
Landform (hillslope, terrace, etc.): Flat Local relief (concave, convex, none): None Slope (%): 0-2 %
Subregion (LRR or MLRA):         USDA LRR-T         Lat:         36.489537 N         Long:         -76.154810 W         Datum:         NAD 83
Soil Map Unit Name: Mapped as Ca - Cape Fear loam NWI classification: None
Are climatic / hydrologic conditions on the site typical for this time of year? Yes No O (If no, explain in Remarks.)
Are Vegetation, Soil, or Hydrology significantly disturbed? Are "Normal Circumstances" present? Yes 🖾 No
Are Vegetation, Soil, or Hydrology naturally problematic? (If needed, explain any answers in Remarks.)
SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.
Hydrophytic Vegetation Present?  Yes No I Is the Sampled Area within a Wetland?  Hydrology Present?  Yes No I Wetland?  Yes No Sampled Area within a Wetland?  Wetland?  No Sampled Area within a Wetland?
HYDROLOGY
Wetland Hydrology Indicators:  Secondary Indicators (minimum of two required)
Primary Indicators (minimum of one is required; check all that apply)  Surface Water (A1)  Aquatic Fauna (B13)  High Water Table (A2)  Marl Deposits (B15) (LRR U)  Saturation (A3)  Water Marks (B1)  Drainage Patterns (B10)  Moss Trim Lines (B16)  Dry-Season Water Table (C2)  Sediment Deposits (B2)  Presence of Reduced Iron (C4)  Drift Deposits (B3)  Recent Iron Reduction in Tilled Soils (C6)  Algal Mat or Crust (B4)  Iron Deposits (B5)  Other (Explain in Remarks)  Marl Deposits (B3)  FAC-Neutral Test (D5)  Sphagnum moss (D8) (LRR T, U)  Fleid Observations:  Surface Soil Cracks (B6)  Sparsely Vegetated Concave Surface (B8)  Drainage Patterns (B10)  Moss Trim Lines (B16)  Dry-Season Water Table (C2)  Crayfish Burrows (C8)  Saturation Visible on Aerial Imagery (C9)  Saturation Visible on Aerial Imagery (C9)  FAC-Neutral Test (D5)  Sphagnum moss (D8) (LRR T, U)
Water Table Present?  Yes No Depth (inches): >36 Inch  Saturation Present?  Yes No Depth (inches): >25 Inch  Wetland Hydrology Present?  Yes No Depth (inches): >25 Inch  Wetland Hydrology Present?  Yes No Secrible Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available: Aerial photography reveals field ditches on both ends
of woods area and subdivision bordering other sides.
Remarks:  Drainage ditches evident throughout the immediate geographic area and some areas on this property are subject to those effects.

Sampling Point: CF-1

	Absolute	Dominant	Indicator	Dominance Test worksheet:
Tree Stratum (Plot size: 30 ft )	% Cover	Species?	Status	Number of Descinant Consiss
1. Pinus taeda	60	<u> </u>	FAC	Number of Dominant Species That Are OBL, FACW, or FAC:3 (A)
2. Acer rubrum	10	—— <del> </del>	FAC	
3. Liquidambar styraciflua	10	<del></del>	FAC	Total Number of Dominant Species Across All Strata: 3 (B)
4. Quercus alba	5		FACU	
5				Percent of Dominant Species That Are OBL, FACW, or FAC:100 (A/B)
6				
7		4 7		Prevalence Index worksheet:
8				Total % Cover of: Multiply by:
50% of total cover:42.5_		Total Cover		OBL species x 1 =
30 % 01 total cover42.3_	_ 20 % 01 (0)	ai covei17		FACW species x 2 =
Sapling/Shrub Stratum (Plot size: 30 ft )	40			FAC species x 3 =
1. Ilex opaca	40	<u> </u>	FACW	FACU species x 4 =
2. Cornus florida	10	<del></del>	FACU	UPL species x 5 =
3. Îlex coricea	5		FAC	Column Totals: (A) (B)
4. Phytolaca americana	5		FACU	
5				Prevalence Index = B/A =
6				Hydrophytic Vegetation Indicators:
7	<del></del>			1 - Rapid Test for Hydrophytic Vegetation
8				2 - Dominance Test is >50%
50% of total cover:30		Total Cover	144	3 - Prevalence Index is ≤3.01
50 % Of Iolai Cover30	_ 20% 01 (0(a)	COVE112	_	Problematic Hydrophytic Vegetation¹ (Explain)
Herb Stratum (Plot size: 30 ft )		_		
1. Arundinaria gigantea	20	<u> </u>	FAC	Indicators of hydric soil and wetland hydrology must be
2. Euthamia spp	5		FAC	present, unless disturbed or problematic.
3				
4				Definitions of Four Vegetation Strata:
5				Tree - Woody plants, excluding vines, 3 in. (7.6 cm) or
6				larger in diameter at breast height (DBH), regardless of
7				height.
8				Sapling/Shrub – Woody plants, excluding vines, less than 3 in. DBH and greater than 3.28 ft. (1m) tall.
9				
10				Herb - All herbaceous (non-woody) plants, regardless
11				of size, and woody plants less than 3.28 ft tall.
12				Woody vine - All woody vines, greater than 3.28 ft. in
		Total Cover		height.
50% of total cover:12.5_	_ 20% or total	cover:5	-	
Woody Vine Stratum (Plot size: 30 ft )				
1 Smilax glauca	5		FAC	
2 Smilax rotundifolia	5	<del>-  </del>	FAC	
3		<del>- = = = = = = = = = = = = = = = = = = =</del>		
		<del>- H</del>		
5	512			
	10 % =	Total Cover		Hydrophytic Vegetation
50% of total cover:5%_			_	Present? Yes No No
Remarks: (If observed, list morphological adaptations below	<i>t</i> ).			
Majority of project consists of mature timber with little avide	naa af timbaa			
Majority of project consists of mature timber with little evided very clean and sparse.	nce of amber	activities. Undei	SIOTY IS	
				*

Depth	Matrix		-0.0	Redox Feat			the absence of indicato	•	
-			-			12	<b>T.</b> A	D	
nches)	Color (moist)	<u>%</u>	Color (moist)		Type <sup>1</sup>	Loc <sup>2</sup>	Texture	Remarks	
)-15 Inch	10 YR 2/1	100					loam		
15-22	10 YR 4/1	90	10 YR 3/2	10		M	Clay loam		
22-36+	10 YR 5/1	90	10 YR 3/2	10	c	M	clay		
Type: C=Co	ncentration, D=Dep	oletion, RM	I=Reduced Matrix,	CS=Cove	red or Coated	Sand Gra	ains. <sup>2</sup> Location:	PL=Pore Lining, M=Matrix	
ydric Soil In	dicators:		_				Indicators for Problem	matic Hydric Solls <sup>3</sup> :	
Histosol (	A1)		Polyvalue	Below Surf	face (S8) (LR	R S, T, U)	1 cm Muck (A9) (Ll	RR O)	
_	pedon (A2)		Thin Dark	Surface (S	9) (LRR S, T,	U)	2 cm Muck (A10) (I	LRR S)	
∐ Black His			= '	•	il (F1) (LRR C	))		8) (outside MLRA 150A,	
= ' '	Sulfide (A4)		Loamy Gle		3.5			in Soils (F19) (LRR P, S,	•
=	Layers (A5)	**	Depleted N	, ,				Loamy Soils (F20) (MLRA	153B)
<b>-</b>	Bodies (A6) (LRR P,		Redox Dar				Red Parent Materia	• •	
_	ky Mineral (A7) (LF						Very Shallow Dark		
_	sence (A8) (LRR U	,	Redox Dep		F0)		☐ Other (Explain in Re	emarks)	
_	k (A9) (LRR P, T) Below Dark Surface	a (Δ11)	Mari (F10) (		) (MLRA 151	١			
<u> </u>		6 (411)		•		,	) 3Indicators of hydron	hytic venetation and wetla	and bydrolog
Thick Dark Surface (A12)  Iron-Manganese Masses (F12) (LRR O, P, T)  Coast Prairie Redox (A16) (MLRA 150A)  Umbric Surface (F13) (LRR P, T, U)  Thick Dark Surface (A12)  must be present, unless disturbed or problematic.									
_	ucky Mineral (S1) (L					•			
			-		•				
_	eyed Matrix (S4)		Reduced V	ertic (F18)	(MLRA 150/	A, 150B)			
_					) (MLRA 150/ Soils (F19) (N		A)		
Sandy GI Sandy Re			Piedmont I	Floodplain	Soils (F19) (A	NLRA 149	A) 149A, 153C, 153D)		
Sandy GI Sandy Re Stripped Dark Surl	edox (S5) Matrix (S6) face (S7) (LRR P, S		Piedmont I	Floodplain	Soils (F19) (A	NLRA 149			
Sandy GI Sandy Re Stripped Dark Surl	edox (S5) Matrix (S6)		Piedmont I	Floodplain	Soils (F19) (A	NLRA 149			
Sandy GI Sandy Re Stripped Dark Surl	edox (S5) Matrix (S6) face (S7) (LRR P, S		Piedmont I	Floodplain	Soils (F19) (A	NLRA 149			
Sandy Gl Sandy Re Stripped Dark Surf Restrictive L Type: Depth (in	edox (S5) Matrix (S6) Face (S7) (LRR P, S ayer (If observed)		Piedmont I	Floodplain	Soils (F19) (A	NLRA 149		Yes⊠	No
Sandy Gl Sandy Re Stripped Dark Surf Restrictive L Type: Depth (in	edox (S5) Matrix (S6) Face (S7) (LRR P, S ayer (If observed)		Piedmont I	Floodplain	Soils (F19) (A	NLRA 149	149A, 153C, 153D)	Yes⊠	No
Sandy Gi Sandy Re Stripped i Dark Surfestrictive L Type: Depth (in	edox (S5) Matrix (S6) Face (S7) (LRR P, S ayer (If observed)	:	Piedmont I	Floodplain s Bright Lo	Soils (F19) (A amy Soils (F2	NLRA 149	149A, 153C, 153D)	Yes⊠	No
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#### ADEQUATE PUBLIC FACILITIES ORDINANCE HISTORY

- August 1994 Anne Mackin, then Moyock Elementary School principal, sent a memo (see attached memo dated August 29, 1994) to Ronnie Capps, Currituck County School Superintendent, stating that with increased enrollment "Facilities, programs, and equipment need to be added and supported yearly to provide sufficient services for our student population. She noted the addition of mobile classrooms and the student crowding in restrooms, cafeteria, and library. She also noted that 'all support services (counseling, special education, chapter 1) needed special attention to accommodate student numbers.' Principal Mackin also itemized the additional staff, copy machines, instructional supplies, consumable materials, furniture textbooks, library materials, computers, and additional bus routes necessary to service the 713 students enrolled at Moyock Elementary. She said 'Overall the increase in enrollment has required Currituck County School System to redesign facilities and programs.'
- September 1994 Ronnie Capps, Currituck County School Superintendent, approached the Planning Board (see attached memo dated September 13, 1994) regarding the "residential development having dramatic impact on each of our schools during recent years." Superintendent Capps detailed the increase in attendance in his memo. He also noted the 38 mobile classroom units installed at schools to accommodate growth of the student population. The memo encourages the Planning Board and the Board of Commissioners to review options relative to the rate of residential development so all county agencies, including the schools, might respond to growth in a timely and positive manner. Superintendent Capps encourage the boards to be proactive instead of reactive to accommodate growth in the student population. Superintendent Capps noted the proposed subdivision "The Plantations" (now Eagle Creek) with 601 residential lots in the Moyock Township and its effect on the school district.
- September 13, 1994 Jack Simoneau Planning and Inspections Director, presented PB 94-42 Currituck County text amendment (attached) to the Planning Board to address adequate public facilities. The language read:
  - 2. Even if the permit issuing board finds that the application complies with all other provisions of this Ordinance, it may still deny the permit if it concludes, based upon the information submitted at the hearing, that if completed as proposed, the development, more probably than not:
    - e. Will exceed the county's ability to provide adequate public facilities, including, but not limited to, schools, fire and rescue, law enforcement, and other county facilities.

The Planning Board minutes for September 13, 2994 are attached for reference. The Planning Board unanimously approved the text amendment.

- October 3, 1994 BOC held a public hearing on the proposed text amendment. (Minutes Attached)
- October 17, 1994 BOC unanimously approved the text amendment. (Minutes attached)
- February 6, 1995 BOC unanimously denied a special use permit for Tate Terrace Realty Investors Inc. request for a 601 lot subdivision (now Eagle Creek) based solely upon the provisions of the above noted text amendment to UDO Section 1402(2)(e). It is important to note that a prior property owner (Moyock Investment Group) received vesting approval to develop at 429 lots on this property.



# CURRITUCK COUNTY PLANNING BOARD September 13, 1994 7:30 p.m.

The Currituck County Planning Board met at the Currituck County Courthouse on September 13, 1994. The following members were present: Walter Jones, Jimmy Waterfield, Aubrey Dowdy, Owen Etheridge, Marguerite Hogge, Shirley Falls, Nolan Alcock, La Ree Lynn, and Walter Banks. Jack Simoneau, Planning and Inspections Director, and H.B. Briggs, Chief Planner were also present.

Mr. Etheridge called the meeting to order and announced a quorum had been met.

Ms. Falls asked if the board wanted her to vote on the Duck Ridge item since her daughter worked for Hearndon Construction. The board agreed that Ms. Falls could vote on this item since she said that her daughter's employment would not bias her opinion.

Mr. Etheridge welcomed Mr. Jimmy Waterfield to the board.

### APPROVAL OF AUGUST 9, 1994, MINUTES

Mr. Lynn motioned to approve the August 9, 1994 minutes as presented. Mr. Jones seconded the motion and the motion passed unanimously.

#### RESOLUTION OF APPRECIATION FOR MR. LUTHER CROCKETT

The board presented the following resolution of appreciation to Mr. Crockett:

### RESOLUTION OF APPRECIATION

WHEREAS, Mr. Luther Crockett has faithfully served as a member of Currituck County's Planning Board from January, 1994 until July, 1994; and,

WHEREAS, Mr. Crockett has devoted numerous hours and energies to serve in this capacity; and,

WHEREAS, Mr. Crockett has served as an integral part in shaping the future of Currituck County; and,

WHEREAS, Mr. Crockett always acted in the best interests of the Citizens of Currituck County as a member of the Planning Board; and,

- Currituck County Board of Commissioners, shall be from \_\_\_\_a. m. to \_\_\_\_p. m.; and,
- 15. Should any conditions within this permit conflict with any other permit issued for this activity, then the more restrictive condition shall prevail.

Ms. Hogge seconded with Mr. Waterfield, Mr. Lynn, Mr. Alcock, and Mr. Jones voting no. (Mr. Etheridge asked for a show of hands to determine those voting in opposition)

DISCUSSION ON IMPACT OF GROWTH INCLUDING ITS IMPACT ON THE SCHOOL SYSTEM BY BOARD OF EDUCATION REPRESENTATIVE

Mr. Ronnie Capps, Ms. Janet Taylor, and Mr. Skip Saunders appeared before the board.

Mr. Capps presented memo to the board with different charts and graphs to show the impact that increased development is having on the school system (see Attachment 1)

Mr. Capps expressed a concern that the county needs to plan better for development. He said that within 3 to 5 years, all of Currituck's public facilities will be overburdened.

Mr. Alcock asked what we had to do now to have the developers start paying for some of the impact they have on the county facilities.

Mr. Simoneau said that you must have a capital improvements plan, establish a level of service standard, compare estimates with proposed development, and to come up with a formula to require developers to pay their fair share of facility cost. He said that this is in the works now, but it will take about 2 - 3 years to get a system in place.

Ms. Taylor explained a problem teachers have in the mobile units. She said that a teacher cannot leave the trailer to take a student to the office for anything (including nurse) since there is not another teacher in the trailer to watch the students. She said this is not fair to the teacher nor the student and that the county should speed up the process in order to get more schools.

Mr. Saunders said that the expense of bringing new trailers was astronomical since special attention has to be given to the electrical requirements, water pressure, meeting handicap regulations, etc. He also said that each school is running out of room for trailers.

Ms. Taylor explained that there is no place for the students in the trailers to hold a fire drill since they have to go

inside and "crouch" against a wall and all of the walls were filled with the students who are in the school already.

Mr. Banks asked about moving the boundary line that said which school a child went to and move more from Moyock to Central, a less crowded school.

Mr. Capps said that this would only move trailers from one school to another and would not solve the problem.

Mr. Simoneau presented the following amendment as requested by the Planning Board at the August meeting as one mean to slow growth as well as a means for determining impact of development:

Currituck County requests the following amendment to the Currituck County Unified Development Ordinance:

PB 94-42 CURRITUCK COUNTY request amendments to Article 14 Section 1402, Articles 9, 10, and 11 Section IV, to provide adequate public facility regulations

BE IT ORDAINED by the Board of Commissioners of the County of Currituck, North Carolina that the Unified Development Ordinance of the County of Currituck be amended as follows:

Part I. That the following section be amended:

Article 14 Section 1402 (2)

- (d) will not be in general conformity with the Land Use Plan, Thoroughfare Plan, or other plan officially adopted by the board: or.
- (e) will exceed the county's ability to provide adequate public facilities, including, but not limited to, schools, fire and rescue, law enforcement, and other county facilities.

Articles 9, 10, and 11 Sections 903, 1003, and 1103

	SKETCH PLAT	PRELIMINARY PLAT	FINAL PLAT
IV. DEVELOPMENT IMPACT STATEME	NT		
1. Physical Analysis (type units expected incl. number of bedrooms; projected vasize and timing of phases, etc.)	lue; X		
2. Housing Market Analysis (delimit market area; project demand, supply and unmet demand; determine net			

capture; identify development profile)	X			
3. Environmental Impact (water consumption estimated per unit type, hydrological report by a licensed engineer identifying available water resources, report outlining sewer generation and means of disposal)	x	e (2)	C= 32	<i>P</i> .
4. Fiscal Analysis (estimated real property valuation, estimated personal property valuation, estimated annual land transfer tax value)	x	-	1 3	
<ol> <li>Traffic Analysis (estimated number of trips generated, volume of existing traffic on roads adjacent to and within 1/2 mile of tract, directional distribution of traffic, capacity analysis)</li> <li>Part II. This ordinance amendment shall and after the day of, 1994.</li> </ol>	X	in eff	ect fr	om
B.U. Evans	. Cha	irman	_	
Attest:	s, cha	- Liwii		
Gwen Tatem, Clerk to the Board		36		
DATE ADOPTED: MOTION TO ADOPT BY COMMISSIONER: SECONDED BY COMMISSIONER:				
VOTE: _ AYES _ NAYES				
Mr. Etheridge asked if the school development will overburden their system the development could be denied?				

Mr. Simoneau that if the school system said that a proposed development would overburden their facilities, then according to the above amendment the Board of Commissioners could deny the request. It was not know if the courts would uphold such a decision.

#### PLANNING BOARD ACTION

Mr. Alcock motioned to approve the above amendment request. Mr. Banks seconded the motion and the motion passed unanimously.

COUNTY: REQUEST TO PB AMENDMENTS 94-40 CURRITUCK SECTION 914, ARTICLE 10 SECTION AND ARTICLE SURETY SECTION 1113 TO DELETE 11 OF THE TDO ADD BONDS AND DEED OF TRUST AND BOND AS ACCEPTABLE BOND TERMS AND METHODS.

Mr. Briggs presented the following memo to the board:

#### MEMORANDUM

TO: Board of Commissioners

FROM: H. B. Briggs, Jr., Chief Planner

DATE: September 2, 1994

SUBJECT: Proposed Amendment to the UDO

Staff has been directed to prepare the attached amendment for presentation to the Planning Board. The purpose of this amendment is two-fold: (1) to add Deeds of Trust and Bonds as acceptable bonding forms and methods; and, (2) to delete the utilization of Surety Bonds by a licensed surety bond company.

#### HISTORY of DEEDS of TRUST

Prior to and included within adoption of the Unified Development Ordinance on April 2, 1989, Deeds of Trust were an acceptable form of guarantee for Maintenance or Performance Bonds. However, when the UDO was updated in November, 1992, the county attorney at the time requested that Deeds of Trust be deleted as an acceptable form of guarantee.

#### ADVANTAGES of DEEDS of TRUST

The main advantage in allowing the use of Deeds of Trust for guarantee purposes is that it allows the developer to "put-up" a portion of the development to insure items will be installed as required (Performance Bond) or that maintenance will be performed as required (Maintenance Bond).

#### DISADVANTAGES of DEEDS of TRUST

(1) although appraisals will be required, market cycles could cause an inflated value on the property, ie. appraisal done during high property values, at selling point property values low;





# Public Schools Post Office Box 40 Currituck, North Carolina 27929

Office of Superintendent

919-232-2223

#### MEMO

TO:

Planning Board Members

Currituck County

FROM:

Ronnie Capps VCC

Superintendent of Schools

DATE:

September 13, 1994

SUBJECT:

Impact of Residential Development on the Public Schools

Residential development has had a dramatic impact on each of our schools during recent years. I will briefly summarize the consequences of this growth.

SCHOOL	9th Month 93/94 ADM	Enrollment 9/1/94	Increase
Moyock	686	715	29
Griggs	444	472	28
Knotts Island	149	168	19
Knapp	419	437	18
Central	338	360	22
CCHS Extended Day	692 40 732	778 32 810	78
	2768	2962	+194

MEMO Page 2 September 13, 1994

As you can see, since school ended in June, our enrollment has increased by 194 students. The four elementary schools and junior high school have each gained the equivalent of an additional class. The high school has gained the equivalent of three additional classes.

Based upon the initial enrollment this school year, our student population has increased by  $\underline{476}$  pupils over the last four years.

Currently, the school district is using 38 mobile classroom units to accommodate growth of the student population. These units are assigned to the schools as follows:

<u>School</u>	Number of Mobile Units
Moyock	8
Central	4
Knapp	8
Currituck High	<u>18</u> 38

Four of these units were sited this summer. Two double-wide units were placed at Moyock Elementary School and two single-wide units were located at the high school.

Moyock Elementary School is virtually at capacity in terms of parking space and locations to site additional mobile classrooms.

Low-flow rest room fixtures are being installed at Central School and Moyock School to maintain design standards for wastewater systems.

Based upon figures supplied by the State Treasurer in April, 1994, the cost of growth in the student population can be illustrated dramatically. The per student county expenditure for capital and operating costs during the 1992-93 school year was \$2,193.67. The costs per student have, of course, increased since the 1992-93 fiscal year, but by extending this cost data to the 194 students enrolled during the summer of 1994 we incur an additional expense of \$425,571.98 given the level of local support provided.

Outside of adding additional mobile classrooms, the overcrowding of our schools will not be relieved until the beginning of the 1996-97 school year when the new high school is completed.

MEMO Page 3 September 13, 1994

Maintaining quality instructional programs and student safety are our primary concerns. Our schools have received high marks in both areas, but our rapid growth makes the job more difficult.

We encourage the Planning Board and the Board of Commissioners to review options relative to the rate of residential development so all county agencies, including the schools, might respond to growth in a timely and positive manner. In recent years, the schools have had to be reactive rather than pro-active to accommodate growth in the student population. By way of example, I received in my office last week the sketch plan for The Plantations, a proposed subdivision of 601 residential lots to be located in Moyock Township. You can imagine the impact this single subdivision will have on the school district. Though the long-range school facility plan prepared by the Board of Education proposed construction of a second school in Moyock Township, funds for this project were not included on the bond referendum and are not currently anticipated from other revenue sources. Assuming this subdivision receives eventual approval, we will find our schools and community in the reactive posture to which we have become accustomed.

WRC/dbs

Attachments:

cc: Board of Education
Bill Richardson

		6.A.r	•
	~	SCHOOL CAPITAL FACILITY NEEDS PER 1993 SURVEY 23,480,	
	<b>&gt;</b>	COUNTY SCIK NITY CAPITA FACIL SH- PERSONAL NEEDS ENT INCOME 199 IE (1991) SUR 3.3 14,658 23,480,	
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0.00	>	1992-93 COUNTY TAX RATE 0.5600	
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28.74	►	LAST YEAR OF REVALUATION FOR COUNTY 1989	3 21-25 Flor
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		Packet Pg. 16	3

									<b>&gt;</b>	COUNTY
									×	1992
	¥	TOTAL COUNTY REVENUES FROM ALL SOURCES FOR ALL PURPOSES	20,906,659	16,038,561	15,266,6 <b>84</b> 100.00	15,857,067 100.00	11,522,248	4,386,504	3	
	•	CURRENT SCHOOL TAX COLLECTED (SEPARATELY I	0.00	0.00	0.00	0.00	0.00	0.00	>	
	na.	FINES AND FORFEITURES	125,372	122,476 0.76	117,231	127,329 0.80	0.00	1.02	5	
	=	COUNTY APPROPRIATIONS TO SCHOOL LOCAL CURRENT EXPENSE FUND	3,024,649	3,599,324	3,179,498	2,764,257	2,208,700	1,260,765	-	
	œ	TOTAL COUNTY COUNTY FUNDING SUBJECT APPROPRIATION TO MAINTENANCE OF TO SCHOOL EFFORT REQUIRENENT LOCAL CHRRENT OF ARTICLE 42 EXPENSE FUND	2,794,193	1,339,287	1,721,502	2,553,894	519,447	112,945	v	1991-92 COUNTY
CURRITUCK 14,566	u.	CHANGE IN P SCHOOL TO CAPITAL EF RESERVE	(640,536)	(311,595)	520,291 3.41	699,452	49,368	(9,919)	æ	
COUNTY - POPULATION -	w	CURRENT YEAR SCHOOL CAPITAL OUTLAY EXPENDITURE	3,434,729	1,650,882	1,201,211	1,854,442	470,079	122,864	σ	1992-93 SCHOOL CAPITAL
	۵	ANNUAL DEBT SERVICE PAYMENTS ON SCHOOL DEBT ISSUED IN 5 8 YEARS PRIOR TO ARTICLE 42	0.00	0.00	0.00	0.00	0.00	0.00	۵	1992-93 PER CAPITA S
6	u	SE ANUAL SCHOOL DEBT SERVICE PAYMENTS	213,751	199,148	199,072	198,979 25.1	199,056 57.1	199,490	0	1997 PER (
	•	RESTRICTED PORTION OF ARTICLE 42 - 1/2 CENT SALES TAX (60% OF RECEIPTS)	278,973 1.33	269,856	274,886	265,268	249,063	0.00	=	1992-93 PER STUDENT
	<	RESTRICTED PORTION OF ARTICLE 42 - 1/2 ARTICLE 40 - 1/2 CENT SALES TAX CENT SALES TAX	139,489	134,977 0.84	137,320	133,037 0.84	124,977	57,778 1.32	×	198 2 834
ii E		YEAR	1993	2661 2661	1991	1990 1990*	1989	5 Year Base	٠.	

### Moyock Elementary School

Post Office Box 160
Moyock, North Carolina 27958
"Success . . . Children First"

9-435-6521

To: Ronnie Capps

From: Anne Mackin af

RE: Enrollment

Date: August 29, 1994

Moyock Elementary has been experiencing increased enrollment over the past three years. Presently, Moyock has an enrollment of 713 students. Facilities, programs, and equipment need to be added and supported yearly to provide sufficient services for our student population.

Mobile units have been added to our campus in order to provide adequate instructional and storage space. Currituck County is still trying to provide appropriate student/teacher ratio which requires more classroom units.

Suitable schedule changes needed to be designed to reduce student crowding in restrooms, cafeteria, and library. All support services (counseling, special education, chapter 1) locations needed special attention to accommodate student numbers.

Moyock Elementary had to increase the professional staff to accommodate student growth . A counselor, Chapter 1 instructor, Special Education instructor, classroom instructors, and office personnel had to be supplemented to provide significant services. Additional materials and equipment needed to be purchased to cope with normal daily demands. The items purchased are as follows: copy machines, instructional supplies, consumable materials, furniture, textbooks, library materials, and computers.

Several changes have been made in bus routes to accommodate new building developments. Additional buses have been added to provide sufficient and timely transportation for all students.

Overall the increase in enrollment has required Currituck County School System to redesign facilities and programs.

Currituck County North Carolina October 17, 1994

The Currituck County Board of Commissioners met in a work session prior to the regular scheduled meeting to discuss the Cooperative Extension Legislative Grants.

The Currituck County Board of Commissioners met at 8:00 PM in regular session on October 17, 1994, in the Currituck County Courthouse with the following members present: Vice-Chairman Wright, Commissioners Gregory, Bowden, and Askew. Chairman Evans was absent.

Vice Chairman Wright called the meeting to order at 8:00 PM.

#### Invocation

Vice Chairman Wright called for a moment of silence.

#### Approval of Agenda

Vice Chairman Wright moved to amend the agenda as follows: move Item 14 to 5B, move Item 19 to 5C and add Consideration of library bids for furnishings. Commissioner Gregory seconded the motion. Motion carried unanimously.

# Approval of September 6 and September 19, 1994 Minutes.

Commissioner Askew moved to approve the minutes of September 6 and September 19, 1994. Commissioner Gregory seconded the motion. Motion carried unanimously.

#### Public Comments

Vice Chairman Wright opened the public comment period.

Jan Smith, with the Humane Society, read a letter that all Board members had received by registered mail. The letter listed various questions and concerns regarding the animal shelter.

There being no further comments, Vice Chairman Wright closed the public comment period.

Public Hearing and Action on PB 94-44 COROLLA FIRE AND RESCUE: special use permit for an addition to the fire and

rescue building located at 827 Whalehead
Drive (intersection of Whalehead Drive and
Dolphin Street) in Whalehead Subdivision in
Corolla, Tax Map 115A, Section 13, Parcel 35A,
Poplar Branch Township.

Jack Simoneau, Planning and Inspections Director, and Marshall Cherry were sworn in prior to giving testimony.

Jack Simoneau reviewed the following case analysis: The applicant is requesting a Special Use Permit (classification #13.200) for two additions to the existing fire station building. Proposed additions will add 2050 s.f. to the existing building. The additions will be used to house additional fire equipment. Property is shown on Tax Map 115A, lot 35A, Poplar Branch Township. Property is located on the northeast corner of Whalehead Drive and Dolphin Street in Whalehead Subdivision near Corolla

#### STAFF FINDINGS

Criteria: (a) Does the applicant meet the standards established in the Unified Development Ordinance (UDO) for the proposed use?

Findings: The applicant proposes an addition to the existing fire station building (Use Classification #13.200) which is a permitted use in a RO1 (Outer Banks Standard Residential) zoned area with a Special Use Permit.

Adjacent Property Owner's Notifications were mailed 9/30/94; property was posted on 10/4/94.

The proposed addition meets setback requirements.

All required parking, vehicle accomodation areas, screening requirements, shading, etc. will be met by applicant.

Criteria: (b) Will the proposed use materially endanger the
 public health or safety?

Findings: The nearest residential dwellings to the site are located across Whalehead Drive and across Dolphin Street.

Limited ingress/egress to site insures safe

traffic movements.

Criteria: (c) Will the proposed use substantially injure

the value of adjoining or abutting property?

Findings: The property is surrounded by the following land

uses:

North: Vacant lot - zoned RO1 South: Residential - zoned RO1 East: Vacant lot - zoned RO1 West: Residential - zoned RO1

Criteria: (d) Will the proposed use be in harmony with the

particular neighborhood or area in which it is to

be located?

Findings: See the findings listed under Criteria "a", "b"

and "c" above.

Criteria: (e) Will the proposed use be in general

conformity with the Land Use Plan, Thoroughfare Plan or other plans officially adopted by the

board?

Findings: The 1990 Land Use Plan classifies this land as

"Limited Transition". The purpose of the limited transition classification is to provide for Development in areas that will have some services, but are suitable for lower densities than those associated with the urban transition class, and/or are geographically remote from existing towns and

municipalities.

The site is located along SR 1420 (Whalehead Drive) which is a Local road according to the 1988

Thoroughfare Plan.

#### STAFF RECOMMENDATION

Staff recommends approval of this request as presented.

#### PLANNING BOARD RECOMMENDATION

On October 11, 1994, the Planning Board voted to recommend approval of this request as presented.

Vice Chairman Wright opened the public hearing.

Marshall Cherry, Corolla Fire Chief, stated that they were adding 3 additional bays to house their equipment.

There being no further comments, Vice Chairman Wright closed the public hearing.

Commissioner Bowden moved to grant the Special Use Permit. Commissioner Gregory seconded the motion. Motion carried unanimously.

Public Hearing and Action on PB 94-45 LOWER CURRITUCK VOLUNTEER FIRE DEPARTMENT,

INC.: special use permit for an addition to the fire department building located in Grandy on the east side of US 158 approximately south of the intersection of SR 1129 550 feet 94, (McHorney Road), Tax Map Parcel Poplar Branch Township.

Jack Simoneau, Planning and Inspections Director and Tommy Brothers were sworn in prior to giving testimony.

Jack Simoneau, reviewed the following case analysis: The applicant is requesting a Special Use Permit (classification #13.200) for an addition to the existing fire station building. The proposed addition will add 2273 s.f. to the existing building. The addition will be used to house additional fire equipment. Property is shown on Tax Map 94, lot 136, Poplar Branch Township. Property is located on the east side of US 158 across from E & S General Merchandise in Grandy.

#### STAFF FINDINGS

Criteria: (a) Does the applicant meet the standards established in the Unified Development Ordinance (UDO) for the proposed use?

Findings: The applicant proposes an addition to the existing fire station building (Use Classification #13.200) which is a permitted use in both a A (Agricultural) and GB (General Business) zoned area with a Special Use Permit.

Adjacent Property Owner's Notifications were mailed 9/30/94; property was posted on 10/4/94.

The proposed addition meets setback requirements.

All required parking, vehicle accomodation areas, screening requirements, shading, etc. will be met by applicant.

Criteria: (b) Will the proposed use materially endanger the
 public health or safety?

Findings: According to county tax maps, the nearest residential dwelling to the site is located directly to the northwest approximately 140'+/-. Limited ingress/egress to site insures safe traffic movements.

Findings: The property is surrounded by the following land uses:

North: Woodlands/Residential - zoned GB & A South: Vacant lot/Residential - zoned GB & A

East: Residential - zoned A

West: Commercial (E & S Gen. Mdse.) - zoned GB

Criteria: (d) Will the proposed use be in harmony with the particular neighborhood or area in which it is to be located?

Findings: See the findings listed under Criteria "a", "b" and "c" above.

Criteria: (e) Will the proposed use be in general conformity with the Land Use Plan, Thoroughfare Plan or other plans officially adopted by the board?

Findings: The 1990 Land Use Plan classifies this land as "Limited Transition". The purpose of the limited transition classification is to provide for Development in areas that will have some services, but are suitable for lower densities than those associated with the urban transition class, and/or are geographically remote from existing towns and municipalities.

The site is located along US 158 which is a Minor Arterial road according to the 1988 Thoroughfare Plan.

#### STAFF RECOMMENDATION

Staff recommends approval of this request as presented.

#### PLANNING BOARD RECOMMENDATION

On October 11, 1994, the Planning Board voted to recommend approval of this request as presented!

Vice Chairman Wright opened the public hearing.

Tommy Brothers, Lower Currituck Fire Department, was present to answer questions.

There being no further comments, Vice Chairman Wright closed the public hearing.

Commissioner Bowden moved to grant special use permit. Commissioner Gregory seconded the motion. Motion carried unanimously.

Action on: PB 94-42 CURRITUCK COUNTY: request amendments to Article 14 Section 1402, Articles 9, 10 and 11 Section IV, to provide adequate public facility regulations.

Commissioner Gregory moved to adopt the following ordinance and change the number of lots where impact statements are required, from 20 to 50 lots and the final number be determined when impact study is complete. Commissioner Bowden seconded the motion. Motion carried unanimously. Chairman Evans was absent.

BE IT ORDAINED by the Board of Commissioners of the County of Currituck, North Carolina that the Unified Development Ordinance of the County of Currituck be amended as follows:

Part I. That the following section be amended:

Article 14 Section 1402 (2)

- (d) will not be in general conformity with the Land Use Plan, Thoroughfare Plan, or other plan officially adopted by the board; or,
- (e) will exceed the county's ability to provide adequate public facilities, including, but not limited to, schools, fire and rescue, law enforcement, and other county facilities.

Part II. That Articles 9, 10, and 11, Sections 903, 1003, and 1103 be amended by adding the following provisions:

For subdivisions containing twenty (20) or more lots, the following information shall be provided.

	SKETCH PLAT	PRELIMINARY PLAT PLA	
IV. DEVELOPMENT IMPACT STAT	EMENT		
1. Physical Analysis (type units expect incl. number of bedrooms; projected size and timing of phases, etc.)			
2. Housing Market Analysis (delimit market area; project demand, suppl and unmet demand; determine net capture; identify development profil			
3. Environmental Impact (water consumestimated per unit type, hydrological report by a licensed engineer identification and sewer generation and means of disposal)	al -		_
4. Fiscal Analysis (estimated real proposal valuation, estimated personal proposal valuation, estimated annual land transfer tax value)			
5. Traffic Analysis (estimated number trips generated, volume of existing traffic on roads adjacent to and with 1/2 mile of tract, directional distribution of traffic, capacity analysis.	hin		

Action on: ALOYSIUS AND JANE KREMPEL request to rename Ninth Street in Point Harbour Beach Subdivision, Poplar Branch Township.

Proposed Name: Rose Lane

Commissioner Bowden moved to approve request to rename street. Commissioner Gregory seconded the motion. Motion carried unanimously. Chairman Evans was absent.

PB 94 - 37ACEY COMMERCIAL Action on PROPERTIES: 18.7 from rezone acres (Agricultural) to (General Business) GB located in Moyock on the southwest side of NC

168 approximately 1364' south of SR 1216 (Puddin Ridge Road), Tax Map 15, Parcel 75A, Moyock Township.

Commissioner Bowden moved to approve the request to rezone from A to GB. Commissioner Gregory seconded the motion. Motion carried unanimously. Chairman Evans was absent.

Action on PB 94-38 C. W. BRYANT AND C. H. BRYANT: special use permit to expand an existing sand mining operation on property located on the east side of NC 168 at the North Carolina/Virginia state line, Tax Map 8, Parcels 15 and 17 and Tax Map 9, Parcels 1 and 2, Moyock Township.

Vice Chairman Wright moved to approve special use permit and hours will be from 7a.m. to 6p.m. Commissioner Gregory seconded the motion. Motion carried unanimously.

Action on: PB 88-62 THE PLANTATIONS (formerly Country Side): sketch plan approval of a 601-lot common open space subdivision located on SR 1215 (Survey Road) in Moyock, Tax Map 15, Parcel 84, Moyock Township.

This item was continued until next meeting at the request of the applicant.

Action on PB 94-40 CURRITUCK COUNTY: request amendments to Article 9 Section 914, Article 10 Section 1013 and Article 11 Section 1113 of the UDO to delete Surety Bonds and add Deed of Trust and Bond as acceptable bond terms and methods.

Vice Chairman moved to table until Chairman Evans was present. Commissioner Gregory seconded the motion. Motion carried unanimously.

CURRITUCK COUNTY: request to name an unnamed lane off of Barnard Road which services the Ezra and Lisa Graley Subdivision in Poplar Branch. Proposed Name: Graley Lane.

Jack Simoneau, Planning and Inspections Director, reviewed the proposed name change. He advised the Board that the two

property owners could not agree on the original name (Tanners Way) so the staff recommended Graley Lane, the subdivision name.

Commissioner Gregory moved to approve the road name. Commissioner Bowden seconded the motion. Motion carried unanimously.

# Chiefs and Captains Advisory Board to discuss permits for bonfires on the beach.

Marshall Cherry, Corolla Fire Department, requests that the Board consider a beach fire notice. Mr. Cherry stated that they had several serious incidents with beach fires this summer and would like some way to the Corolla Fire Dept. could be notified in advance. This is needed only in the RO-1 district.

Commissioner Gregory moved to direct the staff to review proposal for bonfires and bring a recommendation back to the Board. Commissioner Askew seconded the motion. Motion carried unanimously.

### Consideration of Legislative Grants for Cooperative Extension.

Rodney Sawyer, Cooperative Extension Director, was present to review legislative grants.

Vice Chairman Wright commended Mr. Sawyer and his staff on the enormous amount of work they have put into writing these grants.

Commissioner Gregory moved to endorse the legislative grants and provide space for additional staff. Commissioner Askew seconded the motion. Motion carried unanimously.

# Consideration of DOT Agreement for restoration of the Whalehead footbridge.

Vice Chairman Wright moved to approve the DOT agreement for \$58,000. Commissioner Gregory seconded the motion. Motion carried unanimously.

#### Appointment to Whalehead Board of Trustees.

Vice Chairman Wright moved to appoint Rhett White to fill the unexpired term of John Wilson. Commissioner Gregory seconded the motion. Motion carried unanimously.

### Consideration of Application for Archives and History Grant.

Commissioner Bowden moved to approve grant of \$100,000 for the Whalehead Club. Commissioner Gregory seconded the motion. Motion carried unanimously. The project will be assigned to the Whalehead Board of Trustees.

### Discussion on proposed service district for northern Currituck Banks.

Bill Romm, County Attorney, reviewed the process for a proposed service district for street maintenance.

Commissioner Bowden stated that the mere creation of a District would not increase taxes.

Vice Chairman Wright moved to take no action at this time and direct staff to gather information to be presented to the new board in December. Commissioner Gregory seconded the motion. Motion carried unanimously.

### Appointments to Land Transfer Tax Appeals Board.

Commissioner Bowden moved to appoint Gary Montalbaum and reappoint John Whitehurst and Bob Henley. Commissioner Gregory seconded the motion. Motion carried unanimously.

# Release of taxes From September 12 through September 30, 1994.

Commissioner Bowden moved to release the following taxes. Commissioner Gregory seconded the motion. Motion carried unanimously.

Quality Oil	\$194.22
James Cooper	133.92
Thomas Clark	211.50
Cordelia Mack	117.42
James Smith	101.70

# Consideration of Resolution to change Board meeting time.

Commissioner Bowden moved to adopt the following resolution. Commissioner Gregory seconded the motion. Motion carried unanimously.

RESOLUTION

WHEREAS, North Carolina General Statutes 153A-40, authorizes a County Board of Commissioners to fix the time and place of its regular meeting, and

WHEREAS, the Board of Commissioners of the County of Currituck finds that it is desirable to change its regular meeting time from 8:00 p.m. on the first and third Monday of each Monday to 7:00 p.m. on the first and third Monday of each month.

#### NOW, THEREFORE BE IT RESOLVED,

- 1. The regular meeting of the Board of Commissioners of Currituck shall be held at 7:00 p.m. on the first third Monday of each month;
- 2. That the Clerk to the Board is instructed to post a copy of this Resolution on the Courthouse bulletin board and to cause a summary of this Resolution to published.

#### Consideration of Library furnishing bids.

Commissioner Gregory moved to accept bid from Chesson's Business Interiors in the amount of \$55,597.52. Commissioner Askew seconded the motion. Motion carried unanimously.

### Executive Session to discuss a property matter.

Vice Chairman Wright moved to go into executive session to discuss a property matter. Commissioner Gregory seconded the motion. Motion carried unanimously.

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and

ACCOUNT NUMBER	ACCOUNT DESCRIPTION	DEBIT	CREDIT
10-330-486-00 10-640-327-00	Americorp Grant Americorps	\$5,036	\$5,036

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ACCOUNT NU	MBER ACCOUNT DE	ESCRIPTION DEBIT	CREDIT
10-330-487-00	Support our Stude	ents	\$75,000
10-641-020-00	Salaries - Regula	ar \$23,400	
10-641-050-00	FICA Expense	1,790	
10-641-060-00	Insurance Expens	se 2,025	
10-641-070-00	Retirement Exper	nse 2,181	
10-641-110-00	Telephone & Post	tage 600	

10-641-115-00	Printing	500
10-641-140-00	Travel	1,044
10-641-142-00	Transportation	2,240
10-641-145-00	Training & Education	1,766
10-641-210-00	Rent	1,500
10-641-320-00	Supplies	4,626
10-641-450-00	Contracted Services	17, 975
10-641-540-00	Insurance & Bonds	2,223
10-641-890-00	Miscellaneous	2,880
10-641-900-00	Capital Outlay	10,250

### Adjourn

After reconvening from executive session no action was taken. There being no further business the meeting adjourned at 10:50 p.m.

Currituck County
North Carolina
February 6, 1995
Work Sessions held on
January 19, 1995 and January 31, 1995

New High School-

The Currituck County Board of Commissioners met in a work session on January 19, 1995 at 7:00 p.m. with the School Board to discuss the High School bids and other related school projects. The following members were present: Chairman Bowden, Commissioners Gregory, Miller, O'Neal and Etheridge.

Chairman Bowden read the following statement:

• The Board of Commissioners does not intend to provide funding above the current project sheet estimates of \$19,400,000 for the five bond funded capital projects that are specified below:

New Flight Collect	Construction Furnishing	12,264,3	363 750,0	
	Ū	\$ 1	3,995,5	512
Central Renovation	ns- Professional S	Services	\$	65,983
	Construction	824,	785	
	Furnishings		87,	<u>381</u>
		\$	978,	149
"Existing" High School Ro	oof Replacement	\$	200,	000
School Site North		\$	200,	000
New Elem. School- Profe	essional Services	\$ 258,0	640	

Professional Services

981.149

3.233.000

	Furnishings	467,675 \$ 3,959,315
- C -	 	

Construction

- After cutting the project to the greatest extent possible, the Board of Education is requested to re-bid the new high school.
- The County will be a party to the high school construction contract in order to receive sales tax revenue needed for the bond projects.
- The five capital projects are requested to be placed on a five-year building/funding schedule.

- 13. Within sixty (60) days from the date of its first review of the sketch plan, the Planning Board will forward the plat along with it's recommendations, including any conditions or modifications, to the Board of Commissioners. Failure to forward the plat within the allotted time shall have the same effect as a recommendation for approval.
- 14. During the first regularly scheduled monthly meeting of the Board of Commissioners following recommendation by the Planning Board, the Board of Commissioners will hold a public hearing to hear any and all remarks presented by the subdivider/developer, Staff comments and recommendations, Technical Staff comments and others.
- 15.The subdivider/developer or his agent must attend the Board of Commissioners and all subsequent Board meetings for presentation of the application to the appropriate Board and to answer any questions by Board members and others.
- 16.The Board of Commissioners shall discuss with the subdivider/developer or his agent changes deemed advisable, if any, and the kind and extent of improvements to be made by him.
- 17.Upon conclusion of the public hearing, the Board of Commissioners may approve, approve conditionally, disapprove or table the request as set forth in the Board of Commissioner's Rules of Procedures and by State law. Because of the conceptual presentation involved, this shall not constitute an official action of approval of the subdivision for recordation. The reasons for a conditional approval or disapproval shall be placed in the record of the minutes of the proceedings.
- 18.Receiving approval from the Board of Commissioners shall allow the subdivider/developer to proceed with submission of all materials and information required for the preliminary plat review process and to seek all permits as required under this Article.

Part IV. This ordinance amendment shall be in effect from and after the day of , 1995.

Chairman Bowden opened the public hearing. There being no comments, Chairman Bowden closed the public hearing.

Action on PB 94-48 DOVE ROOST: special use permit/sketch plan approval of a 48 lot major subdivision located in Moyock at the southern end of SR 1216 (Puddin' Ridge Road), Tax Map 11, Parcel 2, Moyock Township.

This item was tabled at the request of the developer, James Ferebee, Jr.

Action on: PB 88-62 THE PLANTATIONS (formerly Country Side): sketch plan approval of a 601-lot common open space subdivision located on SR 1215 (Survey Road)

in Moyock, Tax Map 15, Parcel 84, Moyock Township.

Hood Ellis, Attorney, stated that the developer has met all the requirements of the UDO and complied with terms.

Commissioner Gregory moved to deny request as follows. Commissioner O'Neal seconded the motion. Motion carried unanimously.

#### MOTION FOR DENIAL BASED ON THE FOLLOWING:

The proposed 601 lot development fails to meet the provisions of Section 1402(2)(e) of the UDO because it exceeds the county's ability to provide adequate public school facilities based on evidence presented during the public hearings as stated below.

#### FINDINGS:

- 1. The 1992 <u>School Facilities Needs Analysis and Long Range Capital Improvement Plan</u> indicated that the then enrollment of 576 students in Moyock Elementary School required an additional 14,626 square feet of floor space to meet state school facility standards.
- 2. The 1994 school enrollment in Moyock Elementary School is 715 students.
- 3. The state standard for elementary school sites is 10 acres plus 1 acre for every 100 students in membership. The Moyock Elementary School is on a 10 acre site of which only 5.8 acres of the site is usable when utility and drainage areas are removed.
- 4. According to school enrollment figures for 1994, Quail Run, North Point, Quail Point, Moyock Meadows, Duck Ridge, and Old South Landing averaged the following number of students per home: .46 elementary school; .09 junior high school; .14 high school. Assuming these figures hold true for the Plantations subdivision, 601 single-family dwellings will generate 280 elementary students; 54 junior high school students; and 84 high school students at full buildout.
- 5. According to school officials, plans are that by September 1996, Knapp Jr. High will have been converted to an elementary school which will optimally accommodate up to 350 students; Moyock Elementary will optimally accommodate 450 students; and Central Elementary will optimally accommodate up to 375 students (1,175 total elementary students). Based on Planning staff estimates, under a high growth scenario, there will be 1,215 elementary students in

Moyock & Crawford Townships. Under a medium growth scenario, there will be 1,185 elementary students in Moyock and Crawford Townships.

- 6. According to Planning and Inspections, there are nearly 3,000 lots ready for development or having approval for development on the Currituck Mainland, Gibbs Woods and Knotts Island.
- 7. Based on the last 3 years, student enrollment at Moyock Elementary School has increased by 46 students per year.

Action on PB 94-49 THE CURRITUCK ASSOCIATES, RESIDENTIAL PARTNERSHIP AND THE CURRITUCK ASSOCIATES, GOLF PARTNERSHIP: rezone 586.7 from RO1 (Outer Banks Standard Residential) to Planned Unit Development RO1 (Outer Banks Standard Residential) (Limited Business, hotels allowed) located in Corolla on the west side of NC 12 (Ocean Trail) across from Ocean Sands, PUD, Tax Map 117, Parcel 1A, Poplar Branch Township.

This item was continued until the February 20, 1995 meeting.

Action on PB 94-49 THE CURRITUCK CLUB: sketch plan/special use permit for a planned unit development to contain: 476 single family lots, 104 golf course villas, 56 patio homes, 2 commercial areas and 4 employee housing units located in Corolla on the west side of NC 12 extending from northern property lines Pine Island, PUD, and National Audubon Society, Inc. to southern property lines of Ocean Sands, sections X and Y, Tax Map 117, parcel 1A, Poplar Branch Township.

This item was continued until the February 20, 1995 meeting.

Public Hearing on PB 94-51 CURRITUCK COUNTY: request amendments to the UDO in order to establish an expiration date on subdivision plats approved prior to April 2, 1989 where no work has begun; to increase the lot size in agricultural zoning districts to 3 acres and all other zoning districts except RO1 and RO2 to 40,000 sq. ft., to delete multi-family dwellings permitted in the as uses agricultural district; to increase the



### Currituck County Agenda Item Summary Sheet

Agenda ID Number – 3299

Agenda Item Title: PB 18-23 Fost, Phases 6-9, Preliminary Plat/Special Use Permit:

Submitted By: Tammy Glave - Planning & Community Development

Item Type: Quasi-Judicial

Presenter of Item: Tammy Glave

**Board Action:** Action

#### **Brief Description of Agenda Item:**

178 dwelling units located in Moyock on the west side of Caratoke Highway across from Guinea Road, Tax Map 15, Lot 86, Moyock Township.

**Planning Board Recommendation:** 

**Staff Recommendation:** 

**TRC Recommendation:** 



# STAFF REPORT PB 18-23 FOST TRACT PRELIMINARY PLAT/ SPECIAL USE PERMIT, PHASES 6-9 BOARD OF COMMISSIONERS DECEMBER 20, 2021

APPLICATION SUMMARY	
Property Owner:	Applicant:
Moyock Development LLC	Moyock Development LLC
417-D Caratoke Hwy	417-D Caratoke Hwy
Moyock NC 27958	Moyock NC 27958
Case Number: PB 18-23	<b>Application Type:</b> Preliminary Plat/Special Use Permit
Parcel Identification Number:	<b>Existing Use:</b> Planned Development under
0015-000-0086-0000	construction
Land Use Plan Classification: Full Service	Parcel Size (Acres): 91.10
Moyock Small Area Plan Classification: Limited Service	Zoning: PD-R
Number of Units: 178	Overall Project Density (units/acre): 2.19
Required Open Space (Acres): 27.33	Provided Open Space (Acres): 39.07

SURROUNDING PARCELS						
	Land Use	Zoning				
North	Single-family dwellings, retail	AG, GB, SFM				
South	Single-family dwellings, cultivated farmland	AG, SFM				
East	Single-family dwellings, cultivated farmland	GB, SFM				
West	Single-family dwellings, cultivated farmland	C-MXR				

#### STAFF ANALYSIS

The Board of Commissioners approved the original Master Plan (PD-R) for this development on May 6, 2019 and three subsequent amendments thereafter. The developer notes that the goal of Phases 6-9 is to continue building a community that has a creative design, providing a mix of different residential uses in close proximity to one another, while at the same time providing an efficient use of open space that promotes an active lifestyle and strong sense of community. Limited, small scale commercial uses may also be constructed primarily to serve the needs of the residents in this development.

