



**Currituck County**

*Department of Planning and Inspections*  
153 Courthouse Road, Suite 110  
Currituck, North Carolina 27929  
252-232-3055  
FAX 252-232-3026

**MEMORANDUM**

**To:** Rick Willis, Outer Banks Ventures, Inc.  
Mark Bissell, P.E. Bissell Professional Group

**From:** Jennie Turner, Senior Planner

**Date:** December 7, 2023

**Re:** PB 87-56 Monterey Shores, Phase 10 – Corolla Boat Club – 2<sup>nd</sup> Review  
Amended Sketch Plan/Special Use Permit  
Marina Special Use Permit

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The following comments were received at the December 6, 2023 TRC meeting. Please address all comments and resubmit a corrected plan as necessary. TRC comments are valid for six months from the date of the TRC meeting. To be considered for placement on the February 19, 2024 Board of Commissioners meeting agenda, please resubmit an updated plan and requested information by noon on December 14, 2023.

**Planning, Jennie Turner 252-232-6031**

1. There are several outstanding comments from the first TRC memo that need to be addressed. Please provide detailed responses to all comments previously received.

Those are addressed below:

Old comment 3: **NCDOT correspondence is attached.**

Old comment 32: Please describe in detail the proposed operation of the marina, including whether it is transient or permanent. **The marina will be a combination of slips designated for owners of dwellings and transient boaters. There will be a Commodore with office in the clubhouse, who will set and enforce regulations. The marina will be operated in accordance with CAMA's Clean Marina standards. Where will boats launch? Boats will launch from any of several available boat ramps on both the Currituck mainland and the Outer Banks. Are tour operations proposed from marina? If so, additional special use permits may be required. No tour operators have been contacted at this stage. If this becomes an issue, the SUP can be amended at that time. Where will bathrooms associated with this use be provided? A bath house has been added adjacent to the restaurant. Will all owners and guests of the site be afforded access to the pier and marina? Yes. Is it possible to add kayak launches? A 6-foot walkway and short pier has been added, which can be used for launching kayaks.**

Old comment 38: **The marina as proposed appears to comply with these standards.**

**Currituck Building and Fire comments are addressed below.**

- 2 Please provide the overall amended sketch plan exhibit including the new proposed development. [An updated overall PUD sketch plan has been added.](#)
- 3 On page 1 of the amended sketch plan, provide reference to Plat R 372. [The reference has been added.](#)
- 4 Please update existing conditions plan: 804 Ocean Trail does not have a concrete connection to Seaside Farm Market Parcel, the line table is not completely visible. It looks like a building dimension layer is included. Remove proposed pier, walkways, and marina. Remove the word clubhouse. [The existing conditions map has been updated.](#)
2. Non-residential design standards of UDO Section 5.8 apply to the restaurant building. Please describe how the proposed renderings meet these standards. [The Architect's building design uses projections and recesses in the building façade wall with a depth of 18" minimum from the façade plane with a minimum width of 10 feet; the façade color changes which follows the same dimensional standards as the offset standards; incorporation of roofline changes throughout design \(i.e. elevator tower, exterior stairs, restaurant building\); pitched roofs have eaves that exceed minimum of 12 inches from the building face; fenestration includes windows that are visible from adjacent streets; outbuilding \(public restrooms on ground level\) maintain a consistent level of architectural detail, materials and colors on all sides that are compatible with the primary building; and metal siding and/or overly bright, neon, or day-glow colors are NOT being used as the primary exterior building color\).](#)
3. Shopping center design standards of UDO Section 5.9 apply to the commercial buildings with upper story dwelling units. Please describe how the proposed renderings meet these standards. [The building design uses canopies/porticos and roof overhangs above entrance; entry are recessed and covered; gabled roof above entrance; display windows that are directly adjacent to the entrance; design integrates moldings, brackets, and stone work directly above and/or adjacent to the entrance; roof line changes are implemented along the design; sloped roofs in design exceed minimum 4:12 slope while also keeping below the maximum 12:12 slope; metal siding, synthetic stucco \(EIFS\) and vinyl siding are NOT being used.](#)
- 5 We need to discuss the designated commercial use areas. Virginia Lane and Currie Lane are not roads, they are driveways required for serving the commercial uses adjacent to the pond. [As we discussed, the designations are consistent with what was previously approved for this development, and with what was approved for TimBuck II.](#)
- 6 Driveways must extend 20' into the property before establishing drive aisles or parking spaces. [The encroaching parking space was removed; additional feedback is needed on the Currie Lane location.](#)
- 7 The dumpster location at the entrance from Malia Drive does not appear to be best for aesthetics. Is this to be the only refuse collection for the residential portion? Perhaps additional more centrally located trash receptacles could improve the trash collection for residential uses? [An alternate location has not been found, but the area will be completely enclosed with an attractive fence and gate, and will also be landscaped.](#)
- 8 Show connectivity stub connection to the property line. [The stub was extended to the property line.](#)
- 9 There appears to be one CBU located near the upper story dwelling units. Will there be a CBU in the clubhouse or closer to the duplexes? [A second CBU has been added.](#)
- 10 Please label Virginia Lane and Currie Lane as private driveways. [The labels have been added.](#)

- 11 Some duplexes appear to be close to the pond edge, what type of stabilization will be incorporated, a bulkhead is not shown, will a bulkhead be needed? This will be a combination of laying back slopes and low bulkheads, with details to be provided at the next phase of the review.
- 12 Please provide ACOE wetland delineation documentation. A copy of the JD is included.
- 13 The CAMA Coastal Wetland Determination is valid for one year, please provide current determination. The CAMA representative will update the JD tomorrow.
- 14 Use of a TIA for reducing required marina parking spaces isn't allowed by the UDO, please consider use of the alternatives allowed in Section 5.1.6 Alternative Parking Plans. An updated memo is included regarding the use of shared parking. Overall site parking exceeds the UDO requirements.
- 15 Staff suggests a scoping meeting with the Division of Coastal Management and other review agencies regarding the CAMA Major Permit needed for pier/boat slips/gazebo prior to this application being heard by the Board of Commissioners. Please provide an update on conversations with DCM regarding permitting of the proposed marina. WE are in the process of addressing DCM comments, after which other agencies will be brought into the review process.
- 16 Does the applicant want to offer conditions of approval related to the TIA provided with respect to any anticipated traffic impacts?

### **Planning, David Lubelski**

The TIA was based upon traffic counts during the traditional AM and PM commuter peaks. The traffic counts were taken during the 7-9am and 4-6pm period on Wednesday September 1, 2021. This would be representative of a traditional commuter peak condition on the mainland outside of a seasonal recreational area. The main influence on the traffic patterns in Corolla is recreational traffic with a commercial influence and rental turnover. These peaks traditionally occur on weekend during the summer season. Count data encompassing a minimum full week during the summer seasonal week was not presented to support the analysis period presented in the traffic study.

This site is proposed for development with only one means of access. All site traffic must be added to the existing State controlled intersection of Malia Drive with NC 12. The traffic analysis should reflect the conservative and realistic future conditions that not only the average motorist will experience, but also the conditions that response calls for emergency services will experience. If the analysis shows existing and worsening failure conditions and long traffic queues, alternative access may be necessary. The result traffic backups (queue analysis) of an existing traffic signal at NC 12 with Albacore should be known.

The geometric configuration of the Malia Drive intersection with NC 12 is a concern. Malia Drive is misaligned with the opposing shopping center driveway. A misalignment offsetting the intersections is a traffic safety concern. Opposing lefts exiting the two driveways are in conflict. Reconstruction of the access(es) and/or restriping can address the issue. The offset should be addressed in the study.

The TIA report addresses the conditions that are outlined here. In order to mitigate impacts from additional traffic, in addition to the existing left turn lane, left and right turn lanes are being provided at Malia Drive, and a right turn lane and tapers are being added to NC 12 as shown on the updated plan.

**Currituck County Building Inspections & Fire, Richard Godsey 252-232-6020**

Approved

-Will need accessible boat lifts, number to be determined by total number of berths. Accessible lift locations are now indicated on the plan.

-Standpipe required for marina in accordance with NFPA 303. Standpipes have been added to the plan.