The Board of Commissioners approved the below phasing plan as part of the project. The phasing plan remains valid:

					SCHEDULE	ЕВ	
			×	E	HASING SCH	EDULE	
PHASE	AREA (AC.)	DEVELOPMENT INTENSITY (D.U./AC.)	TND T.H.	TND S.F.	CONV. S.F.	TOTAL	OTHER IMPROVEMENTS
1	39.7	1.79	25	12	34	71	Rowland Creek Improvements
2	33.1	1.96	19	9	37	65	Main Lake (Completion)
3	29.2	1.88	16	14	25	55	Village Green & Clubhouse
4	19.9	3.67	27	7	39	73	Multi-Use Path
5	14.4	2.57		17	20	37	Swimming Pool
6	39.7	1.99	21		58	79	Up to 22,000 SF Neighborhood Commercial
7	23.8	2.56	18		43	61	
8	17	1.12			19	19	
9	<u>12</u>	1.58			<u>19</u>	19	
TOTALS	228.8	2.09	126	59	294	479	

The Phased Recording Schedule estimates earliest recording dates as shown below for Phases 6-9:

Phase Numbers	Number of	ESTIMATED EARLIEST
	Dwelling Units	RECORDING DATE
6	79	August 2022
7	61	February 2023
8	19	August 2024
9	19	August 2024

INFRASTRUCTURE	
Water	Public
Sewer	Private Centralized System (connecting to Eagle Creek WWTP)
	Pedestrian: A series of sidewalks and pedestrian trails; The developer must install an 8' multi-use path along the Caratoke Highway property line as required by the Connect Currituck Plan.
Transportation	<ul> <li>Connectivity Score: Minimum = 1.6 Proposed = 1.67</li> <li>One Connection to Caratoke Highway (Main entrance)</li> <li>One Connection to Moyock Farms (proposed development to the Southeast)</li> </ul>
	<ul> <li>Four connections to Flora Farm (proposed development to the West)</li> </ul>
Stormwater/Drainage	See attached map for required stormwater/drainage improvements (PB 18-23 Fost PD-R Rezoning Stormwater Conditions dated 7/2019 prepared by engineering staff)
Design Standards	Multi-family design standards apply to the townhomes.
Lighting	Street lights are proposed and must be full cut-off fixtures
Landscaping	Farmland buffers, street trees, major arterial screening, and site landscaping are required
Compatibility	The BOC determined that this development was compatible with the Land Use Plan, Moyock Small Area Plan, and the surrounding neighborhoods at its May 6, 2019 meeting.
Recreation and Park Area Dedication	The applicant will be paying approximately \$15,978.64 as a fee-in-lieu of recreation and park area dedication for these phases.
Riparian Buffers	Required for the wetlands in these phases.

#### **COMMUNITY MEETING**

The developer held a community meeting on March 23, 2021, at the Eagle Creek Pavilion. There were approximately seven people in attendance. Most of the questions were regarding drainage, traffic, fire hydrants, and wastewater. A community meeting summary prepared by the applicant is attached to this staff report.

#### **SCHOOLS**

There are Adequate Public Facility concerns that Shawboro Elementary is at 100% *Actual Capacity* for this school year, as per superintendent Dr. Lutz at the October 2, 2021, BOC meeting. This development is in the Shawboro Elementary School district and has not started sending children to Shawboro Elementary yet. Phases 6-9 being will send an additional 44 elementary students to Shawboro Elementary. When the complete, the overall development will send approximately 119 elementary students to Shawboro Elementary. These numbers were generated using the Tischler Study. The board should also be aware that Moyock Middle School is at 94% *actual capacity* for this school year and this development would send approximately 38 additional students to Moyock Middle School.

ADEQUATE PUBLIC FACILITIES – SCHOOLS <sup>1</sup>					
School	2021-2022 Actual Capacity <sup>2</sup>	Committed Capacity <sup>3</sup>	Proposed Capacity Changes Number of Students (Phases 6-9)		
Moyock Elementary Shawboro Elementary Central Elementary	128% 99% 96%	131%	+44		
Moyock Middle Currituck Middle	94% 62%	92%	+14		
Currituck High JP Knapp Early College	88% 86%	105%	+24		

<sup>&</sup>lt;sup>1</sup>Does not include minor subdivisions, exempt subdivisions, and subdivisions approved prior to the adoption of the adequate public facilities ordinance (October 1994)

Based upon questions at a previous meeting regarding school capacity and the adequate public facilities ordinance, a brief history of the adequate public facilities ordinance and its application is attached for your review.

#### **STORMWATER**

The stormwater/drainage improvements required as shown on the attached "PB 18-23 Fost PD-R Rezoning Stormwater Conditions dated 7/2019" prepared by engineering staff remain unchanged. On-site stormwater will be conveyed to on-site retention ponds through a combination of curbs with inlets, stormwater pipes, and open vegetated swales.

#### **TRANSPORTATION**

The main subdivision entrance will connect directly to NC 168 (Caratoke Highway). Deceleration and acceleration lanes will be provided in accordance with NCDOT Standards and have been approved by NCDOT. Connectivity will be provided to the proposed development to the southeast, Moyock Farms, and to the proposed development to the west, Flora Farm. According to the application, streets will be in accordance with the approved right-of-way and pavement width characteristics, using the NCDOT Complete Streets Planning Manual as a guide.

Appropriate access has been made for emergency service vehicles in accordance with applicable standards including the North Carolina Fire Code.

#### **WASTEWATER**

The developer proposes to connect to the Eagle Creek wastewater treatment plant (WWTP). Currituck Water and Sewer, LLC (CWS) Major Utility operates the WWTP and is the process of acquiring ownership of the entire wastewater system (collection systems, treatment plant, etc.) from Sandler Utilities Company. A force main is being installed to connect the collection system in Fost development to the Eagle Creek WWTP. A wastewater collection system will be constructed by the developer for the entire Fost development and managed by Currituck Water and Sewer. The utility will be regulated by the North Carolina Utilities Commission and the North Carolina Department of Environmental Quality. CWS has begun the process with state regulatory agencies for this transference.

<sup>&</sup>lt;sup>2</sup>Capacity percentages are based on 2021-2022 school year classroom standards and Sep 2021 ADM

<sup>3</sup> Capacity percentages are based on the 2021-2022 school year classroom standards and Sep 2021 ADM

#### **STAFF REVIEW**

#### **TECHNICAL REVIEW COMMITTEE**

The Technical Review Committee (TRC) reviewed the application and provided the following comments:

- 1. The applicant has not demonstrated that the proposed use will meet the special use permit review standards of the UDO including adequate public facilities (LUP Policy PP2 Adequate Public Facilities) and public health and safety (LUP Purpose of the Plan).
- 2. Noted plan deficiencies:
  - a. Alleys shall only intersect with streets (not other alleys) and shall not have a pavement width more than 16'.
  - b. The Recreation and Parkland fee-in-lieu dedication is incorrect. The fee due prior to final plat approval for theses phases is \$134,288.27. The fee is calculated based on the tax value of the land at subdivision submittal (preliminary plat/SUP). The value of the land based on "pre-closing value" as you requested to make the calculation is not relevant to determining the fee amount. Pre-closing value is typically based upon the then use of the land, often farmland, with a deferred tax value. The Recreation and Parkland fee-in-lieu is not calculated at conditional zoning approval because it is not required for the rezoning, but rather for the subdivision of property.
  - c. Add a note to the plan that the 'Reserved as effluent disposal area' is not permitted, an amended plan is required that must be approved by the BOC, and the BOC must approve the amended plan prior to any land disturbance for any part of the on-site effluent disposal system.

#### Other staff concerns:

- 1. The performance of the Eagle Creek WWTP is under evaluation by NCDEW DWR for non-compliance with the permit standards. Staff has concerns about allowing additional lots from other subdivisions to connect to the Eagle Creek WWTP with the known performance problems of the plant. On August 10, 2021, NCDEQ Division of Water Resources issued a Wastewater Collection Extension Permit for 60,000 gallons per day for 92 four-bedroom homes and 44 three-bedroom townhouses for Fost Phase 1A. On September 17, 2021, NCDEQ Division of Water Resources issued a letter stating "the WWTP must demonstrate consistent performance achieving permit treatment requirements, prior to submittal of a sewer extension permit application. WWTP performance must be satisfactory for approval of any potential sewer extension permits." County staff has not received notification that these permit requirements have been met and a sewer extension permit for additional lots will be issued. (LUP Purpose of the Plan public health, safety, and welfare)
- 2. CWS is in the process with the NC Utilities Commission to acquire the Eagle Creek WWTP and collection system, and to establish a sewer service district and rate structure for the Fost development. There are concerns about the impact of the Consent Judgement on the process with the NCUC for creating the sewer district and applying a rate structure. Please see the Consent Judgement issued by the Superior Court on June 23, 2021 attached to this staff report for more detailed information (North Carolina Division of Environmental Quality v. Sandler Utilities at Mill Run, LLC). The Court ordered Sandler Utilities at Mill Run, LLC to complete a system upgrade plan, and then a timetable to implement the upgrade plan. The timetable to implement the upgrade plan has not been shared by NCDEQ DWR staff due to the legal proceedings. For the protection of the existing users of the system and future users, staff recommends the requirements of the Consent Order be met and certified by NCDEQ and the sewer district established by NCUC approval prior to approving additional lots to be created in

PB 18-23 Fost Tract, Phases 6-9 Preliminary Plat/Special Use Permit Page **5** of **11**  the yet-to-be-established sewer district. (LUP Purpose of the Plan – public health, safety, and welfare)

#### 2006 Land Use Plan and Moyock Small Area Plan

The 2006 Land Use Plan classifies the site as Full Service within the Moyock subarea. The Moyock Small Area Plan classifies the property as Limited Service. At its May 6, 2019, meeting, the BOC determined this development was compatible with the Land Use Plan, Moyock Small Area Plan, and the surrounding neighborhoods because:

- a. The proposed conditions for the property will improve drainage problems on the property and with nearby Ranchland subdivisions if improvements can be made to drainage system on off-site properties, and; (Staff Addition: Potential LUP Policy WQ8)
- b. The PDR is compatible with existing Moyock Township subdivisions. (Staff Addition: Potential LUP Policy HN1)

#### **SPECIAL USE PERMIT REVIEW STANDARDS**

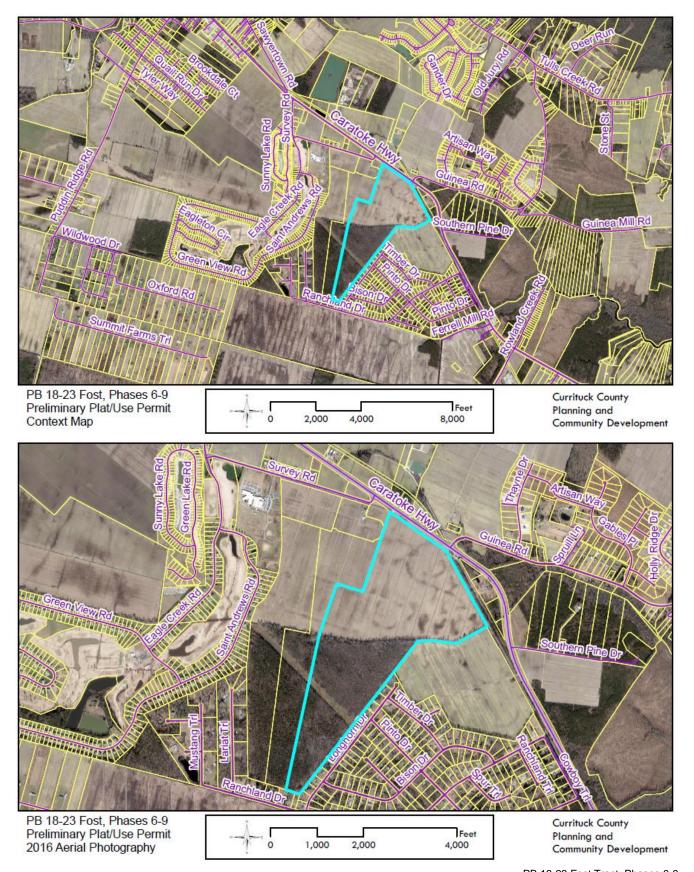
Following an evidentiary hearing, the board shall decide if the application is in accordance with Section 2.3.10, Decision-Making Body Review and Decision, and Section 2.4.6.D, Special Use Permit Review Standards. It is the burden of the applicant to provide evidence to address the Review Standards. The applicant's findings can be found in the "Attachment to application for preliminary plat/special use permit approval for Fost Phases 6-9."

#### Special Use Permit Review Standards

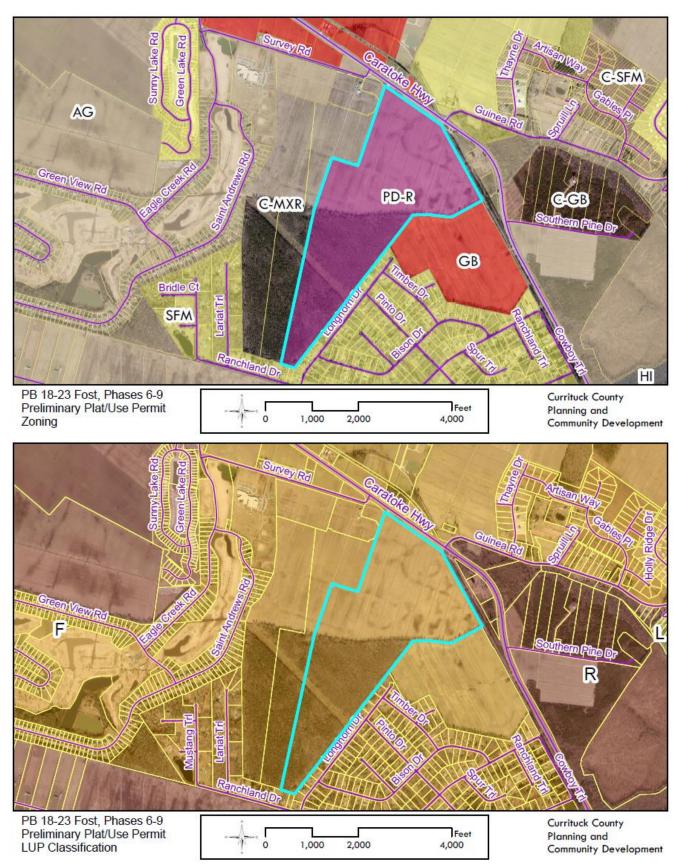
A special use permit shall be approved on a finding that the applicant demonstrates the proposed use will:

- 1. Not endanger the public health or safety.
- 2. Not injure the value of adjoining or abutting lands and will be in harmony with the area in which it is located.
- 3. Be in conformity with the Land Use Plan or other officially adopted plan.
- 4. Not exceed the county's ability to provide adequate public facilities, including but not limited to, schools, fire and rescue, law enforcement, and other county facilities. Applicable state standards and guidelines shall be followed for determining when public facilities are adequate.

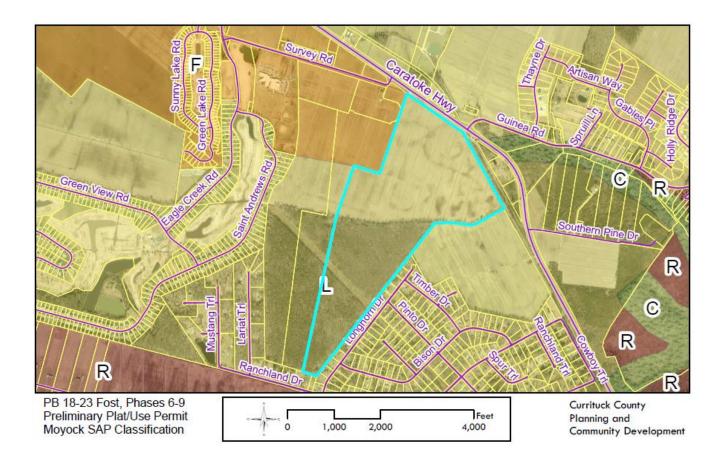
THE APPLICATION AND RELATED MATERIALS ARE AVAILABLE ON THE COUNTY'S WEBSITE Board of Commissioners: www.co.currituck.nc.us/board-of-commissioners-minutes-current.cfm



PB 18-23 Fost Tract, Phases 6-9 Preliminary Plat/Special Use Permit Page **7** of **11** 

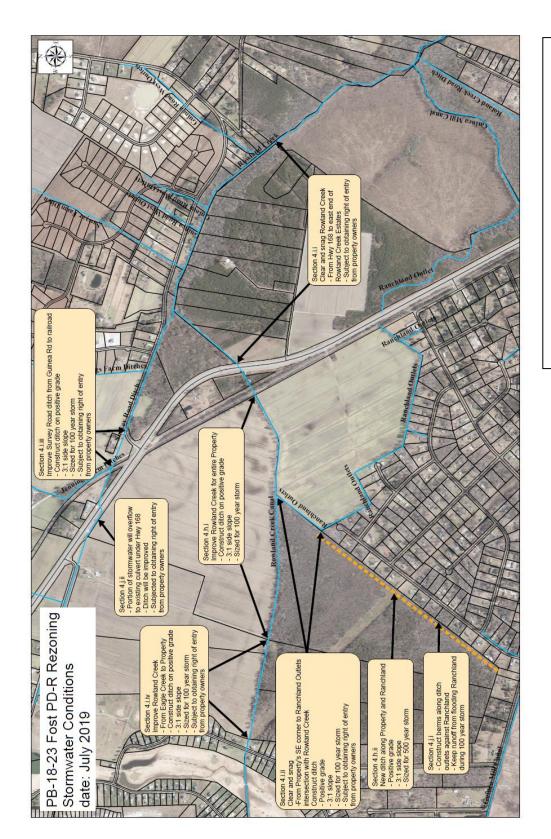


PB 18-23 Fost Tract, Phases 6-9 Preliminary Plat/Special Use Permit Page 8 of 11

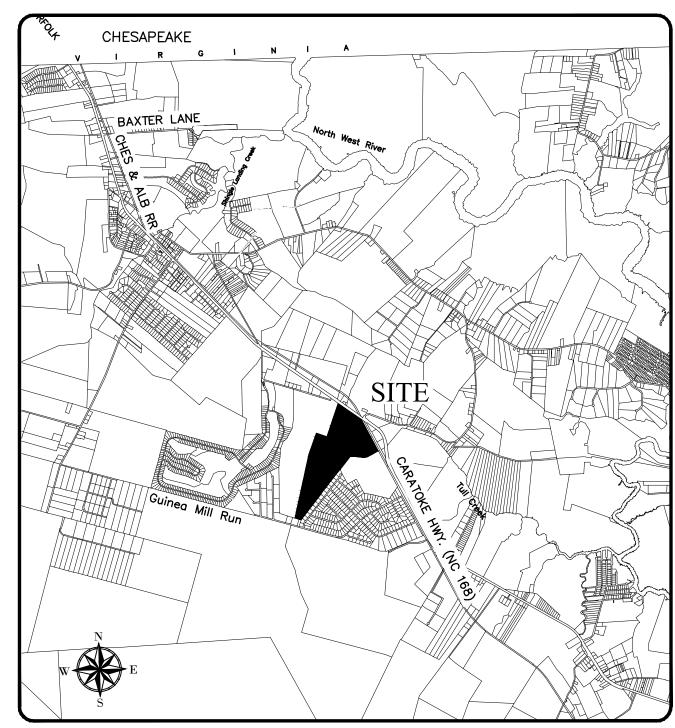


#### Approved Development Standards and Setbacks

	SCHEDULE /	A	
	DEVELOPMENTSTANDAR	DS & SETBACKS	
STYLE:	TND T.H.	TND SFLOT	CONV. SFLOT
Min Lot Size:	1,800 SF	6,900 SF	9,500 SF
Typ. Lot Size:	1,800 – 3,000 SF	7,000 - 9,000 SF	_10,000 - 15,000 SF
Min. Lot Width:	20'	<u>35′</u>	35′
Typ. Lot Width:	20' – 25'	50'-60'	,62'
Front Setback:	15'	20'	20'
Side Setback:	0	10'	10'
Rear Setback:	20'	20'	25'
Corner Side Setback:	15'	15'	15'
Maximum Setback:	25'	75'	,140′
Maximum Height;	35'	35′	35'
Maximum Bldg. Size:	,16,000 <b>SF</b>	N/A	N/A
Maximum Lot Coverage:	100%	60%	45%



Prepared by Currituck County Engineering Staff



#### VICINITY MAP SCALE: 1" = 5,000'

I HEREBY CERTIFY THAT I AM THE OWNER OF THE PROPERTY DESCRIBED HEREON, WHICH PROPERTY IS LOCATED WITHIN THE SUBDIVISION REGULATION JURISDICTION OF CURRITUCK COUNTY, THAT I HEREBY FREELY ADOPT THIS PLAT OF SUBDIVISION AND DEDICATE TO PUBLIC USE ALL AREA SHOWN ON THIS PLAT AS STREETS, UTILITIES, ALLEYS, WALKS, RECREATION AND PARKS, OPEN SPACE AND EASEMENTS EXCEPT THOSE SPECIFICALLY INDICATED AS PRIVATE AND THAT I WILL MAINTAIN ALL SUCH AREAS UNTIL THE OFFER OF DEDICATION IS ACCEPTED BY THE APPROPRIATE PUBLIC AUTHORITY OR HOME OWNERS' ASSOCIATION. ALL PROPERTY SHOWN ON THIS PLAT AS DEDICATED FOR A PUBLIC USE SHALL BE DEEMED TO BE DEDICATED FOR ANY OTHER PUBLIC USE AUTHORIZED BY LAW WHEN SUCH USE IS APPROVED BY THE APPROPRIATE PUBLIC AUTHORITY IN THE PUBLIC INTEREST.

DATE	DATE OWNER				
Ι,	, A NOTARY PUBLIC				
OF	COUNTY, NORTH CAROLINA, DO HEREBY CERTIFY THAT				
APPEARED BEFORE THE FOREGOING CE	PERSONALLY ME THIS DATE AND ACKNOWLEDGED THE DUE EXECUTION OF RTIFICATE.				
WITNESS MY HAND	AND OFFICIAL SEAL THIS DAY OF,				

MY COMMISSION EXPIRES

PRIVATE STREETS OWNER CERTIFICATE I HEREBY CERTIFY THAT THE PRIVATE STREETS SHOWN ON THIS PLAT ARE INTENDED FOR PRIVATE USE AND WILL REMAIN UNDER THE CONTROL, MAINTENANCE, AND RESPONSIBILITY OF THE DEVELOPER AND/OR A HOMEOWNER'S ASSOCIATION AND ACKNOWLEDGE THAT SOME PUBLIC SERVICES MAY NOT BE PROVIDED DUE TO THE PRIVATE NATURE OF THE ROAD(S).

NOTARY PUBLIC

DATE

#### SURVEYOR'S CERTIFICATION

, MICHAEL D. BARR, CERTIFY THAT THIS PLAT WAS DRAWN UNDER MY SUPERVISION FROM AN ACTUAL SURVEY MADE UNDER MY SUPERVISION (DEED AND DESCRIPTION RECORDED IN BOOKS REFERENCED); THAT THE RATIO OF PRECISION AS CALCULATED IS 1: 10,000 AND VERTICAL ACCURACY IS  $\pm$  0.05'; THAT THE GLOBAL POSITIONING SYSTEM (GPS) OBSERVATIONS WERE PERFORMED TO THE GEOSPATIAL POSITIONING ACCURACY STANDARDS, PART 2: STANDARDS FOR GEODETIC NETWORKS AT THE 2-CENTIMETER ACCURACY CLASSIFICATION (95% CONFIDENCE) USING REAL TIME KINEMATIC AND TRADITIONAL TRAVERSE. THAT THIS PLAT WAS PREPARED IN ACCORDANCE WITH G.S. 47-30 AS AMENDED. THAT THIS PLAT MEETS THE REQUIREMENT OF G.S. 47-30 SECTION F-11-C-1AND FURTHER THAT IT MEETS THE REQUIREMENTS OF THE STANDARDS OF PRACTICE FOR LAND SURVEYING IN NORTH CAROLINA (21 NCAC 56. 1600)."

THIS IS TO CERTIFY THAT THIS SURVEY CREATES A SUBDIVISION OF LAND WITHIN THE AREA OF A COUNTY OR MUNICIPALITY THAT HAS AN ORDINANCE THAT REGULATES PARCELS OF LAND.

WITNESS MY ORIGINAL SIGNATURE, REGISTRATION NUMBER AND SEAL THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_\_.

SIGNATURE

## THE FOST TRACT

## MOYOCK TOWNSHIP CURRITUCK COUNTY NORTH CAROLINA PHASES 6-9

PROJECT NAME: THE FOST TRACT

- APPLICANT: MOYOCK DEVELOPMENT, LLC 417-D CARATOKE HIGHWAY MOYOCK, NC 27958
- ADDRESS: CARATOKE HIGHWAY, MOYOCK, NC 27958 PIN: 0015-000-0086-0000 RECORD DOCUMENT(S): D.B.13, PG:E/12 ACREAGE: 228.83 ACRES
  - PROPERTY ZONING: PD-R (PLANNED DEVELOPMENT RESIDENTIAL)
- ZONE X PER F.I.R.M. MAP NOS. 3721803100 K, 3721803000 K, & 3721804000 K ALL HAVING AN EFFECTIVE DATE DECEMBER 21, 2018. USE OF LAND WITHIN A FLOODWAY OR FLOOD PLAIN IS SUBSTANTIALLY RESTRICTED BY CHAPTER 7 OF THE CURRITUCK COUNTY UNIFIED DEVELOPMENT
- THIS PROPERTY CONTAINS ACOE "404' JURISDICTIONAL WETLANDS AS SHOWN AND CONFIRMED BY USACOE ACTION ID# SAW 2018-00838, DATED APRIL 30, 2018, AND MAY REQUIRE U.S. CORP OF ENGINEERS APPROVAL PRIOR TO DEVELOPMENT OF THE PROPERTY.
- A 10' EASEMENT FOR UTILITIES AND DRAINAGE ALONG REAR AND SIDE PROPERTY LINES AND A 15' EASEMENT FOR UTILITIES AND DRAINAGE ALONG FRONT PROPERTY LINE IS HEREBY ESTABLISHED. PEDESTRIAN ACCESS AND STREET TREE EASEMENTS ARE HEREBY PROVIDED PER TYPICAL LOT DETAIL ON SHEET 20. ALL DRAINAGE EASEMENTS ARE HEREBY DEDICATED TO
- A 25' DRAINAGE EASEMENT SHALL BE ESTABLISHED ALONG ALL MAJOR DRAINAGE WAYS SERVING MORE THAN 5 ACRES. A BLANKET DRAINAGE, UTILITY, AND PEDESTRIAN ACCESS EASEMENT IS HEREBY ESTABLISHED ACROSS ALL OPEN SPACE AREAS.
- EXISTING CONDITION INFORMATION BASED ON A COMBINATION OF THE FOLLOWING: 2012 AFRIAL IMAGERY OBTAINED FROM NCONEMAP.COM
- FIELD TOPOGRAPHIC SURVEY DATA BY BISSELL PROFESSIONAL GROUP. ELEVATIONS ARE REFERENCED TO NAVD 1988 VERTICAL DATUM.
- 10. SECTION 7.6.5 OF THE CURRITUCK U.D.O. SUBSTANTIALLY RESTRICTS DEVELOPMENT WITHIN A 30'
- 11. ALL UTILITIES ARE TO BE UNDERGROUND.
- NEAREST HYDRANT (MCH15) FLOW TESTED AT 1,146 GPM. DESIGN FIRE FLOW IS 1,000 GPM FOR SINGLE FAMILY RESIDENCES BASED ON STRUCTURES HAVING LESS THAN 4,800 SQ. FT. AND NOT OVER 2-STORIES HIGH. NFF FOR MULTI-FAMILY STRUCTURES IS BASED ON ISO STANDARDS.
- 13. THERE IS A 50' FARM BUFFER IN ALL AREAS ADJOINING ACTIVE FARM LANDS.
- 14. THIS SUBDIVISION CONTAINS RIGHTS-OF-WAY THAT ARE PLATTED WITH THE INTENT OF BEING EXTENDED AND CONTINUED TO AND FROM ADJOINING PROPERTIES. ACCESS WITHIN THE RIGHTS-OF-WAY FOR STREETS AND UTILITIES SHALL NOT BE RESTRICTED.

**ZONING CONDITIONS:** 

SEE SHEET 18

#### LOT DEVELOPMENT CONFIGURATION: LOT AREAS: VARY FROM 1,809 TO 18,051 SQUARE FEET.

RECREATIONAL AND PARK LAND FEE-IN-LIEU CALCULATION:

REQUIRED UPLAND RECREATION/PARKLAND DEDICATION ACREAGE = 4.539 AC. (178 LOTS \*

- CURRENT PROPERTY ACCESSED VALUE (LAND ONLY) = \$793,300 OR \$3,520.30/AC. (\$793,300/225.35
- FEE-IN-LIEU IS \$15,978.64 (4.539 AC. \* \$3,520.30/AC.)
- PHASES 1-9 STREET CONNECTIVITY INDEX = 89 LINKS/53 NODES = 1.68

**DEVELOPMENT NOTES:** TRACT SUMMARY:

TOTAL AREA OF TRACT: 228.28 AC. PHASES 1-5 AREA: 137.18 AC. PHASES 6-9 AREA 91.10 AC. TOTAL DEVELOPED AREA PHASES 1-9 228.28 AC.

PHASES 6-9 DEVELOPMENT AREA SUMMARY:

PROPOSED LOT AREA: 39.66 AC. PUBLIC PROPOSED R/W AREA: 11.13 AC. PRIVATE PROPOSED R/W AREA: 1.24 AC. REQUIRED OPEN SPACE TOTAL=30% (0.3\*91.10): 27.33 AC. OPEN SPACE PROVIDED: 39.07 AC. (42.9%)

# OF PROPOSED LOTS: 178 (1.95 LOTS/ACRE) OPEN SPACE PROVIDED PHASES 1-9 83.86 AC. (36.7%)

PROPOSED RIGHT-OF-WAY WIDTHS (FT.): PROPOSED PAVED ROADWAY WIDTH: SEE SHEET 20 PHASES 6-9 LINEAR FOOTAGE PUBLIC & PRIVATE ROADWAY: 9,657 L.F.±

Sheet Number	Sheet Title
1	COVER SHEET, DEVELOPMENT NOTES AND SITE LOCATION
2	EXISTING CONDITIONS AND SITE FEATURES PLAN
3	OVERALL PHASING PLAN
4	OVERALL DRAINAGE AND UTILITY PLAN
5	OVERALL HARDSCAPING PLAN
6	LANDSCAPING, LIGHTING, BUFFERING AND SIGNAGE PLAN
7	OVERALL PRELIMINARY PLAT
8-16	DETAILED PRELIMINARY PLAT SHEETS
17	LINE AND CURVE TABLES
18	ZONING TERMS & CONDITIONS
19-20	DETAILS

SHEET INDEX

SURVEY LEGEND PROPERTY BOUNDARY ADJOINING PROPERTY LINE CENTERLINE OF ROADWAY SET IRON ROD EXISTING IRON ROD EXISTING IRON PIPE EXISTING CONCRETE MONUMENT OCP CALCULATED POINT NOW OR FORMERLY TOWNSHIP PLAT CABINET D.B. M.B. MAP BOOK SQ.FT. or S.F. SQUARE FEE MAXIMUM BUILDING LINE M.B.L. AC or AC. ACRES P/0 PART OF TYP. TYPICAL N.T.S. NOT TO SCALE OPEN SPACE 0.S. RIGHT-OF-WAY

	- 30' UNDISTURBED BUFFER (
EWL EWL	- EXISTING WATER LINE
WL WL	PROPOSED WATER LINE (SIZE NOTED)
<b>X</b>	PROPOSED FIRE HYDRANT
<b>)</b>	EXISTING FIRE HYDRANT
×	EXISTING WATER VALVE
	PROPOSED FORCE MAIN SAN SEWER (SIZE AS NOTED)
—— FM —— FM —— FM ——	- EXISTING FORCE MAIN SAN.
SS SS	PROPOSED GRAVITY SANITARY (SIZE AS NOTED)
ss ss ss ss	EXISTING GRAVITY SAN. SEWE
<b>S</b>	PROPOSED SANITARY SEWER MANHOLE
<b>S</b>	EXISTING SANITARY SEWER M
D	PROPOSED CATCH BASIN
D	EXISTING CATCH BASIN
	PROPOSED STORM SEWER PI
	EXISTING STORM SEWER PIPE
	PROPOSED EDGE OF WATER
	EXISTING EDGE OF WATER
	PROPOSED SIDEWALK
=======================================	EXISTING SIDEWALK
EOP BOC	PROPOSED EDGE OF PAVEME BACK OF CURB
<u> </u>	EXISTING EDGE OF PAVEMENT BACK OF CURB
OHE OHE	- EXISTING OVERHEAD ELECTRIC

NO MORE THAN 30% OF ANY LOT SHALL BE COVERED BY IMPERVIOUS STRUCTURES AND MATERIALS, INCLUDING ASPHALT, GRAVEL, CONCRETE, BRICK STONE, SLATE, OR SIMILAR MATERIAL, NOT INCLUDING WOOD DECKING OR THE WATER SURFACE OF SWIMMING POOLS. THIS COVENANT IS INTENDED TO ENSURE COMPLIANCE WITH THE STORMWATER PERMIT NUMBER NORTH CAROLINA. THE COVENANT MAY NOT BE CHANGED OR DELETED WITHOUT

THE CONSENT OF THE STATE. FILLING IN OR PIPING OF ANY VEGETATIVE CONVEYANCES (DITCHES, SWALES, ETC.) ASSOCIATED WITH THIS DEVELOPMENT, EXCEPT FOR AVERAGE DRIVEWAY CROSSINGS, IS STRICTLY PROHIBITED BY ANY PERSON. THE LOT COVERAGE ALLOWANCE PROVIDED IN THE CURRITUCK COUNTY UNIFIED DEVELOPMENT ORDINANCE MAY BE DIFFERENT THAN THE NC STATE STORMWATER PERMIT. THE MOST RESTRICTIVE LOT COVERAGE SHALL APPLY.

REVIEW OFFICER CERTIFICATE STATE OF NORTH CAROLINA COUNTY OF CURRITUCK

REVIEW OFFICER OF CURRITUCK COUNTY, CERTIFY THAT THE MAP OR PLAT TO WHICH THIS CERTIFICATION IS AFFIXED MEETS ALL STATUTORY REQUIREMENTS FOR RECORDING.

<u>PUBLIC STREETS DIVISION OF HIGHWAY DISTRICT ENGINEER CERTIFICATE</u> I HEREBY CERTIFY THAT THE PUBLIC STREETS SHOWN ON THIS PLAT ARE INTENDED FOR DEDICATION AND HAVE BEEN DESIGNED OR COMPLETED IN ACCORDANCE WITH AT LEAST THE MINIMUM SPECIFICATIONS AND STANDARDS OF THE NC DEPARTMENT OF TRANSPORTATION FOR ACCEPTANCE OF SUBDIVISION STREETS ON THE NC HIGHWAY SYSTEM FOR MAINTENANCE.

DISTRICT ENGINEER

I HEREBY CERTIFY THAT THE SUBDIVISION SHOWN ON THIS PLAT IS IN ALL RESPECTS IN COMPLIANCE WITH THE CURRITUCK COUNTY UNIFIED DEVELOPMENT ORDINANCE AND, THEREFORE, THIS PLAT HAS BEEN APPROVED BY THE CURRITUCK COUNTY TECHNICAL REVIEW COMMITTEE AND SIGNED BY THE ADMINISTRATOR, SUBJECT TO ITS BEING RECORDED IN THE CURRITUCK COUNTY REGISTRY WITHIN 90 DAYS OF THE DATE BELOW.

ADMINISTRATOR

REVIEW OFFICER CERTIFICATE STATE OF NORTH CAROLINA COUNTY OF CURRITUCK

REVIEW OFFICER OF CURRITUCK COUNTY, CERTIFY THAT THE MAP OR PLAT TO WHICH THIS CERTIFICATION IS AFFIXED MEETS ALL STATUTORY REQUIREMENTS FOR RECORDING.

DATE REVIEW OFFICER

REQUIRED IMPROVEMENTS CERTIFICATE

I HEREBY CERTIFY THAT ALL IMPROVEMENTS REQUIRED BY THE CURRITUCK COUNTY UNIFIED DEVELOPMENT ORDINANCE HAVE BEEN INSTALLED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS PREPARED BY BISSELL PROFESSIONAL GROUP, AND SAID IMPROVEMENTS COMPLY WITH CURRITUCK COUNTY SPECIFICATIONS.

DATE

REGISTERED LAND SURVEYOR/ENGINEER

03/24/21 AS NOTED WGY

Packet Pg. 193

465100PP3

PROJECT NO:

(COUNTY) ZE AS NITARY SEWER RY SEWER MANHOLE

LEGEND

— · · · — · · · — · · · · — | EXISTING DITCH CENTERLINE

EXISTING DITCH TOP OF BANK

DELINEATED WETLANDS

PROPOSED SWALE HIGH POINT

 $\Rightarrow$  \_\_\_\_ | PROPOSED SWALE W/ FLOW ARROW

~

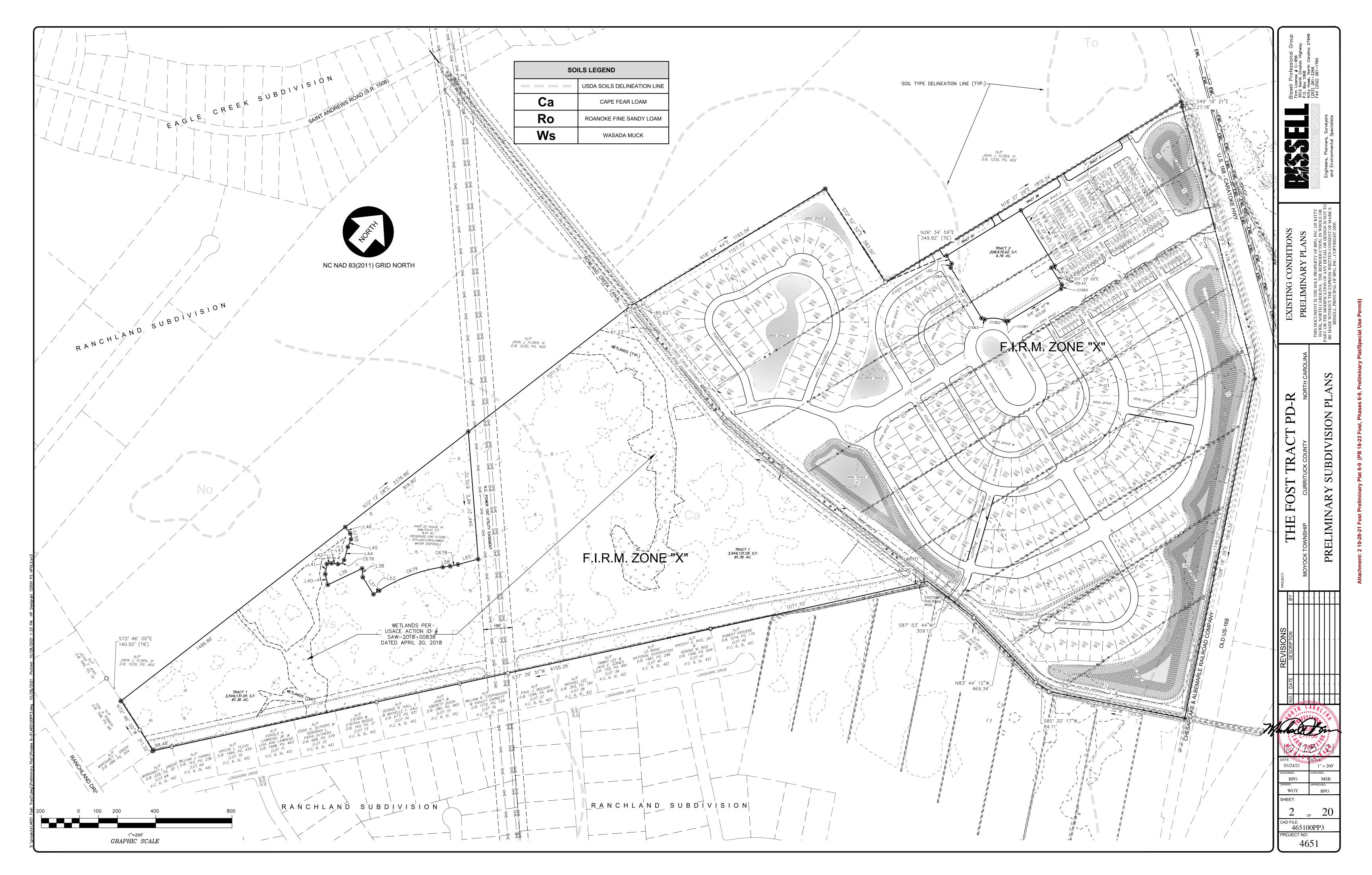
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EXISTING OVERHEAD ELECTRIC LINE EXISTING UTILITY POLE

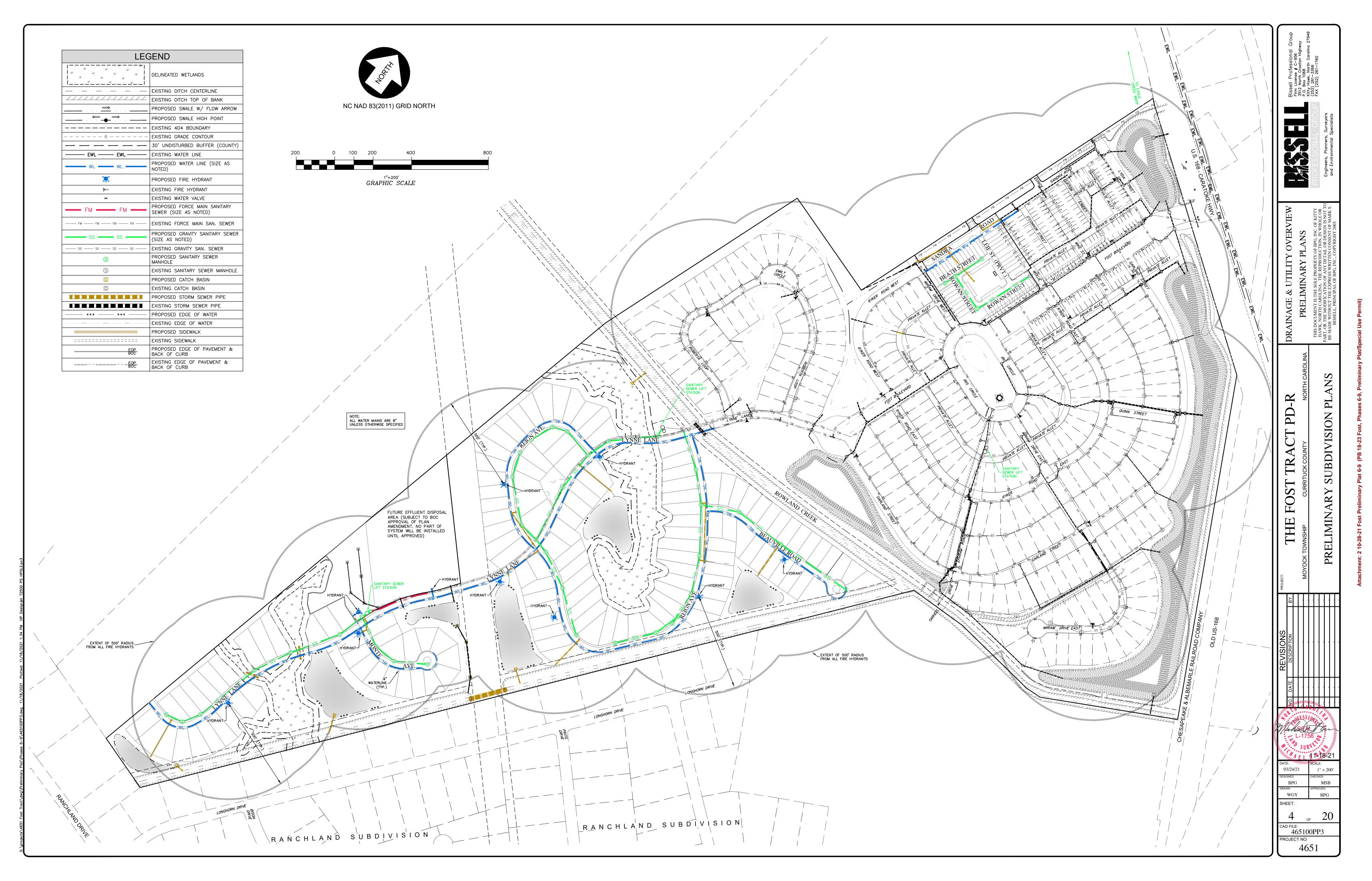
BACK OF CURB EDGE OF PAVEMENT EOP EXISTING EX.

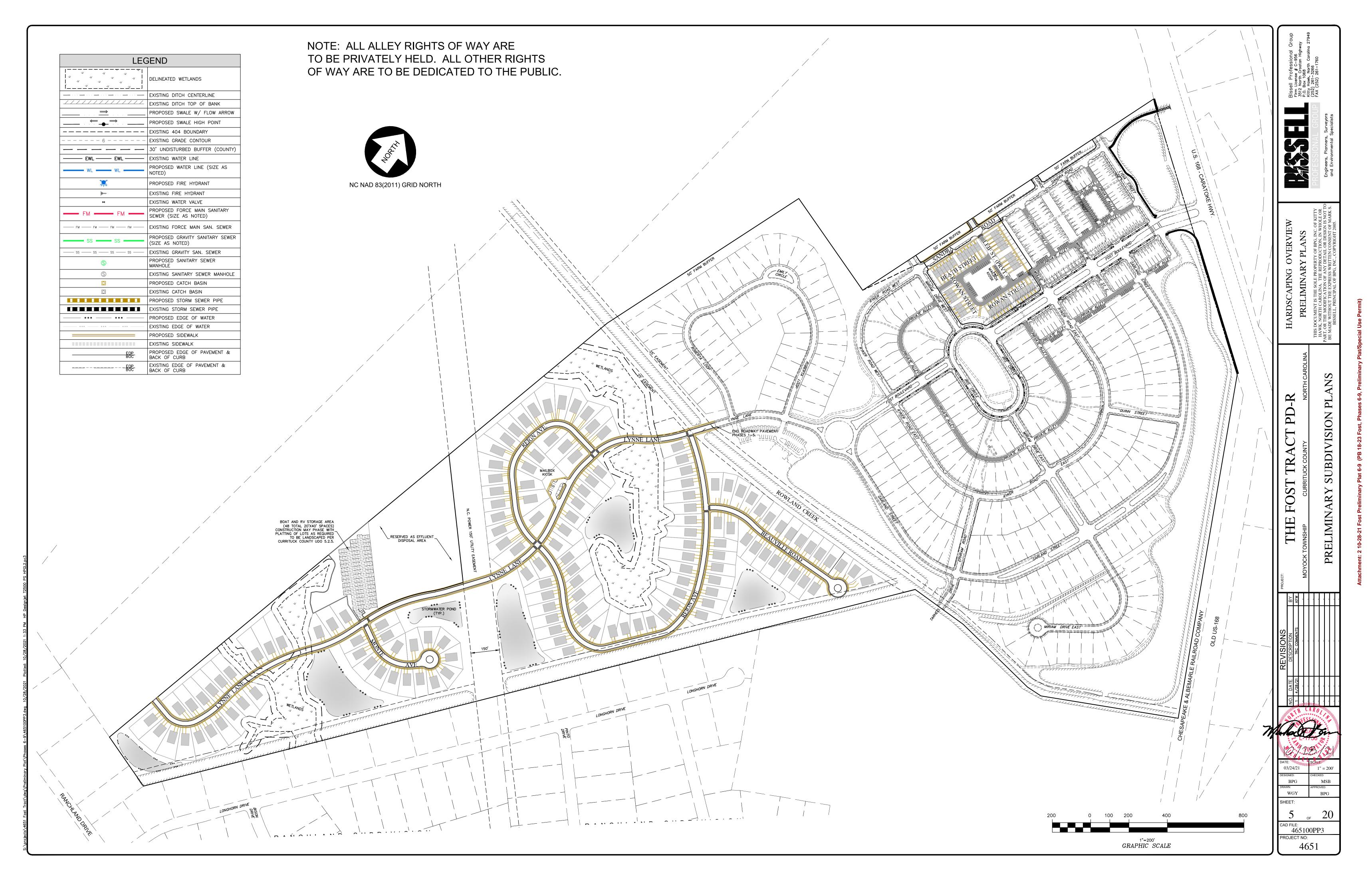
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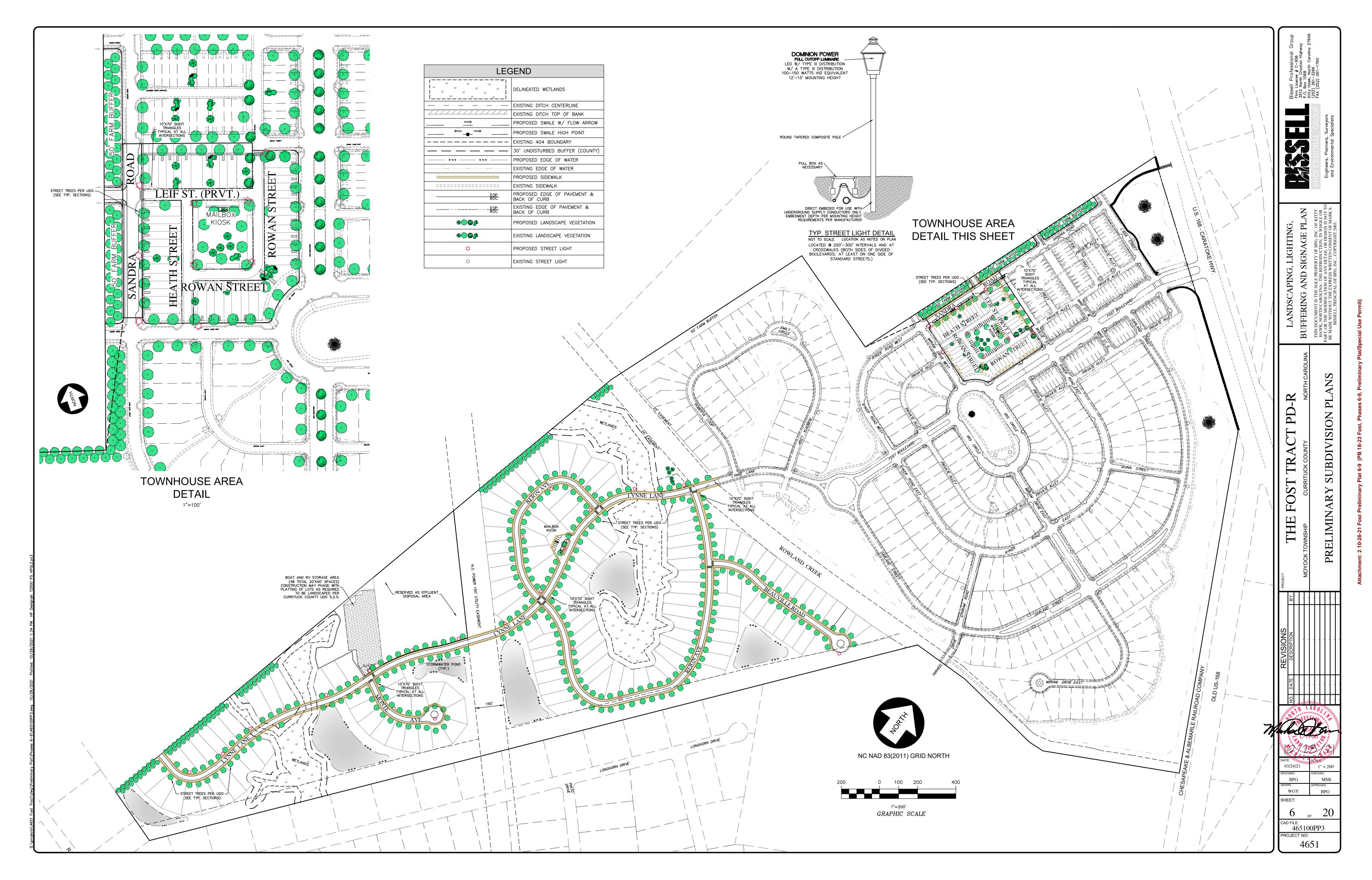
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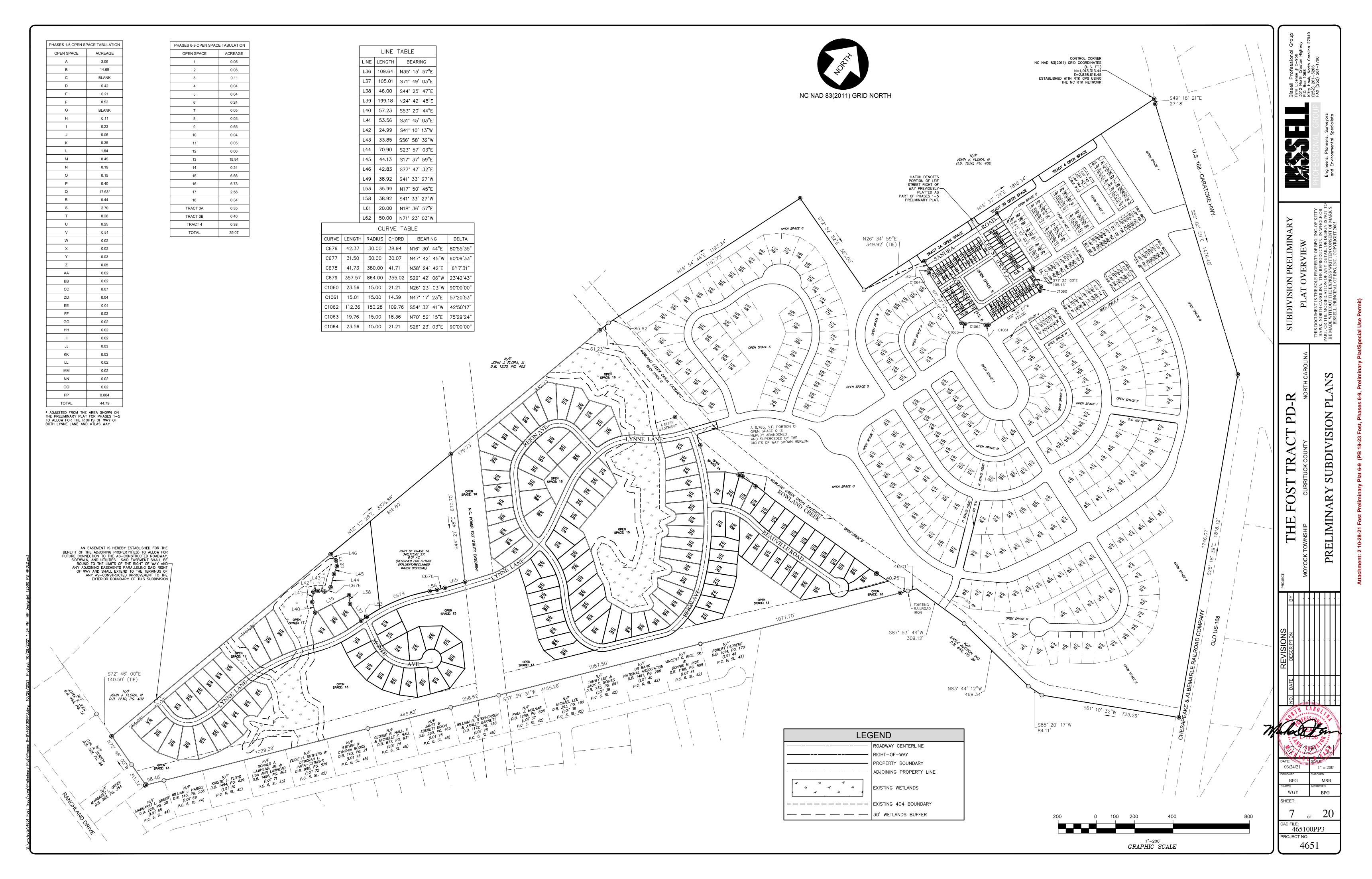


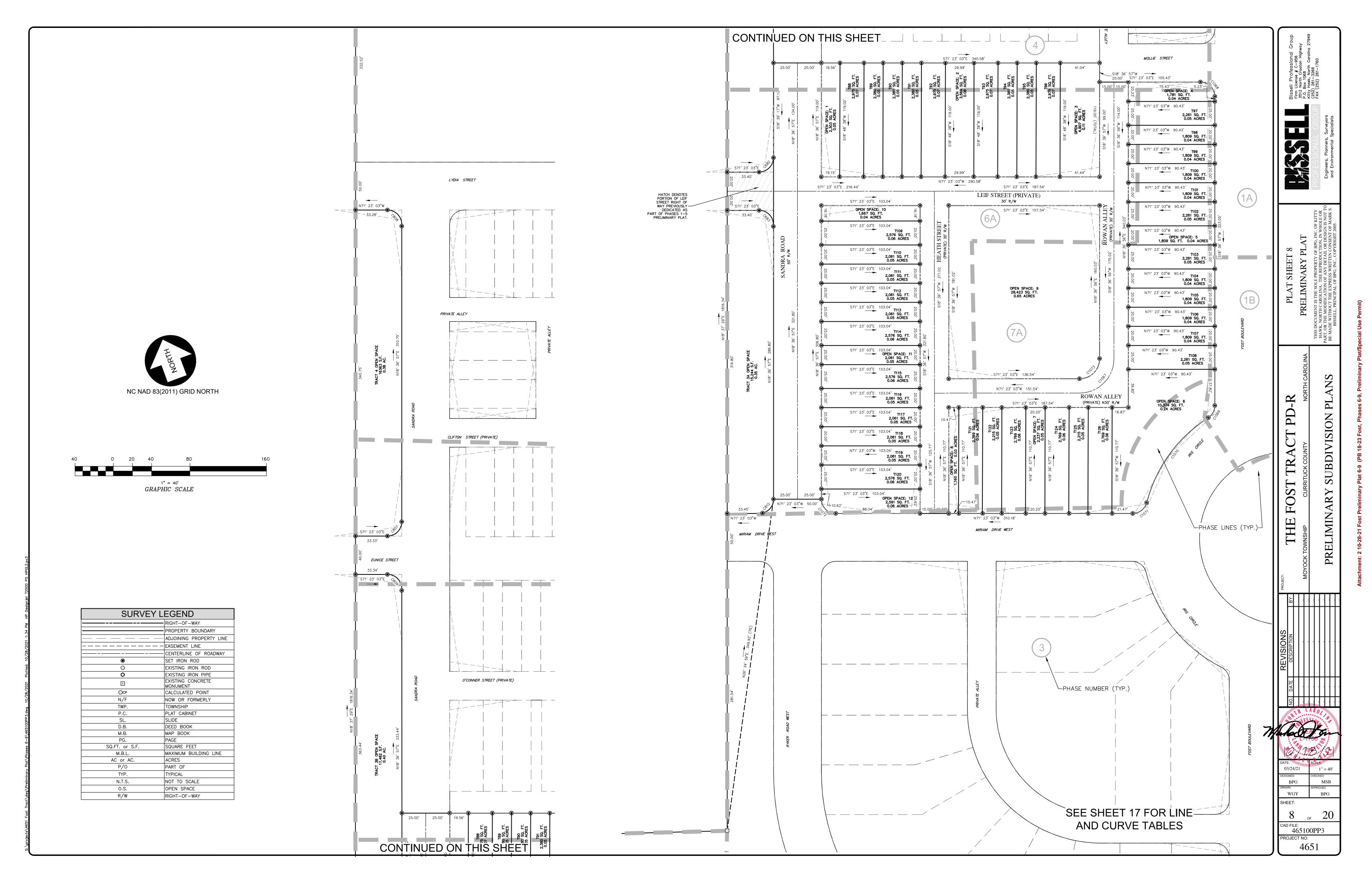


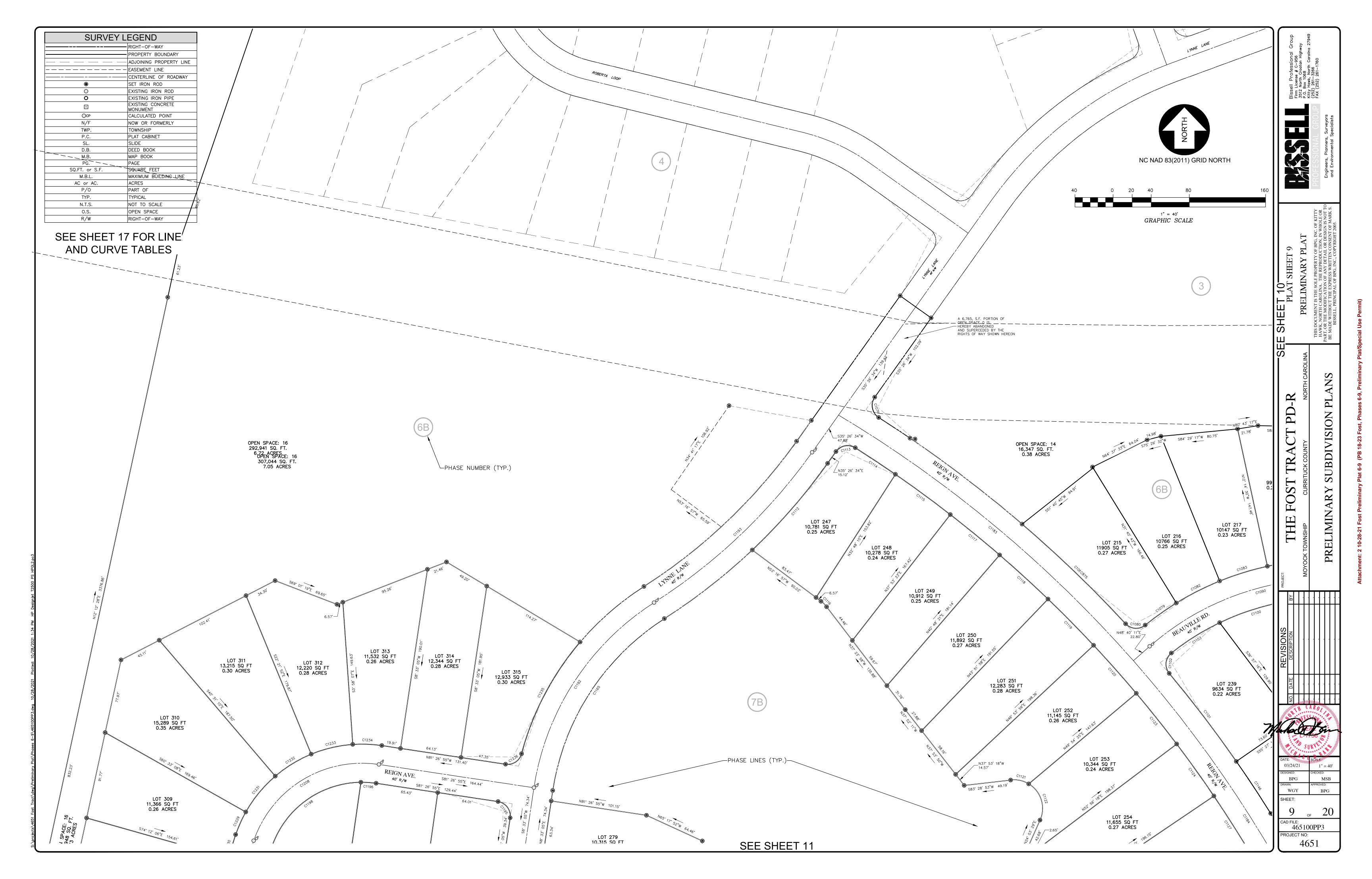




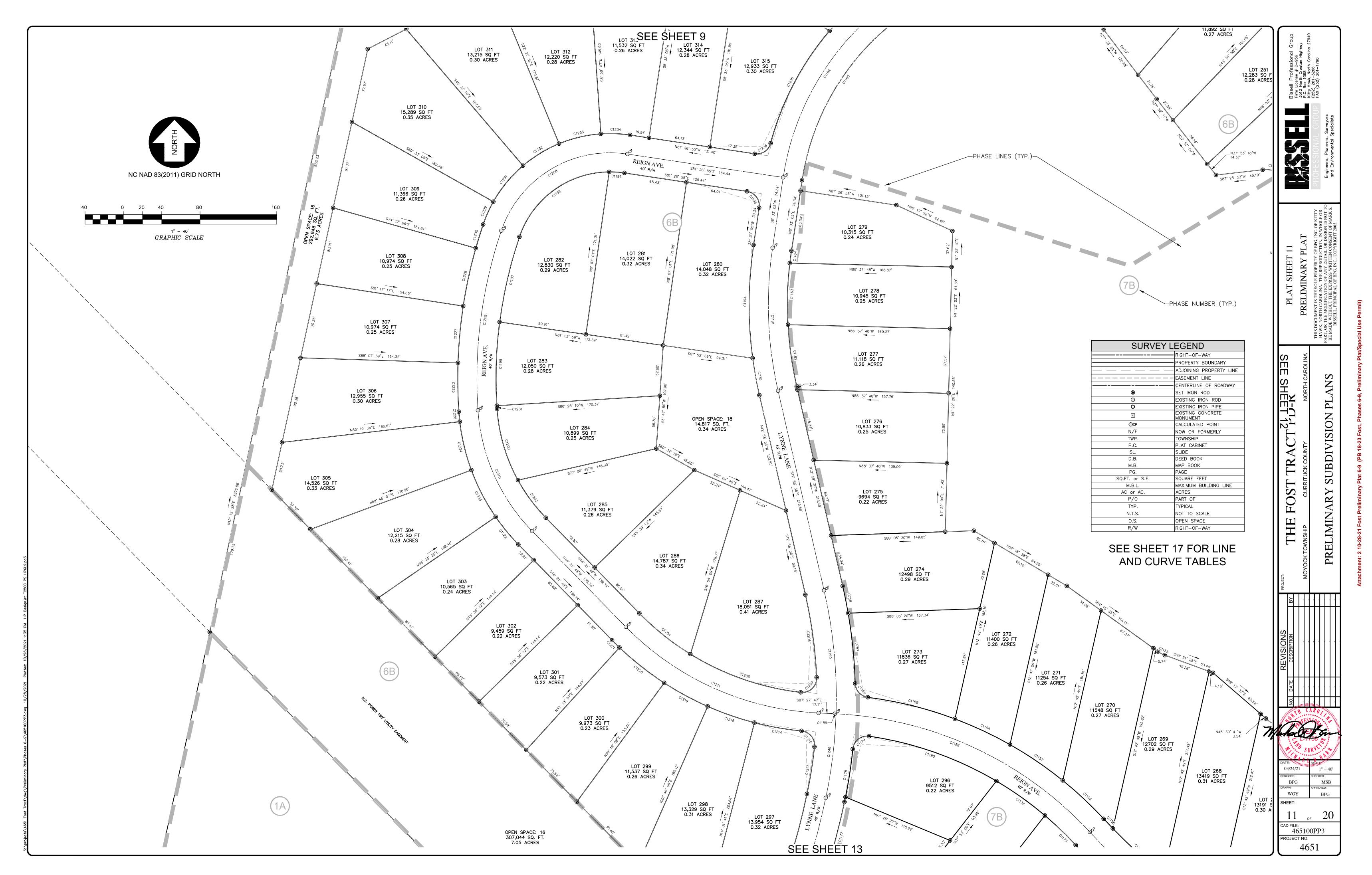


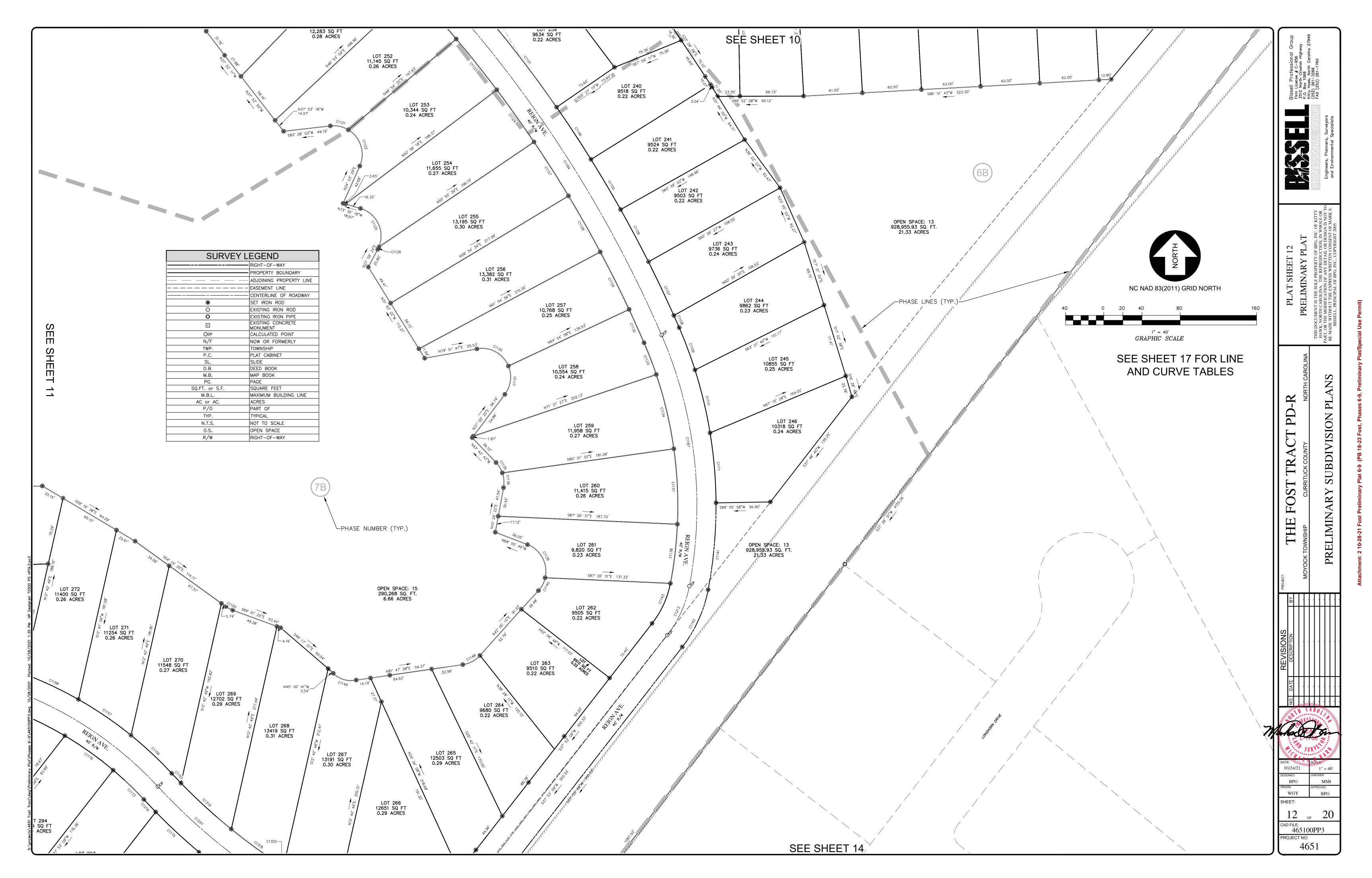


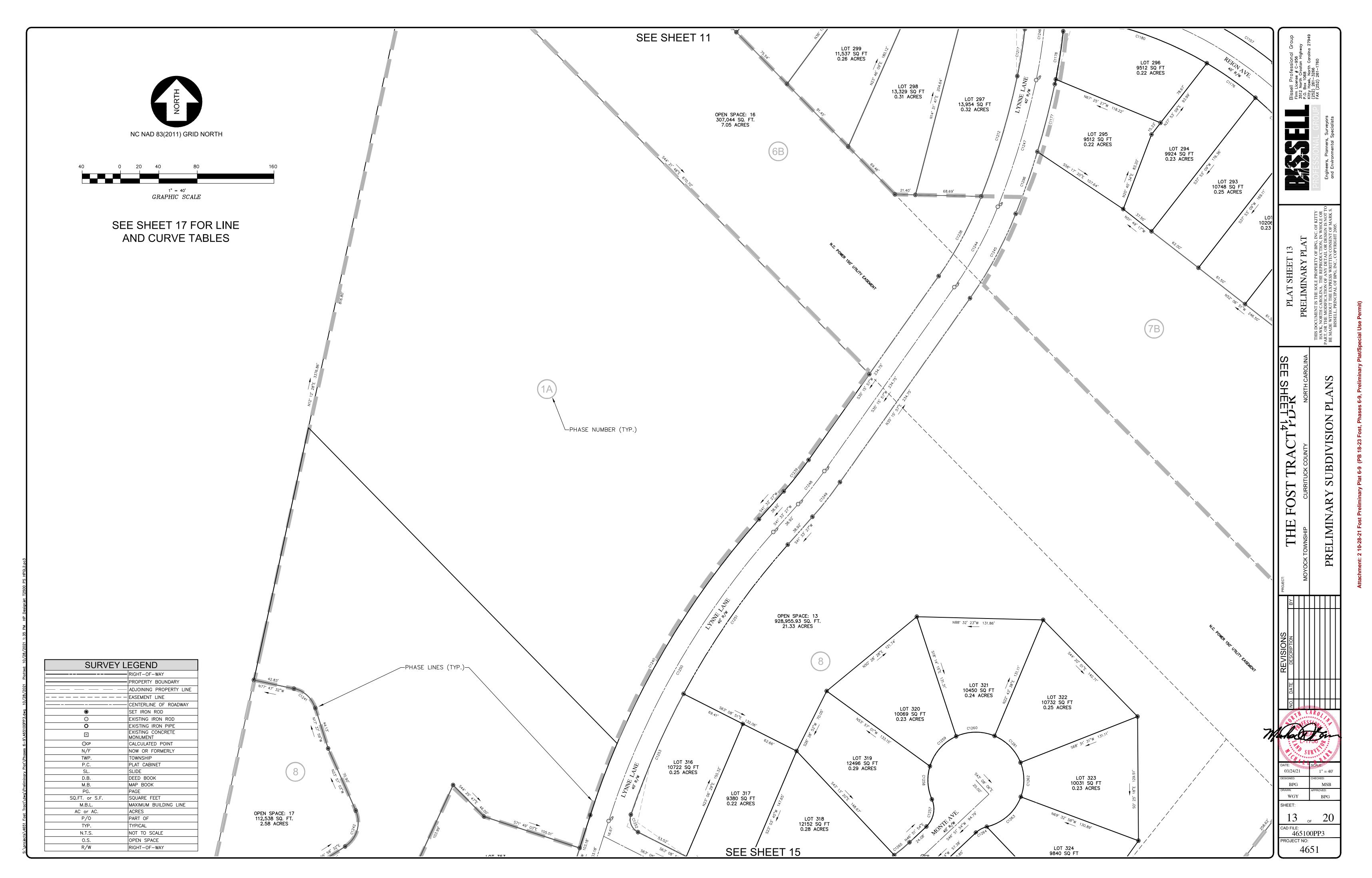


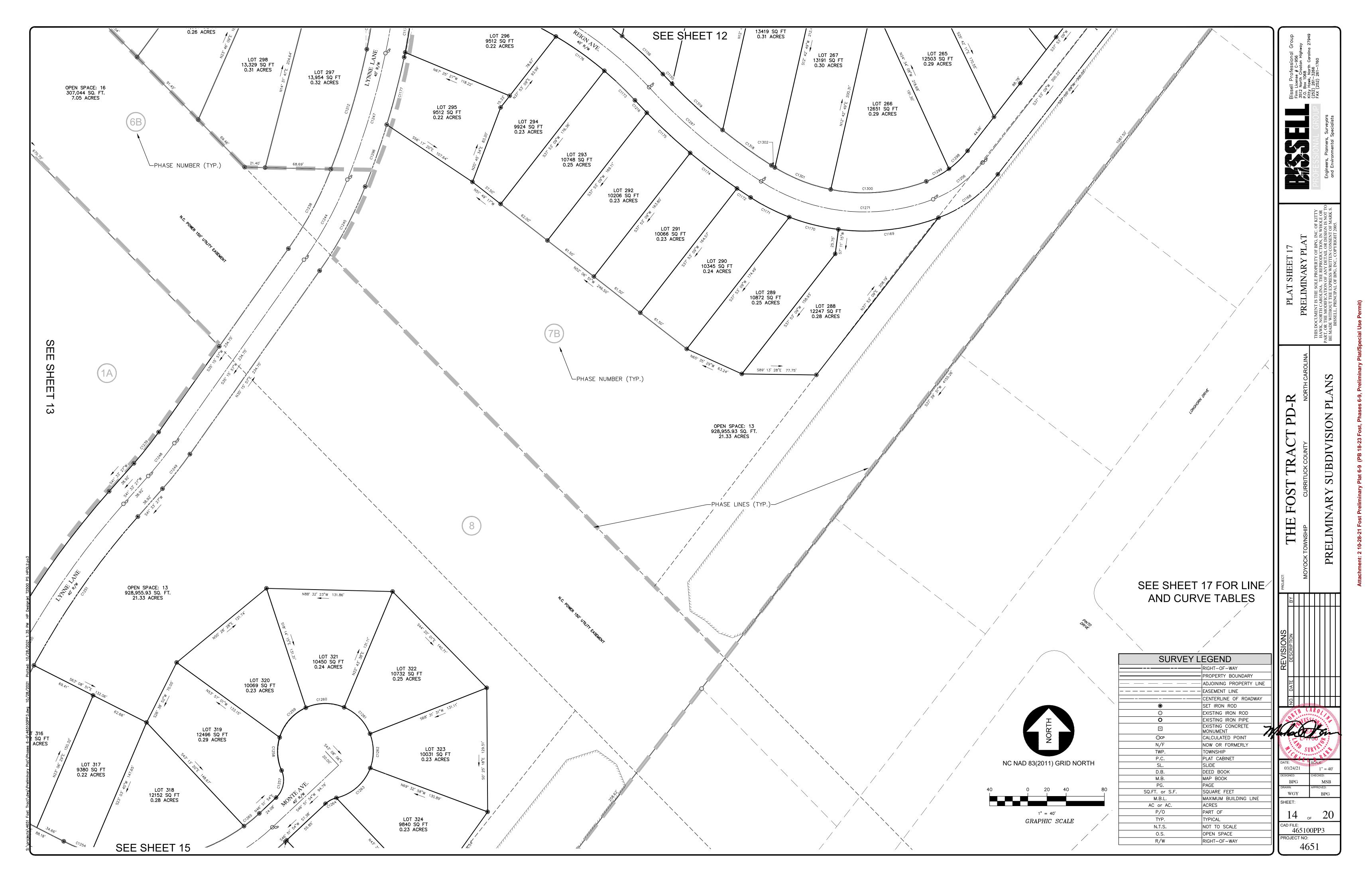


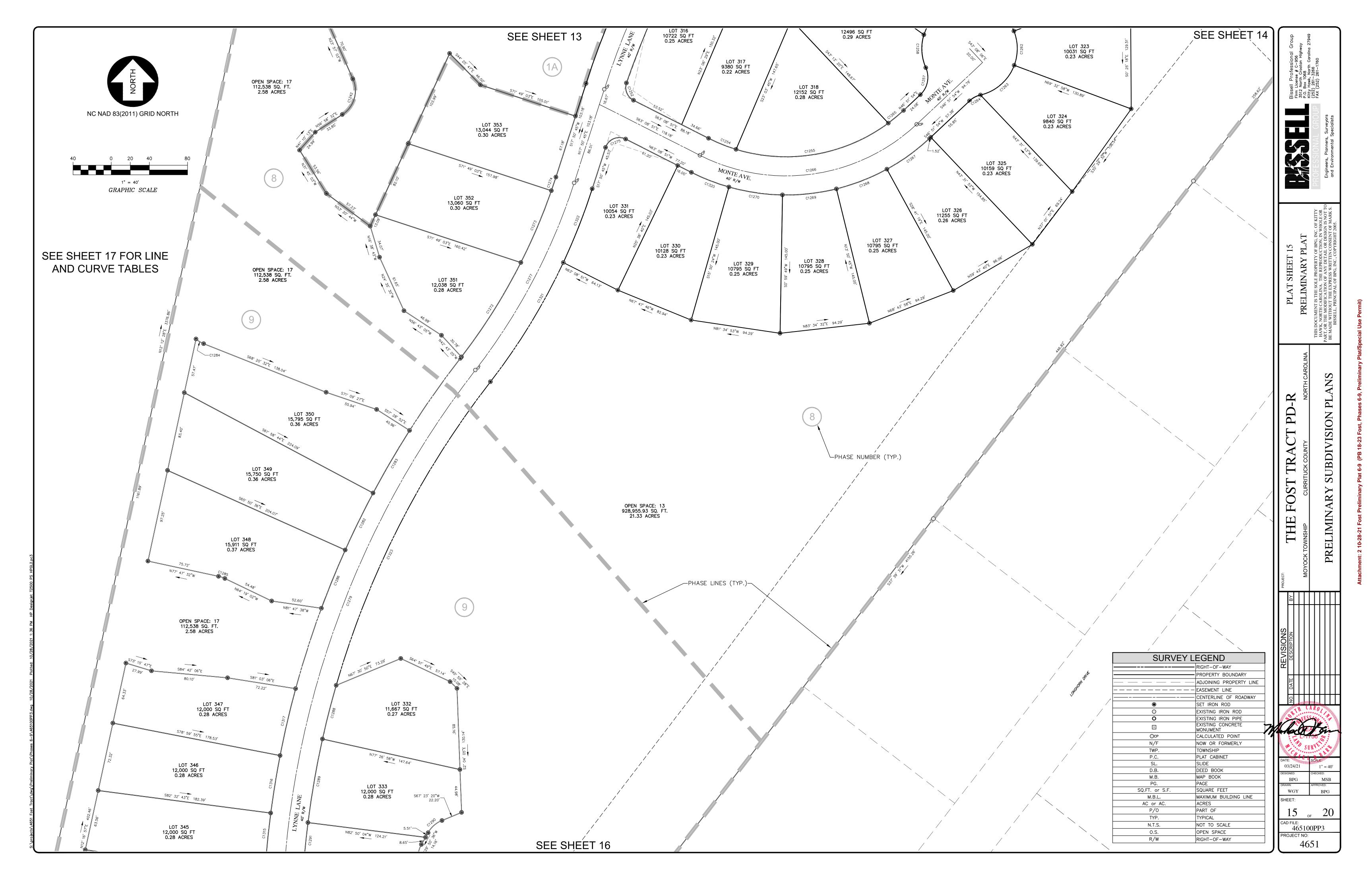


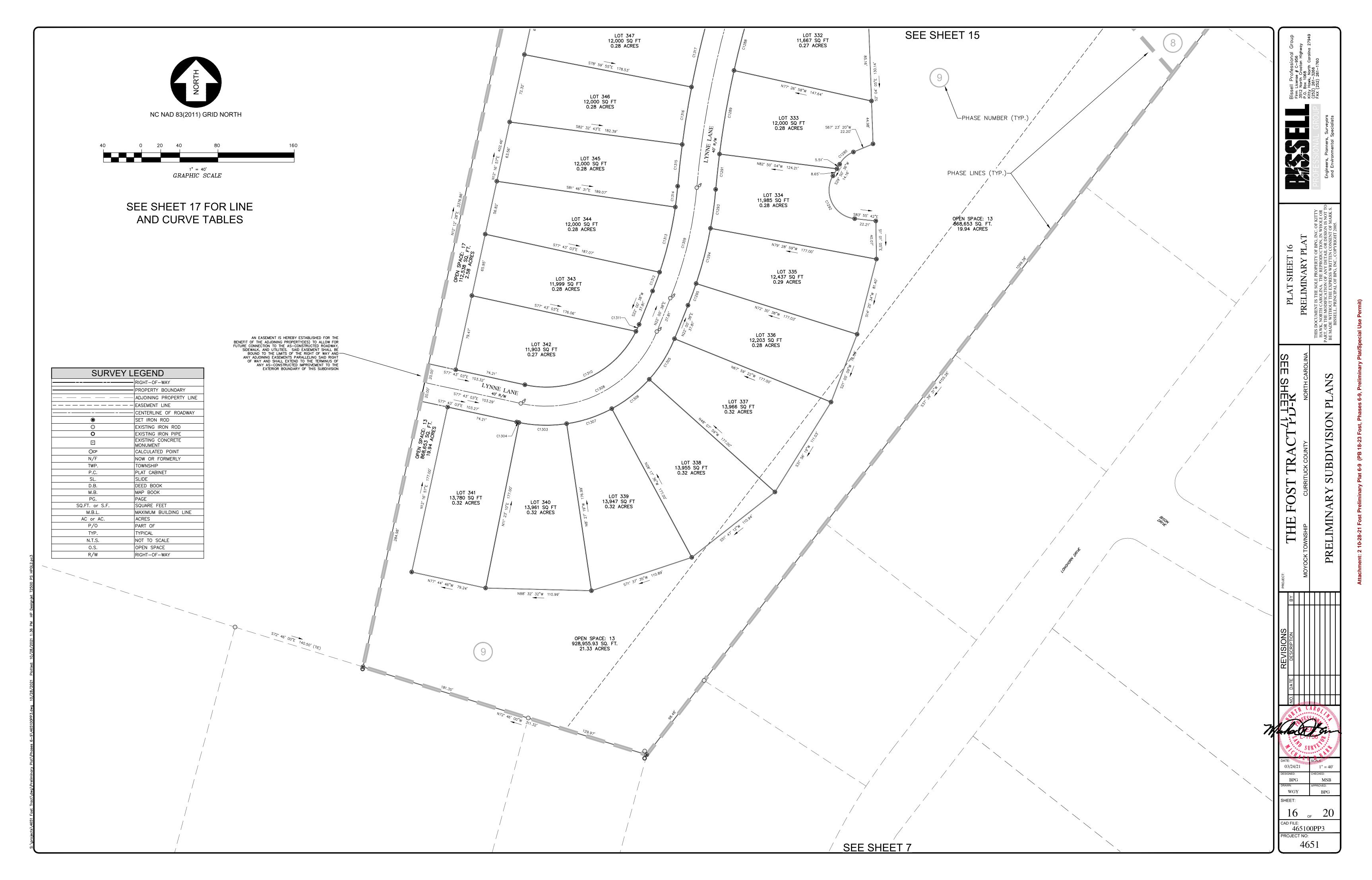












			RVE TA		
CURVE	LENGTH	RADIUS	CHORD	BEARING	DELTA
C757 C758	73.88	520.00 520.00	73.82 28.74	S5° 44' 20"E S11° 23' 35"E	8°08'27" 3°10'02"
C738	39.32	420.00	39.31	S10° 17' 39"E	5*21'53"
C875	150.98	1326.00	150.90	S46° 04' 43"E	6°31'25"
C1076	24.32	15.00	21.74	S11° 00' 14"E	92°53'36"
C1079	40.18	305.00	40.15	S54° 32' 51"W	7°32'52"
C1080	22.62	15.00	20.54	S86° 01' 17"E	86°24'35"
C1081	150.98	1326.00	150.90	N46° 04' 43"W	6*31'25"
C1082	50.99	278.42	50.92	S63° 14' 27"W	10°29'38"
C1083	52.48	286.51	52.40	S72° 42' 11"W	10°29'38"
C1084	55.78	305.00	55.70	S82° 59' 52"W	10°28'44"
C1085 C1086	49.04 19.02	305.00 971.57	48.98 19.02	N87° 08' 31"W	9°12'43" 1°07'19"
C1087	64.63	550.00	64.59	S87° 53' 32"E	6°43'58"
C1088	64.63	550.00	64.59	S87° 53' 32"E	6°43'58"
C1089	18.04	550.00	18.04	N87° 48' 06"E	1°52'46"
C1090	242.71	285.00	235.44	S73° 04' 01"W	48°47'40"
C1091	105.47	570.00	105.32	S87° 50' 13"E	10°36'07"
C1092	18.86	25.00	18.42	N65° 15' 03"E	43°13'20"
C1093	31.60	50.00	31.08	N61° 44' 45"E	36°12'44"
C1095	34.32	50.10	33.65	N80° 33′ 30″W	39°14'50"
C1096	106.65	50.00	87.55	N39° 45' 53"E	122°12'31"
C1097 C1098	51.39 57.78	590.00 590.00	51.38 57.76	N89° 21' 27"E S85° 20' 30"E	4°59'27" 5°36'41"
C1098	73.91	265.00	73.67	S85° 20° 30° E S89° 28' 28" W	15°58'45"
C1100	79.26	265.00	78.97	S72° 54' 58"W	17°08'16"
C1101	116.13	1260.90	116.09	N37° 18' 51"W	5°16'37"
C1102	23.87	15.00	21.43	S5° 46′ 07″W	91 <b>°</b> 10'59"
C1103	60.07	265.00	59.94	S57° 51' 14"W	12 <b>°</b> 59'13"
C1105	60.92	1326.00	60.92	S30° 39' 38"E	2°37'57"
C1106	62.02	1326.00	62.02	S28° 00' 16"E	2°40'48"
C1107	62.16	1326.00	62.15	S25° 19' 17"E	2°41'09"
C1108	13.85	1326.00	13.85	S23° 40' 46"E	0°35'54"
C1109	47.35	470.00	47.33	S20° 29' 39"E	5°46'19"
C1110	67.69 73.55	470.00 470.00	67.64 73.47	S13° 28' 56"E N4° 52' 23"W	8°15'08" 8°57'57"
C11112	120.77	620.00	120.58	N41° 01' 23"E	11°09'39"
C1113	22.76	15.00	20.64	S78° 54' 59"W	86°56'50"
C1114	50.47	1286.00	50.46	N56° 29' 09"W	2°14'54"
C1115	71.48	1286.00	71.47	N53° 46′ 10″W	3°11'04"
C1116	8.32	30.00	8.29	N45° 20′ 28″W	15°52'59"
C1117	66.95	1286.00	66.95	N50° 41' 08"W	2 <b>°</b> 58'59"
C1118	68.69	1286.00	68.68	N47° 39' 50"W	3°03'37"
C1119	68.09	1286.00	68.08	N44° 37' 01"W	3°02'01"
C1120	67.50 19.66	1286.00 30.00	67.49 19.31	N41° 35' 48"W	3°00'26"
C1121 C1122	43.47	30.00	39.77	N77° 44′ 35″W N17° 27′ 17″W	37°33'04" 83°01'32"
C1123	68.04	1286.00	68.03	N38° 34' 38"W	3°01'54"
C1124	67.09	1286.00	67.08	N35° 34' 01"W	2*59'20"
C1125	50.30	30.00	44.61	N25° 28' 22"W	96°03'51"
C1126	2.84	30.00	2.84	N25° 16' 29"E	5°25'51"
C1127	66.93	1286.00	66.93	N32° 34′ 53″W	2°58'56"
C1128	67.47	1286.00	67.46	N29° 35' 15"W	3°00'21"
C1129	67.35	1286.00	67.34	N26° 35' 03"W	3°00'03"
C1130	40.48	30.00	37.48	N61° 29' 06"W	77°18'14"
C1131	31.33	30.00	29.93	N7° 05' 11"E	59°50'19"
C1132 C1133	38.24 35.55	1286.00 430.00	38.24 35.54	N24° 13' 55"W N21° 00' 41"W	1°42'13" 4°44'15"
C1134	80.06	430.00	79.95	N13° 18' 31"W	10°40'05"
C1135	12.71	30.00	12.61	N31° 34' 30"W	24°16'26"
C1136	15.63	30.00	15.45	N4° 30′ 58″W	29 <b>°</b> 50′39″
C1137	79.05	430.00	78.93	N2° 42' 29"W	10°31'57"
C1138	62.40	430.00	62.35	N6° 42' 56"E	8°18'54"
C1139	42.79	30.00	39.25	N28° 04' 16"W	81°42'57"
C1140	15.30	30.04	15.13	N27° 24' 00"E	29°10'17"
C1141	91.46	470.00	91.32	N5° 11' 05"E	11°08'59"
C1142	66.28	140.00	65.66	N24° 19' 22"E	27°07'34"
C1143	46.49	100.00	46.07 39.38	S24° 34' 00"W	26°38'17"
C1145 C1146	40.47 65.46	50.00 1182.57	65.45	N37° 43′ 29″W S33° 23′ 28″E	46°22'39" 3°10'17"
C1148	20.83	30.00	20.42	N61° 53' 49"E	39°47'18"
C1149	25.96	26.42	24.93	S73° 39' 35"E	56°17'48"
C1155	13.70	586.00	13.70	N43° 23' 04"W	1°20'23"
0445-	8.17	30.00	8.14	S62° 03' 30"E	15°35'50"
C1155			<u></u> _		

CURVE C1163 C1164 C1165 C1168 C1169 C1170	LENGTH	CUI	RVE TA	BLE	
C1164 C1165 C1168 C1169 C1170		RADIUS	CHORD	BEARING	DELTA
C1165 C1168 C1169 C1170	64.47	380.00	64.39	S1° 43' 39"W	9°43'12"
C1168 C1169 C1170	13.02	380.00	13.02	S7° 34' 10"W	1°57'49"
C1169 C1170	221.81	260.00	215.15	S32° 59' 30"W	48*52'51"
C1170	76.13	140.00	75.19	N53° 27' 49"E	31°09'20"
	106.52	220.00	105.48	S83° 18' 59"W	27°44'32'
044-	52.97	220.00	52.85	S75° 54' 52"E	13°47'47'
C1171	45.34	220.00	45.26	S63° 06' 44"E	11°48'27"
C1172	17.12	626.00	17.12	S56° 25' 30"E	1°34'02"
C1173	28.39	365.00	28.38	N44° 56′ 34″W	4°27'25"
C1174	61.53	626.00	61.50	S52° 49' 32"E	5°37'54"
C1175	61.75	626.00	61.73	S47° 11' 01"E	5°39'08"
C1176	62.13	365.00	62.06	N54° 34' 42"W	9°45'12"
C1177	72.82	667.86	72.78	N14° 29' 53"E	6°14'49"
C1178	50.81	521.68	50.79	N8° 39' 57"E	5*34'49"
C1179	24.20	15.00	21.66	S52° 05' 30"W	92°26'33'
C1180	141.63	365.00	140.74		
				N70° 34' 16"W	22°13'56"
C1183	404.29	1306.00	402.68	N50° 11' 55"W	17°44'12"
C1184	409.16	1306.00	407.48	N32° 21' 19"W	17°57'01"
C1185	39.89	50.00	38.84	N56° 16′ 42″W	45°42'19"
C1186	31.27	30.00	29.87	N63° 16' 54"W	59°42'45'
C1187	268.13	450.00	264.18	N6° 18' 37"W	34°08'23'
C1188	300.69	385.00	293.11	N65° 05' 20"W	44°44'55'
C1189	1.10	385.00	1.10	N87° 22′ 53"W	0°09'48"
C1190	129.70	500.00	129.34	N5° 32' 43"W	14°51'45"
C1191	150.29	400.00	149.41	S2° 12' 45"E	21°31'41"
C1192	238.88	280.00	231.70	S32° 59′ 30″W	48°52'51"
C1193	230.27	600.00	228.86	N46° 26' 15"E	21 <b>°</b> 59'22"
C1194	118.48	420.00	118.09	S0° 28' 11"W	16°09'48"
C1195	23.56	15.00	21.21	N36° 26' 55"W	90°00'00'
C1196	16.05	100.00	16.04	N86° 02' 51"W	9°11'51"
C1197	91.43	460.00	91.28	S13° 03' 42"W	11°23'18"
C1198	123.22	100.00	115.57	S54° 03' 17"W	70°35'53'
C1199	87.49	460.00	87.36	S1° 55′ 06″W	10°53′52″
C1200	78.07	180.00	77.46	S16° 23' 21"E	24°50'57'
C1201	3.46	460.00	3.46	S3° 44' 45"E	0°25'50"
C1202	48.85	180.00	48.70	S36° 35' 18"E	15°32'59"
C1203	68.18	280.00	68.01	S51° 20' 21"E	13°57'05"
C1204	68.18	280.00	68.01	S51° 20' 21"E	13°57'05"
C1205	122.77	280.00	121.79	S70° 52' 34"E	25°07'21"
C1206	87.25	480.00	87.13	N7° 46' 08"W	10°24'55"
C1207	25.95	15.00	22.83	N47° 00' 03"E	99°07'26'
C1208	167.12	120.00	153.94	S58° 39' 13"W	79 <b>°</b> 47'44'
C1209	190.29	480.00	189.05	S7° 23' 55"W	22°42'52'
C1210	141.04	200.00	138.13	S24° 09' 39"E	40°24'17"
C1211	225.67	300.00	220.39	S65° 54' 48"E	43°05'59'
C1212	130.98	808.00	130.84	N16° 40' 52"E	9°17'16"
C1213	56.81	120.00	56.28	N24° 19' 22"E	27°07'34'
C1214	50.09	320.00	50.04	S79° 57' 23"E	8°58'08"
C1215	23.70	15.00	21.31	N39° 10′ 21″W	90°32'13"
C1216	17.95	626.00	17.95	S43° 32' 10"E	1°38'36"
C1210	49.75	480.00	49.73	N9° 03' 55"E	5°56'17"
				S70° 51' 05"E	9°14'28"
C1218	51.61	320.00	51.56		
C1219	51.61	320.00	51.56	S61° 36′ 37″E	9°14'28"
C1220	59.60	320.00	59.51	S51° 39' 15"E	10°40'16"
C1221	10.92	320.00	10.92	S45° 20' 28"E	1°57'19"
C1222	37.45	220.00	37.41	S39° 29' 11"E	9°45'13"
C1223	55.14	220.00	55.00	S27° 25' 44"E	14°21'41"
C1224	52.12	220.00	52.00	S13° 27' 39"E	13°34'28"
C1225	50.86	500.00	50.84	S1° 02' 31"E	5°49'43"
	10.45	220.00	10.45	S5° 18' 48"E	2°43'15"
C1226	59.68	500.00	59.65	S5° 17' 32"W	6 <b>°</b> 50'22"
C1226 C1227	61.84	500.00	61.80	S12° 15' 18"W	7°05'11"
	26.13	140.00	26.09	S24° 06' 07"W	10°41'31"
C1227	25.81	500.00	25.81	S17° 16' 37"W	2°57'27"
C1227 C1228	20.01	140.00	48.70	S39° 27′ 51″W	20 <b>°</b> 01'58"
C1227 C1228 C1229	48.95		4440	S58° 33' 29"W	18 <b>°</b> 09'18"
C1227 C1228 C1229 C1230		140.00	44.18		•
C1227 C1228 C1229 C1230 C1231 C1232	48.95 44.36				
C1227 C1228 C1229 C1230 C1231 C1232 C1233	48.95 44.36 44.95	140.00	44.76	S76° 50′ 00″W	18 <b>°</b> 23'45"
C1227 C1228 C1229 C1230 C1231 C1232 C1233 C1234	48.95 44.36 44.95 30.59	140.00	44.76 30.53	S76° 50' 00"W N87° 42' 31"W	18°23'45" 12°31'12"
C1227 C1228 C1229 C1230 C1231 C1232 C1233 C1234 C1235	48.95 44.36 44.95 30.59 133.58	140.00 140.00 300.00	44.76 30.53 132.48	S76° 50' 00"W N87° 42' 31"W S27° 41' 13"W	18°23'45" 12°31'12" 25°30'44'
C1227 C1228 C1229 C1230 C1231 C1232 C1233 C1234 C1235 C1236	48.95 44.36 44.95 30.59 133.58 21.89	140.00 140.00 300.00 15.00	44.76 30.53 132.48 20.00	\$76° 50' 00"W \$87° 42' 31"W \$27° 41' 13"W \$56° 44' 28"E	18°23'45" 12°31'12" 25°30'44' 83°37'14"
C1227 C1228 C1229 C1230 C1231 C1232 C1233 C1234 C1235 C1236 C1238	48.95 44.36 44.95 30.59 133.58 21.89 90.70	140.00 140.00 300.00 15.00 380.00	44.76 30.53 132.48 20.00 90.48	S76* 50' 00"W N87* 42' 31"W S27* 41' 13"W N56* 44' 28"E N28* 25' 41"E	18°23'45" 12°31'12" 25°30'44' 83°37'14" 13°40'31"
C1227 C1228 C1229 C1230 C1231 C1232 C1233 C1234 C1235 C1236 C1238 C1238	48.95 44.36 44.95 30.59 133.58 21.89 90.70 41.73	140.00 140.00 300.00 15.00 380.00	44.76 30.53 132.48 20.00 90.48 41.71	S76* 50' 00"W  N87* 42' 31"W  S27* 41' 13"W  N56* 44' 28"E  N28* 25' 41"E  N38* 24' 42"E	18°23'45" 12°31'12" 25°30'44' 83°37'14" 13°40'31" 6°17'31"
C1227 C1228 C1229 C1230 C1231 C1232 C1233 C1234 C1235 C1236 C1238 C1239 C1240	48.95 44.36 44.95 30.59 133.58 21.89 90.70 41.73 357.57	140.00 140.00 300.00 15.00 380.00 380.00 864.00	44.76 30.53 132.48 20.00 90.48 41.71 355.02	S76* 50' 00"W  N87* 42' 31"W  S27* 41' 13"W  N56* 44' 28"E  N28* 25' 41"E  N38* 24' 42"E  S29* 42' 06"W	18°23'45" 12°31'12" 25°30'44" 83°37'14" 13°40'31" 6°17'31" 23°42'43"
C1227 C1228 C1229 C1230 C1231 C1232 C1233 C1234 C1235 C1236 C1238 C1238	48.95 44.36 44.95 30.59 133.58 21.89 90.70 41.73	140.00 140.00 300.00 15.00 380.00	44.76 30.53 132.48 20.00 90.48 41.71	S76* 50' 00"W  N87* 42' 31"W  S27* 41' 13"W  N56* 44' 28"E  N28* 25' 41"E  N38* 24' 42"E	18°23'45" 12°31'12" 25°30'44" 83°37'14" 13°40'31"