-Phone or approved communication device to notify fire department required on pier. A phone location is now shown on the plan.

-Pier shall be provided with fire apparatus access road within 150 of structure and hydrant. Two parking spaces were removed from the restaurant parking lot to provide space for a fire truck to pull up to the wetland buffer and connect to a hydrant and standpipe at that location.

Will need turnaround for fire apparatus. An adequate turning radius appears to be provided.

No parking signs required. No parking signs will be added at the next stage of review.

CMU must be on accessible route. This has been addressed.

Accessible routes must be provided to all amenities. This has also been addressed

**Currituck County Fire and EMS Corolla Division, Captain Sam Dahl**

Reviewed

Currituck County Fire and EMS and Corolla Fire Rescue have the following concerns with the Monterey Shores, PUD, Phase 10A – Corolla Boat Club – Amended Sketch Plan/Special Use Permit:

The turnaround space at the end of Currie Lane is inadequate to accommodate the turning radius of fire apparatus, especially during business hours when the loading zone will be occupied. The turning space is long enough to accommodate an ambulance but not wide enough, especially if the loading zone is occupied. Suggest elongating the west end of the turnaround and the bulkheaded area to 30 feet each and widening the bulkheaded area to one and a half the current width. The bulkheaded area will need to be able to carry the load of the heaviest apparatus which is close to 80,000 pounds. The width has been increased to 30' in both directions.

There is no turnaround space at the end of Virginia Lane to accommodate ambulances and fire apparatus in the restaurant parking lot. Exiting the parking lot will require vehicles to back down the roadway on Virginia Drive to the Currie Drive intersection. Suggest revising the parking lot layout to provide space that allows turning around on the east end of the lot by the existing island. The parking lot has been designed to provide an adequate turning radius.

There should be access for a fire engine to park at the ground-level base of the elevated boardwalk to the dock. A standpipe needs to be installed, covering the entire length of the walkway, dock, and boat slips. The standpipe inlet needs to be at ground level, away from the restaurant structure. A fire hydrant needs to be placed in immediate proximity to the standpipe inlet and the fire engine access. The standpipe should have outlets every 150 feet, 150 feet is the length of hose CCFEMS firefighters carry to connect to standpipes. There should be a standpipe outlet on the outside of the gate immediately prior to the start of the boat slips, this will allow firefighters to connect to the standpipe prior to working near boats in the event of a vessel fire. Two parking spaces were removed from the restaurant parking lot to provide space for a fire truck to pull up to the wetland buffer and connect to a hydrant and standpipe at that location. Standpipes have been added at 150 intervals along the docks.

A dry hydrant to draft water from the stormwater retention pond is recommended as a supplemental, reserve firefighting water supply. This dry hydrant cannot be counted on as a primary water supply. A dry hydrant has been added on Currie Lane.



The fire hydrant on Virginia Lane is very close to R37. Suggest moving the fire hydrant to the island across the street near R32. **The hydrant has been relocated, but kept on the same side of the street to avoid a sewer conflict.**

Will any of the buildings be equipped with fire suppression sprinklers? **Buildings will be designed with fire suppression sprinklers to meet local AHJ requirements per code.**

Will the marina have booms and pads for oil and fuel spill containment? **No fueling facilities are being provided.**

What is the exterior cladding of the single-story duplexes, including soffit and soffit vents? **Exterior cladding material will be cement siding.**

**Stormwater Review, (McAdams, Stormwater Consultant)**

Reviewed

Comments forthcoming

**Currituck County Soil & Stormwater, Dylan Lloyd 252-232-3360**

Approved with Corrections

1) County drain pipe to be extended on Easement Note - Sheet 2, acknowledged. Include measures to promote outlet energy dissipation and avoid short circuiting at Construction drawing phase. **Agreed.**

2) As mentioned in the previous round of comments – the pond shoreline appears to go underneath the foundation of R-42 and the sloped bank underneath R-40. What is the intention here? Will the foundation be on pilings above the permanent pool level? **There will be a combination of laying back slopes and providing low bulkheads, with details to be provided at final site plan review.**

**Currituck County Water, Will Rumsey 252-232-6065**

Approved

**Southern Outer Banks Water, Jim Williams, 252- 453-2620**

No comment

**US Army Corps of Engineers, Anthony Scarbraugh 919-251-4619**

Reviewed

Any impacts to jurisdictional waters or wetlands of the US require prior approval from the US Army Corps of Engineers. **Acknowledged**

**NCDOT, Caitlin Spear, 252-331-4737**

No comments received

**The following items are necessary for resubmittal:**

- 1- PDF digital copy of all revised documents and plans.

**A pdf of all plans and documents is included.**

A special use permit hearing is an evidentiary hearing where the Board of Commissioners must make a Quasi-Judicial Decision.

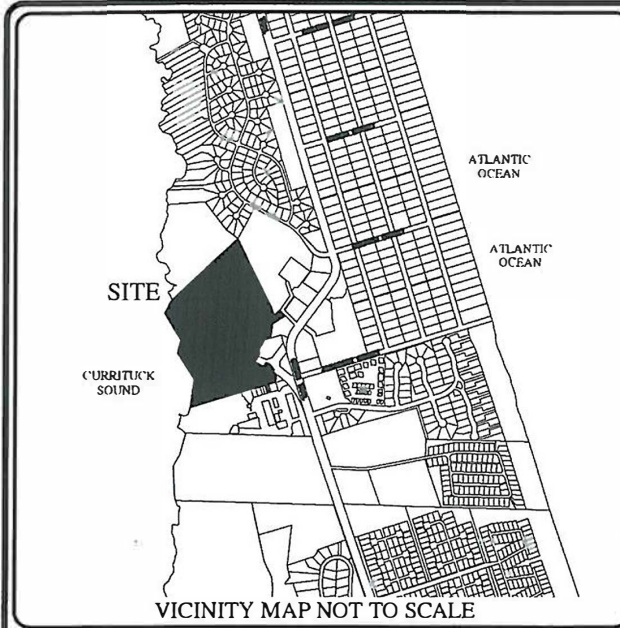
- An evidentiary hearing will be held for the Board of Commissioners to gather competent, material and substantial evidence to establish the facts of the case.
- All testimony is made under oath.
- The applicant or opposing parties shall establish written findings of fact and conclusions of law.
- Parties with standing may participate fully in the evidentiary hearing, including presenting evidence, cross-examining witnesses, objecting to evidence, and making legal arguments.
- Non-parties may present competent, material, and substantial evidence that is not repetitive.

#### Typical Use Permit (Quasi-Judicial) Hearing at BOC

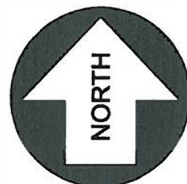
- Swearing in of witnesses/speakers
- Presentation by County Staff on Application
- Required Presentation by Applicant or Authorized Agent (20-25 minutes)
  - Applicant to Present Findings of Fact
- Public Comment Period (typically 3 minutes each)
- Applicant Rebuttal (typically 5 minutes)
- BOC Deliberation & Decision



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| LEGEND      |                                |
|-------------|--------------------------------|
| —           | ROADWAY CENTERLINE             |
| —           | RIGHT-OF-WAY                   |
| —           | PROPERTY BOUNDARY              |
| —           | ADJOINING PROPERTY LINE        |
| ●           | SHORELINE                      |
| □           | EXISTING CONCRETE MONUMENT     |
| □           | SET CONCRETE MONUMENT          |
| ●           | SET IRON ROD                   |
| ○           | EXISTING IRON ROD              |
| ○           | EXISTING IRON PIPE             |
| ○           | PINCHED PIPE                   |
| △           | TELEPHONE PEDestal             |
| ○           | UTILITY POLE                   |
| —           | OVERHEAD UTILITY LINES         |
| —           | GUY WIRE                       |
| ○           | EXISTING SPOT GROUND ELEVATION |
| ○           | WATER METER                    |
| 100         | STREET ADDRESS                 |
| N.T.S.      | NOT TO SCALE                   |
| P.C.        | PLAT CABINET                   |
| D.B.        | DEED BOOK                      |
| SL          | SLIDE                          |
| SF / SQ.FT. | SQUARE FEET                    |
| AC          | ACRES                          |



NC NAD 83 (2011) GRID NORTH

**SURVEYOR'S CERTIFICATION**

I, Michael D. Barr, certify that this plat was drawn under my supervision from an actual survey made under my supervision (deed and description recorded in Books referenced); that the boundaries not surveyed are clearly indicated as drawn from information found in Books referenced; that the ratio of precision or positional accuracy is 1:10,000; and that this map meets the requirements of The Standards of Practice for Land Surveying in North Carolina (21 NCAC 56.1600)."