		CU	IRVE TA	BLE	
CURVE	LENGTH	RADIUS	CHORD	BEARING	DELTA
C1249	46.12	420.00	46.10	N38° 24' 42"E	6°17'31"
C1250	349.29	844.00	346.80	S29° 42′ 06″W	23°42'43'
C1251	327.56	824.00	325.41	S30° 10' 10"W	22°46'35'
C1252	21.45	15.00	19.67	S22° 11' 00"E	81°55'44"
C1253	137.53	740.72	137.33	S23° 33′ 41″W	10°38'17"
C1254	31.06	180.00	31.02	N68° 05' 27"W	9°53'12"
C1255	168.04	180.00	162.00	N80° 13' 16"E	53°29'21"
C1256	65.25	120.00	64.45	N53° 27' 49"E	31°09'20"
C1257	30.77	25.00	28.87	N11° 36' 02"E	70°31'44"
C1258	35.34	50.00	34.61	N3° 25' 01"W	40°29'37'
C1259	42.72	50.00	41.43	S41° 18' 15"W	48°56'55'
C1260	41.22	50.00	40.07	N89° 23' 53"E	47°14'22"
C1261	39.71	50.00	38.68	N44° 13′ 42″W	45°30'27'
C1262	36.59	50.00	35.78	N0° 30' 43"W	41°55'31"
C1263	49.17	50.00	47.21	N48° 37' 16"E	56°20'27'
C1264	13.06	25.00	12.91	S61° 49' 42"W	29°55'35'
C1265	20.77	180.00	20.76	N50° 10' 15"E	6°36'42"
C1266	244.30	200.00	229.39	N81° 51' 31"E	69 <b>°</b> 59 <b>'</b> 15"
C1267	55.47	220.00	55.32	S54° 05' 17"W	14°26'47"
C1268	56.99	220.00	56.83	S68° 43' 58"W	14°50'34"
C1269	56.99	220.00	56.83	S83° 34′ 32"W	14°50'34"
C1270	56.99	220.00	56.83	S81° 34' 53"E	14°50'34"
C1271	187.62	200.00	180.82	S84° 05' 01"E	53°45'00'
C1272	117.09	620.00	116.91	N32° 12' 37"E	10°49'13"
C1273	81.92	620.00	81.86	N23° 00' 53"E	7°34'14"
C1274	14.97	620.00	14.97	N18° 32' 15"E	1*23'01"
C1275	25.92	15.00	22.81	S67° 20' 57"W	99°00'24'
C1277	222.91	640.00	221.78	N27° 49' 25"E	19°57'20"
C1279	554.90	966.50	547.31	S21° 21' 13"W	32°53'44'
C1273		986.50	66.53		3°51'53"
	66.54			S26° 05' 20"W	
C1283	75.94	986.50	75.92	S30° 13′ 35″W	4°24'38"
C1284	9.48	30.00	9.44	S59° 17' 26"E	18 <b>°</b> 06'12"
C1285	7.06	30.00	7.04	S71° 03' 17"E	13°28'30"
C1286	65.87	986.50	65.86	S22° 14' 37"W	3°49'33"
C1288	58.08	946.50	58.07	S14° 18′ 31″W	<b>3°30'57"</b>
C1289	88.96	946.50	88.92	S9° 51' 29"W	5°23'06"
C1290	19.66	30.00	19.31	S48° 36' 58"W	37°32'44'
C1291	37.33	946.50	37.33	S6° 02' 09"W	2°15'35"
C1292	59.57	30.00	50.26	S27° 02' 33"E	113°46'18
C1293	41.13	420.00	41.12	N7° 42' 41"E	5°36'40"
C1294	59.92	420.00	59.87	N14° 36' 16"E	8°10'29"
C1295	24.33	420.00	24.32	N20° 21' 04"E	3°19'07"
C1296	68.55	1188.61	68.54	N19° 16' 30"E	3°18'16"
C1297	153.30	606.00	152.89	S49° 57' 42"E	14 <b>°</b> 29'39"
C1298	27.19	100.00	27.10	S45° 40' 29"W	15°34'40"
C1299	27.19	100.00	27.10	S61° 15' 09"W	15°34'40"
C1299	101.55	180.00	100.21	S85° 12' 15"W	32°19'32"
C1300	63.04	180.00	62.72	S68° 36' 00"E	
					20°04'00'
C1302	4.27	180.00	4.27	N57° 53′ 15″W	1°21′29″
C1303	50.25	145.00	50.00	S88° 32' 32"E	19°51'23"
		145.00	2.27	S78° 09' 57"E	0°53'47"
C1304	2.27				
C1304 C1305	50.25	145.00	50.00	N31° 56' 19"E	
C1304 C1305 C1306	50.25 50.21		50.00 49.96		19°50'23"
C1304 C1305	50.25	145.00	50.00	N31° 56' 19"E N51° 47' 12"E N71° 37' 05"E	19°50'23"
C1304 C1305 C1306	50.25 50.21	145.00 145.00	50.00 49.96	N31° 56′ 19″E N51° 47′ 12″E	19°50'23"
C1304 C1305 C1306 C1307	50.25 50.21 50.17	145.00 145.00 145.00	50.00 49.96 49.92	N31° 56' 19"E N51° 47' 12"E N71° 37' 05"E	19°50'23" 19°49'23" 80°16'19"
C1304 C1305 C1306 C1307 C1308	50.25 50.21 50.17 175.13	145.00 145.00 145.00 125.00	50.00 49.96 49.92 161.15	N31° 56′ 19″E N51° 47′ 12″E N71° 37′ 05″E N62° 08′ 47″E	19°50'23" 19°49'23" 80°16'19" 17°06'17"
C1304 C1305 C1306 C1307 C1308 C1309	50.25 50.21 50.17 175.13 119.41	145.00 145.00 145.00 125.00 400.00	50.00 49.96 49.92 161.15 118.97	N31° 56′ 19″E N51° 47′ 12″E N71° 37′ 05″E N62° 08′ 47″E N13° 27′ 29″E	19°50'23' 19°49'23' 80°16'19" 17°06'17" 75°55'44'
C1304 C1305 C1306 C1307 C1308 C1309	50.25 50.21 50.17 175.13 119.41 139.15	145.00 145.00 145.00 125.00 400.00	50.00 49.96 49.92 161.15 118.97 129.19	N31° 56′ 19″E N51° 47′ 12″E N71° 37′ 05″E N62° 08′ 47″E N13° 27′ 29″E N64° 19′ 05″E	19°50'23' 19°49'23' 80°16'19" 17°06'17" 75°55'44'
C1304 C1305 C1306 C1307 C1308 C1309 C1310 C1311	50.25 50.21 50.17 175.13 119.41 139.15 7.96	145.00 145.00 145.00 125.00 400.00 105.00	50.00 49.96 49.92 161.15 118.97 129.19 7.96	N31° 56′ 19″E N51° 47′ 12″E N71° 37′ 05″E N62° 08′ 47″E N13° 27′ 29″E N64° 19′ 05″E N24° 10′ 55″E	19°50'23' 19°49'23' 80°16'19" 17°06'17" 75°55'44' 4°20'35" 3°11'02"
C1304 C1305 C1306 C1307 C1308 C1309 C1310 C1311 C1312	50.25 50.21 50.17 175.13 119.41 139.15 7.96 21.12	145.00 145.00 145.00 125.00 400.00 105.00 380.00	50.00 49.96 49.92 161.15 118.97 129.19 7.96 21.11	N31° 56′ 19″E N51° 47′ 12″E N71° 37′ 05″E N62° 08′ 47″E N13° 27′ 29″E N64° 19′ 05″E N24° 10′ 55″E N20° 25′ 07″E	19°50'23' 19°49'23' 80°16'19" 17°06'17" 75°55'44' 4°20'35" 3°11'02"
C1304 C1305 C1306 C1307 C1308 C1309 C1310 C1311 C1312 C1313 C1314	50.25 50.21 50.17 175.13 119.41 139.15 7.96 21.12 70.31 22.01	145.00 145.00 145.00 125.00 400.00 105.00 380.00 380.00	50.00 49.96 49.92 161.15 118.97 129.19 7.96 21.11 70.21 22.01	N31° 56′ 19″E N51° 47′ 12″E N71° 37′ 05″E N62° 08′ 47″E N13° 27′ 29″E N64° 19′ 05″E N24° 10′ 55″E N20° 25′ 07″E N13° 31′ 32″E N6° 33′ 55″E	19°50'23' 19°49'23' 80°16'19" 17°06'17" 75°55'44' 4°20'35" 3°11'02" 10°36'07' 3°19'08"
C1304 C1305 C1306 C1307 C1308 C1309 C1310 C1311 C1312 C1313 C1314 C1315	50.25 50.21 50.17 175.13 119.41 139.15 7.96 21.12 70.31 22.01 43.89	145.00 145.00 145.00 125.00 400.00 105.00 380.00 380.00 986.50	50.00 49.96 49.92 161.15 118.97 129.19 7.96 21.11 70.21 22.01 43.88	N31° 56′ 19″E N51° 47′ 12″E N71° 37′ 05″E N62° 08′ 47″E N13° 27′ 29″E N64° 19′ 05″E N24° 10′ 55″E N20° 25′ 07″E N13° 31′ 32″E N6° 33′ 55″E S6° 10′ 49″W	19°50'23' 19°49'23' 80°16'19" 17°06'17" 75°55'44' 4°20'35" 3°11'02" 10°36'07' 3°19'08" 2°32'56"
C1304 C1305 C1306 C1307 C1308 C1309 C1310 C1311 C1312 C1313 C1314 C1315 C1316	50.25 50.21 50.17 175.13 119.41 139.15 7.96 21.12 70.31 22.01 43.89 61.06	145.00 145.00 145.00 125.00 400.00 105.00 380.00 380.00 380.00 986.50	50.00 49.96 49.92 161.15 118.97 129.19 7.96 21.11 70.21 22.01 43.88 61.05	N31° 56′ 19″E  N51° 47′ 12″E  N71° 37′ 05″E  N62° 08′ 47″E  N13° 27′ 29″E  N64° 19′ 05″E  N24° 10′ 55″E  N20° 25′ 07″E  N13° 31′ 32″E  N6° 33′ 55″E  S6° 10′ 49″W  S9° 13′ 41″W	19°50'23' 19°49'23' 80°16'19" 17°06'17" 75°55'44' 4°20'35" 3°11'02" 10°36'07' 3°19'08" 2°32'56" 3°32'47"
C1304 C1305 C1306 C1307 C1308 C1309 C1310 C1311 C1312 C1313 C1314 C1315 C1316 C1317	50.25 50.21 50.17 175.13 119.41 139.15 7.96 21.12 70.31 22.01 43.89 61.06 72.13	145.00 145.00 145.00 125.00 400.00 105.00 380.00 380.00 380.00 986.50 986.50	50.00 49.96 49.92 161.15 118.97 129.19 7.96 21.11 70.21 22.01 43.88 61.05 72.11	N31° 56′ 19″E  N51° 47′ 12″E  N71° 37′ 05″E  N62° 08′ 47″E  N13° 27′ 29″E  N64° 19′ 05″E  N24° 10′ 55″E  N20° 25′ 07″E  N13° 31′ 32″E  N6° 33′ 55″E  S6° 10′ 49″W  S9° 13′ 41″W  S13° 05′ 45″W	19*50'23' 19*49'23' 80*16'19" 17*06'17" 75*55'44' 4*20'35" 3*11'02" 10*36'07' 3*19'08" 2*32'56" 3*32'47" 4*11'21"
C1304 C1305 C1306 C1307 C1308 C1309 C1310 C1311 C1312 C1313 C1314 C1315 C1316 C1317 C1318	50.25 50.21 50.17 175.13 119.41 139.15 7.96 21.12 70.31 22.01 43.89 61.06 72.13 63.09	145.00 145.00 145.00 125.00 400.00 105.00 380.00 380.00 986.50 986.50 986.50	50.00 49.96 49.92 161.15 118.97 129.19 7.96 21.11 70.21 22.01 43.88 61.05 72.11 63.06	N31° 56′ 19″E  N51° 47′ 12″E  N71° 37′ 05″E  N62° 08′ 47″E  N13° 27′ 29″E  N64° 19′ 05″E  N24° 10′ 55″E  N20° 25′ 07″E  N13° 31′ 32″E  N6° 33′ 55″E  S6° 10′ 49″W  S9° 13′ 41″W  S13° 05′ 45″W  N54° 07′ 28″W	19°50'23' 19°49'23' 80°16'19" 17°06'17" 75°55'44' 4°20'35" 3°11'02" 10°36'07' 3°19'08" 2°32'56" 3°32'47" 4°11'21" 6°10'07"
C1304 C1305 C1306 C1307 C1308 C1309 C1310 C1311 C1312 C1313 C1314 C1315 C1316 C1317 C1318 C1319	50.25 50.21 50.17 175.13 119.41 139.15 7.96 21.12 70.31 22.01 43.89 61.06 72.13 63.09 71.45	145.00 145.00 145.00 125.00 400.00 105.00 380.00 380.00 986.50 986.50 986.50 586.00	50.00 49.96 49.92 161.15 118.97 129.19 7.96 21.11 70.21 22.01 43.88 61.05 72.11 63.06 71.40	N31° 56′ 19″E  N51° 47′ 12″E  N71° 37′ 05″E  N62° 08′ 47″E  N13° 27′ 29″E  N64° 19′ 05″E  N24° 10′ 55″E  N20° 25′ 07″E  N13° 31′ 32″E  N6° 33′ 55″E  S6° 10′ 49″W  S9° 13′ 41″W  S13° 05′ 45″W  N54° 07′ 28″W  N47° 32′ 50″W	19°50'23' 19°49'23' 80°16'19" 17°06'17" 75°55'44' 4°20'35" 3°11'02" 10°36'07' 3°19'08" 2°32'56" 3°32'47" 4°11'21" 6°10'07" 6°59'09"
C1304 C1305 C1306 C1307 C1308 C1309 C1310 C1311 C1312 C1313 C1314 C1315 C1316 C1317 C1318 C1319 C1320	50.25 50.21 50.17 175.13 119.41 139.15 7.96 21.12 70.31 22.01 43.89 61.06 72.13 63.09 71.45 42.28	145.00 145.00 145.00 125.00 400.00 105.00 380.00 380.00 986.50 986.50 986.50 586.00 220.00	50.00 49.96 49.92 161.15 118.97 129.19 7.96 21.11 70.21 22.01 43.88 61.05 72.11 63.06 71.40 42.22	N31° 56′ 19″E  N51° 47′ 12″E  N71° 37′ 05″E  N62° 08′ 47″E  N13° 27′ 29″E  N64° 19′ 05″E  N24° 10′ 55″E  N20° 25′ 07″E  N13° 31′ 32″E  N6° 33′ 55″E  S6° 10′ 49″W  S9° 13′ 41″W  S13° 05′ 45″W  N54° 07′ 28″W  N47° 32′ 50″W  S68° 39′ 14″E	19°50'23' 19°49'23' 80°16'19" 17°06'17" 75°55'44' 4°20'35" 3°11'02" 10°36'07' 3°19'08" 2°32'56" 3°32'47" 4°11'21" 6°10'07" 6°59'09" 11°00'45"
C1304 C1305 C1306 C1307 C1308 C1309 C1310 C1311 C1312 C1313 C1314 C1315 C1316 C1317 C1318 C1319 C1320 C1321	50.25 50.21 50.17 175.13 119.41 139.15 7.96 21.12 70.31 22.01 43.89 61.06 72.13 63.09 71.45 42.28 146.76	145.00 145.00 145.00 125.00 400.00 105.00 380.00 380.00 986.50 986.50 986.50 586.00 586.00 220.00	50.00 49.96 49.92 161.15 118.97 129.19 7.96 21.11 70.21 22.01 43.88 61.05 72.11 63.06 71.40 42.22 146.46	N31° 56′ 19″E  N51° 47′ 12″E  N71° 37′ 05″E  N62° 08′ 47″E  N13° 27′ 29″E  N64° 19′ 05″E  N24° 10′ 55″E  N20° 25′ 07″E  N13° 31′ 32″E  N6° 33′ 55″E  S6° 10′ 49″W  S9° 13′ 41″W  S13° 05′ 45″W  N54° 07′ 28″W  N47° 32′ 50″W  S68° 39′ 14″E  N31° 25′ 52″E	19*50'23" 19*49'23" 80*16'19" 17*06'17" 75*55'44' 4*20'35" 3*11'02" 10*36'07" 3*19'08" 2*32'56" 3*32'47" 4*11'21" 6*10'07" 6*59'09" 11*00'45"
C1304 C1305 C1306 C1307 C1308 C1309 C1310 C1311 C1312 C1313 C1314 C1315 C1316 C1317 C1318 C1319 C1320	50.25 50.21 50.17 175.13 119.41 139.15 7.96 21.12 70.31 22.01 43.89 61.06 72.13 63.09 71.45 42.28	145.00 145.00 145.00 125.00 400.00 105.00 380.00 380.00 986.50 986.50 986.50 586.00 220.00	50.00 49.96 49.92 161.15 118.97 129.19 7.96 21.11 70.21 22.01 43.88 61.05 72.11 63.06 71.40 42.22	N31° 56′ 19″E  N51° 47′ 12″E  N71° 37′ 05″E  N62° 08′ 47″E  N13° 27′ 29″E  N64° 19′ 05″E  N24° 10′ 55″E  N20° 25′ 07″E  N13° 31′ 32″E  N6° 33′ 55″E  S6° 10′ 49″W  S9° 13′ 41″W  S13° 05′ 45″W  N54° 07′ 28″W  N47° 32′ 50″W  S68° 39′ 14″E	3°11'02" 10°36'07" 3°19'08" 2°32'56" 3°32'47" 4°11'21" 6°10'07"

	LINE	TABLE
LINE	LENGTH	BEARING
L36	109.64	N35° 15' 57"E
L37	105.01	N71° 49' 03"W
L38	46.00	N44° 25′ 47″W
L39	199.18	N24° 42' 48"E
L40	57.23	S53° 20′ 44″E
L41	53.56	S31° 45′ 03″E
L42	24.99	S41° 10' 13"W
L43	33.85	S56° 58′ 32″W
L44	70.90	S23° 57′ 03″E
L45	44.13	S17° 37' 59"E
L46	42.83	S77° 47′ 32"E
L47	2.00	S29° 53′ 25″E
L48	2.00	N29° 53′ 25″W
L49	38.92	S41° 33′ 27″W
L50	2.00	N66° 04' 57"E
L51	2.00	S66° 04' 57"W
L52	2.00	N66° 04' 57"E
L53	35.99	N17° 50' 45"E
L54	16.53	N23° 55' 03"W
L55	2.00	S66° 04' 57"W
L56	2.00	N87° 04' 59"E
L57	16.53	N23° 55' 03"W
L58	38.92	S41° 33′ 27"W
L61	20.00	N18° 36' 57"E
L62	50.00	N71° 23' 03"W
L65	109.64	N35° 15' 57"E

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4651

- a. THE PHASING PLAN ATTACHED TO THIS ORDINANCE AND INCORPORATED HEREIN BY REFERENCE AS SCHEDULE B (ATTACHED) SHALL BE ADHERED TO EXCEPT THAT THE DEVELOPER MAY DETERMINE THE SEQUENCE IN WHICH PHASES ARE DEVELOPED. THE DEVELOPER SHALL PROVIDE AN ANNUAL REPORT UPDATING THE PHASING PLAN FOR THE DEVELOPMENT. NOTWITHSTANDING THE FORGOING, THE FIRST PHASE OF DEVELOPMENT SHALL NOT EXCEED SEVENTY-ONE (71) RESIDENTIAL UNITS.
- b. DEVELOPMENT ON THE PROPERTY SHALL BE CONNECTED TO A NORTH CAROLINA DEPARTMENT OF ENVIRONMENTAL QUALITY ("NCDEQ") OR NORTH CAROLINA DEPARTMENT OF HEALTH & HUMAN SERVICES ("NCDHHS") PERMITTED AND APPROVED CENTRAL WASTEWATER TREATMENT AND DISPOSAL SYSTEM, AND TO THE CURRITUCK COUNTY WATER SYSTEM. FIRE PROTECTION SHALL BE PROVIDED IN ACCORDANCE WITH THE UDO STANDARDS AND THE N.C. FIRE CODE.
- c. THE DENSITY/INTENSITY STANDARDS, DIMENSIONAL STANDARDS AND DEVELOPMENT STANDARDS FOR DEVELOPMENT OF THE PROPERTY SHALL BE IN ACCORDANCE WITH THE MASTER PLAN AND SCHEDULE A (ATTACHED), SUBJECT TO THE DEGREE OF FLEXIBILITY PROVIDED IN THESE CONDITIONS.
- d. COMMUNITY FORM AND DESIGN FOR DEVELOPMENT OF THE PROPERTY SHALL CONFORM TO THE STREETSCAPE PERSPECTIVES, EXAMPLE MODEL HOME DRAWINGS, MIXED USE, AND CLUBHOUSE PERSPECTIVE DRAWINGS ILLUSTRATED ON THE MASTER PLAN AND AS PROVIDED AND ILLUSTRATED IN APPENDIX TO THE MASTER PLAN SUPPLEMENTAL DOCUMENT. VARIATIONS MAY BE PROVIDED AND SHALL BE PERMITTED IN COLORS, MATERIALS, AND ARCHITECTURAL DETAILING THAT ARE COMPATIBLE WITH THE DESIGN CONCEPT.
- e. TRANSPORTATION: THE MAIN SUBDIVISION ENTRANCE WILL BE CONNECTED DIRECTLY TO N.C. HIGHWAY 168. DECELERATION AND ACCELERATION LANES SHALL BE PROVIDED ALONG THE PROPERTY FRONTAGE IN ACCORDANCE WITH NORTH CAROLINA DEPARTMENT OF TRANSPORTATION, ("NCDOT'), STANDARDS AND SHALL BE APPROVED BY NCDOT PRIOR TO CONSTRUCTION. CONNECTIVITY WILL BE PROVIDED TO THE EXISTING FARMLAND TO THE SOUTHEAST, IDENTIFIED WITH PARCEL IDENTIFICATION NUMBER ("PIN") 002300000070000. ROADWAYS SHALL BE LAID OUT GENERALLY AS SHOWN ON THE ATTACHED MASTER PLAN AND SCHEDULE
- f. POTABLE WATER: WATER SHALL BE SUPPLIED BY CURRITUCK COUNTY VIA AN EXISTING 12" MAIN LOCATED ON N.C. HIGHWAY 168 WHICH WILL BE TAPPED AND LOOPED THROUGH THE SITE. FIRE PROTECTIONS SHALL BE PROVIDED IN ACCORDANCE WITH UDO STANDARD AND THE APPLICABLE INSURANCE SERVICE OFFICE STANDARDS. INDIVIDUAL LOTS AND DWELLINGS SHALL BE METERED. THE DEVELOPER SHALL MODEL THE COUNTY'S WATER SYSTEM TO DEMONSTRATE ADEQUATE WATER FLOW AND PRESSURE FOR FIGHTING FIRES WHILE MEETING THE MAXIMUM DAY DOMESTIC DEMAND.
- g. WASTEWATER: DEVELOPER INTENDS TO CONNECT TO A MAJOR UTILITY, OFF-SITE, OWNED BY CURRITUCK WATER & SEWER, LLC, FOR WASTEWATER TREATMENT AND DISPOSAL. A WASTEWATER COLLECTION SYSTEM WILL BE CONNECTED BY THE DEVELOPER AND MANAGED BY A WASTEWATER UTILITY. THE UTILITY WILL BE REGULATED BY THE NORTH CAROLINA UTILITIES COMMISSION.
- h. ON-SITE STORMWATER: THE FOLLOWING IMPROVEMENTS TO STORMWATER DRAINAGE ("IMPROVEMENTS") ON THE PROPERTY SHALL BE COMPLETED BY THE DEVELOPER PRIOR TO RECORDING THE FINAL PLAT FOR THE FIRST PHASE OF DEVELOPMENT ON THE PROPERTY:
  - 1.1. IMPROVE ROWLAND CREEK FOR THE ENTIRE LENGTH ON THE PROPERTY BY CONSTRUCTION OF A DITCH ON A POSITIVE GRADE WITH 3:1 SIDE SLOPES AND SIZED FOR A 100 YEAR STORM EVENT FROM THE DRAINAGE BASIN IN WHICH THE PROPERTY IS LOCATED.

TND S.F. | CONV. S.F. |

34

30

21

20

58

19

<u>19</u>

294

12

16

14

17

59

TOTAL

42

71

65

51

37

79

19

<u>19</u>

479

OTHER IMPROVEMENTS

Rowland Creek

Improvements

Main Lake (Completion)

Village Green & Clubhouse

Multi-Use Path

Swimming Pool

MAIL AREA

Up to 22,000 SF

Neighborhood Commercial

SCHEDULE B

PHASING SCHEDULE

TND T.H.

25

19

21

126

DEVELOPMENT

INTENSITY (D.U./AC.)

2.3

2.23

2.27

2.06

1.92

3.12

2.48

2.26

2.56

1.02

<u>1.39</u>

2.10

PHASE

1B

SUBTOT PH1

5

6

9

Effluent & RV

TOTALS

AREA (AC.)

18.3

31.3

31.5

26.6

24.7

14.9

35.0

23.8

18.6

13.7

<u>8.18</u>

228.28

INSTALL A NEW DITCH ALONG THE ENTIRE LENGTH OF THE PROPERTY'S
EASTERN COMMON BOUNDARY LINE WITH RANCHLAND SUBDIVISION ON A
POSITIVE GRADE WITH 3:1 SIDE SLOPES AND SIZED FOR A 500 YEAR STORM
EVENT FROM THE DRAINAGE BASIN IN WHICH THE PROPERTY AND A

- PORTION OF RANCHLAND SUBDIVISION ARE LOCATED.

  1.3. THE IMPROVEMENTS SET FORTH IN THIS SECTION SHALL BE MAINTAINED BY THE DEVELOPER, OR A MANAGEMENT ASSOCIATION CREATED BY THE DEVELOPER.
- i. OFF-SITE STORMWATER: SUBJECT TO OBTAINING RIGHT OF ENTRY FROM OFF-SITE PROPERTY OWNERS, I.E. IF CONSENT OF THOSE PROPERTY OWNERS IS OBTAINED, THE FOLLOWING IMPROVEMENTS TO STORM WATER DRAINAGE SYSTEMS OUTSIDE THE BOUNDARIES OF THE PROPERTY SHALL BE COMPLETED BY THE DEVELOPER PRIOR TO RECORDING THE FINAL PLAT FOR THE FIRST PHASE OF THE
  - 1.1. CLEAR AND SNAG ROWLAND CREEK FROM N. C. HIGHWAY 168 TO THE EAST END OF ROWLAND CREEK ESTATES SUBDIVISION (ADJACENT TO PIN 0022000088M0000)
  - 1.2. CLEAR AND SNAG THE RANCHLAND OUTLETS DITCH FROM THE PROPERTY'S SOUTHEAST CORNER ADJACENT TO RANCHLAND SUBDIVISION (ADJACENT TO PIN 0238000004201F2) TO THE RANCHLAND OUTLETS' INTERSECTION WITH ROWLAND CREEK, AND CONSTRUCT A DITCH ON A POSITIVE GRADE WITH 3:1 SIDE SLOPES AND SIZED FOR A 100 STORM EVENT FROM THE DRAINAGE BASIN IN WHICH THE PROPERTY AND RANCHLAND SUBDIVISION ARE LOCATED.
  - 1.3. IMPROVE THE SURVEY ROAD DITCH FROM GUINEA ROAD TO THE RAILROAD (ON PIN 0022000063Y0000) AND CONSTRUCT THE DITCH ON A POSITIVE GRADE WITH 3: 1 SIDE SLOPES AND SIZED FOR A 100 YEAR STORM EVENT FROM THE DRAINAGE BASIN IN WHICH THE PROPERTY IS LOCATED.
  - 1.4. IMPROVE ROWLAND CREEK FROM THE EASTERN BOUNDARY OF EAGLE CREEK (ADJACENT TO PIN 015A00000980000) TO THE WESTERN BOUNDARY OF THE PROPERTY BY CONSTRUCTING A DITCH ON A POSITIVE GRADE WITH 3:1 SIDE SLOPES SIZED FOR A 100 YEAR STORM EVENT
  - 1.5. DEVELOPER OR A MANAGEMENT ASSOCIATION SHALL CONTRIBUTE \$5,000 ANNUALLY TO THE COST OF MAINTENANCE FOR THE OFF-SITE IMPROVEMENTS SET FORTH IN THIS SECTION. SUCH FUNDS SHALL BE DEPOSITED WITHIN AN ASSOCIATION CREATED FOR THE PURPOSE OF MAINTAINING OFF-SITE IMPROVEMENTS. THE FIRST CONTRIBUTION SHALL BE MADE WITHIN 1 YEAR OF THE RECORDING OF THE FIRST PHASE OF DEVELOPMENT, AND SUBSEQUENT CONTRIBUTIONS MADE ANNUALLY FOR THE NEXT 10 YEARS.
  - 1.6. SHOULD DEVELOPER BE UNABLE TO OBTAIN RIGHT OF ENTRY FROM ANY LANDOWNER PRIOR TO RECORDING PHASE 2, THEN DEVELOPER'S ONLY OBLIGATION UNDER THIS SECTION (I), OFF-SITE STORMWATER, SHALL BE TO PROVIDE FEE IN LIEU IN THE AMOUNT OF 115% OF THE COST OF THE OFF-SITE IMPROVEMENTS, SUCH THAT THE COUNTY MAY COMPLETE THESE IMPROVEMENTS IF AND WHEN RIGHT OF ENTRY IS OBTAINED.

#### j. OVERALL STORMWATER CONDITIONS:

- 1.1. THE DEVELOPER SHALL CONSTRUCT BERMS ALONG DITCH OUTLETS AGAINST RANCHLAND TO KEEP PROPOSED DEVELOPMENT'S RUNOFF FROM FLOODING RANCHLAND DURING A 100-YEAR STORM.
- 1.2. ON-SITE STORMWATER WILL BE MANAGED BY CONSTRUCTION A SERIES OF STORMWATER MANAGEMENT PONDS THAT WILL BE INTERCONNECTED AND WILL RETAIN AND SLOW-RELEASE STORMWATER PRIMARILY TO ROWLAND CREEK BOTH DIRECTLY AND INDIRECTLY. A PORTION OF THE STORMWATER WILL ALSO OVERFLOW TO AN EXISTING CULVERT THAT RUNS DIRECTLY

SCHEDULE C ROADWAY STANDARDS			
TYPE	R/W WIDTH	PAV'T.WIDTH (B-B)	
Boulevard w/on-street parking & bike lane	100' – 130'	35' each way	
Local Road w/parking both sides	60'	43'	
Local Road w/parking one side	50' – 55'	35	
Local Road w/no parking	40'	27′	
Private Street	30'	20'	
Boulevard w/out bike lane	80′	16' each way	

UNDER N.C. HIGHWAY 168 NEAR THE NORTHWEST CORNER OF THE PROPERTY, THE DITCH THAT DRAINS THAT OUTLET WILL ALSO BE IMPROVED AS NECESSARY SUBJECT TO OBTAINING RIGHT OF ACCESS REFERENCED ABOVE. IN ADDITION TO MODELING AND RETAINING STORMWATER TO THE UDO AND STORMWATER MANUAL STANDARD FOR THE DIFFERENCE BETWEEN RUNOFF FROM THE 10-YEAR DEVELOPED CONDITION AND RUNOFF FROM A 2-YEAR WOODED CONDITION SITE, STORMWATER SHALL BE MODELED FOR THE 100- YEAR STORM EVENT AND PROPERTY LINE BERMS CONSTRUCTED AS NECESSARY TO MANAGE THE 100-YEAR STORM WITHOUT ADVERSELY IMPACTING NEIGHBORING PROPERTIES.

STORMWATER SHALL BE CONVEYED TO ON-SITE RETENTION PONDS THROUGH A COMBINATION OF CURBS WITH INLETS, STORMWATER PIPES AND OPEN, VEGETATED SWALES.

#### k. PERIMETER COMPATIBILITY SHALL BE ADDRESSED AS FOLLOWS:

- 1.1. TO THE WEST: AS LONG AS THIS PROPERTY REMAINS ZONED AS IS, A 50-FOOT FARM BUFFER SHALL BE PROVIDED, FOLLOWED BY A 60-FOOT ROADWAY CORRIDOR, SO THE NEAREST EXISTING DWELLING UNIT WILL BE AT LEAST 565 FEET FROM THE EXISTING RESIDENCE ON THAT FARM. A VEGETATIVE BUFFER SHALL BE PROVIDED ALONG THAT PROPERTY LINE. IF THIS PROPERTY IS REZONED, THE BUFFER REQUIREMENTS IN THIS CONDITION SHALL NOT APPLY
- 1.2. TO THE NORTH (CARATOKE HIGHWAY): A BERM SHALL BE PROVIDED ALONG THE HIGHWAY ALONG WITH LAKES FLANKING THE MAIN ENTRANCE ROAD. THE NEAREST DWELLING UNIT WILL BE SET BACK AT LEAST 200 FEET FROM THE HIGHWAY RIGHT-OF-WAY AT THE CLOSEST POINT. THE VISUAL IMPACT OF THE DEVELOPMENT SHALL BE MINIMIZED BY SETBACKS, BERMS AND LANDSCAPING.
- 1.3. TO THE EAST: A 50-FOOT VEGETATIVE FARM BUFFER SHALL BE P R O V I D E D ALONG THE EXISTING FARMLAND; TRADITIONAL SINGLE FAMILY DWELLING LOTS ARE BEING PROPOSED IN THE AREAS THAT BACK UP TO EXISTING DWELLING UNITS IN THE ADJACENT RANCHLAND SUBDIVISION. A MINIMUM 50-FOOT BUFFER SHALL BE PROVIDED BETWEEN THE DEVELOPMENTS.
- 1.4. TO THE SOUTH: COMPATIBLE RESIDENTIAL DEVELOPMENT IS BEING PROPOSED AND A MINIMUM 90-FOOT OPEN SPACE BUFFER IS SHOWN TO THE PROPERTY LINE. THE SOUTHERN BUFFER MAY INCLUDE A POND.
- 1.5. LIMITED COMMERCIAL DEVELOPMENT IS LOCATED INTERIOR TO THE DEVELOPMENT AND SHALL FRONT ALONG THE LANDSCAPED ENTRANCE
- I. ENVIRONMENTAL PROTECTION AND MONITORING: WETLANDS SUBJECT TO THE JURISDICTION OF THE US ARMY CORPS OF ENGINEERS HAVE BEEN DELINEATED AND CONFIRMED BY THE CORPS OF ENGINEERS. WETLAND BUFFERS HAVE BEEN SHOWN ON THE MASTER PLAN AND THE DEVELOPMENT PLAN HONORS THOSE BUFFERS. THE ASSOCIATION DOCUMENTS (DECLARATION) WILL INCLUDE PROVISIONS THAT PROHIBIT THE FILLING OF WETLANDS AND PROHIBIT THE CLEARING OF THE BUFFER AREAS OTHER THAN INCIDENTAL TREE CUTTING AND VEGETATION REMOVAL, EXCEPT FOR MINOR IMPACTS ASSOCIATED WITH STORMWATER MANAGEMENT FACILITIES.

THE ASSOCIATION, EITHER ITSELF OR VIA A MANAGEMENT ENTITY, WILL ASSUME RESPONSIBILITY FOR ONGOING OPERATION AND MAINTENANCE OF ALL STORMWATER MANAGEMENT FACILITIES IN ACCORDANCE WITH THE CURRITUCK COUNTY UDO REQUIREMENTS AND ALL NCDEQ PERMIT REQUIREMENTS. THE ASSOCIATION DUES WILL BE STRUCTURED IN A WAY THAT FUNDS ARE PROVIDED FOR THE UPKEEP OF THESE FACILITIES, AS WELL AS PERIODIC IMPROVEMENTS TO ROWLAND CREEK BOTH THROUGH THE DEVELOPMENT, AS WELL AS A CONTRIBUTION TO OFF-SITE MAINTENANCE.

WIND TIDES WILL BE CONSIDERED IN THE DESIGN OF SITE GRADING, WITH STRUCTURES LOCATED ABOVE THE ELEVATION OF THE HISTORIC WIND TIDAL INFLUENCE FROM ROWLAND CREEK.

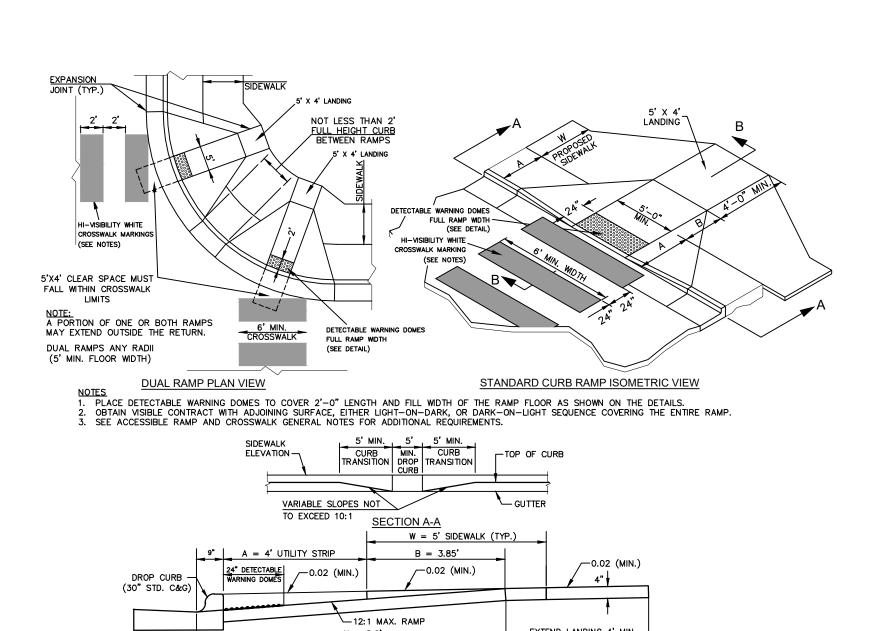
#### ADDITIONAL CONDITIONS OF APPROVAL MADE ON DECEMBER 7, 2020:

- m. ROWLAND CREEK NEAR THE SOUTH OF THE PROPERTY MUST BE CLEARED AND SNAGGED AS SOON AS POSSIBLE.
- n. STORMWATER MANAGEMENT ON ROWLAND CREEK AND AREAS DOWNSTREAM NORTH AND EAST OF THE RAILROAD TRACK ARE WITHIN CAMA JURISDICTION, REQUIRE A 30' SETBACK, AND CAMA PERMITTING IS REQUIRED.
- o. DRAINAGE AND UTILITY REQUIREMENTS IN THE JUNE 24, 2019 APPROVED ORDER REMAIN UNCHANGED.

DE\	DEVELOPMENT STANDARDS & SETBACKS			
		STYLE		
	TND T.H.	TND S.F. LOT	CONV. S.F. LOT	
MIN. LOT SIZE	1,800 SF	6,900 SF	9,500 SF	
TYPICAL LOT SIZE	1,800 - 3,000 SF	7,000 - 9,000 SF	10,000 - 15,000 SF	
MIN. LOT WIDTH	20'	35'	35'	
TYPICAL LOT WIDTH	20'-25'	50'-60'	62'	
FRONT SETBACK	15'	20'	20'	
SIDE SETBACK	0	10'	10'	
REAR SETBACK	20'	20'	25'	
CORNER SIDE SETBACK	15'	15'	15'	
MAXIMUM SETBACK	25'	75'	140'	
MAXIMUM HEIGHT	35'	35'	35'	
MAXIMUM BUILDING SIZE	16,000 SF	N/A	N/A	
MAXIMUM LOT COVERAGE	100%	60%	45%	

DESIGNED: CHECKED:
BPG MSB
DRAWN: APPROVED:
WGY BPG
SHEET:

18 OF 20
CAD FILE:
465100PP3
PROJECT NO:
4651



#### SECTION B-B - 30" VALLEY CURB & GUTTER ACCESSIBLE RAMP DETAILS

B = X - (A+9") SECTION B-B - STANDARD 30" CURB & GUTTER

└─12:1 MAX. RAMP

\* BACK OF SIDEWALK DROP REQUIRED FOR ALL SIDEWALK SLOPES
\*\* BACK OF SIDEWALK DROP REQUIRED FOR SIDEWALK SLOPES 0.04.

18" A = 4' UTILITY STRIP

B = DISTANCE FROM FONT EDGE OF SIDEWALK TO BACK POINT OF 12:1 (8.33%) SLOPE

W = 5' SIDEWALK (TYP.)

NO EXTENDED LANDING REQ'D

ACCESSIBLE RAMP GENERAL NOTES: [NCDOT STD. DETAIL 848.05]

1. CONSTRUCT THE RAMP SURFACE TO BE STABLE, FIRM AND SLIP RESISTANT. CONSTRUCT THE CURB RAMP TYPE AS SHOWN IN THE PAVEMENT MARKING PLANS OR AS DIRECTED BY THE ENGINEER.

2. LOCATE CURB RAMPS AND PLACE PEDESTRIAN CROSSWALK MARKINGS AS SHOWN ON THE PROJECT DRAWINGS OR AS DIRECTED BY THE ENGINEER. WHEN FIELD ADJUSTMENTS REQUIRE MOVING CURB RAMPS OR MARKINGS AS SHOWN, CONTACT AND LOCATE AS DIRECTED BY THE ENGINEER.

- PEDESTRIAN CROSSWALK LINES. 4. SETBACK DISTANCE FROM INSIDE CROSSWALK MARKING TO NEAREST EDGE OF TRAVEL LANE IS 4' MINIMUM. REFER TO PROJECT DRAWINGS FOR STOP BAR LOCATIONS, IF PROVIDED, OR AS DIRECTED BY THE ENGINEER.
- TERMINATE PARKING A MINIMUM OF 20' BACK OF A PEDESTRIAN CROSSWALK.
- 7. CONSTRUCT CURB RAMPS A MINIMUM OF 4' WIDE, OR AS SPECIFIED ON PLANS. 8. CONSTRUCT THE RUNNING SLOPE OF THE RAMP 8.33% (12:1 H:V) MAXIMUM.
- 6. CONSTRUCT THE RONNING SLOPE OF THE RAMP 6.33% (12:1 H:9) MAXIMUM.

  10. WHERE REQUIRED, CONSTRUCT THE SIDE FLARE SLOPE A MAXIMUM OF 10% MEASURED ALONG THE CURB LINE.

  11. CONSTRUCT THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE BASE OF THE CURB RAMP A MAXIMUM OF 5% AND MAINTAIN A SMOOTH TRANSITION.

  12. CONSTRUCT LANDINGS FOR SIDEWALK A MIN. OF 4'X4' WITH A MAXIMUM SLOPE OF 2% IN ANY DIRECTION. CONSTRUCT LANDINGS FOR MEDIAN ISLANDS A MINIMUM OF 5'X5' WITH A MAXIMUM SLOPE OF 2% IN AN DIRECTION. 13. TO USE A MEDIAN ISLAND AS A PEDESTRIAN REFUGE AREA, MEDIAN ISLANDS WILL BE A MINIMUM OF 6' WIDE. CONSTRUCT MEDIAN ISLANDS TO PROVIDE PASSAGE OVER
- OR THROUGHOUT THE ISLAND

  14. SMALL CHANNELIZATION ISLANDS THAT CAN NOT PROVIDE A 5'X5' LANDING AT THE TOP OF A RAMP, WILL BE CUT THROUGH LEVEL WITH THE SURFACE STREET.

  15. CURB RAMPS WITH RETURNED CURBS MAY BE USED ONLY WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP. THE ADJACENT SURFACE IS PLANTI OR OTHER NON-WALKING SURFACE OR THE SIDE APPROACH IS SUBSTANTIALLY OBSTRUCTED.

  16. PLACE A ½" EXPANSION JOINT WHERE THE CONCRETE CURB RAMP JOINS THE CURB AS SHOWN IN ROADWAY STANDARD DRAWING 848.01

  17. CURB RAMPS THROUGH MEDIAN ISLANDS, SINGLE RAMPS AT DUAL CROSSWALKS OR LIMITED R/W SITUATIONS, WILL BE HANDLED BY SPECIAL DETAILS.
- PEDESTRIAN CROSSWALK GENERAL NOTES: [NCDOT STD. DETAIL 1205.07]

  1. USE THE DETAILS ABOVE AND THE FOLLOWING NOTES FOR GUDIANCE IN PLACING CROSSWALK MARKINGS. REFER TO NCDOT ROADWAY STANDARD DRAWINGS, MUTCD AND ADA STANDARDS FOR ADDITIONAL GUIDANCE.

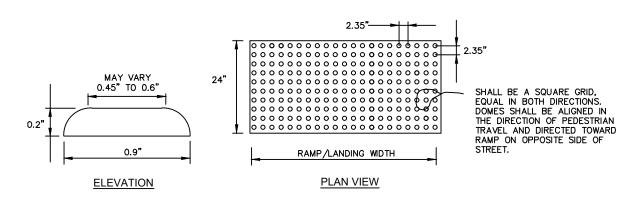
  2. THE LOCATION AND TYPE OF CROSSWALK MARKINGS SHOWN ON THE ABOVE DETAILS ARE FOR REFERENCE ONLY. LOCATE CROSSWALK MARKINGS AS SHOWN ON THE PROJECT DRAWINGS OR AS DIRECTED BY THE ENGINEER. THE CROSSWALK MARKING TYPE, STANDARD OR HI—VISIBILITY, SHALL BE INSTALLED AS SPECIFIED ON THE PROJECT DRAWINGS OR AS DIRECTED BY THE ENGINEER.
- PROJECT DRAWINGS OR AS DIRECTED BY THE ENGINEER..

  3. THE STANDARD CROSSWALK IS THE TWO WHITE 8" MIN. TRANSVERSE LINES WITH A 6' MIN. GAP BETWEEN THE LINES. THE HI—VISIBILITY CROSSWALK IS WHITE 24" MAX. WIDE LONGITUDINAL LINES WITH 24" MIN. GAPS BETWEEN LINES, SEE DETAIL, HI—VISIBILITY CROSSWALKS SHOULD BE A MINIMUM OF 6' WIDE OR AS SPECIFIED ON THE PROJECT DRAWINGS. CURB RAMPS SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS, EXCLUDING AN FLARES.

  4. STOP BARS (IF PROVIDED) SHOULD BE PLACED A 4' MIN. IN ADVANCE OF NEAREST CROSSWALK LINE.

  5. SET BACK DISTANCE FROM INSIDE CROSSWALK MARKING TO NEAREST EDGE OF TRAVEL IS 4' MIN.
- BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE OF 4'X4' MIN. SHALL BE PROVIDED WITHIN THE MARKINGS. SINGLE DIAGONAL CURB RAMPS WITH FLARED SIDES SHALL HAVE A SEGMENT OF CURB 2' MIN. LONG LOCATED ON EACH SIDE OF THE CURB RAMP AND WITHIN THE
- MARKED CROSSING, SEE DETAIL.

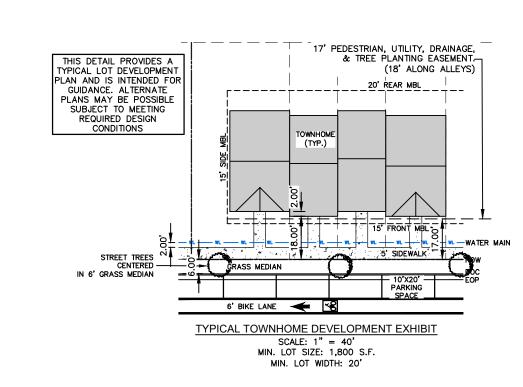
  8. CURB RAMPS SHALL BE CONSTRUCTED IN ACCORDANCE TO THE LATEST NCDOT ROADWAY STANDARD DRAWINGS. CURB RAMPS THROUGH MEDIAN ISLANDS, SINGLE RAMPS AT DUAL CROSSWALKS OR LIMITED R/W SITUATIONS, WILL BE HANDLED BY SPECIAL DETAILS.

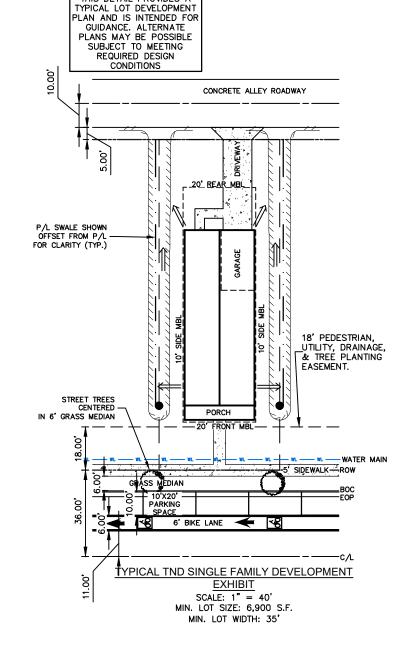


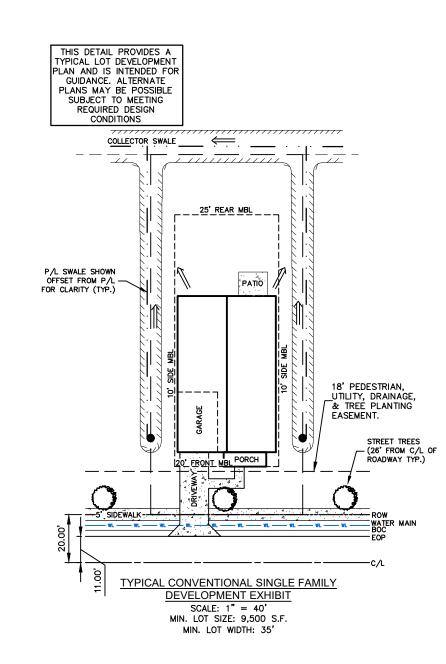
#### TRUNCATED DOME DETECTABLE WARNING DETAIL

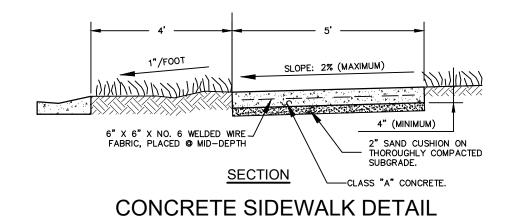
#### DETECTABLE WARNING GENERAL NOTES

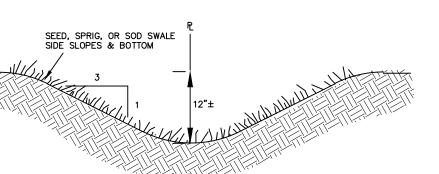
- 1. DETECTABLE WARNING SHALL CONSIST OF RAISED TRUNCATED DOMES MANUFACTURED BY "COTE-L INDUSTRIES, INC.". CALLED "SAFTI-TRAX", WITH POLYURETHANE COATING "DURABAK", OR APPROVED EQUAL. APPLIED ON SMOOTH (NON-GROOVED) CLEAN CONCRETE RAMP, AND SHALL CONFORM TO THE DETAILS IN THE PLANS AND IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND INSTALLATION INSTRUCTIONS.
- 2. ALL DETECTABLE WARNING AREAS SHALL START AT BACK OF CURB, BE 24 INCHES IN DEPTH AND COVER THE COMPLETE WIDTH OF THE RAMP AREA 48 INCHES MIN..
- 3. 70% VISUAL CONTRAST IS REQUIRED. THE COLOR SHALL BE AN INTEGRAL PART OF THE DETECTABLE WARNING MATERIAL, AS SPECIFIED ON THE PLANS. COLOR TO BE DETERMINED BY THE CITY STAFF, SAFETY YELLOW IS THE DEFAULT COLOR. 4. THE SMOOTH AND CLEAN CONCRETE UNDER DETECTABLE WARNING DEVICE AREA SHALL BE
- INCLUDED IN THE COST OF THE CONCRETE CURB RAMP. THE COST OF FURNISHING AND INSTALLING THE DETECTABLE WARNING DEVICE SHALL BE INCLUDED SEPARATELY AS "DETECTABLE WARNING DEVICE" PER SQUARE FOOT OR AS OUTLINED IN THE SPECIFICATIONS. 5. DETECTABLE WARNING SURFACE:
- APPLIED A COATING OF "DURABAK" SLIP-RESISTANT POLYURETHANE COATING TO THE SMOOTH, CLEAN CONCRETE SURFACE. ON TOP OF THE POLYURETHANE COATING APPLY TRUNCATED DOMES FROM A "SAFTY-TRAX" CONTACT SHEET. ON TOP OF THE TRUNCATED DOMES AND INITIAL POLYURETHANE COATING PLACE THREE ADDITIONAL COATS OF "DURABAK" POLYURETHANE COATING. COLOR TO BE DETERMINED BY CITY STAFF OR AS SPECIFIED ON THE PLANS. SAFETY YELLOW IS A DEFAULT COLOR.
- 6. ALL RAMPS AND DETECTABLE WARNING SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON THE OPPOSITE SIDE OF STREET.

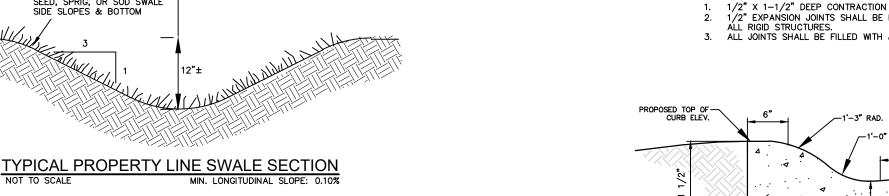


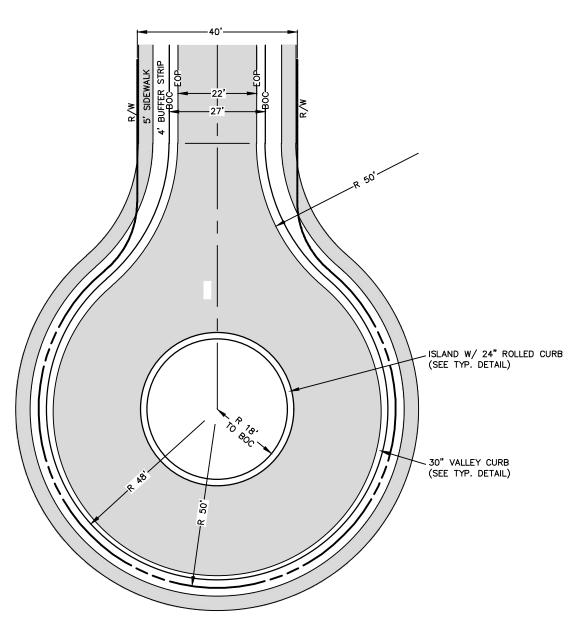


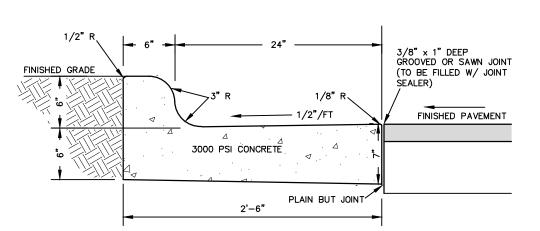








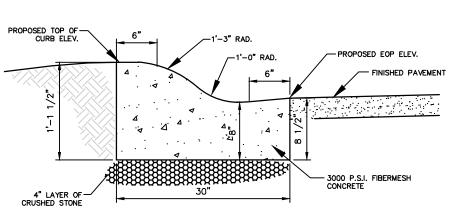




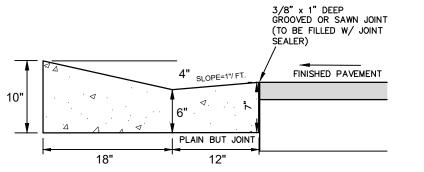
#### **CURB & GUTTER COLLECTION SECTION** NOT TO SCALE NCDOT STD 846.01

GENERAL CURB & GUTTER NOTES:

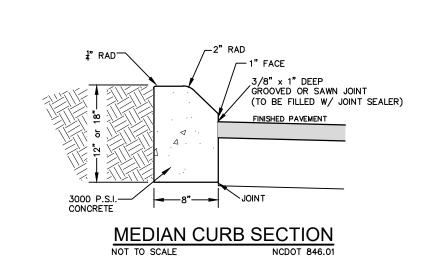
1. 1/2" X 1-1/2" DEEP CONTRACTION JOINTS SHALL BE PLACED AT 10' INTERVALS.
2. 1/2" EXPANSION JOINTS SHALL BE PLACED AT 90' INTERVALS, AND ADJACENT TO ALL RIGID STRUCTURES.
3. ALL JOINTS SHALL BE FILLED WITH JOINT FILLER AND SEALER.