Witness my original signature, registration number and seal this 19 day of July A.D., 2021.

*Michael D. Barr*  
Professional Land Surveyor L-1756



**BISSELL**  
Professional Group  
Professional Land Surveyors  
and Environmental Specialists  
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| WETLAND LINE TABLE |        |               |
|--------------------|--------|---------------|
| LINE               | LENGTH | BEARING       |
| WL21               | 21.81' | N0° 38' 46"E  |
| WL22               | 32.24' | S81° 25' 55"W |
| WL23               | 22.11' | N81° 48' 34"W |
| WL24               | 5.88'  | N36° 32' 29"W |
| WL25               | 76.88' | N24° 51' 51"W |
| WL26               | 68.61' | S80° 51' 31"W |
| WL27               | 58.29' | N17° 30' 45"E |
| WL28               | 57.15' | N11° 58' 17"E |
| WL29               | 78.15' | N6° 08' 56"W  |
| WL30               | 80.75' | N1° 38' 04"W  |
| WL31               | 62.00' | N4° 57' 00"E  |
| WL32               | 43.95' | N0° 45' 52"W  |
| WL33               | 82.37' | N13° 37' 49"E |
| WL34               | 66.65' | N10° 41' 48"E |
| WL35               | 51.15' | N14° 31' 52"E |
| WL36               | 67.25' | N25° 12' 15"E |
| WL37               | 80.94' | N28° 56' 39"E |
| WL38               | 68.58' | N30° 47' 21"E |
| WL39               | 23.70' | N74° 08' 40"E |
| WL40               | 49.09' | N58° 21' 51"E |
| WL41               | 51.55' | N53° 24' 40"E |
| WL42               | 38.70' | N63° 30' 27"E |
| WL43               | 29.57' | N17° 34' 01"W |
| WL44               | 31.62' | N8° 06' 32"E  |
| WL45               | 61.71' | N21° 36' 32"E |
| WL46               | 32.29' | N61° 29' 00"E |
| WL47               | 22.72' | S62° 26' 45"E |
| WL48               | 22.89' | S27° 48' 44"E |
| WL49               | 46.45' | S78° 06' 10"E |

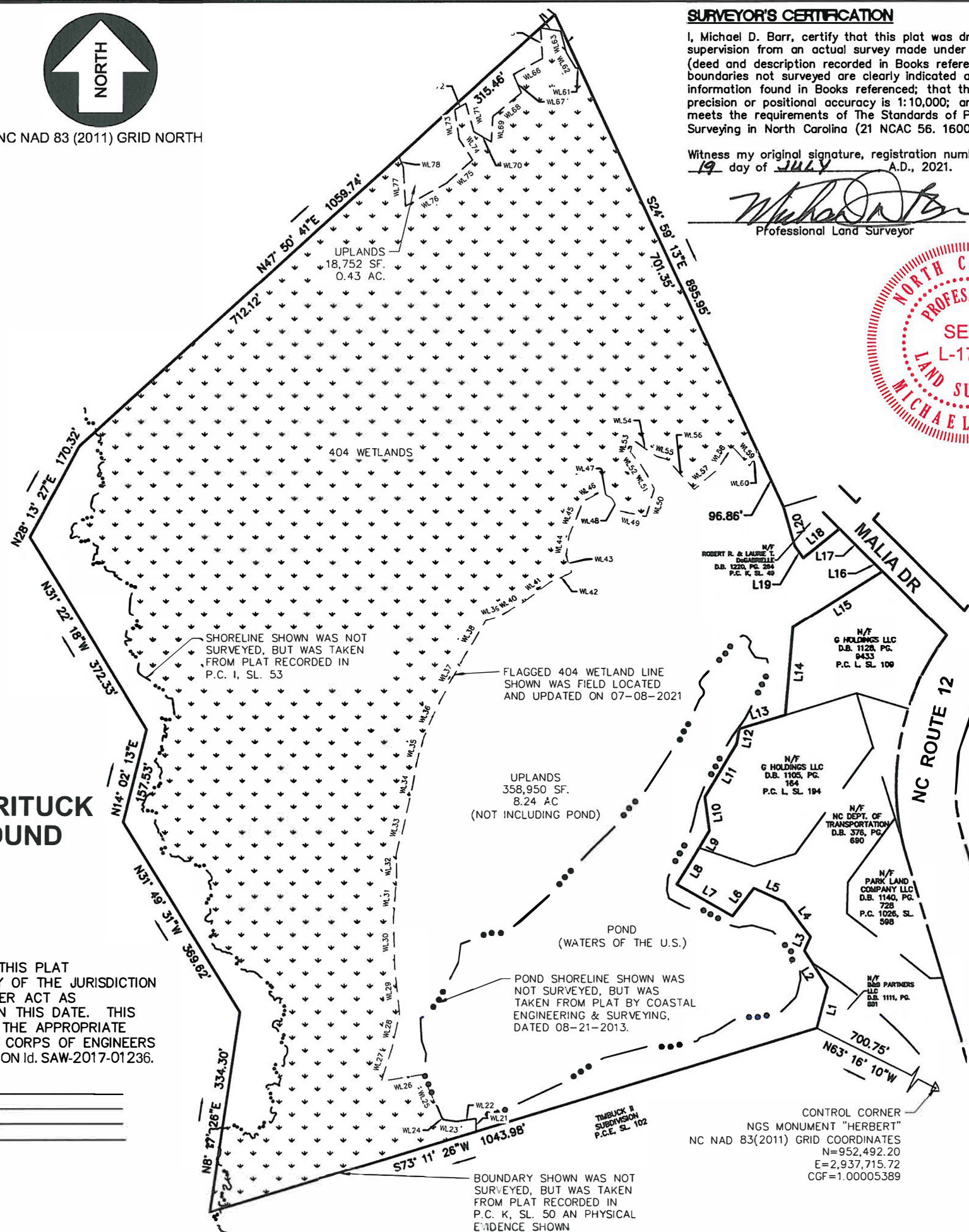
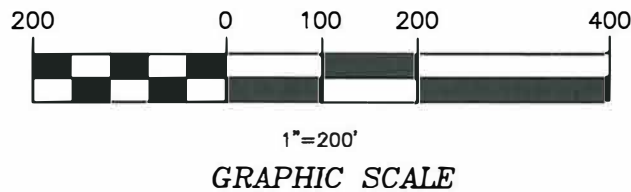
| WETLAND LINE TABLE |        |               |
|--------------------|--------|---------------|
| LINE               | LENGTH | BEARING       |
| WL50               | 46.79' | N17° 05' 18"E |
| WL51               | 37.23' | N28° 19' 01"W |
| WL52               | 36.24' | N36° 46' 28"W |
| WL53               | 19.76' | N13° 59' 55"E |
| WL54               | 47.18' | S54° 42' 20"E |
| WL55               | 18.71' | S73° 18' 11"E |
| WL56               | 64.97' | S41° 04' 55"E |
| WL57               | 51.76' | N51° 02' 32"E |
| WL58               | 44.52' | N32° 57' 19"E |
| WL59               | 42.37' | S41° 50' 18"E |
| WL60               | 25.03' | N35° 01' 51"E |
| WL61               | 7.01'  | N83° 42' 05"W |
| WL62               | 29.81' | N31° 13' 17"W |
| WL63               | 36.85' | N15° 15' 09"W |
| WL64               | 33.42' | N81° 03' 55"W |
| WL65               | 50.82' | S3° 53' 23"E  |
| WL66               | 54.31' | S61° 25' 41"W |
| WL67               | 33.24' | S22° 39' 54"E |
| WL68               | 60.65' | S50° 18' 53"W |
| WL69               | 41.52' | S3° 01' 46"W  |
| WL70               | 34.06' | N60° 23' 49"W |
| WL71               | 48.19' | N15° 50' 11"W |
| WL72               | 21.17' | S60° 18' 12"W |
| WL73               | 48.82' | S8° 59' 03"E  |
| WL74               | 40.95' | S33° 02' 00"E |
| WL75               | 57.29' | S48° 30' 41"W |
| WL76               | 78.28' | S61° 02' 49"W |
| WL77               | 64.44' | N3° 42' 09"W  |
| WL78               | 18.50' | N21° 27' 21"W |