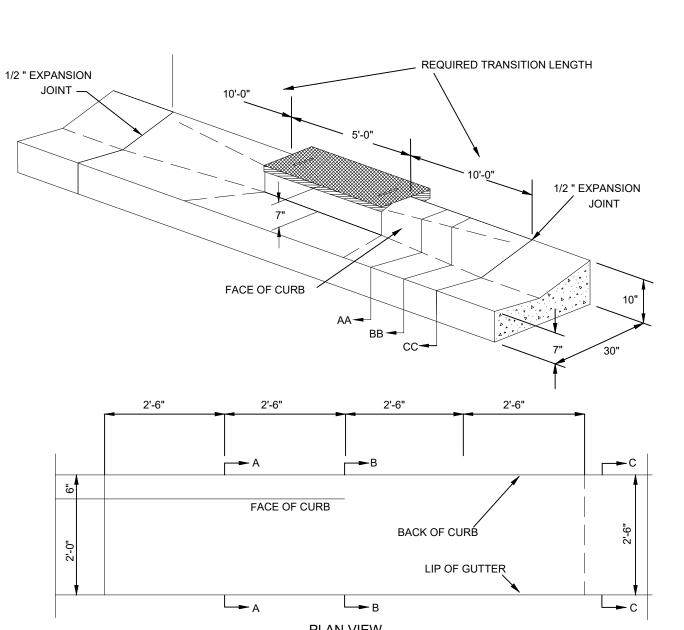


STANDARD ROLLED CURB



**CURB & GUTTER VALLEY SECTION** NOT TO SCALE





~ 2" SF-9.5A OR SF-9.5B ASPHALT PAVING

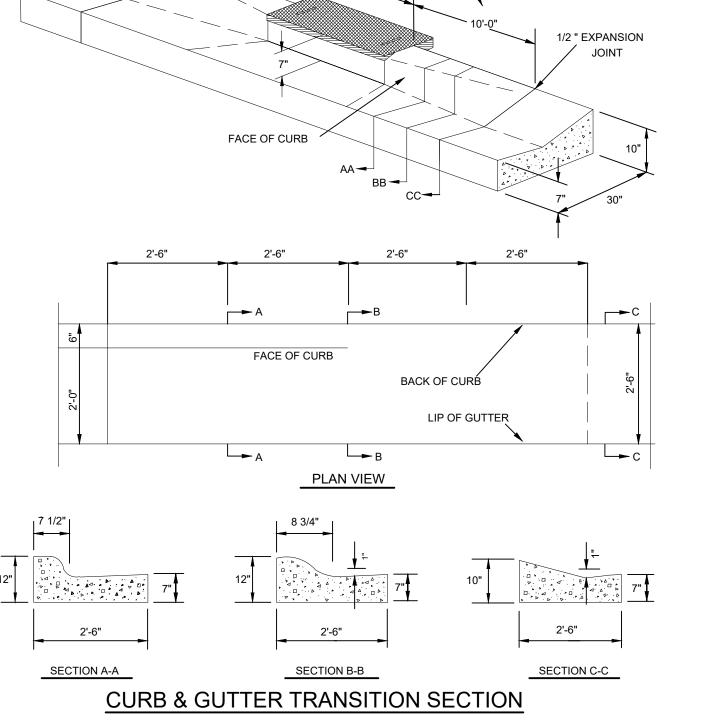
6" BASE COURSE

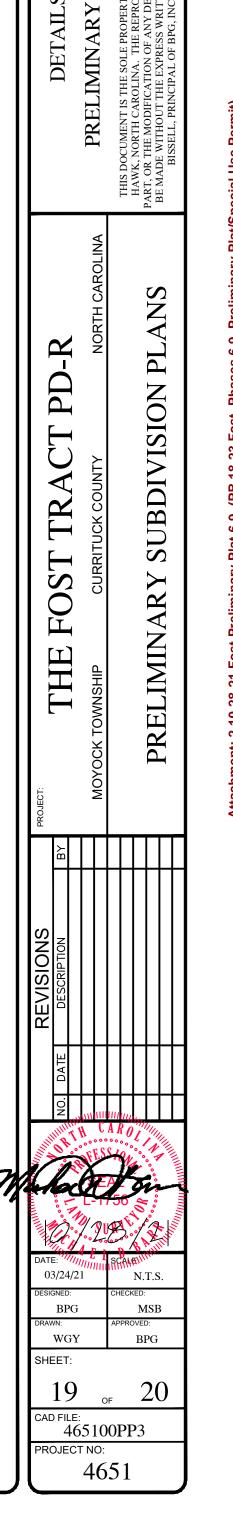
TYPICAL RESIDENTIAL ROADWAY PAVEMENT SECTION

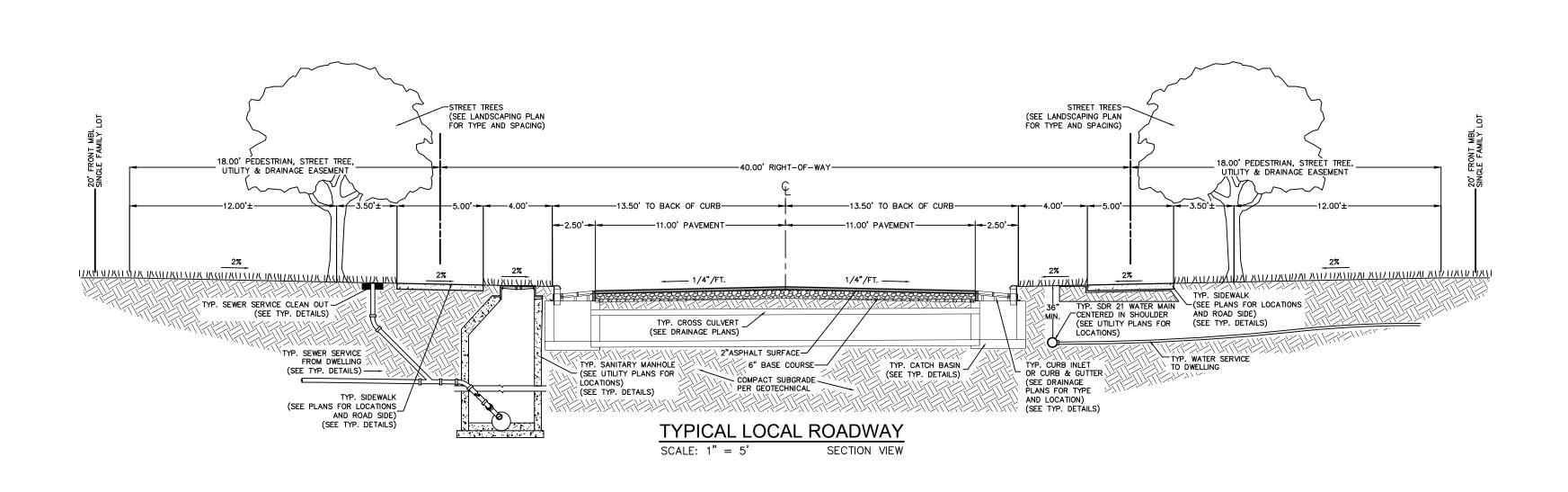
A 6" AGGREGATE BASE COURSE. THE SOIL SUBGRADE BENEATH PAVEMENTS SHALL BE COMPACTED TO AT LEAST 95% OF ASTM D 698 PRIOR TO ANY PLACEMENT OF SUBBASE FILL OR STONE BASE COURSE. ALL SITE PREPARATION AND THE DESIGN AND CONSTRUCTION OF ALL

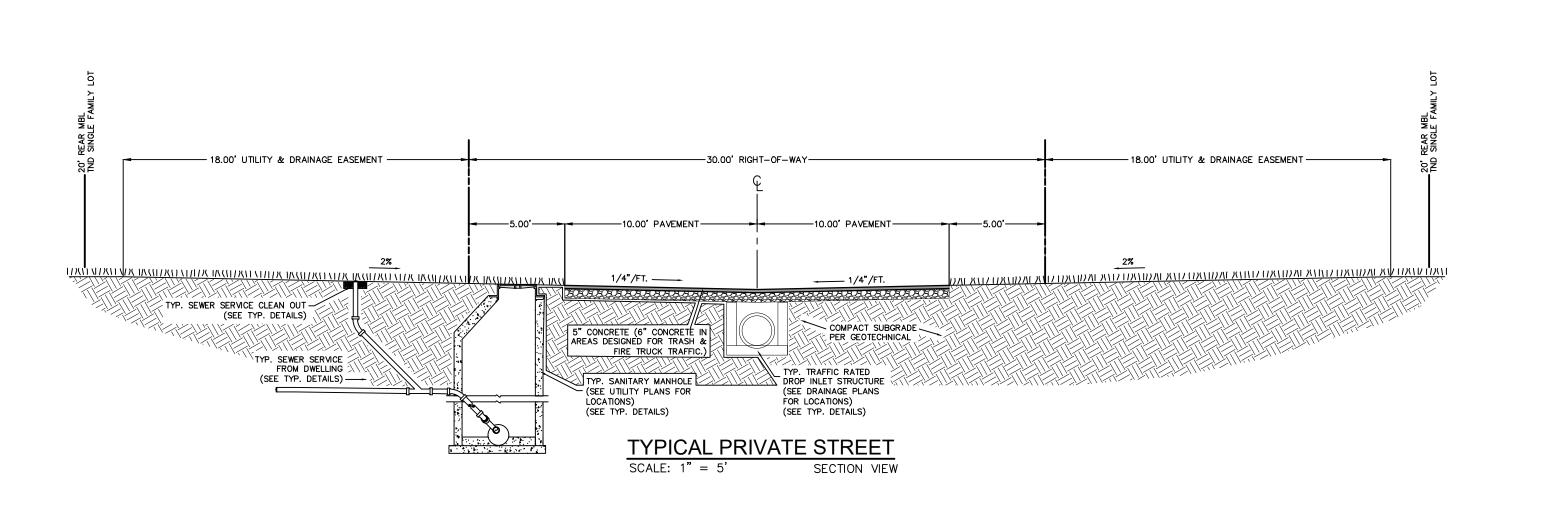
PAVING SHALL CONSIST OF FINE GRADING THE SPECIFIED PARKING & DRIVE AREAS AND INSTALLING 2" SF-9.5A OR SF-9.5B ASPHALT CONCRETE SURFACE COURSE IN CONJUNCTION WITH

FOUNDATIONS, GROUND SLABS, AND PAVEMENTS SHALL BE IN ACCORDANCE WITH RECOMMENDATIONS PROVIDED BY A GEOTECHNICAL ENGINEER.









**JIVISION PL** 

465100PP3



#### **Currituck County**

Department of Planning and Community Development 153 Courthouse Road, Suite 110 Currituck, North Carolina 27929 252-232-3055 FAX 252-232-3026

#### **MEMORANDUM**

To: Mark Bissell, Bissell Professional Group

Justin Old, Allied Properties LLC

From: Tammy D. Glave, CZO, Senior Planner

**Date:** October 14, 2021

**Subject:** PB 18-23 Fost, Preliminary Plat/Special Use Permit, Phases 6-9

The following comments have been received for the October 13, 2021 TRC meeting for Fost, Preliminary Plat/Special Use Permit, Phases 6-9. In order to be scheduled for the November 15, 2021 Board of Commissioners meeting, please address all comments and resubmit a corrected plan by 3:00 p.m. on October 25, 2021. TRC comments are valid for six months from the date of the TRC meeting.

#### Planning (Tammy Glave, 252-232-6025)

Denied:

1. Staff has concerns about the availability of elementary school capacity for the students this development is projected to generate. This development is districted to Shawboro Elementary that has zero % capacity and no plans for expansion. Without school capacity, a finding should not be made that the special use permit review standard of not exceeding the county's ability to provide adequate public facilities is met. (UDO Sections 2.4.6.D and 6.3.6) Section 6.6 of the UDO requires that all required public facilities shall be in place or programmed to be in place within two years after the initial approval of the special use permit. It should also be noted that the high schools are at 99% committed capacity.

2021-2022 Shawboro Elementary	622 Students
School Capacity	
10-4-2021 School Enrollment	622 Students
% Capacity Available	0%
Fost Phases 6-9 Proposed Capacity	+44 Students
Changes	

ADEQUATE PUBLIC FACILITIES – SCHOOLS <sup>1</sup>				
School	2021-2022 Actual Capacity <sup>2</sup>	Committed Capacity <sup>3</sup>	Proposed Capacity Changes Number of Students	
Moyock Elementary Shawboro Elementary Central Elementary	128% 99% 96%	131%	+44	
Moyock Middle Currituck Middle	94% 62%	92%	+14	
Currituck High JP Knapp Early College	88% 86%	105%	+24	

<sup>&</sup>lt;sup>1</sup>Does not include minor subdivisions, exempt subdivisions, and subdivisions approved prior to the adoption of the adequate public facilities ordinance (October 1994)

- 2. While regulated by the NCDEQ Division of Water Resources, staff has concerns about connecting any dwelling units to the WWTP at Eagle Creek until the system is compliant with State standards.
- 3. Please fix the word 'southern' on Sheet 18, k-1.4.
- 4. UDO Section 6.2.1.H Alleys states that alleys shall only intersect with streets (not other alleys) and shall not have a pavement width more than 16'. This was discovered after the preliminary plat approval of previous phases.
- 5. Please verify your calculations for Recreational and Parkland Fee-in-lieu. Staff's calculations are much higher than those shown on Sheet 1. Current property access value (land only) = \$3,942,400 with a resultant fee-in-lieu of \$134,288.27 as opposed to the \$15,978.64 as shown. (UDO Section 6.5)
- 6. Please confirm that the area marked as 'Reserved as effluent disposal area' is not counted as open space. (UDO Section 7.1.3.D)
- 7. Please note on the plan that on-site effluent disposal is not permitted, an amended plan is required that must be approved by the BOC, and the BOC must approve the amended plan prior to any land disturbance for any part of the on-site effluent disposal system.
- 8. Please indicate that drainage easements are dedicated to Currituck County. (UDO Section 7.3.4)

<u>Currituck County Chief Building Inspector/Fire Official (Bill Newns, 252-232-6023)</u>
Approved without comment.

<u>Currituck County Economic Development (Larry Lombardi, 252-232-6015)</u>
Reviewed without comment.

#### Currituck County GIS (Harry Lee, 252-232-4039)

1. Addresses will be assigned by GIS during final plat review.

<sup>&</sup>lt;sup>2</sup>Capacity percentages are based on 2021-2022 school year classroom standards and Sep 2021 ADM

<sup>&</sup>lt;sup>3</sup>Capacity percentages are based on the 2021-2022 school year classroom standards and Sep 2021 ADM

#### Currituck County Parks and Recreation (Jason Weeks, 252-232-3007)

Reviewed without comment.

#### Currituck County Soil and Stormwater (Dylan Lloyd, 252-232-3360)

Reviewed without comment.

#### <u>Currituck County Utilities Director (Will Rumsey, 252-232-2769)/Water Department</u> – Distribution Supervisor (Dave Spence, 252-232-2769)

Reviewed without comment.

#### Currituck County Public Utilities/Backflow (Chas Sawyer, 252-232-6060 ext. 4221)

Reviewed without comment.

#### Albemarle Regional Health Services (Joe Hobbs, 252-232-6603)

Reviewed without comment.

#### NC Department of Transportation - District Engineer (David Otts, 252-331-4860)

Reviewed without comment.

#### NC Division of Coastal Management (Charlan Owens, 252-264-3901)

Previous comments for this subdivision apply.

#### US Army Corps of Engineers (Anthony Scarbraugh, 910-251-4619)

No new comments.

#### The following items are necessary for resubmittal:

- 3 full size copies of revised plans
- 1 − 8.5 x 11" reduced copy
- 1- PDF digital copy of all revised or new documents and plans.

A use permit hearing is an evidentiary hearing where the Board of Commissioners must make a Quasi-Judicial Decision.

- An evidentiary hearing will be held for the Board of Commissioners to gather competent, material and substantial evidence to establish the facts of the case.
- All testimony is made under oath.
- The applicant or opposing parties shall establish written findings of fact and conclusions
  of law
- Parties with standing may participate fully in the evidentiary hearing, including presenting evidence, cross-examining witnesses, objecting to evidence, and making legal arguments.
- Non-parties may present competent, material, and substantial evidence that is not repetitive.

Typical Use Permit (Quasi-Judicial) Hearing at BOC

- Swearing in of witnesses/speakers
- Presentation by County Staff on Application
- Required Presentation by Applicant or Authorized Agent (20-25 minutes)
  - Applicant to Present Findings of Fact
- Public Comment Period (typically 3 minutes each)
- Applicant Rebuttal (typically 5 minutes)
- BOC Deliberation & Decision



Kim Mason, NC Area Director

kmason@mediacomcc.com 216 B Shannonhouse Road

Edenton NC, 27932 Edenton: 252-482-5583 Plymouth: 252-793-2491 Mobile: 252-497-0328

RE: New Build & Development

Dear Development manager;

As you know the key need for all homes in this 21st Century is a solid internet connection, be it for business, education or entertainment, the public demand is here.

With this in mind, as you plan for your development and build out, we would like to encourage you to reach out to us, as you do for other essential utilities. It is most economical and reasonable for you to work with us and have this valuable infrastructure in advance of selling and building the homes. Any build out costs can easily be recouped as the lots are developed and make your neighborhoods more appealing to families and professionals.

We invite, you to partner with us and contact us locally. We will process a ROI for your location to determine partnership feasibility and estimated cost to ensure your development has access to the best internet services available.

Our key contacts are, Kim Mason, Director for North Carolina – information above and our construction coordinator Nathanial Harris at 252-793-5256 or 252-339-9375.

Mediacom launched 1-Gig broadband speeds in the following areas of North Carolina and operates customer service offices in Edenton and Plymouth.

Bertie County	Martin County	Chowan County	Perquimans County
Colerain	Jamesville	Arrowhead / Chowan Beach	Hertford
Kelford	Northampton County	Edenton	Winfall
Lewiston	Conway	Currituck County	Tyrrell County
Powellsville	Galatia	Barco	Columbia
Roxobel	Jackson	Currituck	Washington County
Windsor	Rich Square	Grandy	Creswell
Camden County	Seaboard	Moyock	Plymouth
Camden	Severn	Point Harbor	Roper
Shiloh	Woodland	Poplar Branch	
South Mills		Tulls Bay	

#### About Mediacom Communications

Mediacom Communications Corporation is the 5<sup>th</sup> largest cable operator in the U.S. serving over 1.3 million customers in smaller markets primarily in the Midwest and Southeast. Mediacom offers a wide array of information, communications and entertainment services to households and businesses, including video, high-speed data, phone, and home security and automation. Through Mediacom Business, the company provides innovative broadband solutions to commercial and public sector customers of all sizes and sells advertising and production services under the OnMedia brand. More information about Mediacom is available at www.mediacomcable.com.

We look forward to partnering with you to ensure your projects are successful and your development has the best services available for your buyers.

Best regards,

Kim Mason

Kim Mason

Operations Director, North Carolina



#### Currituck County

Department of Planning and Community Development 153 Courthouse Road, Suite 110 Currituck, North Carolina 27929 252-232-3055 FAX 252-232-3026

#### **MEMORANDUM**

To: Mark Bissell, Bissell Professional Group

Justin Old, Allied Properties LLC

**From:** Tammy D. Glave, CZO, Senior Planner

Date: October 14, 2021 10-28-21 Response to TRC Comments

**Subject:** PB 18-23 Fost, Preliminary Plat/Special Use Permit, Phases 6-9

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- 3. Please fix the word 'southern' on Sheet 18, k-1.4.
- 4. UDO Section 6.2.1.H Alleys states that alleys shall only intersect with streets (not other alleys) and shall not have a pavement width more than 16'. This was discovered after the preliminary plat approval of previous phases. The alleys were designed with a 20' width as required for fire department access, and have a 30' right-of-way width. UDO Section 6.2.1.D has a standard for a local road in a PD-R calling for a 20' pavement width in a 30' right-of-way, so it appears that an easy solution to this problem is to reclassify these streets from alleys to private streets. This appears to solve the problem, so we have assigned street names to these former alleys.
- 5. Please verify your calculations for Recreational and Parkland Fee-in-lieu. Staff's calculations are much higher than those shown on Sheet 1. Current property access value (land only) = \$3,942,400 with a resultant fee-in-lieu of \$134,288.27 as opposed to the \$15,978.64 as shown. (UDO Section 6.5) It is our understanding that the land value was established at the time of preliminary plat approval for the first phases, which should continue through the remaining phases. We have not previously had a request to change to an updated valuation in the middle of implementing a development phasing plan. It does not seem reasonable to require a fee that is more than 8 times what was budgeted based on the first phases, using an increased value that is based on the developer's purchase of the property, but we believe it should instead be based on the pre-closing value and fee schedule that had already been established at the time of the original approvals and budgeting.
- 6. Please confirm that the area marked as 'Reserved as effluent disposal area' is not counted as open space. (UDO Section 7.1.3.D) This area has not been counted as open space.

<sup>&</sup>lt;sup>2</sup>Capacity percentages are based on 2021-2022 school year classroom standards and Sep 2021 ADM

<sup>&</sup>lt;sup>3</sup>Capacity percentages are based on the 2021-2022 school year classroom standards and Sep 2021 ADM

- 7. Please note on the plan that on-site effluent disposal is not permitted, an amended plan is required that must be approved by the BOC, and the BOC must approve the amended plan prior to any land disturbance for any part of the on-site effluent disposal system. A note has been added to the plan on sheet 5.
- 8. Please indicate that drainage easements are dedicated to Currituck County. (UDO Section 7.3.4) The notation has been added to sheet 1.

<u>Currituck County Chief Building Inspector/Fire Official (Bill Newns, 252-232-6023)</u>
Approved without comment.

<u>Currituck County Economic Development (Larry Lombardi, 252-232-6015)</u>
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#### The following items are necessary for resubmittal:

- 3 full size copies of revised plans
- 1 − 8.5 x 11" reduced copy
- 1- PDF digital copy of all revised or new documents and plans.

A use permit hearing is an evidentiary hearing where the Board of Commissioners must make a Quasi-Judicial Decision.

- An evidentiary hearing will be held for the Board of Commissioners to gather competent, material and substantial evidence to establish the facts of the case.
- All testimony is made under oath.
- The applicant or opposing parties shall establish written findings of fact and conclusions of law.
- Parties with standing may participate fully in the evidentiary hearing, including presenting evidence, cross-examining witnesses, objecting to evidence, and making legal arguments.
- Non-parties may present competent, material, and substantial evidence that is not repetitive.

Typical Use Permit (Quasi-Judicial) Hearing at BOC

- Swearing in of witnesses/speakers
- Presentation by County Staff on Application
- Required Presentation by Applicant or Authorized Agent (20-25 minutes)
  - Applicant to Present Findings of Fact
- Public Comment Period (typically 3 minutes each)
- Applicant Rebuttal (typically 5 minutes)
- BOC Deliberation & Decision



#### Kim Mason, NC Area Director

kmason@mediacomcc.com

216 B Shannonhouse Road Edenton NC, 27932 Edenton: 252-482-5583 Plymouth: 252-793-2491 Mobile: 252-497-0328

RE: New Build & Development

Dear Development manager;

As you know the key need for all homes in this 21st Century is a solid internet connection, be it for business, education or entertainment, the public demand is here.

With this in mind, as you plan for your development and build out, we would like to encourage you to reach out to us, as you do for other essential utilities. It is most economical and reasonable for you to work with us and have this valuable infrastructure in advance of selling and building the homes. Any build out costs can easily be recouped as the lots are developed and make your neighborhoods more appealing to families and professionals.

We invite, you to partner with us and contact us locally. We will process a ROI for your location to determine partnership feasibility and estimated cost to ensure your development has access to the best internet services available.

Our key contacts are, Kim Mason, Director for North Carolina – information above and our construction coordinator Nathanial Harris at 252-793-5256 or 252-339-9375.

Mediacom launched 1-Gig broadband speeds in the following areas of North Carolina and operates customer service offices in Edenton and Plymouth.

Bertie County	Martin County	Chowan County	Perquimans County
Colerain	Jamesville	Arrowhead / Chowan Beach	Hertford
Kelford	Northampton County	Edenton	Winfall
Lewiston	Conway	Currituck County	Tyrrell County
Powellsville	Galatia	Barco	Columbia
Roxobel	Jackson	Currituck	Washington County
Windsor	Rich Square	Grandy	Creswell
Camden County	Seaboard	Moyock	Plymouth
Camden	Severn	Point Harbor	Roper
Shiloh	Woodland	Poplar Branch	
South Mills		Tulls Bay	

#### About Mediacom Communications

Mediacom Communications Corporation is the 5<sup>th</sup> largest cable operator in the U.S. serving over 1.3 million customers in smaller markets primarily in the Midwest and Southeast. Mediacom offers a wide array of information, communications and entertainment services to households and businesses, including video, high-speed data, phone, and home security and automation. Through Mediacom Business, the company provides innovative broadband solutions to commercial and public sector customers of all sizes and sells advertising and production services under the OnMedia brand. More information about Mediacom is available at www.mediacomcable.com.

We look forward to partnering with you to ensure your projects are successful and your development has the best services available for your buyers.

Best regards,

Kim Mason

Kim Mason

Operations Director, North Carolina



# Major Subdivision Application

OFFICIAL USE ONLY:		
Case Number:		
Date Filed:		
Gate Keeper:		
Amount Paid:		

Address:    Moyock, NC 27958   Telephone:	APPLICANT:	Moyock Development, LLC	PROPERTY OW	/NER: Same
Telephone:  E-Mail Address:    jold@qhoc.com	Address:	417 Caratoke Hwy., Unit D	Name: Address:	Ounc
Telephone:  E-Mail Address:    jold@qhoc.com				
E-Mail Address:	Telephone:		Telephone	
Request  Physical Street Address:  Caratoke Highway  Parcel Identification Number(s):  Subdivision Name:  The Fost Tract  Number of Lots or Units:  TYPE OF SUBMITTAL  Conservation and Development Plan  Amended Sketch Plan/Use Permit  Amended Sketch Plan/Use Permit  Planned Unit Development  Type I OR XiType II  Construction Drawings (or amended)  Final Plat (or amended)  All information submitted and required as part of this process shall become publication.	-	jold@qhoc.com	•	
Physical Street Address: Caratoke Highway  Parcel Identification Number(s): 0015-000-0086-0000  Subdivision Name: The Fost Tract  Number of Lots or Units: 178 Phase: TYPE OF SUBMITTAL TYPE OF SUBMITTAL TYPE OF SUBMITTAL TYPE OF SUBMITTAL Conservation and Development Plan Conservation Subdivision  Amended Sketch Plan/Use Permit Conservation Subdivision  X Preliminary Plat (or amended) Planned Unit Development  Construction Drawings (or amended)  Final Plat (or amended)  Final Plat (or amended)  The Fost Tract  TYPE OF SUBMITTAL  TYPE OF SUBMITSION  Conservation Development  Conservation Development  Conservation Subdivision  Planned Unit Development  Planned Development  Planned Development  Tractitional Development  Conservation Subdivision  Planned Unit Development  Planned Development  The Fost Tract  TYPE OF SUBMITSION  Tractitional Development  Conservation Subdivision  Planned Unit Development  Planned Development  The Fost Tract  Type OF SUBMITSION  Tractitional Development  Conservation Subdivision  Planned Unit Development  The Fost Tract  Type OF SUBMITSION  Tractitional Development  Tractitional Development			Sa	me
Physical Street Address:  Caratoke Highway  Parcel Identification Number(s):  O015-000-0086-0000  Subdivision Name:  The Fost Tract  Number of Lots or Units:  178  Phase:  TYPE OF SUBMITTAL  Conservation and Development Plan  Amended Sketch Plan/Use Permit  Amended Sketch Plan/Use Permit  Conservation Subdivision  X Preliminary Plat (or amended)  Type I OR XiType II  Construction Drawings (or amended)  Final Plat (or amended)  Final Plat (or amended)  Planned Development  physical Street Address:  TYPE OF SUBDIVISION  Traditional Development  Conservation Subdivision  Planned Unit Development  Planned Development  Planned Development  All information submitted and required as part of this process shall become publication.  Table 12.1		OF AFFECANT TO TROTERT		
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applicable standards. All information submitted and required as part of this process shall become publicated.	u Tindi Fic	n (or amenaea)		
Property Owners / Ampliform*	applicable stand record.	dards. All information submitted and	ty for purposes of c required as part of	this process shall become public
Dule Dule	Property Owner	st/Applicant*		Date
NOTE: Form must be signed by the owner(s) of record, contract purchaser(s), or other person(s) having a	recognized prope	rty interest. If there are multiple proper	y owners/applicants	a signature is required for each.
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Major Stibdivision Application Page 5 of 12

Revised 8/29/2018

#### ATTACHMENT TO APPLICATION FOR PRELIMINARY PLAT/USE PERMIT APPROVAL for FOST Phases 6-9

#### Purpose of the Use Permit and Project Narrative

The goal is to continue building a community that has a creative design, providing a mix of different residential uses in close proximity to one another, while at the same time providing an efficient use of open space that promotes an active lifestyle and strong sense of community. Limited, small scale commercial uses may also be constructed primarily to serve the needs of the residents in this development.

The ways in which these goals and objectives will be achieved is shown graphically on the approved Master Plan, and is further described in the following paragraphs:

- Several different housing types are proposed, including traditional rear load traditional neighborhood development style townhomes, TND style homes with rear loaded driveways and garages, and more conventional frontloaded single family development lots.
- The Community will be pedestrian friendly, with open spaces and greenways strategically located throughout the community, and with a combination of sidewalks, multi-use paths/cart paths, a village green and a clubhouse with large outdoor recreation areas.
- The mixing of housing styles will also provide a mix of lot sizes, densities and streetscapes.
- The development is sensitive to existing wetlands, which have been delineated and confirmed
  by the Corps of Engineers; buffers have been shown and are being honored, existing drainage
  ways are being maintained and improved. Lower areas that are more flood prone are being set
  aside as open space, some of which will be used for the construction of lakes and ponds for the
  management of stormwater.

#### **USE PERMIT CRITERIA:**

#### 1. The use will not endanger the public health or safety.

Transportation, portable water, wastewater and stormwater management will be addressed in accordance with the following paragraphs:

<u>a.</u> <u>Transportation</u>: The main subdivision entrance will be connected directly to NC 168 (Caratoke Highway). Deceleration and acceleration lanes will be provided in accordance with NCDOT Standards and have been approved by NCDOT. Connectivity will be provided to the existing farmland to the southeast (Moyock Farms) and to the west (Flora Farm).

Roadways have been laid out generally as shown on the Master Plan and will be in accordance

with the approved right-of-way and pavement width characteristics, using the NCDOT complete streets planning manual as a guide.

Appropriate access has been made for emergency service vehicles in accordance with applicable standards including the N.C. Fire Code.

- <u>b.</u> Potable Water: Water will be supplied by Currituck County via an existing 12" main located on Caratoke Highway, which will be tapped and looped through the site. Fire protection will be provided in accordance with UDO Standards and the applicable ISO standards. The water system will be modeled to demonstrate adequate flow and pressure for fighting fires while meeting the maximum day domestic demand.
- <u>c.</u> <u>Wastewater</u>: A centralized off-site wastewater treatment and disposal facility will be utilized (Currituck Water & Sewer Major Utility), and connected by running a force main from the Fost Tract to the treatment facility at Eagle Creek. A wastewater collection system will be constructed by the Developer and managed by the same wastewater utility. The utility will be regulated by the North Carolina Utilities Commission.
- d. Stormwater Management: A major drainage way known as Rowland Creek Canal runs through the development property. Due to the importance of this drainage feature not only to the development but to other properties located both upstream and downstream of this property, the Developer has committed to cleaning and laying back the slopes and stabilizing the portion of Rowland Creek that runs from Eagle Creek to the railroad next to NC 168. The Developer has also committed to improving the common property line ditch along Ranchland and the Moyock Farms property, and to excavate a new ditch that would be available to Ranchland along the common property line from the Moyock Farms site to south of the power easement. The Developer will also improve the sections of Rowland Creek downstream of the Fost property that require maintenance.

On-site stormwater will be managed by constructing a series of stormwater management ponds that will be interconnected and will retain and slow-release stormwater to existing outlets. In addition to modeling and retaining stormwater to the UDO and Stormwater Manual standard of the difference between runoff from the 10-year developed condition and runoff from a 2-year wooded condition site, stormwater will be modeled for the 100-year storm to determine improvements that are needed without adversely impacting neighboring properties. Implementation of this plan should improve drainage for neighboring properties compared to existing conditions.

Stormwater will be conveyed to on-site retention ponds through a combination of curbs with inlets, stormwater pipes, and open vegetated swales.

## 2. The use will not injure the value of adjoining or abutting lands and will be in harmony with the area in which it is located.

Land use to the west is agricultural, with one dwelling unit located approximately 435 feet from the nearest property line and approximately 565 feet from the nearest structure. Land use to the north, across the railroad and Caratoke Highway is commercial, land to the east is currently farmland, which has been approved for residential development, and existing residential lots, and land to the south is residential lots. Perimeter compatibility is being addressed as follows:

- a. To the west: A 50 foot farm buffer is being provided to existing farmland; however, the adjacent Flora farm has now been rezoned to C-MXR for compatible residential development.
- b. To the north (Caratoke Highway): A berm is being provided along the highway along with stormwater ponds flanking the main entrance road. The nearest dwelling unit will be set back at least 175 feet from the highway right-of-way at the closest point. The visual impact of the development will be minimized by setbacks, berms and landscaping.
- c. To the east: A 50 foot vegetative farm buffer is being provided along the existing farmland (which has been approved for residential development); traditional single family dwelling lots are being proposed in the areas that back up to existing dwelling units in the adjacent Ranchland subdivision. Even though the dwelling types are believed to be compatible, a minimum 50 foot buffer is being provided between the developments.
- d. To the south: Compatible residential development is being proposed and a minimum 90 foot open space buffer is shown to the property line. The buffer may contain a pond.
- e. Limited commercial development is proposed to be located interior to the development and will front along the landscaped entrance boulevard.

Lot size ratios and density ratios are comparable to other developments that have been approved in the immediate area and in other parts of Moyock.

The applicant is not aware of any adverse impacts on land value in the surrounding area. The applicant's proposed stormwater improvements will benefit the property values of the nearby Ranchland community. The Development is anticipated to **increase** the values of nearby properties by improving existing drainage problems.

The site is adjacent to non-agricultural activities including the Ranchland neighborhood to the south and southeast, and commercial and residential uses to the east. The site is also in close proximity to the Eagle Creek neighborhood. The proposed use is compatible with these adjacent uses and furthers the County's goal of concentrating new residential development in the vicinity of existing residential development. Nearby communities include existing planned communities of 588 lots to the west, a 275 lot subdivision to the east and 426 lots under development to the north of Caratoke Highway, all in the vicinity of the Fost tract. The development is appropriate for the location as it continues existing

development patterns in this part of Moyock. Lot area ratios and density ratios are consistent with other developments that have been approved recently nearby and in other parts of Moyock.

- 3. The use is consistent with the goals, objectives, and policies of the Land Use Plan or other applicable county-adopted plans:
  - a. The use is consistent with the following policies of the Currituck County Land Use Plan ("LUP"): The project is consistent with the Land Use Plan policies addressing density, recreational and open space, transportation infrastructure, and utilities:
  - *i.* **Density:** The project is located within the Full Service Area of the Land Use Plan, and proposes a density of 2.1 units per acre. The proposed use will include residential development in village like-clusters and will include open spaces surrounding the clusters, helping to prevent traditional suburban sprawl. The applicant is proposing centralized sewer, and the Full Service Area designation recognizes that higher density (above 2 units per acre) is appropriate here. The proposed use will not interfere with nearby resource production activities. This is consistent with the County's density designations because development is occurring along Caratoke Highway, a major transportation corridor, and is an infill site between two existing residential subdivisions but with its own access to Hwy 168. This is consistent with the following Land Use Plan policies:

POLICY AG6: For areas experiencing intense development pressure, new residential development may be allowed to locate in COMPACT, VILLAGE-LIKE CLUSTERS, PREFERABLY NEAR EXISTING, NON-AGRICULTURAL ACTIVITIES AND SERVICES, or in other locations that will not interfere with resource production activities. Overall density shall remain very low, with permanent open space, dedicated during the development review process, surrounding such clusters of homes. LUP, p. 9-7.

POLICY HN1: Currituck County shall encourage development to occur at densities appropriate for the location. LOCATION AND DENSITY FACTORS shall include whether the development is within an environmentally suitable area, the type and capacity of sewage treatment available to the site, the adequacy of transportation facilities providing access to the site, and the proximity of the site to existing and planned urban services. For example, projects falling within the Full Services areas of the Future Land Use Map would be permitted a higher density because of the availability of infrastructure as well as similarity to the existing development pattern. Such projects could be developed at a density of two (2) or more dwelling units per acre.... LUP, p. 9-7.

POLICY HN3: Currituck County shall especially encourage two forms of residential development, each with the objective of avoiding traditional suburban sprawl...COMPACT, MIXED USE DEVELOPMENTS or DEVELOPMENTS NEAR A MIXTURE OF USES that promote a return to balanced, self-supporting community centers generally served by centralized water and sewer. The types of development are contemplated for the Full Service Areas identified on the Future Land Use Map. LUP, p. 9-7.

#### ii. Utilities:

<u>Stormwater</u>: The applicant is aware of stormwater drainage issues in the surrounding properties and has conducted a stormwater study to investigate causes in Rowland Creek, upstream and downstream from the property. That study has shown significant restrictions to flow on both on- and off-site properties, which the applicant is willing to remove and restore with the off-site

landowners' consent. The applicant is also placing significant drainage facilities on site to collect and control its own stormwater. Where adjacent landowners are unwilling to allow the applicant to remove off-site obstructions, the applicant is willing to provide a additional stormwater storage on site to minimize flooding. The applicant plans to model the 100-year storm event and to manage stormwater runoff from that event. These commitments will provide a public benefit to the County by improving existing drainage problems with private funds, and providing a responsible mechanism for the site and surrounding areas to drain.

<u>Wastewater</u>: The applicant is proposing to connect to an off-site wastewater treatment and disposal facility. This will give the existing system new funds for upgrades, repair, and maintenance. Thus, the development will have a positive impact on sewer facilities and will not strain the County's infrastructure, consistent with the Full Service Area designation which anticipates additional density on this site. The applicant is also setting aside land to receive effluent and/or groundwater from the off-site facility.

This is consistent with the following Land Use Plan policies:

**POLICY WS7**: Currituck County allows for the appropriate use of PACKAGE SEWAGE TREATMENT PLANTS as a means of achieving more efficient land use, while properly disposing of waste. Such systems shall have a permanent organizational ownership to guarantee their proper management, including operation, maintenance and replacement needs. Depending on their location in the county, such systems may be required to have a design that allows for assimilation into a centralized system at a future date. LUP, p. 9-13.

<u>POLICY WQ3</u>: Currituck County supports policies, plans and actions that help protect the water quality of the county's estuarine system by preventing SOIL EROSION AND SEDIMENTATION, and by controlling the quantity and quality of STORMWATER RUNOFF entering the estuary. LUP, p. 9-17.

<u>POLICY WQ4</u>: RUNOFF AND DRAINAGE from development, forestry and agricultural activities shall be of a quality and quantity as near to natural conditions as possible. Post-development runoff shall not exceed pre-development volumes. LUP, p. 9-17.

*iii.* Recreational and Open Space: The project will provide for sidewalks, greenways, and walking trails to encourage mobility by pedestrians and bicyclists. It also preserves a significant amount of open space and natural features throughout to promote recreation and preserve natural areas. This is consistent with the following Land Use Plan policies:

**POLICY TR9**: BIKEWAY FACILITIES shall be encouraged as energy-efficient, healthful, and environmentally sound alternatives to the automobile. The inclusion of bikeways, sidewalks, trails, and other alternatives to the automobile shall be encouraged in both public and private developments. LUP, p. 9-11.

<u>POLICY PR4</u>: The County shall seek to identify, plan for and develop a system of OPEN SPACE GREENWAYS, HIKING and BIKING TRAILS as opportunities may allow. The use of (1) natural corridors such as streams and floodplains, and (2) man-made corridors such as utility and transportation rights-ofway and easements, shall be emphasized. LUP, p. 9-14.

**POLICY PR6**: All new residential development shall provide for ADEQUATE OPEN SPACE AND RECREATION IMPROVEMENTS including, as may be appropriate, funding in proportion to the demand created by the development. The amount of open space and improvements may be determined according to the number of dwelling units in the development and/or by a percentage of the total acreage in the development. Fees in lieu of land dedication shall be based on the inflation adjusted assessed value of the development or subdivision for property tax purposes. LUP, p. 9-14.

*iv. Transportation infrastructure:* As set forth in the master plan, the project will provide for paved public roadways and drainage infrastructure, and will include convenient circulation within the proposed neighborhood. The project will seek to preserve existing buffering along NC 168 where feasible. The project will have direct access to and frontage on NC 168, and will not include access to higher intensity development through an area of lower intensity development. This is consistent with the following Land Use Plan policies:

**POLICY TR12**: New residential developments shall provide for the installation of PAVED PUBLIC ROADWAY AND DRAINAGE INFRASTRUCTURE at the time of development. This policy is intended to prevent the creation of substandard developments that must later correct for infrastructure problems that could have been avoided, had they been installed properly from the beginning.... LUP, p. 9-11.

<u>POLICY CA1</u>: The important economic, tourism, and community image benefits of attractive, functional MAJOR HIGHWAY CORRIDORS through Currituck County shall be recognized. Such highway corridors, beginning with US 158 and NC 168, shall receive priority attention for improved appearance and development standards, including driveway access, landscaping, buffering, signage, lighting and tree preservation. LUP, p. 9-19.

**POLICY TR11**: ACCESS TO HIGHER INTENSITY DEVELOPMENT shall generally not be permitted through an area of lower intensity development. For example, access to a multi-family development, a major park facility or other large traffic generator shall not be permitted through a local street serving a single-family residential neighborhood. LUP, p. 9-11.

<u>POLICY TR8</u>: Local streets shall be designed and built to allow for convenient CIRCULATION WITHIN AND BETWEEN NEIGHBORHOODS and to encourage mobility by pedestrians and bicyclists. Care shall be taken to encourage local street "connectivity" without creating opportunities for cut-through traffic from outside the connected areas. LUP, p. 9-11.

b. The proposed use is consistent with the following provisions of the Moyock Area Policy Emphasis of the Land Use Plan:

**Moyock Area Policy Emphasis**: The policy emphasis of this plan is on properly managing the increased urban level of growth that this area is sure to experience over the next decade and beyond. Residential development densities should be medium to high depending upon available services. In areas where on-site wastewater is proposed and other County services are limited, development density should be limited to 1-2 units per acre. However in areas where central sewer is proposed or existing, additional services are available and the character of the surrounding areas supports it, higher density ranging from 3-4 units per acre could be considered through the use of overlay zones.

Clustered housing developments, with open space required by ordinance, will need to be encouraged. If sewage treatment collection systems are built, whether publicly or privately, all such

systems should be designed so as to allow them to be tied together into a single system at some point in the future. LUP, p. 11-4.

The proposed use will include clustered housing accompanied by open spaces. The applicant is proposing to connect to a privately developed major sewer utility to avoid strain on County infrastructure, since private central sewer is existing in the area and available. Because the wastewater system will be a central sewer system, and the 2.1 units/acre is well below the 3-4 units per acre allowed where central sewer is available, the proposed use is consistent with the Moyock Area Policy Emphasis.

#### c. The proposed use is consistent with the Future Land Use Map.

The Currituck County Future Land Use Map designates the site as a Full Service area. In Full Service areas, residential density can be increased to 3-4 units per acre depending on the available services. A greater diversity in housing types is also appropriate in Full Service Areas. The proposed use will have 2.1 units per acre, which is below the contemplated development densities from Section 3.7.3 of the UDO for PD-R districts (up to three units per acre). The proposed use will also include diversity in housing types, including townhomes, cottage style single-family lots and traditional single family lots.

### d. The proposed use is consistent with the following policies and action items of the Moyock Area Small Area Plan ("SAP"):

The proposed use will include open spaces, sidewalks, greenways, and walking trails to for recreational purposes, and a clubhouse for community gathering. The plan will provide for connections to the Flora Farm and to Moyock Farms, as appropriate. The applicant has studied the existing stormwater drainage issues and will ensure that the project improves these issues.

- **POLICY TR 2**: Ensure that development is designed with an interconnected, multi-modal transportation network between neighborhoods, activity centers, and other destinations to improve mobility and emergency access. Development of an interconnected road network east and west of Highway 168 that allows north-south movement for local residential traffic is strongly encouraged. SAP, p. 16.
- **POLICY IS 1**: The costs of infrastructure, facilities, and services related to new growth and development should be borne primarily by those creating the demand. SAP, p. 17.
- **POLICY IS 4**: Ensure that stormwater runoff, soil erosion, and sedimentation is properly managed to reduce nuisance flooding and pollution of sensitive environmental areas. SAP, p. 17.
- **POLICY IS 5**: Encourage retrofitting of NCDOT drainage infrastructure to manage nuisance flooding. SAP, p. 18.
- <u>ACTION IS 5A</u>: Coordinate with NCDOT to develop a plan of action for addressing ... Ranchland subdivision street flooding.... SAP, p. 18.
- <u>POLICY R-1</u>: Expand and develop recreational opportunities for all ages and users including access to the water and natural environment, walking trails, multi-purpose fields, multi-purpose community building, and other non-traditional types of recreational opportunities that are consistent with the Currituck County Parks and Recreation Master Plan. SUP, p. 21.

### The use will not exceed the county's ability to provide adequate public facilities, including, but not limited to, schools, fire and rescue, law enforcement, and other county facilities.

The site has adequate access to NC 168 and will be adequately served by, police, fire, and emergency services. The applicant has completed a stormwater study and improvements plan to ensure that stormwater management will be enhanced not only for the development but also for the surrounding community. Sewer service will be provided off-site and will not burden the county's wastewater system. The Development will not exceed the County's ability to provide adequate public facilities with respect to schools because the site is entirely in the Shawboro Elementary school district. In 2020, school capacity in this district was already allocated to students generated by this development, and these students have been included in the County's committed capacity tool. At the time of the hearing on this matter, there will be actual capacity in this district, and the County is expected to have a program in place for expansion of other nearby schools in the district which will deliver additional capacity to the elementary and middle school in this area within 2 years of approval.

#### The Fost Tract - Phases 6-9

March 23, 2021

Scheduled Time/Place: 6:00PM at Eagle Creek Event Pavilion

The meeting began at 6:00PM/Meeting Ended: at approximately 6:40PM

**Attendees:** (See attached sign-in sheets)

Also: Jeannie Turner, Currituck County Justin Old, Developer Mark Bissell, Engineer

Comments from the Community	How Addressed
Will the project tie in to Guinea Mill canal and will it be part of the Guinea Mill tax district?	No, the property drains away from Guinea Mill to Rowland Creek and 168. It will not be included in the drainage district
Will there be a traffic light and can Ranchland connect to it to get out to 168 at a light?	There will be a traffic signal at the main entrance to Fost. The timing for its installation will be up to NCDOT based on traffic counts. The Moyock Farms community will connect to Fost and be able to utilize the light, but the Ranchland property that borders Moyock Farms is all privately owned and the key will be finding a way to get a road connected from Ranchland to Moyock Farms.
Concerning the new ditch along Ranchland: where will it stop? Will it turn at the end?	The ditch will run to the end of the development and stop without turning.
Will it connect to Guinea Mill?	There are developed lots in between Guinea Mill and this property and all drainage will be in the opposite direction (toward Rowland).
There is a pile of dirt bermed up adjacent to the new ditch. Will that impede the drainage?	That is a temporary stockpile during the construction of the ditch. Any berming will be done on the Fost side of the Ranchland ditch so that Ranchland can benefit from it as much as possible.
How is it designed and how do you know what direction it will drain?	It was engineered and placed on a positive grade to drain toward Rowland Creek.
How will wastewater be handled?	A pump station will pump wastewater from Fost to the Eagle Creek wastewater treatment

	plant where it will be treated. Some of the clean water will be disposed of at Eagle Creek and eventually some of it will be returned to the Fost Tract and disposed of on a site that has been set aside for the purpose.
Who owns the wastewater system?	A privately owned public utility company is in the process of acquiring the Eagle Creek facility, and will own and operate it.
How many lots will there be?	479 including townhomes.
Will there be fire hydrants? And who maintains the hydrants?	There will be fire hydrants and they will be maintained by the County water department.
I heard that no one is in charge of the water department at this point.	Will Rumsey is the utilities director in charge of both water and sewer for the county, and Dave Spence is the water distribution supervisor.
Will there be dry hydrants for when the hydrants run dry?	There will not be dry hydrants, but there will be fire hydrants located throughout the subdivision which should not run dry.
I understand that the BOC is amending the ordinance to keep new development from raising lot elevations.	We have not heard of any such proposal. The lot grades in Fost will be raised, but the berming and ditching is being provided to protect Ranchland and other adjacent properties.

#### **Summary:**

Four residents from Ranchland were in attendance along with the developer, a representative from Currituck County, and the Engineer. Several people stayed to talk informally and look at the development plan. The meeting was adjourned at approximately 6:40PM.

### **Community Meeting Sign-In Sheet The Fost Tract** March 23, 2021 6:00PM

NAME	ADDRESS	TELEPHONE	E-MAIL S
Mark Bissell	PO 1068 K.H	(252)261-3266	mark@bissellprefessionelgrup
Justin	131 sprinte la	(252/435-276	Jula 10 Witecocon
Jennie Turner	Carrituax County	(252)232-6031	jennie tvrnere curi trul connhyne
Jeff O'itan	121 P.No Or	* 252 - 425. 9201	6-9, Pr
LON DIXON	114 Longhorn DR	435 9318	ases
mykastrasie	=133 Pint Dk	252-43-6089	moyockked a yahoo con &
BRIAL PETTET	129 Langtory DR	757-681-930	noyockkeda yahoo con &
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			Attach
			Packet Pg. 237

# STATE OF NORTH CAROLINA COUNTY OF CURRITUCK

#### IN THE GENERAL COURT OF JUSTICE SUPERIOR COURT DIVISION 21 CVS 78

STATE OF NORTH CAROLINA, ex rel., NORTH CAROLINA DEPARTMENT OF ENVIRONMENTAL QUALITY,  Plaintiff, v.	CONSENT JUDGMENT
SANDLER UTILITIES AT MILL RUN, LLC, Defendant.	) ) ) ) )

Plaintiff, the State of North Carolina, by and through the North Carolina Department of Environmental Quality ("Plaintiff" or "DEQ"), and Defendant Sandler Utilities at Mill Run, LLC ("Defendant") hereby agree to the entry of this Consent Judgment in order to resolve the matters in controversy between Plaintiff and Defendant.

The Court makes, and Plaintiff and Defendant (collectively "the Parties") hereby stipulate to, the following findings of fact and conclusions of law:

#### FINDINGS OF FACT

1. Plaintiff is the sovereign State of North Carolina. This action was brought on the relation of the North Carolina Department of Environmental Quality ("DEQ"), the State agency established pursuant to N.C. Gen. Stat. § 143B-279.1 et seq., and vested with the statutory authority to enforce the State's environmental pollution laws, including laws enacted to protect the water quality of the State. The Division of Water Resources ("DWR") is a division within DEQ and all actions taken by DWR are necessarily actions of the Plaintiff.

- 2. Defendant is a limited liability corporation formed in North Carolina with its principal office in Virginia and doing business in North Carolina. Defendant is the owner and operator of the wastewater collection system serving the Eagle Creek Subdivision, Eagle Creek Golf Club, and Moyock Middle School in Moyock, Currituck County, North Carolina ("Collection System").
- 3. The Eagle Creek Subdivision is a development in Moyock, Currituck County, North Carolina. The development includes approximately 420 single-family homes and is generally situated between Roland Creek Canal to the north and Guinea Mill Run Canal to the south. Stormwater swales run beside the roadways in the development and between homes. Those swales drain to the Roland Creek Canal to the north and Guinea Mill Run Canal to the south of the development.
- On May 2, 2013, DWR issued non-discharge permit number WQCS00290
   ("Permit") to Defendant for operation of the Collection System.
- 5. The Collection System consists of 4.8 miles of vacuum sewer and utilizes vacuum pumps to maintain a constant negative pressure within the sewer pipes. Domestic waste from individual homes connected to the system collects in containment vessels, commonly referred to as "pits," with each pit serving two homes. When the level of waste within the pit reaches a determined level, pneumatic pressure triggers the opening of a valve to the piping connected to the sewer line. The vacuum withdraws waste and wastewater from the pit into the sewer line. When the waste level within the pit drops, the valve connecting the pit to the sewer line is closed, allowing waste to again collect within the pit and maintaining the vacuum in the sewer line.
- 6. Wastewater from the Collection System is conveyed to the Sandler Utilities Wastewater Treatment Plant for treatment and disposal as authorized by a separate permit,

WQ0014306.

- A release of wastewater from a wastewater collection system such as Defendant's is referred to as a sanitary sewer overflow or "SSO."
- 8. Condition I.2 of the Permit requires that the Collection System "shall be effectively managed, maintained and operated at all times so that there is no SSO to land or surface waters, nor any contamination of groundwater." In the event of a system failure, the permittee is required to "take immediate corrective actions, including actions that may be required by [DWR] such as the construction of additional or replacement sewer lines and/or equipment."
- 9. Condition II.5 of the Permit provides that "for each pump station without pump reliability...at least one fully operational spare pump capable of pumping peak flow shall be maintained on hand."
- 10. Condition II.11 of the Permit provides that, if an SSO occurs, Defendant "shall restore the system operation, remove visible solids and paper, sanitize any ground area and restore the surroundings."
- 11. Condition IV.2 of the Permit provides that SSOs of over 1,000 gallons and any SSO that reaches surface waters, must be verbally reported to DWR as soon as possible, but no later than "24 hours following the occurrence or first knowledge of the occurrence."
- 12. On September 29, 2020, DWR's Washington Regional Office began receiving complaints from Eagle Creek residents indicating that the Collection System was not operating properly at homes throughout Eagle Creek.
- 13. On September 30, 2020, DWR inspectors Sarah Toppen and Victoria Herdt from the Washington Regional Office conducted a site inspection to investigate the complaints. DWR's investigation confirmed that the system had not been operating properly since September 27, 2020

due to failure of one of the two vacuum pumps. The vacuum pump failure caused SSOs that drained into stormwater swales in front of and between homes.

- 14. Upon learning of the incident, Defendant called in technical support from Airvac, the initial pump supplier, to assist in bringing the Collection System back on line. Due to insufficient replacement parts available on site, Defendant purchased replacement parts for the vacuum pumps and mechanical equipment located in the pits to address the reported issues.
- 15. Defendant provided cleanup and lime application at any location showing evidence of overflow and in all roadside ditches around October 9, 2021. Performance issues in the Collection System were resolved by October 11, 2020.
- 16. On October 26, 2020, the Washington Regional Office again began receiving complaints indicating that the Collection System was experiencing further performance issues resulting in SSOs.
- 17. On October 27, 2020, Ms. Toppen and another DWR inspector, Allen Stewart, conducted a site inspection to investigate the complaints and learned that a high water alarm within the Collection System had failed, causing the water to overflow and flood both of the system's vacuum pumps, taking them offline. The vacuum pumps' failure resulted in SSOs, and a water sample from the stormwater swale at 125 Eagleton Circle contained fecal coliform (bacteria from fecal matter) that were too numerous to count.
- 18. In response to this incident, Defendant requested assistance from technicians from Airvac and FloVac (another vacuum system supplier), and the vacuum pumps were restored and fully operational by October 29, 2020. Among other equipment and parts, Defendant ordered new parts for the malfunctioning high water alarm, which were installed on November 5, 2020.
  - 19. On November 13, 2020, the Washington Regional Office began receiving new

complaints stating that, beginning on or around November 11, 2020, the Collection System was not operating properly causing SSOs.

- 20. On November 14, 2020, David May, DWR Regional Supervisor for the Washington Regional Office, conducted a site visit to investigate the complaints. DWR's investigation confirmed the Collection System was experiencing performance issues resulting in SSOs. A substantial number of houses and customers remained without functional sewer service through at least November 16, 2020.
- 21. Complaints to the Washington Regional Office recommenced on November 20, 2020. Residents indicated that the Collection System was not functioning properly, that residents were once again without sewer service due to vacuum leaks disrupting operation of the Collection System, and that some pits were overflowing. A number of houses remained without functional sewer service or sporadic sewer service through about November 25, 2020.
- 22. On December 16, 2020, the Washington Regional Office once again received complaints from Eagle Creek residents regarding disruption of sewer service and SSOs. DWR's investigation suggested that the problem was limited to one section of the Eagle Creek development due to a disconnected line in a pit and sustained rainfall. Residences affected by the disconnected line remained without functional sewer service or sporadic sewer service through approximately December 18, 2020.
- 23. Additional complaints to the Washington Regional Office were received on January 26, 2021, with residents complaining that they lacked sewer service and describing other performance issues including SSOs.
- 24. Defendant has been subject to various enforcement actions by the DWR related to the Permit as a result of the incidents described above.

- 25. On October 7, 2020, DWR issued a Notice of Violation and Notice of Intent to Enforce ("First NOV") to Defendant pertaining to SSOs and system failures occurring from September 27, 2020 and ongoing as of the date the NOV was issued. The First NOV alleged the following violations:
  - Violation of Permit Condition I.2 for failure to effectively manage, maintain and operate the Collection System at all times so there are no SSOs to land or surface waters;
  - Violation of Permit Condition II.11 for failure to restore the system operation,
     remove visible solids and paper, sanitize any ground area and restore
     surroundings after an SSO;
  - c. Violation of Permit Condition IV.2 for failure to properly report SSOs.
- 26. In a letter dated October 27, 2020, Defendant responded to the First NOV. Defendant stated that replacement parts for the failed vacuum pumps were not readily available, causing a delay in fixing the Collection System. Once the vacuum pump was replaced, Defendant noted that two sewage pumps malfunctioned, forcing the entire system offline again for cleaning. Defendant acknowledged that its operator's personnel resources were stretched during the event limiting the ability to address the Collection System problems in a timely manner and that additional support from a vacuum sewer specialist was called in. Defendant stated that cleanup of any discharges was performed and the System was ultimately repaired.
- 27. DWR assessed a civil penalty of \$62,517.96 against Defendant arising out of the allegations in the First NOV on December 10, 2020 ("Civil Penalty") for fifteen violations of Permit Condition I.2 between September 27, 2020 and October 11, 2020.
  - 28. On November 23, 2020, DWR issued a Second Notice of Violation and Notice of

Intent to Enforce ("Second NOV") to Defendant pertaining to SSOs and system failures occurring during the period from October 26, 2020 through November 5, 2020. In addition to alleging violations of Permit Conditions I.2, II.11 and IV.2, the Second NOV alleged violations of Permit Condition II.5<sup>1</sup> for failure to maintain operational replacement pumps for stations without pump reliability and N.C. Gen. Stat. § 143-215.1(a)(1) for making an outlet into waters of the State without a permit.

- 29. In a letter dated December 15, 2020, Defendant responded to the Second NOV. Defendant stated that a "very large investment has been made into new equipment and parts" and that new equipment and parts were ordered. Defendant also stated that spills were addressed with lime application and solid waste overflow was physically removed. In response to the incident, Defendant installed a new vacuum pump and motor, acquired a spare vacuum pump and motor, purchased a new sewer pump, and replaced multiple controllers and valves within individual pits. Defendant further replaced parts needed to ensure functionality of the high-level alarm (which was the precipitating cause of this incident) to alleviate water intake into the vacuum sewer pumps causing failure.
- 30. On December 16, 2020, DWR issued a Third Notice of Violation and Notice of Intent to Enforce ("Third NOV") to Defendant pertaining to SSOs and system failures occurring during the period from November 11, 2020 through November 16, 2020. The Third NOV alleged violations of Conditions I.2, II.11, and IV.2 as well as N.C. Gen. Stat. § 143-215.1(a)(1).
- 31. On January 14, 2021, DWR issued a Fourth Notice of Violation and Notice of Intent to Enforce ("Fourth NOV") to Defendant pertaining to SSOs and system failures occurring during the period from November 20 through November 25, 2020. The Fourth NOV alleged violations

A typographical error in the Second NOV identifies a violation of Permit Condition II.6, not II.5. The narrative description of the violation refers to Permit Condition II.5, however.

of Conditions I.2, II.11, and IV.2 as well as N.C. Gen. Stat. § 143-215.1(a)(1).

- 32. DEQ filed the instant action on March 4, 2021, seeking injunctive relief for existing or threatened violations of various laws and rules governing the protection of water quality pursuant to N.C. Gen. Stat. § 143-215.6C.
- 33. The Parties have reached a mutually agreeable and reasonable resolution of the injunctive relief sought by DEQ through this suit which they seek to memorialize in this Consent Judgment.
- 34. The Court has reviewed the pleadings and supporting materials in this matter. Counsel for the Parties have represented to the Court that their respective clients have reviewed and approved the substance of the proposed Consent Judgment and that the Parties supported a request that this Court approve a Consent Judgment embodying their agreement.

#### **CONCLUSIONS OF LAW**

- 1. This matter is properly before this Court, which has jurisdiction over the Parties and subject matter of this action pursuant to N.C. Gen. Stat. § 143-215.6C and other provisions of law.
  - 2. Venue is proper pursuant to under N.C. Gen. Stat. §§ 1-79 and 143-215.6C.
- 3. Defendant is subject to non-discharge collection system permit number WQCS00290. Pursuant to that permit, as well as North Carolina laws and regulations, Defendant is expressly prohibited from discharging collected sewage and wastewater to land and waters of the State. See 15A NCAC subchapter 2T ("Waste Not Discharged to Surface Waters"); N.C. Gen. Stat. § 143-215.1(a)(1) (disallowing any person from making "any outlets into waters of the State" without receiving a permit to do so).
  - 4. Whenever DEQ has reasonable cause to believe that any person has violated or is

threatening to violate any of the provisions of the State's environmental laws or administrative rules, including State water quality laws and rules, DEQ is authorized to "request the Attorney General to institute a civil action in the name of the State upon the relation of [DEQ] for injunctive relief to restrain the violation or threatened violation." N.C. Gen. Stat. § 143-215.6C. That section further provides that "[u]pon a determination by the court that the alleged violation of the provisions of this Part or the regulations of the Commission has occurred or is threatened, the court shall grant the relief necessary to prevent or abate the violation or threatened violation." N.C. Gen. Stat. § 143-215.6C.

- 5. As of the date of entry of this Consent Judgment, Defendant has repeatedly violated the conditions of its Permit and North Carolina's water quality laws, including, but not limited to, failing to properly manage, maintain and operate the Collection System to prevent SSOs, failing to maintain replacement equipment to prevent SSOs, and failure to restore consistent service to Eagle Creek residents.
- 6. The current state of the Collection System presents an ongoing threat that the Eagle Creek Development will continue to experience performance issues and SSOs into nearby surface water in violation of the Collection System Permit, and in violation of N.C. Gen. Stat. § 143-215.1(a)(1).
- 7. The State is entitled to permanent injunctive relief against Defendant to abate the repeated previous violations and prevent the threatened violations set forth in the Complaint pursuant to N.C. Gen. Stat. § 143-215.6C.
- 8. Based on the Court's review of the pleadings and materials submitted, the Court has concluded that the relief reflected in the Consent Judgment represents a lawful, fair, and reasonable resolution of this matter, consistent with the purposes of N.C. Gen. Stat. § 143-215.1,

and this Court further concludes that it is fully authorized and justified in entering this Consent Judgment.

9. The Parties expressly waive any argument that the recitation of the above Findings of Fact and Conclusions of Law is insufficient to support the injunctive relief ordered below.

Based upon the foregoing Findings of Fact and Conclusions of Law, IT IS THEREFORE ORDERED, ADJUDGED, AND DECREED as follows:

Defendant is ordered to do the following:

- Fully comply with all terms and conditions of the Permit and State water quality laws, including by maintaining the Collection System in a manner that prevents the discharge of waste onto to land or into surface waters;
- 2. Report to a DWR staff member as soon as possible but in no case more than 24 hours following knowledge of the occurrence of any material equipment failure, any material system failure, and any SSO regardless of volume and file a written report within five days outlining actions taken or proposed to address the equipment failure, system failure, and SSO and prevent recurrence. An equipment or system failure shall be considered material if there is any SSO associated with the failure or the failure affects four or more residences in the Eagle Creek Development. The reports referenced in this subparagraph shall be submitted to the following email address:

David May, DWR david.may@ncdenr.gov

- 3. Within 30 calendar days of entry of this Consent Judgment:
  - a. Secure all pits to ensure only Defendant and its operators have pit access;

- b. Submit to DWR for approval (which may include conditions) a plan for Defendant's operators to receive any necessary training in operating and maintaining the Collection System, including specialized training in vacuum system operation, or provide documentation that such training has been received ("Operator Training Plan");
- c. Submit to DWR for review and approval (which may include conditions) a plan to prevent future SSOs, restore and sanitize areas impacted by prior SSOs, and expeditiously restore and maintain service to homeowners in the event of any system failures pending an engineering evaluation of the Collection System ("Interim Service and Restoration Plan");
- d. Provide to DWR a complete engineering evaluation detailing actions necessary to prevent future SSOs and system failures, including but not limited to necessary upgrades to the design and physical infrastructure of the Collection System ("Engineering Evaluation").
- 4. Within 60 calendar days of entry of this Consent Judgment:
  - a. Submit to DWR for review and approval (which may include conditions) a plan with actions to be taken to address necessary and proposed upgrades included in the Engineering Evaluation ("System Upgrade Plan"). The System Upgrade Plan must include a proposed schedule with dates by which each activity will be completed and, as appropriate, the frequency with which those activities will be repeated.
- If DWR requires plan revisions in order for any of the plans listed above to be approved,
   Defendant shall resubmit the plan incorporating said revisions within 15 days of written

notification by DWR that such revisions are required. DWR's discretion to require plan revisions shall be limited to revisions necessary to ensure compliance with North Carolina's water quality laws and regulations including provisions applicable to wastewater collection systems.

- 6. Once each submission is approved (the Operator Training Plan, the Interim Service and Restoration Plan, and the System Upgrade Plan) including any conditions added by DWR to each, Defendant shall execute each plan in accordance with the dates included therein.
- 7. Once the work set forth in the approved System Upgrade Plan, including any conditions added by DWR, is completed, Defendant shall submit a final report documenting the results of the activities set forth in each respective plan. The final report shall include a certification from a licensed professional engineer.
- 8. Unless otherwise indicated, Defendant shall submit all plans and reports referenced above to:

David May, DWR david.may@ncdenr.gov

- 9. It is further ordered that this Consent Judgment shall take effect immediately and shall remain in effect until the Permittee can adequately demonstrate that the collection system can operate in a reliable manner and maintain compliance with North Carolina water quality laws and regulations on a consistent basis. The Defendant may request termination of this Consent Judgment by the Court at no time sooner than one year following submittal of the final report described in Paragraph 7 above and subsequent approval by DWR. The Parties shall comply with all terms of this Consent Judgment.
- 10. This Consent Judgment shall be binding upon Defendant's successors and assigns.

Defendant shall not transfer any of the assets that are the subject of the Complaint, including the Permit or the Collection System, unless and until Defendant moves to join the transferee as a defendant in this case such that this Consent Judgment shall be binding upon the transferee and the Court issues an order granting such motion. DEQ agrees that it will not oppose dismissal of Defendant in the event the assets that are the subject of the Complaint have been transferred and the transferee has been added as a defendant in this case and is bound by this Consent Judgment. Nothing in this Consent Judgment relieves Defendant of its duty to abide by the terms of the Permit and State water quality law. DEQ retains its authority, in accordance with applicable law, to initiate any and all enforcement actions that would otherwise be available to it in the absence of this Consent Judgment.

- 11. The Court shall retain continuing jurisdiction in this case to enforce the terms and conditions of this Consent Order, to modify this Consent Order, and to resolve disputes arising under this Consent Order until all parties have complied with all provisions of this Consent Judgment.
- 12. The contempt provisions of Article 2, Chapter 5A of North Carolina General Statutes shall be available to enforce this Consent Judgment.
- 13. This Consent Judgment may be signed out-of-court, out-of-term, out-of-county, and may be signed in multiple counterpart originals, all of which, taken together, shall be considered one and the same document. Facsimile or scanned signatures will be sufficient to render this Consent Judgment effective. Original signatures will be substituted at a later date.
- 14. Each undersigned representative of a party to this Consent Judgment certifies that the

representative is authorized to enter into the terms and conditions of this Consent Judgment, and to execute and legally bind such party to this Consent Judgment.

Superior County Jude Currituck County

[ADDITIONAL SIGNATURES ON FOLLOWING PAGES]

CONSENTED TO BY:

NORTH CAROLINA DEPARTMENT OF ENVIRONMENTAL QUALITY

By:

Regional Supervisor, Washington Regional Office, Division of Water Resources

CONSENTED TO BY:

SANDLER UTILITIES AT MILL RUN, LLC

Ву:

Raymond Gottlieb Manager

## SANDLER UTILITIES AT MILL RUN, LLC

April 1, 2021

Mr. William Grantmyre Staff Attorney North Carolina Public Staff Utilities Commission 4326 Mail Service Center Raleigh, NC 27699-4300

> Re: Docket No. W-1130, Sub 8 Eagle Creek Subdivision

Dear Mr. Grantmyre:

Sandler Utilities at Mill Run, LLC ("Sandler Utilities") sincerely regrets the recent wastewater system backups into the homes of customers caused by controller and valve failures due to water intrusion into the value pit packages of the Eagle Creek Vacuum System ("Vacuum System"). As discussed in our January 22, 2021 letter, Sandler Utilities performed substantial capital improvements to the Vacuum System since the Commission's Recommended Order Granting Rate Increase, Requiring Refund, and Customer Notice was issued on December 11, 2015. As discussed in detail on our January 22, 20121 letter, more recently, Sandler Utilities (i) retained the services of FloVac to provide expert recommendations about how to address and correct the problems with the Vacuum System; (ii) enlisted FloVac's assistance in training the operator of the system, Envirolink, Inc. ("Envirolink"), as to how to prevent water ingestion in the system; (iii) installed 120 new controllers from October 12, 2020 through November 23, 2020 that are capable of processing small amounts of water and being submerged up to five feet with no impact to performance; (iv) performed proper installation of in-sump breather hoses on several valve pits; and (v) installed one new valve pit in December 2020. Also, Sandler Utilities has requested recommendations from FloVac about whether to install a monitoring system at each valve pit so that the operator of the system can timely respond to any issues with the system.

In addition to those actions and capital improvements, Sandler has performed, or will be performing, further remedial actions and improvements to the system to reduce rainwater intrusion and minimize flooding of the valve pits. Below is a list of the actions items and completion date for the additional improvements to reduce rainwater intrusion and minimize flooding of the valve pits.

Action Item	Recommended Response	Completion Schedule for Remedial Action
Rainwater intrusion. Reduce rainwater intrusion and minimize flooding in valve pits. Most rainwater intrusion is believed to be caused by I&I from the homeowners" laterals causing vacuum systems to struggle with inundation during heavy rain.	Install a monitoring system to detect in real time issues, such as missing clean out caps that the homeowners may have removed to relieve wastewater system back-ups. It will also alleviate time spent searching for leaks in the system, and response time is critical to keep problems from escalating.	FloVac has submitted proposals for the monitoring system to both Sandler Utilities and Envirolink, the potential purchaser of the Vacuum System. Sandler Utilities and Envirolink are reviewing FloVac's proposal, and determining the most effective method of addressing the problems with the system.

448 Viking Drive, Suite 220, Virginia Beach, Virginia 23452
Mailing Address: Post Office Box 8790, Virginia Beach, Virginia 23450
Telephone 757-463-5000 / Telefax 757-463-3358

April 1, 2021 Page 2

Continuous operation of vacuum pump. The vacuum pump frequently operates continuously during periods of high water levels within the vacuum holding tank, and the continuous operation of the vacuum pump creates the potential for motor failure.	Repair high-level lock-out valve.	This repair was completed in December 2020.
System monitoring. Appropriate and effective system monitoring during off-hours has been accomplished.	Sandler Utilities is committed to ensuring that Envirolink is appropriately staffed so that any required service to the system will be performed timely. Sandler Utilities has instructed Envirolink to have a 24/7 presence at the plant. Envirolink's 24/7 presence at the plant will address previous issues of response time, such as when issues occurred during weekends when Envirolink was not physically on-site.	At the instruction of Sandler Utilities, effective on 3/19/2021, Envirolink has a physical presence at the site for 7-day, 20-hour shifts. Also, Envirolink will have at least two technicians on call during the remaining 4 hours of the day in which the plant is not physically staffed by an operator. During projected wet weather events, Sandler Utilities will be proactive and request that Envirolink ensure that 24-7 physical coverage is maintained until the wet weather event has subsided.
Outsource controllers. Outsource controllers and valves to be rebuilt so fully functional ones are available when needed.	37 controllers that had been waiting for technician repair were delivered to FloVac for repair and rebuilding by their expert technicians. FloVac repaired the controllers and delivered them to the Eagle Creek plant.	The repairs to the controllers were completed in December 2021. In the event that Envirolink has a back-log for repair of the controllers in the future, Sandler Utilities has informed Envirolink to deliver the controllers to FloVac for repair.
Controller failure. Controller failure from water ingestion was the result of new controllers being properly fitted to the valves within the collection network.  Operator's technical experience.	Envirolink technicians were trained about the proper procedure for removing water from the valve upper before fitting a new controller to prevent water ingestion.  Envirolink has been gaining experience and expertise in operating a vacuum wastewater system since the EnviroTech team who had previously operated the system left in	FloVac provided training to Envirolink during the week beginning November 20, 2020.  In November 2002, Sandler Utilities hired a former EnviroTech employee with experience in operating vacuum systems. This former EnviroTech employees is leading operations at the facility, and he has been

	September 2020. Training in vacuum systems for all technicians who work at or will potentially work at this plant is critical.	instrumental in leading his team to detect and address potential issues. Also, Sandler Utilities has requested that Envirolink continue a training regimen with its operators to ensure that the operators will be proficient with the vacuum system. Mike Myers with Envirolink has that training has occurred and that additional training by a technician who leads training efforts with both AirVac and FloVac will be scheduled.
Extended outages. A delay in receipt of the initial vacuum pump replacement caused an extended outage that resulted serious problems to the system. The ongoing disassembly and reassembly of the valve pit by pump truck operators to clear the pits left the valve pits vulnerable to failures.	Once the system was stabilized, technicians analyzed each valve pit, and tested, repaired, reconnected, and replaced items as necessary. During this analysis, a technician discovered that a valve pit had collapsed. Therefore, a new valve pit was ordered and installed to replace the failed valve pit.	The analysis of each valve pit was conducted and completed during December 2020. As a result of the analysis, most of the valve pits that had sustained damage during the September and October outages were repaired. For example, each valve was fired multiple times by hand and then analyzed for hose orientation and correct hose connection and timing.
Problem identification. Identify problems early that cannot be readily observed.	Envirolink operators are required to record daily run times during their daily station "walk-through."	An example of a log and recording document was provided to Envirolink in November 2020 to use as an example. This document is now utilized as part of Envirolink's daily protocol.
Pump station alarm. The excessive pump run alarm is the most effective warning to keep the system from collapsing and causing a complete failure. Therefore, the pump alarm must be operating properly at all times.	Envirolink is required to check the pump alarm on a weekly basis, and check the control panel and sensors associated with all alarm systems at the plant on a weekly basis.	Since January 2021, Envirolink has been inspecting these systems to make sure they are operating properly. By the end of 2021, Sandler Utilities plans to provide and overhaul the control panel.
Couplings wearing prematurely to the motor of the sewer pumps.	Alignment of the motor on the pumps has been checked.	Pearson pumps completed this work during their repair and replacement efforts in November 2020.
Nonoperational check valves on the vacuum main.	Replace or repair as needed.	Valves are exercised weekly and are all currently in working order.

Inadequate stock levels.	Ensure multiple spare parts are available at all times.	From October 2020 through January 2021, numerous controllers, valves, pumps, motors, and valve pits have been ordered and kept on-site for Envirolink to use when needed.
Excessive activations caused by inflow and infiltration	Install a monitoring system to identify locations of excessive	In December 2020, FloVac submitted proposals for a monitoring system to
from the homeowners'	activations.	Sandler Utilities and Envirolink.
gravity laterals.		Sandler Utilities and Envirolink are considering the proposals.
The length of time it takes	Install a monitoring system to	In December 2020, FloVac submitted
Envirolink to identify the	eliminate the need for this	proposals for a monitoring system to
location of a valve that is	search.	Sandler Utilities and Envirolink.
stuck open.		Sandler Utilities and Envirolink are
		considering the proposals.
Short cycling of sewage	Clean and inspect the collection	The vacuum tank was cleaned and
<u>pumps</u> likely caused by	tank thoroughly.	thoroughly inspected in October
debris in the vacuum		2020.
collection tanks.		

Sandler Utilities is committed to ensuring that the wastewater system for the Eagle Creek Subdivision is both safe and reliable. We are continuing our efforts to ensuring that the problems with the Vacuum System are satisfactorily addressed.

If you have any question about our plan to address the problems with the system, please let me know.

Sincerely,

Brittney M. Willis – Project Manager Sandler Utilities at Mill Run, LLC

Currituck Water & Sewer, LLC 4700 Homewood Ct., Suite 108 Raleigh, North Carolina 27609

March 25, 2021

Mr. Justin Olds Allied Properties, LLC 417 Caratoke Hwy., Unit D Moyock, NC 27958

Re: Fost PD-R, Phases 6-9, Moyock North Carolina Sanitary Sewer Utility, Willingness to Serve

To Whom it May Concern,

Currituck Water & Sewer (CWS) is currently working through the regulatory approvals to allow to acquire the Eagle Creek Wastewater Treatment Facility serving the Eagle Creek community. CWS intends to provide wastewater services to surrounding developments as approved by the County. We understand the project to include 139 residential dwelling units, 39 Town homes and approximately 22,000 square feet of commercial for an estimated 45,000 gpd of sewer flow. The Eagle Creek WWTP has capacity and is able to adequately treat wastewater from this project.

Upon approval by Currituck County, Currituck Water and Sewer, LLC intends to provide sanitary sewer utility services to the 479 residential dwelling units known as the Fost Project.

Should you have any questions, please do not hesitate to contact me directly at (252) 235-4900 or by email at mmyers@envirolinkinc.com.

Thank you for your attention to this matter.

Sincerely,

Currituck Water and Sewer, LLC

Currituck Water & Sewer, LLC 4700 Homewood Ct., Suite 108 Raleigh, North Carolina 27609

August 26, 2021

Mr. Justin Olds Allied Properties, LLC 417 Caratoke Hwy, Unit D Moyock, NC 27958

Re: Flora Farm Phases 1-5 Sanitary Sewer Utility, Willingness to Serve

To Whom it may Concern,

Currituck Water & Sewer (CWS) is currently working through the regulatory approvals to acquire Eagle Creek Wastewater Treatment Facility serviing the Eagle Creek community. CWS intends to provide wastewater service to the Fost Development. We understand the project to include 277 residential dwelling units and approximately 7,500 square feet of commercial for an estimated 62,847 gpd.

Eagle Creek WWTP has a NC DEQ approved 321,000 gpd of permitted capacity with actual plant capacity of 175,000 gpd and annual average flow of 53,917 gpd. In accordance with NC DEQ regulation, CWS will expand the capacity from 175,000 gpd on or before the annual average flow exceeds 157,500 gpd.

The following summarizes the capacity commitments for the Eagle Creek Wastewater Treatment System.

Description	Allocation	Remaining Permitted	Plant Capacity
7		Capacity	
Permitted Capacity		321,000 gpd	175,000 gpd
Eagle Creek Community	53,917 gpd	267,083 gpd	121,083 gpd
Golf Club	1,462 gpd	265,621 gpd	119,621 gpd
Moyock Middle School	3,546.91 gpd	262,074 gpd	116, 047 gpd
Fost, phase 1-5	63,511 gpd	198,563 gpd	52,563 gpd
Fost, phase 6-9	37,758 gpd	160,805 gpd	14,805 gpd
Additional 175,000 gpd capacity			189,805 gpd
Flora	62,847 gpd	97 958 gpd	316,763 gpd
Total	223, 042 gpd		

Prior to submitting for an NC DEQ collection system permit, CWS anticipates completion of the expansion from 175,000 gpd to 350,000 gpd.

As such upon approval by Currituck County, Currituck Water & Sewer, LLC intends to provide sanitary sewer service to Fost phases 6-9 and Flora.

Should you have any questions, please do not hesitate to contact me directly at 252-235-4900 or email at <a href="mailto:mmyers@envirolinkinc.com">mmyers@envirolinkinc.com</a>

Sincerely

Michael Myers

Currituck Water & Sewer, LLC 4700 Homewood Ct., Suite 108 Raleigh, North Carolina 27609

August 26, 2021

Mr. Justin Olds Allied Properties, LLC 417 Caratoke Hwy, Unit D Moyock, NC 27958

Re: Fost PD-R, Phases 6-9 Sanitary Sewer Utility, Willingness to Serve

To Whom it may Concern,

Currituck Water & Sewer (CWS) is currently working through the regulatory approvals to acquire Eagle Creek Wastewater Treatment Facility serviing the Eagle Creek community. CWS intends to provide wastewater service to the Fost Development. We understand the project to include 139 residential dwelling units, 39 Town homes and approximately 22,000 square feet of commercial for an estimated 37,758 gpd.

Eagle Creek WWTP has a NC DEQ approved 321,000 gpd of permitted capacity with actual plant capacity of 175,000 gpd and annual average flow of 53,917 gpd. In accordance with NC DEQ regulation, CWS will expand the capacity from 175,000 gpd on or before the annual average flow exceeds 157,500 gpd.

The following summarizes the capacity commitments for the Eagle Creek Wastewater Treatment System.

Description	Allocation	Remaining Permitted	Plant Capacity
		Capacity	
Permitted Capacity		321,000 gpd	175,000 gpd
Eagle Creek Community	53,917 gpd	267,083 gpd	121,083 gpd
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Moyock Middle School	3,546.91 gpd	262,074 gpd	116, 047 gpd
Fost, phase 1-5	63,511 gpd	198,563 gpd	52,563 gpd
Fost, phase 6-9	37,758 gpd	160,805 gpd	14,805 gpd
Additional 175,000 gpd capacity			189,805 gpd
Flora	62,847 gpd	97 958 gpd	316,763 gpd
Total	223, 042 gpd		

Prior to submitting for an NC DEQ collection system permit for Fost Phases 6 -9, CWS anticipates completion of the expansion from 175,000 gpd to 350,000 gpd.

As such upon approval by Currituck County, Currituck Water & Sewer, LLC intends to provide sanitary sewer service to Fost phases 6-9 and Flora.

Should you have any questions, please do not hesitate to contact me directly at 252-235-4900 or email at <a href="mailto:mwyers@envirolinkinc.com">mmyers@envirolinkinc.com</a>

Sincerely

Michael Myers

## SANDLER UTILITIES AT MILL RUN, LLC

Michael Myers

Currituck Water & Sewer, LLC

4700 Homewood Ct., Suite 108

Raleigh, NC 27609

Re: Acceptance of Additional Flow, Fost Tract

Dear Mr. Myers,

Sandler Utilities at Mill Run, LLC ("Sandler Utilities") provides this written confirmation that the wastewater agreement permits the acceptance of the flow from the Fost project.

The current capacity of our plant is 175,000 gpd with permitted disposal capacity of 350,000 gpd. Our actual average inflow is 52,695 gpd, leaving remaining capacity of 122,305 gpd prior to expansion of the facility. As such, we have the ability to accept 104,805 gpd of flow prior to expansion to 350,000 gpd. The Fost Tract is only expected to generate 57,480 gpd of flow.

As you are aware, Sandler Utility and Currituck Water & Sewer are in the process of transferring the Eagle Creek wastewater system, so responsibility for expansion will be on Currituck Water & Sewer.

Sincerely,

Brittney M. Willis – Project Manager

Sandler Utilities at Mill Run, LLC

#### Conceptual Drainage Plan Narrative

The Fost Tract PD-R – Permitting Phases 6-9 Planned Residential Development Moyock Currituck County Submittal March 22, 2021



## General

The Fost Tract PD-R project is a proposed Residential Development to be located on a (+/-) 225.35 acre parcel located on the west side of NC 168 to the southwest of the intersection of Survey Road and NC 168 in Moyock, NC. The development is segregated into North and South "Project Areas" by Rowland Creek, which flows in an west-east direction and bisects the property. Additionally, the survey road ditch to the north and a NC 168 culvert located at the northeast corner of the Ranchland Subdivision to the south serve as interconnected outfalls.

The North Project Area contains the densest development within the project and, accordingly, an NCDEQ High Density Stormwater Permit is being pursued for the North Project Area.

The South Project Area contains lower density single family residential development and accordingly, an NCDEQ Low Density Stormwater Permit is being pursued for the South Project Area.

To meet Currituck County's peak flow mitigation requirements, a stormwater management system consisting of multiple interconnected wet ponds and multiple outlet control structures draining to the multiple existing outfalls has been designed which will mitigate post-construction peak flows from the developed area.

## **Summary of Existing Conditions**

The project site consists of a 225.35 acre parcel located on the west side of NC 168 to the southwest of the intersection of Survey Road and NC 168 in Moyock, NC. Before construction started, the North Project Area consisted of farm fields planted in row crops and the South Project Area consisted primarily of woods. A series of ditches drained the farm field to the Survey Road Ditch to the north and to the Rowland Creek Canal to the south. And a single ditch on the east side of the South Project Area drains the Project as well as a significant portion of the Ranchland Subdivision. This ditch will be referred to as the Ranchland Ditch from this point forward. There is an interconnected ditch that flows from the Ranchland Ditch to an outfall culvert located at the northeast corner of the Ranchland Subdivision – from this point forward, this culvert will be referred to as the "Ranchland Outfall Culvert." Therefore, the three outfalls that drain this project site are the Survey Road Ditch, Rowland Creek, and the Ranchland Outfall Culvert.

Development of the project has started under a phased development approach. The bulk of the proposed development within the North Project Area is currently underway whereas the South Project Area remains largely undeveloped, except for the installation of a trailer Parking Area and wastewater infrastructure.

## Improvements to Existing Common Drainage Features

At the request of Currituck County, improvements are being made to Rowland Creek and to the Ranchland Ditch in order to improve drainage conveyance for off-site, upstream properties. These drainage improvements are included in the phase of construction currently underway.

## <u>Summary of Proposed Conditions – Phases 6-9</u>

Phases 6-9 represent ultimate build-out of the proposed Master Plan, consisting of the installation of townhomes within the North Project Area and Single Family Residential Lots within the South Project Area. These phases will install drainage infrastructure in the North Project Area adequate to drain the proposed townhome sections to the Pond Infrastructure installed with earlier phases. Within the south project area, an additional series of wet ponds and drainage infrastructure will be installed to mitigate peak flows from the proposed residential subdivision.

For each outfall, post-construction runoff resulting from a 10-yr, 24 hr storm event will be retained and released at a peak flow rate which does not exceed the 2-yr, 24 hr peak runoff rate for the site in a theoretical wooded predevelopment condition. Post-construction runoff resulting from a 100-yr, 24 hr storm event will be retained and released at a peak flow rate which does not exceed the actual calculated pre-construction 100-yr, 24 hr peak runoff rate for the site.

## Stormwater Collection, Treatment, Storage and Disposal

The topography of the drainage area, existence of low permeability soils, clearance to groundwater, and peak flow mitigation requirement indicate that wet ponds would be the preferable BMP to specify for this site. Runoff will be collected and treated in multiple wet ponds, with control structures located at the connection of the system with the existing outfalls.

#### Collection

The stormwater runoff will be collected and directed to the wet ponds via a combination of site grading, curb & gutter, storm piping, and conveyance swales.

#### **Treatment**

The proposed system will offer several methods of stormwater runoff treatment prior to release. The majority of runoff from developed areas will flow overland to the collection system, providing initial filtration and opportunities for infiltration. The collection system is designed such that runoff is conveyed through vegetated swales either upstream or downstream of piped conveyances, providing treatment of runoff within the swales. The large pond system provides an additional treatment opportunity which is not typical of low-density designs. The primary purpose of the pond system is peak flow mitigation, but considerable water quality treatment will occur within the pond system prior to any runoff being allowed to leave the site.

#### Storage

Currituck County's Stormwater Ordinance requires that the runoff rate from the 10-yr, 24-hr storm in the post-construction condition not exceed the runoff rate from the 2-yr, 24-hr storm in the pre-construction, theoretical fully wooded condition.

#### **Disposal**

A small amount of the runoff will be infiltrated or be disposed of via infiltration and evapotranspiration. The primary source of disposal, however, will be via discharge to the four outfalls via the Outlet Control Structures.

## **Soils**

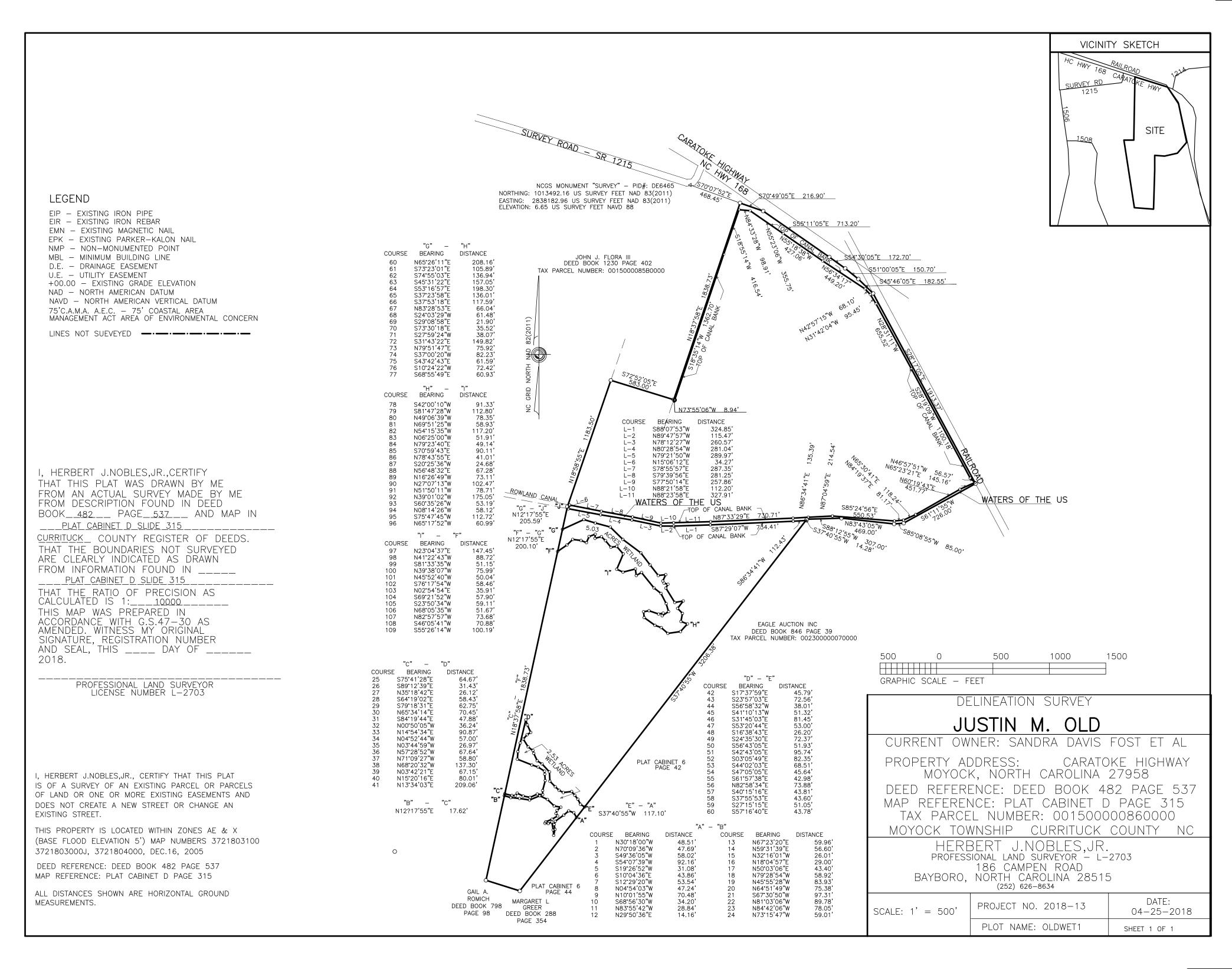
Information collected on site indicates that the soils found throughout this site are composed primarily of sandy loam. These soil types will have slow permeability. These findings generally correlate with the description mapped and discussed in the United States Department of Agriculture, Soil Conservation Service, Soil Survey of Currituck County, North Carolina, which map the soil for this site as follows:

RoA – Roanoke silt loam, Permeability is very low to moderately high Ca – Cape Fear loam, Permeability is moderately low to moderately high Ws – Wada muck, Permeability is low

A soils report has been included in the appendix of this narrative. Water levels in the proposed Wet Ponds will be regulated via the weir structures.

## **Conclusions**

The proposed stormwater management plan for this site will be designed to handle the difference in runoff between the pre-development 2-yr, 24hr and the post-development 10-yr, 24hr storm events, as prescribed in the Currituck County Stormwater Management Ordinance. Additionally, the design mitigates peak flows from the 100-yr, 24hr storm events and provides treatment of the NCDEQ required water quality volume. This proposed system will be designed to more than adequately serve the stormwater management requirements of this site.



# U.S. ARMY CORPS OF ENGINEERS WILMINGTON DISTRICT

Action Id. SAW-2018-00838 County: Currituck U.S.G.S. Quad: Lambs Corner NE

#### NOTIFICATION OF JURISDICTIONAL DETERMINATION

Property Owner/Applicant: Sandra D. Fost

Address: 121 Soundshore Drive

Currituck, North Carolina 27929

Telephone Number: 252-232-3005

Size (acres) Nearest Waterway

**USGS HUC** 

228.8 acres

Roland Creek 03010205 Nearest Town Snowden

River Basin Coordinates Albemarle-Chowan
Latitude: 36.491912 N

Longitude: -76.150370 W

Location description: Property is an approximate 228.8-acre tract located off US Highway 168 and Survey Road, adjacent to Roland Creek and the Northwest River, near Snowden, in Currituck County, North Carolina. The project area consists of an approximate 102-acre wooded area immediately adjacent to the existing agricultural fields on the property. Currituck County PARID: 001500000860000. TAXID: 130052. NC PIN 8031-80-3559.

#### **Indicate Which of the Following Apply:**

#### A. Preliminary Determination

There are waters, including wetlands, on the above described property that may be subject to Section 404 of the Clean Water Act (CWA)(33 USC § 1344) and/or Section 10 of the Rivers and Harbors Act (RHA) (33 USC § 403). The waters, including wetlands, have been delineated, and the delineation has been verified by the Corps to be sufficiently accurate and reliable. Therefore this preliminary jurisdiction determination may be used in the permit evaluation process, including determining compensatory mitigation. For purposes of computation of impacts, compensatory mitigation requirements, and other resource protection measures, a permit decision made on the basis of a preliminary JD will treat all waters and wetlands that would be affected in any way by the permitted activity on the site as if they are jurisdictional waters of the U.S. This preliminary determination is not an appealable action under the Regulatory Program Administrative Appeal Process (Reference 33 CFR Part 331). However, you may request an approved JD, which is an appealable action, by contacting the Corps district for further instruction.

There are wetlands on the above described property, that may be subject to Section 404 of the Clean Water Act (CWA)(33 USC § 1344) and/or Section 10 of the Rivers and Harbors Act (RHA) (33 USC § 403). However, since the waters, including wetlands, have not been properly delineated, this preliminary jurisdiction determination may not be used in the permit evaluation process. Without a verified wetland delineation, this preliminary determination is merely an effective presumption of CWA/RHA jurisdiction over all of the waters, including wetlands, at the project area, which is not sufficiently accurate and reliable to support an enforceable permit decision. We recommend that you have the waters of the U.S. on your property delineated. As the Corps may not be able to accomplish this wetland delineation in a timely manner, you may wish to obtain a consultant to conduct a delineation that can be verified by the Corps.

#### B. Approved Determination

There are Navigable Waters of the United States within the above described property subject to the permit requirements of Section 10 of the Rivers and Harbors Act (RHA) (33 USC § 403) and Section 404 of the Clean Water Act (CWA)(33 USC § 1344). Unless there is a change in law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.

There are waters of the U.S., including wetlands, on the above described property subject to the permit requirements of Section 404 of the Clean Water Act (CWA) (33 USC § 1344). Unless there is a change in the law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.

We recommend you have the waters of the U.S. on your property delineated. As the Corps may not be able to accomplish this wetland delineation in a timely manner, you may wish to obtain a consultant to conduct a delineation that can be verified by the Corps.

#### SAW-2018-00838 Fost Farm

\_ The waters of the U.S., including wetlands, on your project area have been delineated and the delineation has been verified by the Corps. If you wish to have the delineation surveyed, the Corps can review and verify the survey upon completion. Once verified, this survey will provide an accurate depiction of all areas subject to CWA and/or RHA jurisdiction on your property which, provided there is no change in the law or our published regulations, may be relied upon for a period not to exceed five years.

The waters of the U.S., including wetlands, have been delineated and surveyed and are accurately depicted on the plat signed by the Corps Regulatory Official identified below on Date. Unless there is a change in the law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.

There are no waters of the U.S., to include wetlands, present on the above described property which are subject to the permit requirements of Section 404 of the Clean Water Act (33 USC 1344). Unless there is a change in the law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.

Direg

The property is located in one of the 20 Coastal Counties subject to regulation under the Coastal Area Management Act (CAMA). You should contact the Division of Coastal Management in Elizabeth City, NC at 252-264-3901 to determine their requirements.

Placement of dredged or fill material within waters of the US, including wetlands, without a Department of the Army permit may constitute a violation of Section 301 of the Clean Water Act (33 USC § 1311). Placement of dredged or fill material, construction or placement of structures, or work within navigable waters of the United States without a Department of the Army permit may constitute a violation of Sections 9 and/or 10 of the Rivers and Harbors Act (33 USC § 401 and/or 403). If you have any questions regarding this determination and/or the Corps regulatory program, please contact Raleigh W. Bland, PWS at (910) 251-4564 or Raleigh.w.bland@usace.army.mil.

- C. Basis For Determination: The delineated wetlands on this site meets the wetland criteria as described in the 1987 Corps Wetland Delineation Manual and the Atlantic and Gulf Coastal Plain Regional Supplement to the 1987 Wetland Delineation Manual. The wetlands are a part of a broad continium of wetlands connected to Roland Creek and the Northwest River.
- **D. Remarks:** Department of the Army authorization is required to work within any jurisdictional waters and or wetlands on the property.

#### E. Attention USDA Program Participants

This delineation/determination has been conducted to identify the limits of Corps' Clean Water Act jurisdiction for the particular site identified in this request. The delineation/determination may not be valid for the wetland conservation provisions of the Food Security Act of 1985. If you or your tenant are USDA Program participants, or anticipate participation in USDA programs, you should request a certified wetland determination from the local office of the Natural Resources Conservation Service, prior to starting work.

## F. Appeals Information (This information applies only to approved jurisdictional determinations as indicated in B. above)

This correspondence constitutes an approved jurisdictional determination for the above described site. If you object to this determination, you may request an administrative appeal under Corps regulations at 33 CFR Part 331. Enclosed you will find a Notification of Appeal Process (NAP) fact sheet and request for appeal (RFA) form. If you request to appeal this determination you must submit a completed RFA form to the following address:

US Army Corps of Engineers South Atlantic Division Attn: Jason Steele, Review Officer 60 Forsyth Street SW, Room 10M15 Atlanta, Georgia 30303-8801

In order for an RFA to be accepted by the Corps, the Corps must determine that it is complete, that it meets the criteria for appeal under 33 CFR part 331.5, and that it has been received by the Division Office within 60 days of the date of the NAP. Should you decide to submit an RFA form, it must be received at the above address by March 30, 2017.

SAW-2018-00838 Fost Farm
\*\*It is not necessary to submit an RFA form to the Division Office if you do not object to the determination in this correspondence.\*\*

Corps Regulatory Official:

Date: April 30, 2018 Expiration Date: N/A for PJD

The Wilmington District is committed to providing the highest level of support to the public. To help us ensure we continue to do so, please complete our Customer Satisfaction Survey, located online at http://corpsmapu.usace.army.mil/cm apex/f?p=136:4:0.

Copy Furnished: CESAW/RG-W/Bland

	NOTIFICATION OF ADMINISTRATIVE APPEAL OPTIONS AND PROCESS AND REQUEST FOR APPEAL					
A	pplicant: Fost Farm	File Number:SAW 2018-00838	April 30, 2018			
A	ttached is:		See Section below			
	INITIAL PROFFERED PERMIT (Stand	ard Permit or Letter of permission)	A			
	PROFFERED PERMIT (Standard Permi	t or Letter of permission)	В			
	PERMIT DENIAL		С			
	APPROVED JURISDICTIONAL DETE	ERMINATION	D			
X	PRELIMINARY JURISDICTIONAL D	ETERMINATION	E E			

SECTION I - The following identifies your rights and options regarding an administrative appeal of the above decision.

Additional information may be found at <a href="http://www.usace.army.mil/Missions/CivilWorks/RegulatoryProgramandPermits.aspx">http://www.usace.army.mil/Missions/CivilWorks/RegulatoryProgramandPermits.aspx</a> or Corps regulations at 33 CFR Part 331.

#### A: INITIAL PROFFERED PERMIT: You may accept or object to the permit.

- ACCEPT: If you received a Standard Permit, you may sign the permit document and return it to the district engineer for final
  authorization. If you received a Letter of Permission (LOP), you may accept the LOP and your work is authorized. Your
  signature on the Standard Permit or acceptance of the LOP means that you accept the permit in its entirety, and waive all
  rights to appeal the permit, including its terms and conditions, and approved jurisdictional determinations associated with the
  permit.
- OBJECT: If you object to the permit (Standard or LOP) because of certain terms and conditions therein, you may request that the permit be modified accordingly. You must complete Section II of this form and return the form to the district engineer. Your objections must be received by the district engineer within 60 days of the date of this notice, or you will forfeit your right to appeal the permit in the future. Upon receipt of your letter, the district engineer will evaluate your objections and may: (a) modify the permit to address all of your concerns, (b) modify the permit to address some of your objections, or (c) not modify the permit having determined that the permit should be issued as previously written. After evaluating your objections, the district engineer will send you a proffered permit for your reconsideration, as indicated in Section B below.

#### B: PROFFERED PERMIT: You may accept or appeal the permit

- ACCEPT: If you received a Standard Permit, you may sign the permit document and return it to the district engineer for final
  authorization. If you received a Letter of Permission (LOP), you may accept the LOP and your work is authorized. Your
  signature on the Standard Permit or acceptance of the LOP means that you accept the permit in its entirety, and waive all
  rights to appeal the permit, including its terms and conditions, and approved jurisdictional determinations associated with the
  permit.
- APPEAL: If you choose to decline the proffered permit (Standard or LOP) because of certain terms and conditions therein,
  you may appeal the declined permit under the Corps of Engineers Administrative Appeal Process by completing Section II of
  this form and sending the form to the division engineer. This form must be received by the division engineer within 60 days
  of the date of this notice.
- C: PERMIT DENIAL: You may appeal the denial of a permit under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the division engineer. This form must be received by the division engineer within 60 days of the date of this notice.
- D: APPROVED JURISDICTIONAL DETERMINATION: You may accept or appeal the approved JD or provide new information.
- ACCEPT: You do not need to notify the Corps to accept an approved JD. Failure to notify the Corps within 60 days of the
  date of this notice, means that you accept the approved JD in its entirety, and waive all rights to appeal the approved JD.
- APPEAL: If you disagree with the approved JD, you may appeal the approved JD under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the district engineer. This form must be received by the division engineer within 60 days of the date of this notice.

#### SAW-2018-00838 Fost Farm

E: PRELIMINARY JURISDICTIONAL DETERMINATION: You do not need to respond to the Corps regarding the preliminary JD. The Preliminary JD is not appealable. If you wish, you may request an approved JD (which may be appealed), by contacting the Corps district for further instruction. Also you may provide new information for further consideration by the Corps to reevaluate the JD.

#### SECTION II - REQUEST FOR APPEAL or OBJECTIONS TO AN INITIAL PROFFERED PERMIT

REASONS FOR APPEAL OR OBJECTIONS: (Describe your reasons for appealing the decision or your objections to an initial proffered permit in clear concise statements. You may attach additional information to this form to clarify where your reasons or objections are addressed in the administrative record.)

ADDITIONAL INFORMATION: The appeal is limited to a review of the administrative record, the Corps memorandum for the record of the appeal conference or meeting, and any supplemental information that the review officer has determined is needed to clarify the administrative record. Neither the appellant nor the Corps may add new information or analyses to the record. However, you may provide additional information to clarify the location of information that is already in the administrative record.

#### POINT OF CONTACT FOR QUESTIONS OR INFORMATION:

If you have questions regarding this decision and/or the appeal process you may contact:

District Engineer, Wilmington Regulatory Division,

Attn: Raleigh W. Bland, PWS

2407 West 5th Street

Washington, North Carolina 27889

910-252-4558

If you only have questions regarding the appeal process you may

Mr. Jason Steele, Administrative Appeal Review Officer

**CESAD-PDO** 

U.S. Army Corps of Engineers, South Atlantic Division

60 Forsyth Street, Room 10M15 Atlanta, Georgia 30303-8801 Phone: (404) 562-5137

RIGHT OF ENTRY: Your signature below grants the right of entry to Corps of Engineers personnel, and any government consultants, to conduct investigations of the project site during the course of the appeal process. You will be provided a 15 day notice of any site investigation, and will have the opportunity to participate in all site investigations.

Date:

Telephone number:

Signature of appellant or agent.

For appeals on Initial Proffered Permits send this form to:

District Engineer, Wilmington Regulatory Division, Raleigh W. Bland, PWS, 2407 West 5th Street Washington, North Carolina 27889

For Permit denials, Proffered Permits and approved Jurisdictional Determinations send this form to:

Division Engineer, Commander, U.S. Army Engineer Division, South Atlantic, Attn: Mr. Jason Steele, Administrative Appeal Officer, CESAD-PDO, 60 Forsyth Street, Room 10M15, Atlanta, Georgia 30303-8801 Phone: (404) 562-5137

#### ADEQUATE PUBLIC FACILITIES ORDINANCE HISTORY

- August 1994 Anne Mackin, then Moyock Elementary School principal, sent a memo (see attached memo dated August 29, 1994) to Ronnie Capps, Currituck County School Superintendent, stating that with increased enrollment "Facilities, programs, and equipment need to be added and supported yearly to provide sufficient services for our student population. She noted the addition of mobile classrooms and the student crowding in restrooms, cafeteria, and library. She also noted that 'all support services (counseling, special education, chapter 1) needed special attention to accommodate student numbers.' Principal Mackin also itemized the additional staff, copy machines, instructional supplies, consumable materials, furniture textbooks, library materials, computers, and additional bus routes necessary to service the 713 students enrolled at Moyock Elementary. She said 'Overall the increase in enrollment has required Currituck County School System to redesign facilities and programs.'
- September 1994 Ronnie Capps, Currituck County School Superintendent, approached the Planning Board (see attached memo dated September 13, 1994) regarding the "residential development having dramatic impact on each of our schools during recent years." Superintendent Capps detailed the increase in attendance in his memo. He also noted the 38 mobile classroom units installed at schools to accommodate growth of the student population. The memo encourages the Planning Board and the Board of Commissioners to review options relative to the rate of residential development so all county agencies, including the schools, might respond to growth in a timely and positive manner. Superintendent Capps encourage the boards to be proactive instead of reactive to accommodate growth in the student population. Superintendent Capps noted the proposed subdivision "The Plantations" (now Eagle Creek) with 601 residential lots in the Moyock Township and its effect on the school district.
- September 13, 1994 Jack Simoneau Planning and Inspections Director, presented PB 94-42 Currituck County text amendment (attached) to the Planning Board to address adequate public facilities. The language read:
  - 2. Even if the permit issuing board finds that the application complies with all other provisions of this Ordinance, it may still deny the permit if it concludes, based upon the information submitted at the hearing, that if completed as proposed, the development, more probably than not:
    - e. Will exceed the county's ability to provide adequate public facilities, including, but not limited to, schools, fire and rescue, law enforcement, and other county facilities.

The Planning Board minutes for September 13, 2994 are attached for reference. The Planning Board unanimously approved the text amendment.

- October 3, 1994 BOC held a public hearing on the proposed text amendment. (Minutes Attached)
- October 17, 1994 BOC unanimously approved the text amendment. (Minutes attached)
- **February 6, 1995** BOC unanimously denied a special use permit for Tate Terrace Realty Investors Inc. request for a 601 lot subdivision (now Eagle Creek) based solely upon the provisions of the above noted text amendment to UDO Section 1402(2)(e). It is important to note that a prior property owner (Moyock Investment Group) received vesting approval to develop at 429 lots on this property.

Minute De

#### CURRITUCK COUNTY PLANNING BOARD September 13, 1994 7:30 p.m.

The Currituck County Planning Board met at the Currituck County Courthouse on September 13, 1994. The following members were present: Walter Jones, Jimmy Waterfield, Aubrey Dowdy, Owen Etheridge, Marguerite Hogge, Shirley Falls, Nolan Alcock, La Ree Lynn, and Walter Banks. Jack Simoneau, Planning and Inspections Director, and H.B. Briggs, Chief Planner were also present.

Mr. Etheridge called the meeting to order and announced a quorum had been met.

Ms. Falls asked if the board wanted her to vote on the Duck Ridge item since her daughter worked for Hearndon Construction. The board agreed that Ms. Falls could vote on this item since she said that her daughter's employment would not bias her opinion.

Mr. Etheridge welcomed Mr. Jimmy Waterfield to the board.

### APPROVAL OF AUGUST 9, 1994, MINUTES

Mr. Lynn motioned to approve the August 9, 1994 minutes as presented. Mr. Jones seconded the motion and the motion passed unanimously.