| LINE TABLE |         |               |
|------------|---------|---------------|
| LINE       | LENGTH  | BEARING       |
| L1         | 69.81'  | S14° 36' 25"W |
| L2         | 71.75'  | S32° 26' 53"E |
| L3         | 24.88'  | S24° 21' 21"W |
| L4         | 72.84'  | S36° 53' 44"E |
| L5         | 54.23'  | S65° 23' 21"E |
| L6         | 57.93'  | N37° 34' 20"E |
| L7         | 106.34' | S60° 26' 58"E |
| L8         | 70.75'  | S31° 08' 08"W |
| L9         | 32.69'  | S31° 14' 41"W |
| L10        | 56.14'  | S7° 52' 57"E  |
| L11        | 101.58' | S30° 51' 08"W |
| L12        | 25.83'  | S10° 18' 28"W |
| L13        | 78.87'  | S73° 11' 21"W |
| L14        | 145.59' | S4° 43' 46"W  |
| L15        | 175.00' | S58° 09' 22"W |
| L16        | 30.17'  | S45° 13' 47"E |
| L17        | 70.44'  | S45° 13' 47"E |
| L18        | 76.92'  | N51° 11' 37"E |
| L19        | 25.28'  | S34° 57' 06"E |
| L20        | 67.05'  | S15° 57' 18"E |

**CURRITUCK SOUND**

THIS CERTIFIES THAT THIS COPY OF THIS PLAT ACCURATELY DEPICTS THE BOUNDARY OF THE JURISDICTION OF SECTION 404 OF THE CLEAN WATER ACT AS DETERMINED BY THE UNDERSIGNED ON THIS DATE. THIS DETERMINATION WAS MADE UTILIZING THE APPROPRIATE REGIONAL SUPPLEMENT TO THE 1987 CORPS OF ENGINEERS WETLANDS DELINEATION MANUAL. ACTION ID. SAW-2017-01236.

REGULATORY OFFICIAL \_\_\_\_\_  
TITLE \_\_\_\_\_  
DATE \_\_\_\_\_  
USACE ACTION ID \_\_\_\_\_



PROJECT: **OUTER BANKS VENTURES, INC**  
**PARCEL 10 MONTERAY SHORES P.U.D.**  
POPLAR BRANCH BCH. CURRITUCK COUNTY NORTH CAROLINA  
**404 WETLAND PLAT**