#### RESOLUTION OF APPRECIATION FOR MR. LUTHER CROCKETT

The board presented the following resolution of appreciation to Mr. Crockett:

## RESOLUTION OF APPRECIATION

WHEREAS, Mr. Luther Crockett has faithfully served as a member of Currituck County's Planning Board from January, 1994 until July, 1994; and,

WHEREAS, Mr. Crockett has devoted numerous hours and energies to serve in this capacity; and,

WHEREAS, Mr. Crockett has served as an integral part in shaping the future of Currituck County; and,

WHEREAS, Mr. Crockett always acted in the best interests of the Citizens of Currituck County as a member of the Planning Board; and,

- Currituck County Board of Commissioners, shall be from \_\_\_\_a. m. to \_\_\_\_p. m.; and,
- 15. Should any conditions within this permit conflict with any other permit issued for this activity, then the more restrictive condition shall prevail.

Ms. Hogge seconded with Mr. Waterfield, Mr. Lynn, Mr. Alcock, and Mr. Jones voting no. (Mr. Etheridge asked for a show of hands to determine those voting in opposition)

DISCUSSION ON IMPACT OF GROWTH INCLUDING ITS IMPACT ON THE SCHOOL SYSTEM BY BOARD OF EDUCATION REPRESENTATIVE

Mr. Ronnie Capps, Ms. Janet Taylor, and Mr. Skip Saunders appeared before the board.

Mr. Capps presented memo to the board with different charts and graphs to show the impact that increased development is having on the school system (see Attachment 1)

Mr. Capps expressed a concern that the county needs to plan better for development. He said that within 3 to 5 years, all of Currituck's public facilities will be overburdened.

Mr. Alcock asked what we had to do now to have the developers start paying for some of the impact they have on the county facilities.

Mr. Simoneau said that you must have a capital improvements plan, establish a level of service standard, compare estimates with proposed development, and to come up with a formula to require developers to pay their fair share of facility cost. He said that this is in the works now, but it will take about 2 - 3 years to get a system in place.

Ms. Taylor explained a problem teachers have in the mobile units. She said that a teacher cannot leave the trailer to take a student to the office for anything (including nurse) since there is not another teacher in the trailer to watch the students. She said this is not fair to the teacher nor the student and that the county should speed up the process in order to get more schools.

Mr. Saunders said that the expense of bringing new trailers was astronomical since special attention has to be given to the electrical requirements, water pressure, meeting handicap regulations, etc. He also said that each school is running out of room for trailers.

Ms. Taylor explained that there is no place for the students in the trailers to hold a fire drill since they have to go

inside and "crouch" against a wall and all of the walls were filled with the students who are in the school already.

Mr. Banks asked about moving the boundary line that said which school a child went to and move more from Moyock to Central, a less crowded school.

Mr. Capps said that this would only move trailers from one school to another and would not solve the problem.

Mr. Simoneau presented the following amendment as requested by the Planning Board at the August meeting as one mean to slow growth as well as a means for determining impact of development:

Currituck County requests the following amendment to the Currituck County Unified Development Ordinance:

PB 94-42 CURRITUCK COUNTY request amendments to Article 14 Section 1402, Articles 9, 10, and 11 Section IV, to provide adequate public facility regulations

BE IT ORDAINED by the Board of Commissioners of the County of Currituck, North Carolina that the Unified Development Ordinance of the County of Currituck be amended as follows:

Part I. That the following section be amended:

Article 14 Section 1402 (2)

- (d) will not be in general conformity with the Land Use Plan, Thoroughfare Plan, or other plan officially adopted by the board: or.
- (e) will exceed the county's ability to provide adequate public facilities, including, but not limited to, schools, fire and rescue, law enforcement, and other county facilities.

Articles 9, 10, and 11 Sections 903, 1003, and 1103

	SKETCH PLAT	PRELIMINARY PLAT	FINAL PLAT
1. Physical Analysis (type units expected incl. number of bedrooms; projected value and timing of phases, etc.)			
2. Housing Market Analysis (delimit market area; project demand, supply and unmet demand; determine net			

capture; identify development profile)	X		
3. Environmental Impact (water consumption estimated per unit type, hydrological report by a licensed engineer identifying available water resources, report outlining sewer generation and means of disposal)	X	8 42 (A	11 133
4. Fiscal Analysis (estimated real property valuation, estimated personal property valuation, estimated annual land transfer tax value)	x		=== - 
5. Traffic Analysis (estimated number of trips generated, volume of existing traffic on roads adjacent to and within 1/2 mile of tract, directional distribution of traffic, capacity analysis)  Part II. This ordinance amendment sha and after the day of, 1994.	X	in effect	from
B.U. Evan	s Cha	irman	_
Attest:			
Gwen Tatem, Clerk to the Board		ii.	
DATE ADOPTED: MOTION TO ADOPT BY COMMISSIONER: SECONDED BY COMMISSIONER:			
VOTE: _ AYES _ NAYES			*
Mr. Etheridge asked if the school development will overburden their system			

the development could be denied?

Mr. Simoneau that if the school system said that a proposed development would overburden their facilities, then according to the above amendment the Board of Commissioners could deny the request. It was not know if the courts would uphold such a decision.

#### PLANNING BOARD ACTION

Mr. Alcock motioned to approve the above amendment request. Mr. Banks seconded the motion and the motion passed unanimously.

REQUEST TO PB COUNTY: AMENDMENTS 94-40 CURRITUCK SECTION 914, ARTICLE 10 SECTION AND ARTICLE SURETY 1113 TO DELETE 11 SECTION OF THE TDO ADD ACCEPTABLE BONDS AND DEED OF TRUST AND BOND AS BOND TERMS AND METHODS.

Mr. Briggs presented the following memo to the board:

#### MEMORANDUM

TO: Board of Commissioners

FROM: H. B. Briggs, Jr., Chief Planner

DATE: September 2, 1994

SUBJECT: Proposed Amendment to the UDO

Staff has been directed to prepare the attached amendment for presentation to the Planning Board. The purpose of this amendment is two-fold: (1) to add Deeds of Trust and Bonds as acceptable bonding forms and methods; and, (2) to delete the utilization of Surety Bonds by a licensed surety bond company.

#### HISTORY of DEEDS of TRUST

Prior to and included within adoption of the Unified Development Ordinance on April 2, 1989, Deeds of Trust were an acceptable form of guarantee for Maintenance or Performance Bonds. However, when the UDO was updated in November, 1992, the county attorney at the time requested that Deeds of Trust be deleted as an acceptable form of guarantee.

#### ADVANTAGES of DEEDS of TRUST

The main advantage in allowing the use of Deeds of Trust for guarantee purposes is that it allows the developer to "put-up" a portion of the development to insure items will be installed as required (Performance Bond) or that maintenance will be performed as required (Maintenance Bond).

#### DISADVANTAGES of DEEDS of TRUST

(1) although appraisals will be required, market cycles could cause an inflated value on the property, ie. appraisal done during high property values, at selling point property values low; Attachment et 1



# Public Schools Post Office Box 40 Currituck, North Carolina 27929

Office of Superintendent

919-232-2223

#### MEMO

TO:

Planning Board Members

Currituck County

FROM:

Ronnie Capps VCc

Superintendent of Schools

DATE:

September 13, 1994

SUBJECT:

Impact of Residential Development on the Public Schools

Residential development has had a dramatic impact on each of our schools during recent years. I will briefly summarize the consequences of this growth.

SCHOOL	9th Month 93/94 ADM	Enrollment 9/1/94	Increase
Moyock	686	715	29
Griggs	444	472	28
Knotts Island	149	168	19
Knapp	419	437	18
Central	338	360	22
CCHS Extended Day	692 40 732	778 32 810	78
	2768	2962	+194

MEMO Page 2 September 13, 1994

As you can see, since school ended in June, our enrollment has increased by 194 students. The four elementary schools and junior high school have each gained the equivalent of an additional class. The high school has gained the equivalent of three additional classes.

Based upon the initial enrollment this school year, our student population has increased by  $\underline{476}$  pupils over the last four years.

Currently, the school district is using 38 mobile classroom units to accommodate growth of the student population. These units are assigned to the schools as follows:

School	Number of Mobile Units
Moyock	8
Central	4
Knapp	8
Currituck High	<u>18</u> 38

Four of these units were sited this summer. Two double-wide units were placed at Moyock Elementary School and two single-wide units were located at the high school.

Moyock Elementary School is virtually at capacity in terms of parking space and locations to site additional mobile classrooms.

Low-flow rest room fixtures are being installed at Central School and Moyock School to maintain design standards for wastewater systems.

Based upon figures supplied by the State Treasurer in April, 1994, the cost of growth in the student population can be illustrated dramatically. The per student county expenditure for capital and operating costs during the 1992-93 school year was \$2,193.67. The costs per student have, of course, increased since the 1992-93 fiscal year, but by extending this cost data to the 194 students enrolled during the summer of 1994 we incur an additional expense of \$425,571.98 given the level of local support provided.

Outside of adding additional mobile classrooms, the overcrowding of our schools will not be relieved until the beginning of the 1996-97 school year when the new high school is completed.

MEMO Page 3 September 13, 1994

Maintaining quality instructional programs and student safety are our primary concerns. Our schools have received high marks in both areas, but our rapid growth makes the job more difficult.

We encourage the Planning Board and the Board of Commissioners to review options relative to the rate of residential development so all county agencies, including the schools, might respond to growth in a timely and positive manner. In recent years, the schools have had to be reactive rather than pro-active to accommodate growth in the student population. By way of example, I received in my office last week the sketch plan for The Plantations, a proposed subdivision of 601 residential lots to be located in Moyock Township. You can imagine the impact this single subdivision will have on the school district. Though the long-range school facility plan prepared by the Board of Education proposed construction of a second school in Moyock Township, funds for this project were not included on the bond referendum and are not currently anticipated from other revenue sources. Assuming this subdivision receives eventual approval, we will find our schools and community in the reactive posture to which we have become accustomed.

WRC/dbs

Attachments:

cc: Board of Education
Bill Richardson

A	8										PER T	ايترى	0.D.I
A	Page 28									N	SCHOOL CAPITAL FACILITY NEEDS PER	SURVEY	23,480,
A										<b>&gt;</b>	COUNTY PER CAPITA PERSONAL	(1991)	(
A										×	1992 COUNTY UNEN-	PLOYMENT	3.3
A		<b>¥</b>	TOTAL COUNTY REVENUES FROM ALL SOUNCES FOR ALL PURPOSES	20,906,659	16,038,561	15,266,6 <b>6</b> 4 100.00	15,857,067 100.00	11,522,248	4,386,504	3	1992-93 COLMITY TAX	COLLECTION RATE	g .
A ARTICLE 40 - 1/2 C ARTICLE 40		7		0.00	0.00	0.00	0.00	0.00	0.00	>	1992-93 COUNTY	TAX	0.5600
A ARTICLE 40 - 1/2 C AR ARTICLE 40 - 1/2 C ARTICLE			FINES AND FORFEITURES	125,372	122,476 0.76	117,231 0.77	127,329 0.80	0.00	1.02	5	ANUARY 1, 1992 COUNTY	ASSESSED VALUATION	1,408,483,015
A ARTICLE 40 - 1/2 C AR ARTICLE 40 - 1/2 C ARTICLE		=	COUNTY APPROPRIATIONS TO SCHOOL LOCAL CURRENT EXPENSE FUND	3,024,649	3,599,324	3,179,498	2,764,257	2,208,700	1,260,765	<b></b>		REVALUATION FOR COUNTY	19861
A ARTICLE 40 - 1/2 C AR ARTICLE 40 - 1/2 C ARTICLE		œ	TOTAL COUNTY FINDING SUBJECT MAINTENANCE OF FFORT REQUIREMENT OF ARTICLE 42	2,794,193	1,339,287	1,721,502	2,553,894	519,447	112,945	<b>v</b>	1991-92 COUNTY FUND BALANCE AS A % OF	GENERAL FUND Expenditures	89.79
A ARTICLE 40 - 1/2 C AR ARTICLE 40 - 1/2 C ARTICLE	14,566	<b>L</b>		(640,536)	(311,595)	520,291 3.41	699,452	49,368	(9,919)	æ	1991-92 COUNTY FUND	BALANCE AVAI LABLE	5,599,500
A ARTICLE 40 - 1/2 C AR ARTICLE 40 - 1/2 C ARTICLE	COUNTY - POPULATION -	ш	CURRENT YEAR CUTLAY EXPENDITURE	3,434,729	1,650,882	1,201,211	1,854,442	470,079	122,864	σ	1992-93 CHOOL CAPITAL UNDING AS A X OF CURRENT	PROPERTY TAX	38.14
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Attachment: 18 9-13-1994 PB Minutes APFO (PB 18-23 Fost, Phases 6-9, Preliminary Plat/Special Use Permit)

6.B.r

SCHOOL
CAPITAL
FACILITY
NEEDS PER
1993
SSURVEY

## Moyock Elementary School

Post Office Box 160
Moyock, North Carolina 27958
"Success . . . Children First"

9-435-6521

To: Ronnie Capps

From: Anne Mackin and

RE: Enrollment

Date : August 29, 1994

Moyock Elementary has been experiencing increased enrollment over the past three years. Presently, Moyock has an enrollment of 713 students. Facilities, programs, and equipment need to be added and supported yearly to provide sufficient services for our student population.

Mobile units have been added to our campus in order to provide adequate instructional and storage space. Currituck County is still trying to provide appropriate student/teacher ratio which requires more classroom units.

Suitable schedule changes needed to be designed to reduce student crowding in restrooms, cafeteria, and library. All support services (counseling, special education, chapter 1) locations needed special attention to accommodate student numbers.

Moyock Elementary had to increase the professional staff to accommodate student growth . A counselor, Chapter 1 instructor, Special Education instructor, classroom instructors, and office personnel had to be supplemented to provide significant services. Additional materials and equipment needed to be purchased to cope with normal daily demands. The items purchased are as follows: copy machines, instructional supplies, consumable materials, furniture, textbooks, library materials, and computers.

Several changes have been made in bus routes to accommodate new building developments. Additional buses have been added to provide sufficient and timely transportation for all students.

Overall the increase in enrollment has required Currituck County School System to redesign facilities and programs.

Currituck County North Carolina October 17, 1994

The Currituck County Board of Commissioners met in a work session prior to the regular scheduled meeting to discuss the Cooperative Extension Legislative Grants.

The Currituck County Board of Commissioners met at 8:00 PM in regular session on October 17, 1994, in the Currituck County Courthouse with the following members present: Vice-Chairman Wright, Commissioners Gregory, Bowden, and Askew. Chairman Evans was absent.

Vice Chairman Wright called the meeting to order at 8:00 PM.

#### Invocation

Vice Chairman Wright called for a moment of silence.

#### Approval of Agenda

Vice Chairman Wright moved to amend the agenda as follows: move Item 14 to 5B, move Item 19 to 5C and add Consideration of library bids for furnishings. Commissioner Gregory seconded the motion. Motion carried unanimously.

# Approval of September 6 and September 19, 1994 Minutes.

Commissioner Askew moved to approve the minutes of September 6 and September 19, 1994. Commissioner Gregory seconded the motion. Motion carried unanimously.

#### Public Comments

Vice Chairman Wright opened the public comment period.

Jan Smith, with the Humane Society, read a letter that all Board members had received by registered mail. The letter listed various questions and concerns regarding the animal shelter.

There being no further comments, Vice Chairman Wright closed the public comment period.

Public Hearing and Action on PB 94-44 COROLLA FIRE AND RESCUE: special use permit for an addition to the fire and

rescue building located at 827 Whalehead
Drive (intersection of Whalehead Drive and
Dolphin Street) in Whalehead Subdivision in
Corolla, Tax Map 115A, Section 13, Parcel 35A,
Poplar Branch Township.

Jack Simoneau, Planning and Inspections Director, and Marshall Cherry were sworn in prior to giving testimony.

Jack Simoneau reviewed the following case analysis: The applicant is requesting a Special Use Permit (classification #13.200) for two additions to the existing fire station building. Proposed additions will add 2050 s.f. to the existing building. The additions will be used to house additional fire equipment. Property is shown on Tax Map 115A, lot 35A, Poplar Branch Township. Property is located on the northeast corner of Whalehead Drive and Dolphin Street in Whalehead Subdivision near Corolla

#### STAFF FINDINGS

Findings: The applicant proposes an addition to the existing fire station building (Use Classification #13.200) which is a permitted use in a RO1 (Outer Banks Standard Residential) zoned area with a Special Use Permit.

Adjacent Property Owner's Notifications were mailed 9/30/94; property was posted on 10/4/94.

The proposed addition meets setback requirements.

All required parking, vehicle accomodation areas, screening requirements, shading, etc. will be met by applicant.

**Criteria:** (b) Will the proposed use materially endanger the public health or safety?

Findings: The nearest residential dwellings to the site are located across Whalehead Drive and across Dolphin Street.

Limited ingress/egress to site insures safe

traffic movements.

Criteria: (c) Will the proposed use substantially injure

the value of adjoining or abutting property?

Findings: The property is surrounded by the following land

uses:

North: Vacant lot - zoned RO1 South: Residential - zoned RO1 East: Vacant lot - zoned RO1 West: Residential - zoned RO1

Criteria: (d) Will the proposed use be in harmony with the

particular neighborhood or area in which it is to

be located?

Findings: See the findings listed under Criteria "a", "b"

and "c" above.

Criteria: (e) Will the proposed use be in general

conformity with the Land Use Plan, Thoroughfare Plan or other plans officially adopted by the

board?

Findings: The 1990 Land Use Plan classifies this land as

"Limited Transition". The purpose of the limited transition classification is to provide for Development in areas that will have some services, but are suitable for lower densities than those associated with the urban transition class, and/or are geographically remote from existing towns and

municipalities.

The site is located along SR 1420 (Whalehead Drive) which is a Local road according to the 1988

Thoroughfare Plan.

#### STAFF RECOMMENDATION

Staff recommends approval of this request as presented.

#### PLANNING BOARD RECOMMENDATION

On October 11, 1994, the Planning Board voted to recommend approval of this request as presented.

Vice Chairman Wright opened the public hearing.

Marshall Cherry, Corolla Fire Chief, stated that they were adding 3 additional bays to house their equipment.

There being no further comments, Vice Chairman Wright closed the public hearing.

Commissioner Bowden moved to grant the Special Use Permit. Commissioner Gregory seconded the motion. Motion carried unanimously.

Public Hearing and Action on PB 94-45 LOWER CURRITUCK VOLUNTEER FIRE DEPARTMENT,

INC.: special use permit for an addition to the fire department building located in Grandy on the east side of US 158 approximately

550 feet south of the intersection of SR 1129

Tax Map

94,

Parcel

136,

Poplar Branch Township.

(McHorney

Jack Simoneau, Planning and Inspections Director and Tommy Brothers were sworn in prior to giving testimony.

Road),

Jack Simoneau, reviewed the following case analysis: The applicant is requesting a Special Use Permit (classification #13.200) for an addition to the existing fire station building. The proposed addition will add 2273 s.f. to the existing building. The addition will be used to house additional fire equipment. Property is shown on Tax Map 94, lot 136, Poplar Branch Township. Property is located on the east side of US 158 across from E & S General Merchandise in Grandy.

#### STAFF FINDINGS

Criteria: (a) Does the applicant meet the standards established in the Unified Development Ordinance (UDO) for the proposed use?

Findings: The applicant proposes an addition to the existing fire station building (Use Classification #13.200) which is a permitted use in both a A (Agricultural) and GB (General Business) zoned area with a Special Use Permit.

Adjacent Property Owner's Notifications were mailed 9/30/94; property was posted on 10/4/94.

The proposed addition meets setback requirements.

All required parking, vehicle accomodation areas, screening requirements, shading, etc. will be met by applicant.

Criteria: (b) Will the proposed use materially endanger the
 public health or safety?

Findings: According to county tax maps, the nearest residential dwelling to the site is located directly to the northwest approximately 140'+/-.

Limited ingress/egress to site insures safe traffic movements.

Findings: The property is surrounded by the following land uses:

North: Woodlands/Residential - zoned GB & A South: Vacant lot/Residential - zoned GB & A

East: Residential - zoned A

West: Commercial (E & S Gen. Mdse.) - zoned GB

Criteria: (d) Will the proposed use be in harmony with the particular neighborhood or area in which it is to be located?

Findings: See the findings listed under Criteria "a", "b" and "c" above.

Criteria: (e) Will the proposed use be in general conformity with the Land Use Plan, Thoroughfare Plan or other plans officially adopted by the board?

Findings: The 1990 Land Use Plan classifies this land as "Limited Transition". The purpose of the limited transition classification is to provide for Development in areas that will have some services, but are suitable for lower densities than those associated with the urban transition class, and/or are geographically remote from existing towns and municipalities.

The site is located along US 158 which is a Minor Arterial road according to the 1988 Thoroughfare Plan.

#### STAFF RECOMMENDATION

Staff recommends approval of this request as presented.

#### PLANNING BOARD RECOMMENDATION

On October 11, 1994, the Planning Board voted to recommend approval of this request as presented!

Vice Chairman Wright opened the public hearing.

Tommy Brothers, Lower Currituck Fire Department, was present to answer questions.

There being no further comments, Vice Chairman Wright closed the public hearing.

Commissioner Bowden moved to grant special use permit. Commissioner Gregory seconded the motion. Motion carried unanimously.

Action on: PB 94-42 CURRITUCK COUNTY: request amendments to Article 14 Section 1402, Articles 9, 10 and 11 Section IV, to provide adequate public facility regulations.

Commissioner Gregory moved to adopt the following ordinance and change the number of lots where impact statements are required, from 20 to 50 lots and the final number be determined when impact study is complete. Commissioner Bowden seconded the motion. Motion carried unanimously. Chairman Evans was absent.

BE IT ORDAINED by the Board of Commissioners of the County of Currituck, North Carolina that the Unified Development Ordinance of the County of Currituck be amended as follows:

Part I. That the following section be amended:

Article 14 Section 1402 (2)

- (d) will not be in general conformity with the Land Use Plan, Thoroughfare Plan, or other plan officially adopted by the board; or,
- will exceed the county's ability to provide public facilities, including, adequate but not limited schools, fire and rescue, law enforcement, and other county facilities.

Part II. That Articles 9, 10, and 11, Sections 903, 1003, and 1103 be amended by adding the following provisions:

For subdivisions containing twenty (20) or more lots, the following information shall be provided.

	SKETCH PLAT	PRELIMINARY PLAT PLAT	
IV. DEVELOPMENT IMPAC	CT STATEMENT		
1. Physical Analysis (type uni incl. number of bedrooms; size and timing of phases,	projected value;		
2. Housing Market Analysis (of market area; project demand and unmet demand; determinent capture; identify developments)	nd, supply nine net		
3. Environmental Impact (wat estimated per unit type, hy report by a licensed engine available water resources, outlining sewer generation means of disposal)	drological er identifying report		_
4. Fiscal Analysis (estimated valuation, estimated person valuation, estimated annuatransfer tax value)	nal property		
5. Traffic Analysis (estimated trips generated, volume of traffic on roads adjacent to 1/2 mile of tract, directions distribution of traffic, capa	existing and within al		

Action on: ALOYSIUS AND JANE KREMPEL request to rename Ninth Street in Point Harbour Beach Subdivision, Poplar Branch Township.

Proposed Name: Rose Lane

Commissioner Bowden moved to approve request to rename street. Commissioner Gregory seconded the motion. Motion carried unanimously. Chairman Evans was absent.

ACEY PB 94 - 37COMMERCIAL Action on PROPERTIES: 18.7 from rezone acres (Agricultural) (General Business) to GB located in Moyock on the southwest side of NC 168 approximately 1364' south of SR 1216 (Puddin Ridge Road), Tax Map 15, Parcel 75A, Moyock Township.

Commissioner Bowden moved to approve the request to rezone from A to GB. Commissioner Gregory seconded the motion. Motion carried unanimously. Chairman Evans was absent.

Action on PB 94-38 C. W. BRYANT AND C. H. BRYANT: special use permit to expand an existing sand mining operation on property located on the east side of NC 168 at the North Carolina/Virginia state line, Tax Map 8, Parcels 15 and 17 and Tax Map 9, Parcels 1 and 2, Moyock Township.

Vice Chairman Wright moved to approve special use permit and hours will be from 7a.m. to 6p.m. Commissioner Gregory seconded the motion. Motion carried unanimously.

Action on: PB 88-62 THE PLANTATIONS (formerly Country Side): sketch plan approval of a 601-lot common open space subdivision located on SR 1215 (Survey Road) in Moyock, Tax Map 15, Parcel 84, Moyock Township.

This item was continued until next meeting at the request of the applicant.

Action on PB 94-40 CURRITUCK COUNTY: request amendments to Article 9 Section 914, Article 10 Section 1013 and Article 11 Section 1113 of the UDO to delete Surety Bonds and add Deed of Trust and Bond as acceptable bond terms and methods.

Vice Chairman moved to table until Chairman Evans was present. Commissioner Gregory seconded the motion. Motion carried unanimously.

CURRITUCK COUNTY: request to name an unnamed lane off of Barnard Road which services the Ezra and Lisa Graley Subdivision in Poplar Branch. Proposed Name: Graley Lane.

Jack Simoneau, Planning and Inspections Director, reviewed the proposed name change. He advised the Board that the two

property owners could not agree on the original name (Tanners Way) so the staff recommended Graley Lane, the subdivision name.

Commissioner Gregory moved to approve the road name. Commissioner Bowden seconded the motion. Motion carried unanimously.

# Chiefs and Captains Advisory Board to discuss permits for bonfires on the beach.

Marshall Cherry, Corolla Fire Department, requests that the Board consider a beach fire notice. Mr. Cherry stated that they had several serious incidents with beach fires this summer and would like some way to the Corolla Fire Dept. could be notified in advance. This is needed only in the RO-1 district.

Commissioner Gregory moved to direct the staff to review proposal for bonfires and bring a recommendation back to the Board. Commissioner Askew seconded the motion. Motion carried unanimously.

# Consideration of Legislative Grants for Cooperative Extension.

Rodney Sawyer, Cooperative Extension Director, was present to review legislative grants.

Vice Chairman Wright commended Mr. Sawyer and his staff on the enormous amount of work they have put into writing these grants.

Commissioner Gregory moved to endorse the legislative grants and provide space for additional staff. Commissioner Askew seconded the motion. Motion carried unanimously.

# Consideration of DOT Agreement for restoration of the Whalehead footbridge.

Vice Chairman Wright moved to approve the DOT agreement for \$58,000. Commissioner Gregory seconded the motion. Motion carried unanimously.

### Appointment to Whalehead Board of Trustees.

Vice Chairman Wright moved to appoint Rhett White to fill the unexpired term of John Wilson. Commissioner Gregory seconded the motion. Motion carried unanimously.

# Consideration of Application for Archives and History Grant.

Commissioner Bowden moved to approve grant of \$100,000 for the Whalehead Club. Commissioner Gregory seconded the motion. Motion carried unanimously. The project will be assigned to the Whalehead Board of Trustees.

# Discussion on proposed service district for northern Currituck Banks.

Bill Romm, County Attorney, reviewed the process for a proposed service district for street maintenance.

Commissioner Bowden stated that the mere creation of a District would not increase taxes.

Vice Chairman Wright moved to take no action at this time and direct staff to gather information to be presented to the new board in December. Commissioner Gregory seconded the motion. Motion carried unanimously.

# Appointments to Land Transfer Tax Appeals Board.

Commissioner Bowden moved to appoint Gary Montalbaum and reappoint John Whitehurst and Bob Henley. Commissioner Gregory seconded the motion. Motion carried unanimously.

# Release of taxes From September 12 through September 30, 1994.

Commissioner Bowden moved to release the following taxes. Commissioner Gregory seconded the motion. Motion carried unanimously.

Quality Oil	\$194.22
James Cooper	133.92
Thomas Clark	211.50
Cordelia Mack	117.42
James Smith	101.70

# Consideration of Resolution to change Board meeting time.

Commissioner Bowden moved to adopt the following resolution. Commissioner Gregory seconded the motion. Motion carried unanimously.

RESOLUTION

WHEREAS, North Carolina General Statutes 153A-40, authorizes a County Board of Commissioners to fix the time and place of its regular meeting, and

WHEREAS, the Board of Commissioners of the County of Currituck finds that it is desirable to change its regular meeting time from 8:00 p.m. on the first and third Monday of each Monday to 7:00 p.m. on the first and third Monday of each month.

## NOW, THEREFORE BE IT RESOLVED,

- 1. The regular meeting of the Board of Commissioners of Currituck shall be held at 7:00 p.m. on the first third Monday of each month;
- 2. That the Clerk to the Board is instructed to post a copy of this Resolution on the Courthouse bulletin board and to cause a summary of this Resolution to published.

## Consideration of Library furnishing bids.

Commissioner Gregory moved to accept bid from Chesson's Business Interiors in the amount of \$55,597.52. Commissioner Askew seconded the motion. Motion carried unanimously.

# Executive Session to discuss a property matter.

Vice Chairman Wright moved to go into executive session to discuss a property matter. Commissioner Gregory seconded the motion. Motion carried unanimously.

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and

ACCOUNT NUMBER	ACCOUNT DESCRIPTION	DEBIT	CREDIT
10-330-486-00 10-640-327-00	Americorp Grant Americorps	\$5,036	\$5,036

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ACCOUNT NUMBER	ACCOUNT DESCRIPTION	DEBIT	CREDIT
10-330-487-00	Support our Students		\$75,000
10-641-020-00	Salaries - Regular	\$23,400	
10-641-050-00	FICA Expense	1,790	
10-641-060-00	Insurance Expense	2,025	
10-641-070-00	Retirement Expense	2,181	
10-641-110-00	Telephone & Postage	600	

10-641-115-00	Printing	500
10-641-140-00	Travel	1,044
10-641-142-00	Transportation	2,240
10-641-145-00	Training & Education	1,766
10-641-210-00	Rent	1,500
10-641-320-00	Supplies	4,626
10-641-450-00	Contracted Services	17, 975
10-641-540-00	Insurance & Bonds	2,223
10-641-890-00	Miscellaneous	2,880
10-641-900-00	Capital Outlay	10,250

# Adjourn

After reconvening from executive session no action was taken. There being no further business the meeting adjourned at 10:50 p.m.

Currituck County
North Carolina
February 6, 1995
Work Sessions held on
January 19, 1995 and January 31, 1995

New High School-

The Currituck County Board of Commissioners met in a work session on January 19, 1995 at 7:00 p.m. with the School Board to discuss the High School bids and other related school projects. The following members were present: Chairman Bowden, Commissioners Gregory, Miller, O'Neal and Etheridge.

Chairman Bowden read the following statement:

• The Board of Commissioners does not intend to provide funding above the current project sheet estimates of \$19,400,000 for the five bond funded capital projects that are specified below:

	Construction Furnishing	12,26		7 <u>50,00</u> 750,51	
	Professional Construction Furnishings		4,78±	\$ 5 <u>87,3</u> 978,1	
"Existing" High School Roof	Replacement		\$	200,0	00
School Site North			\$	200,0	00
New Elem. School- Profess	sional Services Construction		8,640 3,000		

Professional Services

981,149

467,675 \$ 3,959,315

• After cutting the project to the greatest extent possible, the Board of Education is requested to re-bid the new high school.

**Furnishings** 

- The County will be a party to the high school construction contract in order to receive sales tax revenue needed for the bond projects.
- The five capital projects are requested to be placed on a five-year building/funding schedule.

- 13. Within sixty (60) days from the date of its first review of the sketch plan, the Planning Board will forward the plat along with it's recommendations, including any conditions or modifications, to the Board of Commissioners. Failure to forward the plat within the allotted time shall have the same effect as a recommendation for approval.
- 14. During the first regularly scheduled monthly meeting of the Board of Commissioners following recommendation by the Planning Board, the Board of Commissioners will hold a public hearing to hear any and all remarks presented by the subdivider/developer, Staff comments and recommendations, Technical Staff comments and others.
- 15.The subdivider/developer or his agent must attend the Board of Commissioners and all subsequent Board meetings for presentation of the application to the appropriate Board and to answer any questions by Board members and others.
- 16.The Board of Commissioners shall discuss with the subdivider/developer or his agent changes deemed advisable, if any, and the kind and extent of improvements to be made by him.
- 17.Upon conclusion of the public hearing, the Board of Commissioners may approve, approve conditionally, disapprove or table the request as set forth in the Board of Commissioner's Rules of Procedures and by State law. Because of the conceptual presentation involved, this shall not constitute an official action of approval of the subdivision for recordation. The reasons for a conditional approval or disapproval shall be placed in the record of the minutes of the proceedings.
- 18.Receiving approval from the Board of Commissioners shall allow the subdivider/developer to proceed with submission of all materials and information required for the preliminary plat review process and to seek all permits as required under this Article.

Part IV. This ordinance amendment shall be in effect from and after the day of , 1995.

Chairman Bowden opened the public hearing. There being no comments, Chairman Bowden closed the public hearing.

Action on PB 94-48 DOVE ROOST: special use permit/sketch plan approval of a 48 lot major subdivision located in Moyock at the southern end of SR 1216 (Puddin' Ridge Road), Tax Map 11, Parcel 2, Moyock Township.

This item was tabled at the request of the developer, James Ferebee, Jr.

Action on: PB 88-62 THE PLANTATIONS (formerly Country Side): sketch plan approval of a 601-lot common open space subdivision located on SR 1215 (Survey Road)

in Moyock, Tax Map 15, Parcel 84, Moyock Township.

Hood Ellis, Attorney, stated that the developer has met all the requirements of the UDO and complied with terms.

Commissioner Gregory moved to deny request as follows. Commissioner O'Neal seconded the motion. Motion carried unanimously.

## MOTION FOR DENIAL BASED ON THE FOLLOWING:

The proposed 601 lot development fails to meet the provisions of Section 1402(2)(e) of the UDO because it exceeds the county's ability to provide adequate public school facilities based on evidence presented during the public hearings as stated below.

#### FINDINGS:

- 1. The 1992 <u>School Facilities Needs Analysis and Long Range Capital Improvement Plan</u> indicated that the then enrollment of 576 students in Moyock Elementary School required an additional 14,626 square feet of floor space to meet state school facility standards.
- 2. The 1994 school enrollment in Moyock Elementary School is 715 students.
- 3. The state standard for elementary school sites is 10 acres plus 1 acre for every 100 students in membership. The Moyock Elementary School is on a 10 acre site of which only 5.8 acres of the site is usable when utility and drainage areas are removed.
- 4. According to school enrollment figures for 1994, Quail Run, North Point, Quail Point, Moyock Meadows, Duck Ridge, and Old South Landing averaged the following number of students per home: .46 elementary school; .09 junior high school; .14 high school. Assuming these figures hold true for the Plantations subdivision, 601 single-family dwellings will generate 280 elementary students; 54 junior high school students; and 84 high school students at full buildout.
- 5. According to school officials, plans are that by September 1996, Knapp Jr. High will have been converted to an elementary school which will optimally accommodate up to 350 students; Moyock Elementary will optimally accommodate 450 students; and Central Elementary will optimally accommodate up to 375 students (1,175 total elementary students). Based on Planning staff estimates, under a high growth scenario, there will be 1,215 elementary students in

Moyock & Crawford Townships. Under a medium growth scenario, there will be 1,185 elementary students in Moyock and Crawford Townships.

- 6. According to Planning and Inspections, there are nearly 3,000 lots ready for development or having approval for development on the Currituck Mainland, Gibbs Woods and Knotts Island.
- 7. Based on the last 3 years, student enrollment at Moyock Elementary School has increased by 46 students per year.

Action on PB 94-49 THE CURRITUCK ASSOCIATES, RESIDENTIAL PARTNERSHIP AND THE CURRITUCK ASSOCIATES, GOLF PARTNERSHIP: rezone 586.7 from RO1 (Outer Banks Standard Residential) to Planned Unit Development RO1 (Outer Banks Standard Residential) (Limited Business, hotels allowed) located in Corolla on the west side of NC 12 Trail) across from Ocean Sands, PUD, Tax Map 117, Parcel 1A, Poplar Branch Township.

This item was continued until the February 20, 1995 meeting.

Action on PB 94-49 THE CURRITUCK CLUB: sketch plan/special use permit for a planned unit development to contain: 476 single family lots, 104 golf course villas, 56 patio homes, 2 commercial areas and 4 employee housing units located in Corolla on the west side of NC 12 extending from northern property lines Pine Island, PUD, and National Audubon Society, Inc. to southern property lines of Ocean Sands, sections X and Y, Tax Map 117, parcel 1A, Poplar Branch Township.

This item was continued until the February 20, 1995 meeting.

Public Hearing on PB 94-51 CURRITUCK COUNTY: request amendments to the UDO in order to establish an expiration date on subdivision plats approved prior to April 2, 1989 where no work has begun; to increase the lot size in agricultural zoning districts to 3 acres and all other zoning districts except RO1 and RO2 to 40,000 sq. ft., to delete multi-family dwellings permitted in the as uses agricultural district; to increase the



# Currituck County Agenda Item Summary Sheet

**Agenda ID Number** – (ID # 3305)

**Agenda Item Title:** Consideration and Action on a Grant Agreement Between Currituck County and Corolla Volunteer Fire Department for the Purchase of Fire Apparatus.

**Submitted By:** Samantha Evans – County Manager

**Presenter of Item:** 

**Board Action:** Action

## **Brief Description of Agenda Item:**

FEAB voted to support Corollas VFD's request for an interest-free loan from the county in the amount of \$265,000.00 to purchase a used 2013 Rosenbauer Fire Engine (rescue pumper) (to be deducted in equal annual installments from their county operating budget for the next ten years) to purchase a used fire apparatus.

Is this item regulated by plan, regulation or statute?

**Manager Recommendation:** 

This instrument has been preaudited in the manner required by the Local Government Budget and Fiscal Control Act.

Finance Officer

## GRANT AGREEMENT

THIS GRANT AGREEMENT (the "Agreement"), made this \_\_\_\_ day of \_\_\_\_\_\_, 2021 by and between the COUNTY OF CURRITUCK, NORTH CAROLINA, a body corporate and politic existing pursuant to the laws of the State of North Carolina, (hereinafter called the "Grantor"), and COROLLA FIRE AND RESCUE SQUAD, INC., a North Carolina nonprofit corporation, (hereinafter called the "Grantee");

## WITNESSETH:

WHEREAS, pursuant to N.C. Gen. Stat. §153A-449 a county may contract with and appropriate money to any corporation to carry out any public purpose that the county is authorized by law to engage in; and

WHEREAS, pursuant to N.C. Gen. Stat. §153A-233 a county may provide financial assistance to incorporated volunteer fire departments and contract with incorporated volunteer fire departments for firefighting or prevention services; and

WHEREAS, Grantor and Grantee have entered a contractual relationship for Grantee to provide fire protection and rescue services for the health, safety and welfare of Grantor's citizens and the protection of property within Grantor's boundaries; and

WHEREAS, Grantee needs financial assistance for the acquisition of a 2013 Rosenbauer Rescue Pumper for protection of firefighters and rescue personnel and enhancement of fire and rescue functions.

NOW, THEREFORE, in consideration of One Dollar (\$1.00) and other good and valuable consideration, the receipt and sufficiency of which are hereby acknowledged, the Grantor and Grantee covenant and agree as follows:

Section 1. <u>The Grant</u>. In accordance with the terms of this Agreement, the Grantor will provide a grant to the Grantee up to the sum of TWO HUNDRED

SIXTY-FIVE THOUSAND AND NO/100 DOLLARS (\$265,000.00) for the purchase of a 2013 Rosenbauer Rescue Pumper, (the "Apparatus").

- Section 2. <u>Disbursement of Grant Proceeds</u>. Grantor shall disburse to Grantee's vendor the amount of the grant set forth in Section 1 of this Agreement upon presentation of a final invoice from Grantee's vendor to the Grantor.
- Section 3. <u>Use of Grant Proceeds</u>. The proceeds of the Grant are to be used only for purchase of the Apparatus.
- Section 4. <u>Mutuality of Obligation</u>. The Grantee will accept all disbursements made by the Grantor on behalf of the Grantee pursuant to this Agreement up to the full amount of the Grant.
- Section 5. <u>Inspections; Independent Inspecting Representative</u>. The Grantor shall have the right to inspect the Apparatus. Should there be any discrepancy in the Apparatus purchased from that provided for in this Agreement, the Grantor shall be relieved of the obligation to advance any undisbursed Grant proceeds until such time as the discrepancy shall have been corrected to the satisfaction of the Grantor.
- Section 6. <u>Repayment of Funding.</u> Beginning with Grantor's 2021-2022 Fiscal Year Budget, Grantor shall deduct from funds allocated annually by Grantor to Grantee for fire and rescue services \$26,500.00 and a like amount every fiscal year thereafter until the full amount of funding provided Grantee under this Agreement is retained by Grantor.
- Section 7. No Third-Party Beneficiary; No Warranties. All conditions precedent to the obligation of the Grantor to make disbursements hereunder are imposed solely and exclusively for the benefit of the Grantor and its assigns. No other person shall have standing to require satisfaction of such conditions in accordance with their terms or be entitled to assume that the Grantor will refuse to make disbursements in the absence of strict compliance with any or all thereof. No person other than the Grantee shall, under any circumstances, be deemed to be a beneficiary of this Agreement, or any of the terms or conditions hereof, any or all of which may be freely waived in whole or in part by the Grantor at any time if in its sole discretion it deems it advisable to do so.
- Section 8. <u>Incorporation by Reference</u>. The terms, conditions, warranties, representations and agreements contained in any document executed in connection with the Grant are incorporated herein by reference and made a part hereof as fully and completely as if set out herein verbatim and any default therein shall constitute a default hereunder.

- Section 9. <u>Events of Default</u>. The following shall constitute defaults (each a "default") hereunder:
  - (a) The failure of the Grantee to keep, perform or observe any covenant, agreement, term or condition herein required to be kept, performed or observed by the Grantee.
  - (b) If any representation or certification given or at any time hereafter required to be given hereunder shall be false or erroneous in any material respect when made.
- Section 10. <u>Agreement to Survive</u>. This Agreement shall survive the initial disbursement of funds and shall remain in full force and effect until such time as the Grant shall have been paid in full.
- Section 11. <u>Counterparts</u>. This Agreement may be executed in any number of counterparts, all of which taken together shall constitute but one and the same instrument.
- Section 12. <u>Successors and Assigns</u>. The covenants, terms and conditions herein contained shall bind (and the benefits and powers shall inure to) the respective heirs, executors, administrators, successors and assigns of the parties hereto. The Grantee, however, shall not assign its rights or obligations under this Agreement unless such assignment has been consented to by the Grantor in writing. Whenever used herein, the singular number shall include the plural, the plural the singular, and the term the "Grantor" shall include any transferee or assignee thereof, whether by operation of law or otherwise.
- Section 12. <u>Governing Law</u>. This Agreement shall be governed by and construed in all respects under the laws of the State of North Carolina without regard to principles of conflict of laws.

[THE REMAINDER OF THIS PAGE HAS BEEN INTENTIONALLY LEFT BLANK]

IN TESTIMONY WHEREOF, this Agreement has been executed under seal by the parties hereto.

	GRANTEE:
ATTEST:	COROLLA FIRE AND RESCUE SQUAD, INC.
Secretary	By: President
(CORPORATE SEAL)	
	GRANTOR:
ATTEST:	COUNTY OF CURRITUCK
Clerk to the Board	By: Michael H. Payment, Chairman Board of Commissioners
(COUNTY SEAL)	

TRUCKS FOR SALE \*

FIRE TRUCK FINDER®



**SELL YOUR TRUCK** 

#### **SERVICE & REPAIR**

# 2013 Rosenbauer Rescue Pumper

# **Call For Custom Quote**









59 <del>+</del>







Print

Listing





Add to Favorites Truck Type

Used

Pumpers and

**Engines** 

Manufacturer Rosenbauer

Year Built

2013

Mileage

51,993

Location

USA -

Southwest

Stock#

14013

Price

Call For Custom Quote



View Truck Video



Thank you for your interest in the 2013 Rosenbauer Rescue Pumper! Fill out the form below and we'll immediately start the process to get you more information.

Fi	rst	Name*	

Richard

Last Name\*

Shortway

Email Address\*

chief6corolla@gmail.com

Phone

**+1 ▼ 2526194442** 

**Fire Department** 

Corolla Fire and Rescue

**Select Country** 

United States

**Select State** 

North Carolina

**Select Purchase Timeframe** 

Less than 1 Month

I am also interested in the f...

Stock #14013

**Submit Inquiry** 

Or Call 256-776-7786

# **Truck Details**

# **General Specs**

Stock#: 14013

2013

Rosenbauer Fire Body

**Rosenbauer Chassis** 

Custom

Length: 35'5"

Truck Height: 10'10"

Wheelbase: 234"

GVWR: 48,500

Seating for 6; 5 SCBA seats

Mileage: 51,993

Engine Hours: 3,721

**Cummins ISX 500 HP Diesel Engine** 

**Engine Brake** 

Allison 4000 evs Automatic Transmission

Additional equipment not included with purchase unless otherwise listed.

**Interior Medical Cabinet** 

## Pump - Tank

Hale Qmax 1500 GPM Top-Mount Pump

Pump Hours: 254

1000 Gallon Polypropylene Tank

25 Gallon (Class A) Foam Cell

25 Gallon (Class B) Foam Cell

Driver's Side Discharges: (2) 2 1/2"

Front Discharges: (1) 1 1/2"

Crosslays/Speedlays: Crosslays: (2) 1 1/2"

Officer's Side Discharge: (1) 2 1/2" (1) 3"

Rear Discharges: (2) 2 1/2"

Driver's Side Suction: (1) 6", (1) 2 1/2"

Officer's Side Suction: (1) 6"

# **Electrical - Lighting**

**Electric Reel** 

**Smart Power Generator** 

(2) Telescoping lights

**LED Lighting** 

**Arrowstick Traffic Indicator** 

# **Options**

**Air Conditioning** 

**Aluminum Hose Bed Cover** 

Hydraulic Ladder Rack

**Backup Camera** 

Federal Q Siren

Extended Front Bumper

# Apparatus Information

Today, Rosenbauer is one of the world's leading manufacturers of fire fighting vehicles and is represented by a sales and customer services network in over 100 countries. The Group's core competence lies in mobile fire and disaster protection, particularly with regard to the development and production of the very latest fire fighting systems and fire fighting vehicle superstructures. Rosenbauer produces a complete portfolio of fire fighting vehicles according to both European and US standards and provides the fire services with a full range of equipment through the combination of vehicles with extinguishing systems and an extensive equipment program. The Rosenbauer Group has been doing business for over 140 years. The business has survived numerous economic hardships and still has not faltered. Take a look and see what the company has seen unfold and has overcome during its many years in business.

http://www.rosenbaueramerica.com/corporate/history/index.php

# Here are other options that meet your search criteria.

2013 Ferrara Ember Rescue Pumper



# Currituck County Agenda Item Summary Sheet

**Agenda ID Number** – (ID # 3307)

**Agenda Item Title:** Budget Amendments

**Submitted By:** Samantha Evans – County Manager

Presenter of Item:

**Board Action:** Action

**Brief Description of Agenda Item:** 

Is this item regulated by plan, regulation or statute?

**Manager Recommendation:** 

# **BUDGET AMENDMENT**

			Debit	(	Credit	
Account Number	Account Description		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
10535-516000 10535-557100	Repairs and Maintenance Software	\$	1,410	\$	1,410	
		\$	1,410	\$	1,410	
	Communications (10535) - Transfer budge:  : Operating Fund (10) - No change.			on equipment.	spano.	
Minute Book #	, Page #					
lournal #		Clark to t	he Board			

# **BUDGET AMENDMENT**

			Debit		Credit	
Account Number	Account Description		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
10510-590000	Capital Outlay	\$	17,000			
10380-488000	ABC Education Donations			\$	12,000	
10380-488200	ABC Law Enforcement Distribution			\$	5,000	
		\$	17,000	\$	17,000	
Net Budget Effect	: Operating Fund (10) - Increased by \$17,000	).				
Minute Book #	, Page #					
lournal #		Clark to t	the Board			

# **BUDGET AMENDMENT**

			Debit		Credit	
Account Number	Account Description		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
51848-597009	MES Modular Unit - Dec 2021	\$	940,000			
51390-495041	T F - School Capital Reserve			\$	940,000	
41990-587051	T T - School Construction	\$	940,000			
41390-499900	Appropriated Fund Balance			\$	940,000	
		\$	1,880,000	\$	1,880,000	
Net Budget Effec	• • • • • • • • • • • • • • • • • • • •	•				
	School Construction Fund (51) - Increase	a by \$940,000.				
Minute Book #						
	, Page #					

# **BUDGET AMENDMENT**

The Currituck County Board of Commissioners, at a meeting on the 20th of December 2021, passed the following amendment to the budget resolution for the fiscal year ending June 30, 2022.

		Debit	Credit
Account Number	Account Description	 ease Revenue or ease Expense	 ase Revenue or rease Expense
51848-597008	MES - Addition 2021	\$ 13,159,800	
51390-495041	T F - School Capital Reserve		\$ 3,150,000
51390-495042	T F - Transfer Tax Capital Reserve		\$ 10,009,800
41990-587051	T T - School Construction	\$ 1,500,000	
41390-499900	Appropriated Fund Balance		\$ 1,500,000
42450-587051	T T - School Capital Construction	\$ 10,009,800	
42390-499900	Appropriated Fund Balance		\$ 10,009,800
		\$ 24,669,600	\$ 24,669,600

**Explanation:** 

School Capital Construction (51848) - Increase appropriations for addition to the Moyock Elementary School. The Guaranteed Maximum Price (GMP) is \$14,259,800. This funding also includes \$100,000 for unexpected contingencies outside of the GMP and credit for the \$1,200,000 appropriated in FY 2021 for the design.

Net Budget Effect: School Capital Reserve Fund (41) - Increased by \$1,500,000. Transfer Tax Capital Fund (42) - Increased by \$10,009,800. School Construction Fund (51) - Increased by \$13,159,800.

Minute Book #, Page #	
lournal #	Clerk to the Board

# **BUDGET AMENDMENT**

			Debit		Credit	
Account Number	Account Description		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
10430-503000	Salaries - Part Time	\$	1,500			
10430-503430	Poll Workers	\$	21,000			
10430-505000	FICA	\$	1,725			
10340-450505	Administration and Filing Fees			\$	24,225	
		\$	24,225	\$	24,225	
Net Budget Effect	: Operating Fund (10) - Increased by \$	524,225.				
	, Page#					
Journal #		Clerk to	the Board			

# **BUDGET AMENDMENT**

		I	Debit	(	Credit	
Account Number	Account Description		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
10410-553000	Dues & Subscriptions	\$	1,500			
10410-557100	Software License Fees	\$	16			
10410-506000	Health Insurance Expense			\$	1,516	
		\$	1,516	\$	1,516	
	Administration (10410) - Transfer budgeted : Operating Fund (10) - No change.	a tunds for increase in	dues and software	license rees to	r F Y 2022.	
Minute Book #	, Page #					
Journal #		Clerk to t	he Board			

# **BUDGET AMENDMENT**

			Debit	(	Credit	
Account Number	Account Description		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
61818-511010	Data Transmission	\$	10,000			
61818-561000	Professional Services	\$	1,000			
61818-506000	Health Insurance Expense			\$	5,000	
61818-590000	Capital Outlay			\$	6,000	
		\$	11,000	\$	11,000	
Net Budget Effect	: Mainland Water Fund (61) - No change	<b>.</b>				



# Currituck County Agenda Item Summary Sheet

Agenda ID Number – (ID # 3306)

Agenda Item Title: Project Ordinance-Moyock Elementary School Modular

**Submitted By:** Samantha Evans – County Manager

Presenter of Item:

**Board Action:** Action

**Brief Description of Agenda Item:** 

Is this item regulated by plan, regulation or statute?

**Manager Recommendation:** 

# COUNTY OF CURRITUCK CAPITAL PROJECT ORDINANCE

BE IT ORDAINED by the Currituck County Board of Commissioners, North Carolina that pursuant to Section 13.2 of Chapter 159 of the General Statutes of North Carolina, the following capital project ordinance is hereby adopted:

**SECTION 1.** The project authorized is purchase and set-up of modular classrooms at Moyock Elementary School.

**SECTION 2.** The following amounts are appropriated for the project:

Moyock Elementary Modular Unit - Dec 2021

\$ 940,000

\$ 940,000

**SECTION 3.** The following funds are available to complete this project:

Transfer from School Capital Reserve

\$ 940,000

\$ 940,000

**SECTION 4.** The Finance Director is hereby directed to report, on a quarterly basis, on the financial status of each project element delineated in Section 2 above.

## **SECTION 5. SPECIAL APPROPRIATIONS AND RESTRICTIONS**

The Budget Officer is hereby authorized to transfer appropriations within the fund as contained herein under the following conditions:

a. He may transfer amounts between object line items within the fund up to One Thousand dollars (\$1,000).

#### SECTION 6. CONTRACTUAL OBLIGATIONS

The County Manager is hereby authorized to execute contractual documents under the following conditions:

- a. He may execute contracts for construction or repair projects which do not require formal competitive bid procedures.
- b. He may execute contracts for (1) purchases of apparatus, supplies, and materials, or equipment which are within the budgeted departmental appropriations; (2) leases of personal property for a duration of one year or less and within budgeted

- departmental appropriations; and (3) services which are within budgeted departmental appropriations.
- c. He may execute contracts, as the lessor or lessee of real property, which are of a duration of one year or less which are within the budgeted departmental appropriations.

#### **SECTION 7. USE OF BUDGET ORDINANCE**

The Budget Officer and the Finance Director shall use this capital project ordinance for administration of the budget and for the accounting system.

ADOPTED this 20th day of December 2021.		
	Michael Payment, Chairman Board of Commissioners	
ATTEST:		
Samantha Evans Deputy Clerk to the Board		



# Currituck County Agenda Item Summary Sheet

Agenda ID Number – (ID # 3303)

**Agenda Item Title:** Resolution Authorizing the Purchase of Powered Ambulance Stretchers and Stair Chairs From Stryker Corporation Through Sole Source Purchase Pursuant To N.C GEN. STAT. §143-129(e)(6)

Submitted By: Samantha Evans – County Manager

Presenter of Item:

**Board Action:** Action

## **Brief Description of Agenda Item:**

Sole Source Resolution for purchase of SMART Power systems and stair chairs FY22

Is this item regulated by plan, regulation or statute?

**Manager Recommendation:** 



# **COUNTY OF CURRITUCK**

RESOLUTION AUTHORIZING THE PURCHASE OF POWERED AMBULANCE STRETCHERS -AND STAIR CHAIRS FROM STRYKER CORPORATION THROUGH SOLE SOURCE PURCHASE PURSUANT TO N.C. GEN. STAT. §143-129(e)(6)

WHEREAS, N.C. Gen. Stat. §143-129(e)(6) authorizes a unit of local government to purchase apparatus, supplies, materials or equipment when standardization or compatibility is an overriding consideration; and

WHEREAS, Currituck County Fire-EMS needs two (2) ambulance stretchers and two (2) stair chairs; and

WHEREAS, Currituck Fire-EMS must comply with North Carolina Office of Emergency Medical Services rules and regulations for equipment; and

WHEREAS, Stryker Corporation is the only entity capable of providing Fire-EMS with ambulance stretchers and stair chairs compatible with current North Carolina Office of Emergency Medical Services rules and regulations; and

WHEREAS, the County has been using Stryker Corporation to purchase emergency response, power loaded stretchers and ambulance equipment; and

WHEREAS, as the sole and exclusive distributor of the Stryker Power-PRO XT and Stair-PRO, Stryker Corporation is supplying Fire-EMS with two (2) Power-PRO XT ambulance stretchers and two (2) Stair Chairs and accessories at a cost of \$86,788.81; and

WHEREAS, the total cost of the purchase is \$86,788.81.

NOW, THEREFORE, BE IT RESOLVED by the Board of Commissioners for Currituck County, North Carolina as follows:

Section 1. The County of Currituck is authorized to enter into a contract in the amount of \$86,788.81 with Stryker Corporation for the sole source purchase of Power-PRO XT ambulance stretchers and Stair-PRO chairs in accordance with the sole source provision requirements set forth by N.C. Gen. Stat. \$143-129(e)(6). Further, the County Manager is authorized to execute the agreement with Stryker for the acquisition of apparatus, materials, and equipment described in this resolution and the proposed contract.

Section 2. This resolution shall be effecti	ive upon its adoption.
This the day of 2021.	
	Michael H. Payment, Chairman Board of Commissioners
ATTEST:	
Samantha Evans Deputy Clerk to the Board of Commissioners	
(COUNTY SEAL)	



# Currituck County Agenda Item Summary Sheet

**Agenda ID Number** – (ID # 3304)

Agenda Item Title: Resolution Authorizing the Moyock Volunteer Fire Department to Expend

\$8000 of Capital Project Fund to Replace Flooring

**Submitted By:** Samantha Evans – County Manager

Presenter of Item:

**Board Action:** Action

# **Brief Description of Agenda Item:**

Moyock VFD requesting to use \$8000 that was slated for painting the building to replace the floors instead.

Is this item regulated by plan, regulation or statute?

**Manager Recommendation:** 



December 6, 2021 Minutes – Regular Meeting of the Board of Commissioners

#### 6:00 PM CALL TO ORDER

The Board of Commissioners held a regular meeting at 6:00 PM in the Board Meeting Room of the Historic Courthouse, 153 Courthouse Road, Currituck, North Carolina.

Attendee Name	Title	Status	Arrived
Michael H. Payment	Chairman	Present	
Paul M. Beaumont	Vice Chairman	Present	
J. Owen Etheridge	Commissioner	Present	
Mary "Kitty" Etheridge	Commissioner	Absent	
Selina S. Jarvis	Commissioner	Present	
Kevin E. McCord	Commissioner	Present	
Bob White	Commissioner	Present	

Chairman Payment called the meeting to order. He acknowledge the absence of Commissioner Mary Etheridge.

## A) Invocation & Pledge of Allegiance

Commissioner Beaumont offered the Invocation and led the Pledge of Allegiance.

## B) Approval of Agenda

Commissioner McCord moved to amend the agenda and add a Resolution of a Life Saving Event recognizing Ted Fox as Item A under Administrative Report moving current Items to B and C. Commissioner Beaumont also requested to amend the agenda and add Item D under New business for Discussion of Employee Holiday Wages and move Consent Agenda to Item E.

Commissioner Jarvis seconded the motion. The motion carried, 6-0.

Approved agenda:

December 6, 2021

#### 6:00 PM Call to Order

- A) Invocation & Pledge of Allegiance
- B) Approval of Agenda

#### **Election of Board Chair**

## **Election of Board Vice-Chair**

#### **Public Comment**

Please limit comments to matters other than those appearing on this agenda as a Public Hearing. Public comments are limited to 3 minutes.

#### Commissioner's Report

#### **County Manager/County Attorney Reports**

#### Administrative Reports

- A) Amended Item- Resolution of Appreciation to Ted Fox
- B) State Representative Bobby Hanig, Discussion Regarding State Efforts to Resolve Eagle Creek Wastewater Issues.
- C) Televate presentation of the County Public Safety Radio System Analysis and Recommendations.

#### Public Hearings

A) **PB 90-07 Pine Island Phase 5B:** Request for Amended Preliminary Plat/Special Use Permit to approve an additional unit of density in the form of an upper story dwelling unit over the cabana amenity and to designate a small commercial area within Pine Island PUD.

#### **New Business**

- A) Consideration of the Guaranteed Maximum Price for Moyock Elementary School Addition and Renovation Project.
- B) Discussion Regarding Albemarle Regional Health Services Vaccination Requirements for Employees and Impact to Currituck County.
- C) Consideration of National Opioid Settlement Agreement and Authorize Interim County Manager/County Attorney to Execute Settlement Agreements.
- D) Amended Item- Discussion Regarding Employee Holiday Wages
- E) Consent Agenda
  - Consideration of Lease Agreement with the North Carolina Department of Agriculture and Consumer Services for Forest Service Office.
  - Second Amendment to License Agreement for Placement of the Communications Facility on Currituck County-Owned Tower.
  - 3. Resolution in support of Individual Freedom Over Personal Vaccinations Status
  - 4. Resolution Requesting Appropriation of Additional State Funds to Reduce Impacts of COVID-19 Related to Family Violence Expenses
  - 5. Resolution Opposing Proposed Shrimp Fishery Management Plan Amendment 2
  - 6. Contract to Audit Accounts-Amended
  - 7. Project Ordinance- Historic Corolla Park Playground
  - 8. Budget Amendments
  - 9. Approval of Minutes-11/15/21 & SM 11/29/21

### Special Meeting of the Tourism Development Authority

#### **Budget Amendment-TDA**

#### Adjourn TDA and Reconvene Regular Meeting

### **Closed Session**

Closed Session under G. S. 143-318.11(a)(3) to receive from and discuss with the County Attorney privileged and confidential legal matters and to consult with the County Attorney regarding the case entitled Marc Sisino, d/b/a Complete Auto Credit v. Currituck County.

#### <u>Adjourn</u>

RESULT: APPROVED [UNANIMOUS]

MOVER: Kevin E. McCord, Commissioner

SECONDER: Selina S. Jarvis, Commissioner

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord,

Commissioner, Bob White, Commissioner

**ABSENT:** Mary "Kitty" Etheridge, Commissioner

#### **ELECTION OF BOARD CHAIR**

North Carolina General Statute requires a Board of Commissioners to re-organize each year at its first meeting in December. Interim County Manager/County Attorney Ike McRee opened the floor to receive nominations for Chairman. Commissioner Jarvis nominated Commissioner Payment to serve as Chair. No others were nominated and the motion carried 6-0.

#### **ELECTION OF BOARD VICE-CHAIR**

Chairman Payment opened the floor to receive nominations for Vice-Chair. Commissioner McCord nominated Commissioner Beaumont. Commissioner White seconded the motion. No others were nominated and the motion carried 6-0.

#### **PUBLIC COMMENT**

Please limit comments to matters other than those appearing on this agenda as a Public Hearing. Public comments are limited to 3 minutes.

Chairman Payment opened the Public Comment period.

Mark Chappell, Corporal with the Currituck County Sheriffs office requested consideration of a supplement at a reduced rate for retirement.

Barbara Snowden, Currituck, invited the public to the Tree Lighting Service at the Historic Courthouse on Saturday, December 11, 2021 at 6:00pm.

Chief Richard Shortway with the Corolla Fire and Rescue, thanked Commissioner McCord for the Christmas Parade invitation and expressed gratitude for the ability to participate.

No others were signed up nor wished to speak and the Public Comment period was closed.

### **COMMISSIONER'S REPORT**

Commissioner J. Owen Etheridge attended the Public Safety Center Event recognizing Mr. Stanley Griggs and commended Mr. Griggs for his commitment to public health and safety of our county citizens. Mr. Etheridge wished everyone a Merry Christmas.

Commissioner McCord thanked all who assisted with the Annual Christmas Parade. Mr. McCord praised the Cooperative Extension staff, Volunteers and the public for attending.

Commissioner Beaumont thanked Commissioner McCord for his hard work and commended the Christmas Parade. He thanked the Board for their support.

Communication: Approval of 12-6-2021 Minutes (Approval Of Minutes for 12-6-2021)

Chairman Payment expressed appreciation to the Board for allowing him to serve as Chairman for another year. He shared the loss of former Commissioner Newton Hampton, who served the Board from 1980-1984. Mr. Payment expressed gratitude to all first responders and invited all to the Lower Currituck Fire Station for Breakfast with Santa on December 18, 2021.

Commissioner White addressed the funds added for the Historic Corolla Park Playground and he reminded the public of the December 20, 2021 Regular meeting will be held this year.

Commissioner Jarvis attended Coastal Economic Summit on November 17, 2021, and found it very informational. She thanked the Chamber of Commerce for hosting the event. She also attended the dedication to Mr. Stanley Griggs at the Public Safety Center and that portion of the building is training the next generations of first responders. She congratulated Mr. Griggs. Mrs. Jarvis praised the Christmas Parade and involvement of the community.

### **COUNTY MANAGER/COUNTY ATTORNEY REPORTS**

Interim County Manager/County Attorney, Ike McRee updated Commissioner on the Corolla Civic Association lawsuit and anticipates a decision by the end of the year or early next year. Mr. McRee announced the Sound Park playground equipment is ready for installation and expected to be completed in the next two weeks. He reminded the Board of the Employee Holiday Christmas Luncheon scheduled for December 22, 2021.

### ADMINISTRATIVE REPORTS

### A. Amended Item- Resolution of Appreciation to Ted Fox

Commissioner McCord recognized citizen Ted Fox who assisted in the rescue of an individual from an active fire at a Tulls Creek residence. Commissioner McCord described the event and the actions of Mr. Fox to bring the individual to safety. Mr. Fox was presented a framed copy of a Resolution of the Board of Commissioners acknowledging those efforts.

RESOLUTION OF THE CURRITUCK COUNTY BOARD OF COMMISSIONERS IN RECOGNITION AND APPRECIATION FOR THE SERVICE OF TED FOX TO THE CITIZENS OF CURRITUCK COUNTY, NORTH CAROLINA

**WHEREAS**, on August 4, 2021 Ted Fox observed fire and smoke coming from a residential structure near his home in Tulls Bay Colony; and

**WHEREAS,** Ted Fox ran to the residential structure where he found one occupant of the house confined in life threatening circumstances; and

**WHEREAS**, after breaking a window, Ted Fox assisted the occupant's escape from the burning structure to safety prior to arrival of first responders; and

**WHEREAS**, Ted Fox's immediate and selfless lifesaving action rendered to a neighbor is a credit to him for which the citizens of Currituck County are grateful.

**NOW, THEREFORE, BE IT RESOLVED**, by the Currituck County Board of Commissioners, that on behalf of the citizens of Currituck County, the Board of Commissioners acknowledges the lifesaving action of Ted Fox and extends its appreciation for his dedication to community and the need of others.

ADOPTED this 6th day of December, 2021.

Michael Payment, Chairman Board of Commissioners

Samantha M. Evans Deputy Clerk to the Board

# B. State Representative Bobby Hanig, Discussion Regarding State Efforts to Resolve Eagle Creek Wastewater Issues.

Chairman Payment introduced State Representative Bobby Hanig to discuss matters regarding state efforts to resolve the Eagle Creek sewer issues.

Mr. Hanig discussed the communication and timeline of the ongoing issue of Eagle Creek sewer. Rep. Hanig stated a judge has required Sandler to develop a resolution by December 20, 2021 and Sandler will be held accountable. Rep. Hanig provided his contact for citizens to send any questions, concerns or information.

# C. Televate presentation of the County Public Safety Radio System Analysis and Recommendations.

Director of Emergency Management and Communications, Mary-Beth Newns, introduce Dominic Currie with Televate, that was awarded a contract for the county's current radio system and provided recommendations ensuring a radio system that provides optimal safety and reliability for customers, longevity of the systems core and functionality at capacity to accommodate the county. Mr. Currie's presentation of the studies data and project plans reviewed a five site system called Simulcast that simultaneously broadcasts identical information to all county towers at the same time in effort to cover the entire county with wireless coverage for first responders and linking the 911 center through dispatch consoles on a microwave network. A microwave systems has a lifespan expectance of 15 years. Mr. Currie discussed recommended short term actions and potential enhancements.

Mrs. Newns provided a project timeline to show approximately how long each phase will take and discussed the cost effective ways to maintain radio communication equipment. The upgrade move to the new site will require furniture, radio equipment, phone equipment and computer workstations that will be primarily funded with the current North Carolina 911 fund balance and additional Nyenrode funds recently approved totaling under \$815,000.00. The additional funds must be used prior to 2022. Mrs. Newns recommended a path study to determine the height of the new tower and effectiveness of adding two additional booster sites and consideration of development and tree growth. Mrs. Newns will be reapplying for a grant through the North Carolina 911 Board.

At 7:35 PM, Chairman Payment called a brief recess. The meeting reconvened at 7:45 PM.

### **PUBLIC HEARINGS**

#### A. PB 90-07 Pine Island Phase 5B:

APPLICATION SUMMARY	
Property Owner: Turnpike Properties, LLC 4400 Silas Creek Pkwy, Suite 302 Winston Salem, NC 27104	Applicant: Same
Case Number: PB90-07	Application Type: Amended Preliminary Plat/Special Use Permit Phase 5B
Parcel Identification Numbers: 0128-000- 002H-0000 (Phase 5B)	Existing Use: Planned Unit Development
2006 Land Use Plan Classification: Full Service	PUD Parcel Size (Acres): 366.22 Phase 5B: 25.15
Request: Amended Preliminary Plat/Special Use Permit	Zoning: SFO with PUD Overlay
PUD Number of Units: 304 units Phase 5B: 23 units + 1 upper story dwelling unit	PUD Density: .87 units per acre Phase 5B: .95 units per acre
PUD Required Open Space: 128.18 acres (35%) Phase 5B: 5.03 acres (20%) (Mixed Use)	<b>PUD Provided Open Space:</b> 137.72 acres (37.6%) <b>Phase 5B:</b> 6.77 acres (26.9%)

SURROUNDING PA	RCELS	
	Land Use	Zoning
North	Hotel	SFO with PUD Overlay
South	Single Family Dwellings	SFO with PUD Overlay
East	Atlantic Ocean	SFO with PUD Overlay
West	Pine Island Open Space and Air Strip	SFO with PUD Overlay

# **Application Summary**

The applicant is requesting an amended preliminary plat/special use permit to amend an existing approved 23 lot residential subdivision to add an upper story dwelling unit over the cabana amenity within the Pine Island Planned Unit Development (PUD). The upper story dwelling unit and cabana amenity including supporting features are proposed to be designated as commercial area.

On October 18, 2021, the Board of Commissioners approved an amended sketch plan/special use permit for Pine Island PUD to allow Phase 5B (Lot 4R) to be developed as 23 single-family dwelling lots and one additional upper story dwelling unit on condition that the side setbacks shall be a minimum of 15' on the proposed lots. The amended sketch plan/special use permit approval also designated the area of the upper story dwelling unit and cabana as commercial area. The property is located adjacent to the Atlantic Ocean, south of the Hampton Inn in Corolla. Paved sidewalks are proposed within the subdivision

and connections will be made to the existing sidewalk along NC12. Community water access is available on the North and South of the property and each owner of oceanfront property may construct a private beach access way.

INFRASTRUCTURE	
Water	Public
Sewer	Private Centralized System
Transportation	Pedestrian: Proposed sidewalk will connect to path along NC1
	Connectivity Score: Minimum = 1.2 Proposed = 1.5
Stormwater/Drainage	Reviewed by Soil and Stormwater Manager.
Lighting	None proposed.
Landscaping	Street Trees will be required.
Parking	Adequate parking will be provided on each lot as well as at the
	proposed cabana amenity.
Compatibility	The use is compatible with the 2006 Land Use Plan.
Recreation and Park Area	A fee-in-lieu will be provided. The fee will be based on the
Dedication	assessed value at the time of final plat.
Riparian Buffers	CAMA regulations apply to oceanfront lots.

# STAFF REVIEW TECHNICAL REVIEW COMMITTEE

The Technical Review Committee (TRC) reviewed the application and provided the following comments (these are carried over from prior Preliminary Plat/Special Use Permit):

- 1. The application complies with all applicable review standards of the UDO.
- 2. The applicant demonstrates the proposed use will meet the use permit review standards of the UDO.
- 3. The conditions of approval necessary to ensure compliance with the review standards of the UDO and to prevent or minimize adverse effects of the development application on surrounding lands include:
  - a. Side setbacks shall be a minimum of 15' for principal structures.
  - b. A fee in lieu is required and shall be paid prior to final plat.

## 2006 Land Use Plan

The 2006 Land Use Plan classifies this site as Full Service within the Corolla subarea. The Full-Service designation allows for a greater diversity of housing types. The policy emphasis for the Corolla subarea is to allow for predominantly medium density residential development (2 to 3 units per acre) with minimal commercial development arranged in clusters. An overall density of no more than 3 units per acre should apply to PUDs, the prevailing development form in the Corolla area.

The following policies of the plan may apply to the proposed request:

<u>POLICY HN1</u>: Currituck County shall encourage development to occur at densities appropriate for the location. LOCATION AND DENSITY FACTORS shall include whether the development is within an environmentally suitable area, the type and capacity of sewage treatment available to the site, the adequacy of transportation facilities providing access to

the site, and the proximity of the site to existing and planned urban services. For example, projects falling within the Full Services areas of the Future Land Use Map would be permitted a higher density because of the availability of infrastructure as well as similarity to the existing development pattern. Such projects could be developed at a density of two (2) or more dwelling units per acre. Projects within areas designated as Limited Service would be permitted a density of one (1) to one and one half (1.5) units per acre depending upon the surrounding development pattern and availability of resources. Projects within areas designated as Rural or Conservation by the Future Land Use Plan would be permitted a much lower density of 1 dwelling unit per 3 acres because of the lack of infrastructure in the area, the existing low density development pattern, and presence of environmentally sensitive natural areas.

<u>POLICY HN3</u>: Currituck County shall especially encourage two forms of residential development, each with the objective of avoiding traditional suburban sprawl:

- 1. OPEN SPACE DEVELOPMENTS that cluster homes on less land, preserving permanently dedicated open space and often employ on-site or community sewage treatment. These types of developments are likely to occur primarily in the Conservation, Rural, and to a certain extent the Limited Service areas identified on the Future Land Use Map.
- 2. COMPACT, MIXED USE DEVELOPMENTS or DEVELOPMENTS NEAR A MIXTURE OF USES that promote a return to balanced, self-supporting community centers generally served by centralized water and sewer. The types of development are contemplated for the Full Service Areas identified on the Future Land Use Map.

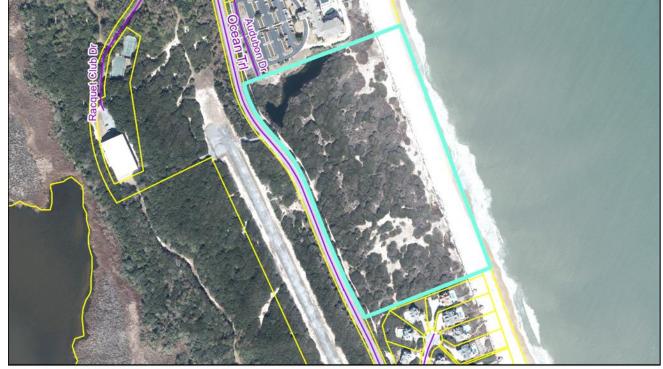
### **SPECIAL USE PERMIT REVIEW STANDARDS**

Following an evidentiary hearing, the board shall decide if the application is in accordance with Section 2.3.10, Decision-Making Body Review and Decision, and Section 2.4.6.D, Special Use Permit Review Standards.

## Special Use Permit Review Standards

A special use permit shall be approved on a finding that the applicant demonstrates the proposed use will:

- 1. Not endanger the public health or safety.
- 2. Not injure the value of adjoining or abutting lands and will be in harmony with the area in which it is located.
- 3. Be in conformity with the Land Use Plan or other officially adopted plan.
- 4. Not exceed the county's ability to provide adequate public facilities, including but not limited to, schools, fire and rescue, law enforcement, and other county facilities. Applicable state standards and guidelines shall be followed for determining when public facilities are adequate.



Pine Island PUD Phase 5B Preliminary Plat/Use Permit Aerial







Preliminary Plat/Use Permit

Land Use Plan Classification

Currituck Development

Planning and Community

THE APPLICATION AND RELATED MATERIALS ARE AVAILABLE ON THE COUNTY'S WEBSITE

Board of Commissioners: www.co.currituck.nc.us/board-of-commissioners-minutes-current.cfm

Parties were sworn and Senior Planner, Jennie Turner, reviewed the request for amended, currently approved on October 18, 2021, preliminary plat and special use permit in Pine Island Phase 5B to add an additional unit of density in the form of an upper story dwelling unit over the cabana amenity and to designate a small commercial area within Pine Island PUD. A powerpoint was used to display location, zoning and uses of the site. A summary of the request with no changes and the sketch plan were reviewed.

Attorney, John Morrison, requesting the Board to take judicial notice of the finding facts entered 45 days prior.

Interim County Manager/County Attorney Ike McRee recommended to take the judicial notice and adopted the finding facts from the previous order entered 45 days of this matter.

Commissioner White moved to accept the Judicial notice that it was previously established in the areas in harmony on October 18, 2021 meeting when the applicant appeared before the Board. Chairman Payment seconded the motion. The motion carried 6-0

No others were signed up nor wished to speak and the Public Hearing was closed.

Commissioner White moved to approve PB 90-07, Pine Island PUD (Phase 5B) amended preliminary plat/special use permit because the applicant has demonstrated the proposed use meets the review standards of the UDO. The TRC Review comments provided in the staff report shall become conditions of approval. The use will not endanger the public health or safety; the use will not injure the value of adjoining or abutting lands and will be in harmony with the area in which it is located; the use will be in conformity with the 2006 Land Use Plan or other officially adopted plans. The use will not exceed the county's' ability to provide adequate public facilities.

Commissioner Jarvis seconded the motion. The motion carried 6-0

RESULT: APPROVED [UNANIMOUS]

MOVER: Bob White, Commissioner

SECONDER: Selina S. Jarvis, Commissioner

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord,

Commissioner, Bob White, Commissioner

**ABSENT:** Mary "Kitty" Etheridge, Commissioner

### **NEW BUSINESS**

# A. Consideration of the Guaranteed Maximum Price for Moyock Elementary School Addition and Renovation Project.

County Manager/County Attorney Ike McRee, discussed the last phase of the design build process to establish the guaranteed maximum price for Moyock Elementary School addition and renovation project in the amount of \$14,259,800.00.

Commissioner J.Owen Etheridge motioned to approve the maximum price \$14,259,800.00. Commissioner White seconded the motion. Motion carried 6-0

RESULT: APPROVED [UNANIMOUS]

MOVER: J. Owen Etheridge, Commissioner

**SECONDER:** Bob White, Commissioner

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord,

Commissioner, Bob White, Commissioner

ABSENT: Mary "Kitty" Etheridge, Commissioner

# B. Discussion Regarding Albemarle Regional Health Services Vaccination Requirements for Employees and Impact to Currituck County.

Commissioner Beaumont discussed Albemarle Regional Health adopted policy regarding vaccination requirements for employees and expressed his concerns of the effects that Currituck County may encounter. He expressed concern for Currituck inspections becoming back logged. During discussion Chairman Payment reminded the Board that Albemarle Regional Health is a separate entity and expressed discussing a separate entity's personnel matters being inappropriate. Mr. Payment provided current information of one vacancy in Currituck County and two new hires with active authorizations to work unassisted immediately. He said there are thirteen Environmental Health Specialist that rotate around the district as needed and currently inspections are at about two and a half week delay which is ahead of schedule from the prior year. Commissioner Beaumont expressed a lack of confidence with the way the Albemarle Health Advisory Board and Commissioner McCord echoed Mr. Beaumont concerns.

Chairman Payment said that he would provide monthly updates to the Board.

# C. Consideration of National Opioid Settlement Agreement and Authorize Interim County Manager/County Attorney to Execute Settlement Agreements.

Interim County Manager/County Attorney Ike McRee discussed the National Opioid Settlement Agreement. The county along with many other counties, cities and states around the country are involved with the ongoing litigation about opioids through companies controlling and recording how the opioids are being distributed. The settlement will allow for a broad use of funds for various uses including intervention, treatment, education and recovery services and the state and local governments left to decide what debts best fit their communities. The North Carolina Department of Justice agrees that the county government is best suited to use and distribute the proceeds in each County's jurisdiction. Mr. McRee recommended the Board authorize the execution of the settlement agreement on behalf of

Communication: Approval of 12-6-2021 Minutes (Approval Of Minutes for 12-6-2021)

Currituck County and authorize Mr. McRee as Interim County Manager to execute them on behalf of the county.

Commissioner White moved for approval. Commissioner McCord seconded the motion. Motion carried 6-0

RESULT: APPROVED [UNANIMOUS]
MOVER: Bob White, Commissioner
SECONDER: Kevin E. McCord. Commissioner

SECONDER: Kevin E. McCord, Commissioner

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord,

Commissioner, Bob White, Commissioner Mary "Kitty" Etheridge, Commissioner

### D. Amended Item- Discussion Regarding Employee Holiday Wages

Commissioner Beaumont requested clarification of the county's recognized holidays and for the wages of employees working to be reviewed.

Interim County Manager/County Attorney Ike McRee explained the county follows the state holiday schedule. Mr. McRee addressed the issue that the years when a holiday like Christmas falls on a Saturday employees like the Sheriffs department or EMS who are working do not receive regular time and overtime, whereas their fellow employees who may work on Friday which is observed as the state holiday are paid regular and overtime wages.

Commissioner Beaumont requested staff to review the personnel policy and possibly modify the structure for holiday pay.

### E) Consent Agenda

**ABSENT:** 

ABSENT:

Commissioner Beaumont moved for approval of the Consent Agenda. Commissioner White seconded the motion. The motion carried, 6-0, and Consent Agenda items were approved.

RESULT: APPROVED [UNANIMOUS]

MOVER: Paul M. Beaumont, Vice Chairman

**SECONDER:** Bob White, Commissioner

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord,

Commissioner, Bob White, Commissioner Mary "Kitty" Etheridge, Commissioner

1. Consideration of Lease Agreement with the North Carolina Department of Agriculture and Consumer Services for Forest Service Office.

2. Second Amendment to License Agreement for Placement of the Communications Facility on Currituck County-Owned Tower.

3. Resolution in support of Individual Freedom Over Personal Vaccinations Status

# RESOLUTION IN SUPPORT OF INDIVIDUAL FREEDOM OVER PERSONAL VACCINATION STATUS

WHEREAS, the freedom to make personal health choices is one of the most fundamental human rights protected under the Bill of Rights of the United States Constitution; and

WHEREAS, informed consent is the bedrock of ethical practice in medicine, consisting of three essential elements: access to complete information, patient comprehension, and voluntariness; and

WHEREAS, private and governmental entities across the United States and around the world are mandating COVID-19 vaccination, coupled with punitive restrictions on freedom, commerce, employment and medical care for non-compliance; and

WHEREAS, there are various reasons why Americans choose not to be vaccinated, including, but not limited to, established medical conditions, previous infection and recovery, preference for therapeutics; robust immune systems; prevention of potential adverse events; religious beliefs; intolerance to vaccine ingredients; previous anaphylactic and other reactions; and

**WHEREAS**, during the 2021 legislative session House Bill 572," No Vaccine Mandate by EU Rule, or Agency" passed the North Carolina House of Representatives; and

**WHEREAS**, during the 2021 legislative session House Bill 686," An Act Prohibiting State and Local Government Retribution Regarding Refusal of Vaccines" is currently in committee; and

WHEREAS, on September 9, 2021, despite his promise to the contrary, President Biden directed the Occupational Safety and Health Administration rule adoption for mandated vaccinations and/or testing requirements for businesses with over 100

employees; and

NOW, THEREFORE, BE IT RESOLVED by the Currituck County Board of Commissioners that the board upholds the United States Constitution and fully supports the individual freedom to choose whether to be vaccinated or unvaccinated and, while recognizing the historical efficacy of proven vaccines in public health, rejects policies of mandatory vaccination regardless of Food and Drug Administration approval, rejects proposals for statewide, nationwide, or global "

vaccine passports," "health passes" or similar policies. The board further thanks and supports members of the North Carolina General Assembly, local government officials and other elected officials for their continued efforts to ensure that individual freedom to be vaccinated or unvaccinated without penalty or discrimination is fully protected.

ADOPTED this 6th day of December, 2021.

Michael H. Payment, Chairman Board of Commissioners

ATTEST:

Samantha Evans, Deputy Clerk

(COUNTY SEAL

4. Resolution Requesting Appropriation of Additional State Funds to Reduce Impacts of COVID-19 Related to Family Violence Expenses

RESOLUTION REQUESTING APPROPRIATION OF ADDITIONAL STATE FUNDS TO REDUCE IMPACTS OF COVID-19 RELATED FAMILY VIOLENCE EXPENSES

WHEREAS, counties in the State of North Carolina are receiving Coronavirus Local Fiscal Recovery Funds from the federal American Rescue Plan Act (ARPA); and

WHEREAS, these funds are designated to be spent on public health emergencies and any negative economic impacts with respect to COVID-19, and/or to provide premium pay to essential workers, and/or to supplement general revenue loss sustained, and/or to invest in eligible projects related to water, sewer, and broadband infrastructure; and

WHEREAS, area social services agencies have seen an increase of child abuse and neglect, elder abuse and neglect as well as domestic violence reports in the wake of the COVID-19 pandemic as individuals have begun to resume their daily routines; and

WHEREAS, the financial impact of the increased caseload has been significant to counties; and

WHEREAS, the State of North Carolina appropriates monies to assist counties with some of the costs associated with family violence and abuse case work; and

**WHEREAS,** the amount of funding to counties is determined by the State of North Carolina; and

WHEREAS, counties are unable to use the ARPA monies appropriated to offset these expenses.

**NOW, THEREFORE, BE IT RESOLVED** by the Currituck County Board of Commissioners that:

Section 1. Believing that all persons in the county and State of North Carolina should be afforded adequate protection and services regardless of local fiscal constraints, Currituck County requests that the North Carolina General Assembly act to increase local appropriations for social services specifically for child abuse, elder abuse and domestic violence case expenses.

Section 2. The Clerk to the Board of Commissioners shall forward a copy of this resolution to all North Carolina counties.

**ADOPTED** this 6<sup>th</sup> day of December, 2021.

Michael H. Payment, Chairman Board of Commissioners

ATTEST:

Samantha Evans, Deputy Clerk

(COUNTY SEAL)

# 5. Resolution Opposing Proposed Shrimp Fishery Management Plan Amendment 2

# RESOLUTION OPPOSING PROPOSED SHRIMP FISHERY MANAGEMENT PLAN AMENDMENT 2

WHEREAS, the Currituck County Board of Commissioners is advised of relevant issues with detrimental effect on the Currituck County recreational and commercial fishermen; and

WHEREAS, members of the county's local fishing community, some of whom are recreational fishermen and head boat operators familiar with the North Carolina fisheries, are concerned about proposed Shrimp Fishery Management Plan Amendment 2; and

WHEREAS, the Currituck County Board of Commissioners is also concerned about proposed Shrimp Fishery Management Plan Amendment 2 based in part on the following:

- The North Carolina Division of Marine Fisheries' ("NCDMF") recommendations include area closures to shrimp trawling in State coastal waters of almost 69,000 acres.
- The many close-shore areas that will be closed will require shrimpers go further into open waters where weather and rough seas could make conditions life-threatening for smaller vessels.
- A large percentage of our shrimp trawl fleet is composed of boats less than 50 feet.

- The areas recommended for closures are not where finfish tend to spawn; they are connecting areas between nurseries.
- The recommended options were not made available to the public by the NCDMF until Monday, November 1st, allowing limited time for public comment.
- The potential benefits of the area closures are unquantifiable, but the devastating economic impacts to small vessel owners are undeniable.
- The proposed amendment will have devastating impacts to the fishing economies of Currituck County and could potentially destroy the county's fishing heritage and put local fishermen out of business; and

WHEREAS, shrimp are the most consumed seafood in the United States, and:

- Health experts are continually telling citizens of the United States to eat more seafood;
- Nearly 90% of the shrimp consumed in the United States is estimated to be imported;
- The United States Department of Agriculture inspects only 1-2% of imported seafood;
- North Carolina is an important source of shrimp for consumers:
- Shrimp are sustainably fished in North Carolina;
- Trawling is the main way shrimp are harvested to provide for consumers;
- North Carolina is one of the few States that has commercial fisheries for three species ofshrimp (brown, pink, and white);
- North Carolina has had an important shrimp trawl fishery for many years;
   and

WHEREAS, the Currituck County Board of Commissioners supports fishery management that incorporates the whole body of available evidence and considers the biology of the fish, environmental conditions, prior management actions, and uncertainties about the data.

**NOW, THEREFORE, BE IT RESOLVED** by the Currituck County Board of Commissioners that Currituck County reaffirms its commitment to North Carolina's fishing industry and opposes Shrimp Fishery Management Plan Amendment 2 or any other action that would cause harm to the county's recreational and commercial fishermen.

**BE IT FURTHER RESOLVED** that the Currituck County Board of Commissioners directs the Clerk to the Board to submit this Resolution to the North Carolina Fisheries Advisory Committee as public comment in opposition to Shrimp Fishery Management Plan Amendment 2.

**ADOPTED,** the 6<sup>th</sup> day of December, 2021.

Michael H. Payment, Chairman Board of Commissioners

ATTEST:

Samantha Evans, Deputy Clerk

(COUNTY SEAL)

- 6. Contract to Audit Accounts-Amended
- 7. Project Ordinance- Historic Corolla Park Playground
- 8. Budget Amendments

		Debit	Credit				
		Decrease Revenue or	Increase Revenue or				
ccount Number	Account Description	Increase Expense	Decrease Expense				
iooodiit i tairiboi	71000ain Booonphon	morodos Exponos	Booloado Expondo				
50442-590001	Connect Corolla Parking Project		\$ 15,115				
50442-590002	Connecting Corolla Bike Path		2,035,031				
50442-590003	Connecting Corolla East Side Walkway		14,232				
50442-590005	Connecting Corolla Albacore Sidewalk		158,538				
50448-590000	Restroom - 2016 Whalehead		250,000				
50510-590002	Shooting Range		236.195				
50512-590000	Incinerator		156,546				
50512-594500	Contract Services - Animal Shelter		2.196.120				
50512-596100	Professional Services - Animal Shelter		262,427	State 90%	Other		
50550-592007	Parallel Taxiway 36237.8.6.1		1,273,334	1,157,576.36	115.757.64		
50550-592009	Rehab Taxiway A 36244.10.3.1		179.330	163,027,27	16.302.73		
50550-592010	Airport - Matching Funds		160,132	,	160,132.00		
50550-592011	Taxiway 36244.10.4.1		510,670	464.245.45	46,424.55		
50550-592012	COA Taxi/Apron 36244.10.5.1		110,030	100,027.27	10,002.73		
50550-592013	Airfield Improvements 36237.8.10.1		166,667	151,515.45	15,151.55		
50550-592014	Airport Taxi/Apron 36244.10.5.2		441,562	401,420,00	40.142.00		
50550-592016	Airport Fuel Upgrades 36237.8.10.1		103,055	93,686.36	9,368.64		
50550-592017	S Parallel Taxi 36244.10.6.1		2,154,069	1,958,244.55			
50550-592018	Corp Area Apron 36237.8.13.1		107,336	97.578.18	9.757.82		
50550-592019	Corp Apron Ph II 86237.8.12.1		885,905	805,368.18	80,536.82		
50795-590005	Veteran's Park Dock Improvements		687,614	5,392,689.09		Fund through investment earnings for	r actual
50795-594500	Soccer Fields		2,734,629	5,552,005.05	000,400.01	Turid through investment carriings for	actual
50795-594600	Baseball/Softball Fields		5,736,663				
50796-590012	Carova Park FY 2018		18.552				
50848-587051	TT - School Construction Fund		2,250,000				
50848-587067	T T - Moyock Central Sewer		378.000				
50848-592003	Community Parks		2,449,000				
50442-588000	Connecting Corolla Contingencies	\$ 202,983	2,445,000				
50390-495015	T F - Occupancy Tax	9,792,110					
50390-495010	T F - Operating Fund	3,732,110	707,405				
50390-495040	T F - Capital Improvements		69,485				
50390-495042	T F - Transfer Tax Capital Fund	9,479,714	05,405				
50330-493042	Park Grant	249,905					
50330-447500	State Aid to Airports	5.392.689					
50330-449900	Miscellaneous Grants	2,000					
50380-481000	Investment earnings	724,401					
50390-495051	T F - School Construction Fund	603.840					
JUGGU-43000 I	11 - GCHOOL CONSTRUCTION 1 UNIO	003,040					
		\$ 26,447,642	\$ 26,447,642	\$ -			
Explanation:	County Government Facilities Fund (50) - To clo	se out County Governmental con	struction projects that have				
	been completed in prior years.						

		Debit	Credit
		Decrease Revenue or	Increase Revenue or
Account Number	Account Description	Increase Expense	Decrease Expense
10511-545000	Contract Services	\$ 6,000	
10320-411000	Article 39 Sales Tax		\$ 6,000
		\$ 6,000	\$ 6,000
Explanation:	Detention Center (10511) - Increase ap	propriations for extraordinary inmate medi-	cal bill.
Net Budget Effe	ct: Operating Fund (10) - Increased by	\$6,000.	
		Debit	Credit
		Debit	Credit
		Decrease Revenue or	Increase Revenue or
Account Number	Account Description	Increase Expense	Decrease Expense
10541-531000	Fuel	\$ 1,000	
10541-514600	Public Education		\$ 1,000
		\$ 1,000	\$ 1,000
Explanation:	County Fire Services (10541) - Transfer	r budgeted funds for fuel for the remainder	of this fiscal year.
Net Budget Effe	ct: Operating Fund (10) - No change.		
		Debit	Credit
		Decrease Revenue or	Increase Revenue or
Account Number	Account Description	Increase Expense	Decrease Expense
51848-592004	Griggs HVAC replacement - June 20	018	\$ 59,020
51848-592006	Griggs - HVAC A Wing South		\$ 115,000
51848-592007	Griggs - Roof replacement Center V	Vina	\$ 113,250
51848-592008	Griggs - Roof replacement A Wing S		\$ 80,000
51848-592009	Griggs - Roof replacement A Wing I		\$ 80,000
51848-590001	Jarvisburg - HVAC April 2019		\$ 32,250
51848-594003	MMS - Energy Mgmt Upgrade June	2018	\$ 40,000
51848-593005	KIES - HVAC June 2016		\$ 27,570
51848-596001	Shawboro - HVAC April 2019		\$ 18,225
51848-595007	CCHS HVAC Dec 2017		\$ 30,500
51848-595008	CCHS Energy Mgmt June 2018		\$ 40,000
51380-425001	Lottery Proceeds	\$ 635,815	
		\$ 635,815	\$ 635,815
Explanation:	School Multi-year Construction (51949)	) - To close out completed school projects	funded with lottery
Expialiation.	proceeds.	10 51036 our completed school projects	Tanaca with lottery
Net Budget Effe	ct: School Multi-vear Construction Fun	d (51) - No changes to overall fund balance	 e.
			<u>.                                      </u>

				Debit		Credit
			D	D	l	D
A a a a cont Normalia a r		Associat Description		se Revenue or		se Revenue o
Account Number		Account Description	increa	ase Expense	Decre	ase Expense
220548-506000		Health Insurance			\$	4,000
220548-516015		Repairs & Maintenance - Knotts Island	\$	4,000		
			\$	4,000	\$	4,000
Explanation:		otts Island Fire Services (220548) - Transfer faintenance for tank monitor level for fire suppre				
Net Budget Effe	ct:	Knotts Island Fire District (220) - No change.				
		, ,		Debit		Credit
			Decrea	se Revenue or	Increas	se Revenue o
Account Number		Account Description	Increa	ase Expense	Decre	ase Expense
10510-590000		Capital Outlay	\$	50,000		
10340-450420		Beach Parking Permits	<b>T</b>	00,000	\$	50,000
			\$	50,000	\$	50,000
			Ψ	30,000	Ψ	30,000
Explanation:	Sh	leriff (10510) - Increase appropriations to replacea.	ce one of the	Jeeps used primarily	y for monitorii	ng the 4 X 4
Net Budget Effe	ct:	Operating Fund (10) - No change.				
				Debit		Credit
			Decrea	se Revenue or	Increas	se Revenue o
Account Number		Account Description	Increa	ase Expense	Decre	ase Expense
50448-590002		Historic Corolla Park Playground	\$	750,000		
50390-495015		T F - Occupancy Tax Fund			\$	750,000
			\$	750,000	\$	750,000
				,	, , , , , , , , , , , , , , , , , , ,	· ·
Explanation:	-	ounty Governmental Facilities (50448) - Increa storic Corolla Park.	se appropriati	ions to provide fundin	ng for a playgr	ound at
Net Budget Effe	ct:	Operating Fund (10) - Increased by \$50,000.				

				Debit		Credit
			Decrease	e Revenue or	Increa	se Revenue o
Account Number		Account Description		se Expense		ease Expense
26535-590535		Capital - 911 Grant Funds	\$	290,955		
26330-445000		Emergency Management Grants			\$	290,955
50531-590000		Capital Outlay	\$	646,486		
50390-495042		T F - Transfer Tax Capital Fund			\$	646,486
42450-587050		TT - Co Govt Facilities Fund	\$	646,486		
42320-414000		Land Transfer Tax			\$	646,486
			\$	1,583,927	\$	1,583,927
Explanation:	eq So	bunty Governmental Construction - Public Safet uipment for the Public Safety building. This an olutions for \$817,826.25, Path and Site Survey funded through Land Transfer Tax and a grant	nendment will services for \$4	cover quote dated 44,615.38 and \$75,	11/29/21 from 000 continge	m Motorola ency. This wi
	eq So be	uipment for the Public Safety building. This an slutions for \$817,826.25, Path and Site Survey funded through Land Transfer Tax and a grant Transfer Tax Capital Fund (42) - Increased by	mendment will services for \$4 from the Eme \$646,486.	cover quote dated 44,615.38 and \$75, ergency Telephone	11/29/21 from 000 continge	m Motorola ency. This wil
	eq So be	uipment for the Public Safety building. This an alutions for \$817,826.25, Path and Site Survey funded through Land Transfer Tax and a grant Transfer Tax Capital Fund (42) - Increased by County Governmental Construction (50) - Incre	nendment will services for \$4 from the Eme \$646,486. eased by \$646	cover quote dated 44,615.38 and \$75, ergency Telephone 53,486.	11/29/21 from 000 continge	m Motorola ency. This wil
	eq So be	uipment for the Public Safety building. This an slutions for \$817,826.25, Path and Site Survey funded through Land Transfer Tax and a grant Transfer Tax Capital Fund (42) - Increased by	nendment will services for \$4 from the Eme \$646,486. eased by \$646 ed by \$290,95	cover quote dated 44,615.38 and \$75, ergency Telephone 53,486.	11/29/21 from 000 continge	m Motorola ency. This wil
Explanation:  Net Budget Effe	eq So be	uipment for the Public Safety building. This an alutions for \$817,826.25, Path and Site Survey funded through Land Transfer Tax and a grant Transfer Tax Capital Fund (42) - Increased by County Governmental Construction (50) - Incre	nendment will services for \$4 from the Eme \$646,486. eased by \$646 ed by \$290,95	cover quote dated 44,615.38 and \$75, ergency Telephone 5,486.	11/29/21 from 000 contingents System (PS.	m Motorola ency. This wil AP) fund.
Net Budget Effe	eq So be	uipment for the Public Safety building. This an alutions for \$817,826.25, Path and Site Survey funded through Land Transfer Tax and a grant Transfer Tax Capital Fund (42) - Increased by County Governmental Construction (50) - Incre	nendment will services for \$4 from the Eme \$646,486. eased by \$646 ed by \$290,95	cover quote dated 44,615.38 and \$75, ergency Telephone 53,486. 5. Debit	11/29/21 from the continued of the conti	m Motorola ency. This wil AP) fund.  Credit  se Revenue c
	eq So be	uipment for the Public Safety building. This an olutions for \$817,826.25, Path and Site Survey funded through Land Transfer Tax and a grant Transfer Tax Capital Fund (42) - Increased by County Governmental Construction (50) - Increased Emergency Telephone System (26) - Increased	nendment will services for \$4 from the Eme \$646,486. eased by \$646 ed by \$290,95	cover quote dated 44,615.38 and \$75, ergency Telephone 53,486. 5. Debit	11/29/21 from the continued of the conti	m Motorola ency. This wil AP) fund.
Net Budget Effe	eq So be	uipment for the Public Safety building. This an plutions for \$817,826.25, Path and Site Survey funded through Land Transfer Tax and a grant Transfer Tax Capital Fund (42) - Increased by County Governmental Construction (50) - Increased Emergency Telephone System (26) - Increased Account Description	nendment will services for \$4 from the Eme \$646,486. eased by \$646 ed by \$290,95	cover quote dated 44,615.38 and \$75, ergency Telephone 53,486. 5. Debit	11/29/21 from 1000 continger System (PS. Increa	m Motorola ency. This wil AP) fund.  Credit se Revenue cease Expense
Net Budget Effe  Account Number  10330-431000	eq So be	uipment for the Public Safety building. This an alutions for \$817,826.25, Path and Site Survey funded through Land Transfer Tax and a grant Transfer Tax Capital Fund (42) - Increased by County Governmental Construction (50) - Increased Emergency Telephone System (26) - Increased Account Description  DSS Administration	nendment will services for \$4 from the Eme \$646,486. eased by \$646 ed by \$290,95	cover quote dated 44,615.38 and \$75, ergency Telephone 53,486. 5. Debit e Revenue or se Expense	11/29/21 from 1000 continger System (PS. Increa	m Motorola ency. This wil AP) fund.  Credit se Revenue cease Expense
Net Budget Effe  Account Number  10330-431000 10390-499900	eq So be	uipment for the Public Safety building. This an alutions for \$817,826.25, Path and Site Survey funded through Land Transfer Tax and a grant  Transfer Tax Capital Fund (42) - Increased by County Governmental Construction (50) - Increased Emergency Telephone System (26) - Increased Account Description  DSS Administration  Appropriated Fund Balance	nendment will services for \$4 from the Eme \$646,486. eased by \$646 ed by \$290,955    Decrease Increase	cover quote dated 44,615.38 and \$75, ergency Telephone 53,486. 5. Debit e Revenue or se Expense 5,065	11/29/21 from 1000 continger System (PS. Increa	m Motorola ency. This wil AP) fund.  Credit se Revenue cease Expense
Net Budget Effe  Account Number  10330-431000 10390-499900  10760-519504	eq So be	uipment for the Public Safety building. This an alutions for \$817,826.25, Path and Site Survey funded through Land Transfer Tax and a grant  Transfer Tax Capital Fund (42) - Increased by County Governmental Construction (50) - Increased Emergency Telephone System (26) - Increased Account Description  DSS Administration  Appropriated Fund Balance  Low Income Water Assistance Program	nendment will services for \$4 from the Eme \$646,486. eased by \$646 ed by \$290,955    Decrease Increase	cover quote dated 44,615.38 and \$75, ergency Telephone 53,486. 5. Debit e Revenue or se Expense 5,065	11/29/21 froi 000 continge System (PS. Increa	m Motorola ency. This wi AP) fund.  Credit se Revenue cease Expense 5,068
Net Budget Effe  Account Number  10330-431000 10390-499900  10760-519504	eq So be	uipment for the Public Safety building. This an alutions for \$817,826.25, Path and Site Survey funded through Land Transfer Tax and a grant  Transfer Tax Capital Fund (42) - Increased by County Governmental Construction (50) - Increased Emergency Telephone System (26) - Increased Account Description  DSS Administration  Appropriated Fund Balance  Low Income Water Assistance Program	nendment will services for \$4 from the Eme \$646,486. eased by \$646 od by \$290,95:  Decrease Increase \$	cover quote dated 44,615.38 and \$75, ergency Telephone 56,486. 5. Debit e Revenue or 56 Expense 5,065 25,328	11/29/21 from 1000 continger System (PS. Increase Decree State	m Motorola ency. This wil AP) fund.  Credit se Revenue co ease Expense  5,065  25,328  30,393

## 9) Approval of Minutes-11/15/21 & SM 11/29/21

- 1. Approval of Minutes- 11/15/21
- 2. Approval of Minutes-SM 11/29/21

## SPECIAL MEETING OF THE TOURISM DEVELOPMENT AUTHORITY

The Regular Meeting of the Board of Commissioners recessed to hold a special meeting of the Tourism Development Authority. The Special Meeting took place in the Board Meeting Room of the Historic Courthouse, 153 Courthouse Road, Currituck, North Carolina, for the purpose of considering Budget Amendments.

## F. Budget Amendment-TDA

Commissioner White moved for approval of the TDA Budget Amendments. Commissioner Jarvis seconded the motion. The motion carried, 6-0.

					Debit		(	Credit
				Decreas	se Revenue or		Increase	e Revenue or
Account Number		Account Description		Increa	se Expense		Decrea	se Expense
15447-545004		Corolla Wild Horse Fund		\$	11,750			
15320-415000		Occupancy Tax					\$	11,750
				\$	11,750		\$	11,750
Explanation:		urism Related Expenses (154 ncing damaged by fall storms.	,	- Increas	se appropriations t	o repaid	d Corolla	Wild Horse
Net Budget Effect	ct:	Occupancy Tax Fund (15) - I	ncr	eased by	\$11,750.			

			Debit	(	Credit
		Decreas	se Revenue or	Increase	e Revenue or
Account Number	Account Description	Increa	se Expense	Decrea	se Expense
15448-502000	Salaries	\$	28,693		
15448-505000	FICA	\$	2,195		
15448-506000	Health Insurance	\$	9,299		
15448-507000	Retirement	\$	5,233		
15448-561900	Administration			\$	45,420
		\$	45,420	\$	45,420
Explanation:	Historic Corolla Park (1544 salaries to hire one Educat second Education Speciali	ion Specialist - W	'ildlife Center to star	t January 1, 2	
Net Budget Effect	ct: Occupancy Tax Fund (	15) - No change.		-	

				Debit		Credit
			Decrea	se Revenue or	Increas	e Revenue or
Account Number		Account Description	Increa	ase Expense	Decrea	ase Expense
15442-526200		Promotions			\$	12,520
15442-590000		Capital Outlay	\$	12,100		
15442-511010		Data Transmission	\$	420		
			\$	12,520	\$	12,520
Explanation:		urism Promotions (15442) - 7 ditional data transmission fee	_	geted funds to repair	two mobile sta	ages and for
Net Budget Effect	ct:	Occupancy Tax Fund (15) -	No change.			

			Debit		Credit
		Decrea	ase Revenue or	Increas	e Revenue or
Account Number	Account Description	Incre	ase Expense	Decrease Expen	
15447-587050	T T - Co Govt Facilities Fund	d \$	750,000		
15320-415000	Occupancy Tax			\$	750,000
		\$	750,000	\$	750,000
Explanation:	Tourism Related Expenses (154 construction of playground at Hi	,		fund design a	nd
Net Budget Effect	ct: Occupancy Tax Fund (15) - I	ncreased b	y \$750,000.		

RESULT: APPROVED [UNANIMOUS]
MOVER: Bob White, Commissioner
SECONDER: Selina S. Jarvis, Commissioner

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord,

Commissioner, Bob White, Commissioner

**ABSENT:** Mary "Kitty" Etheridge, Commissioner

## ADJOURN TDA AND RECONVENE REGULAR MEETING

The Board had no further business and the regular meeting was reconvened.

### **CLOSED SESSION**

Commissioner White moved the board into Closed Session under G. S. 143-318.11(a)(3) to receive from and discuss with the County Attorney privileged and confidential legal matters and to consult with the

County Attorney regarding the case entitled Marc Sisino, d/b/a Complete Auto Credit v. Currituck County.

### **ADJOURN**

### **Motion to Adjourn Meeting**

The Board had no further business and Chairman Payment moved to adjourn. Commissioner Beaumont seconded the motion. The motion carried, 6-0, and the meeting of the Board of Commissioners was adjourned at 9:15 PM.

RESULT: APPROVED [UNANIMOUS]

MOVER: Michael H. Payment, Chairman

SECONDER: Paul M. Beaumont, Vice Chairman

AYES: Michael H. Payment, Chairman, Paul M. Beaumont, Vice Chairman, J. Owen

Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord,

Commissioner, Bob White, Commissioner

**ABSENT:** Mary "Kitty" Etheridge, Commissioner