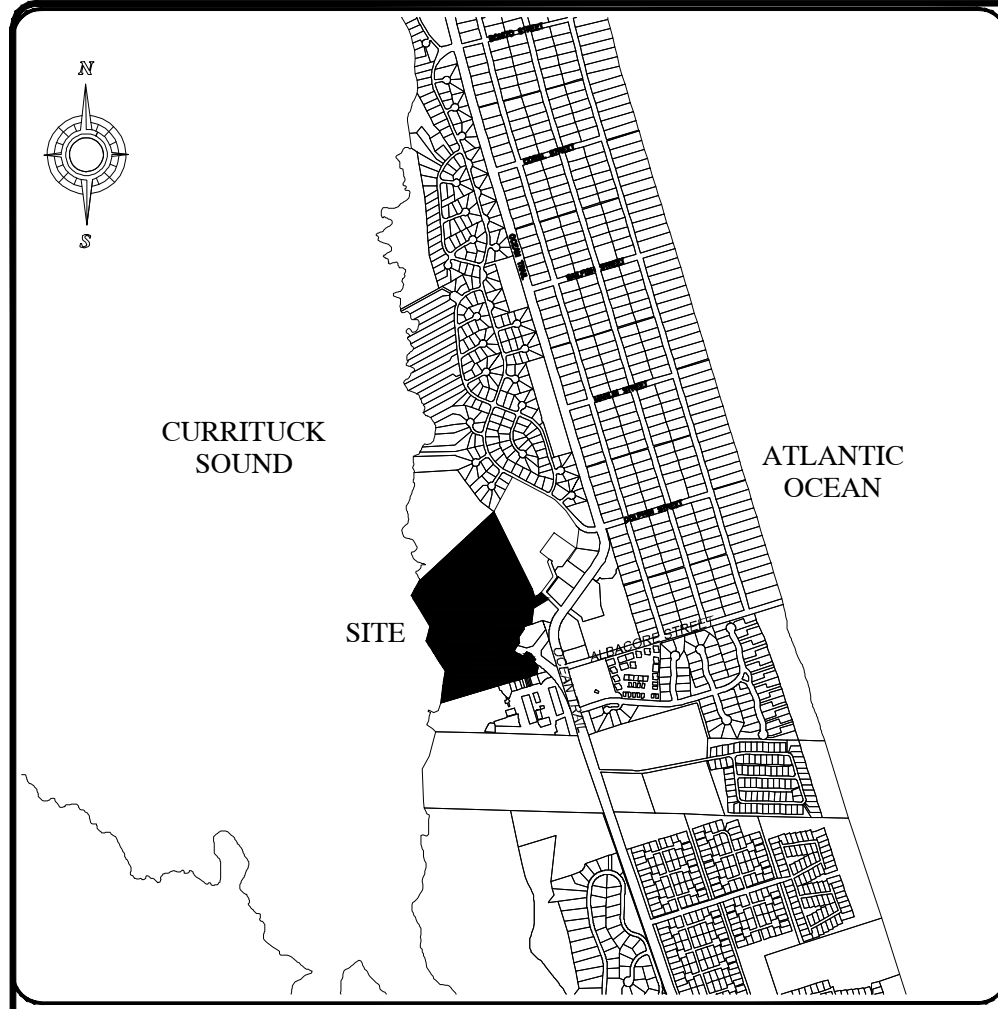
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|-----------|------|-------------|-------------|----|
| NO.       | DATE | DESCRIPTION | BY          |    |
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|           |      |             |             |    |
|           |      |             |             |    |

DATE: 07-19-2021 SCALE: 1"=200'  
DESIGNED: \_\_\_\_\_ CHECKED: \_\_\_\_\_  
DRAWN: \_\_\_\_\_ APPROVED: \_\_\_\_\_  
MDB BPG  
SHEET: 1 of 1  
CAD FILE: 459600WL2  
PROJECT NO: 4596



# AMENDED SKETCH PLANS FOR COROLLA BOAT CLUB

AMENDMENT FOR DEVELOPMENT OF PHASE 10  
POPLAR BRANCH TOWNSHIP CURRITUCK COUNTY NORTH CAROLINA



VICINITY MAP  
SCALE: 1" = 1000'

**GENERAL NOTES:**

- PROJECT NAME: MONTEREY SOUND SHORE
- OWNER/APPLICANT: OUTER BANKS VENTURES, INC.  
P.O. BOX 549  
COROLLA, NC 27927
- PROPERTY DATA:  
PARCEL ID#: 0116-0000-010A-0000, 0116-0000-010B-0000 & 0116-0000-010C-0000  
PRIMARY ADDRESS: MALIA DRIVE, COROLLA, NC  
RECORDED REFERENCES: D.B. 1161, PG. 734; P.C. K, SL. 49; P.C. R, SL. 372  
PROPERTY ZONING: SFO-PUD
- F.I.R.M. DATA:  
ZONES X, AE (3') AND SHADED X PER F.E.M.A. F.I.R.M. MAP NUMBER 3721803200 K,  
EFFECTIVE DATE DECEMBER 21, 2018. USE OF LAND WITHIN A FLOODWAY OR FLOOD PLAIN  
IS SUBSTANTIALLY RESTRICTED BY CHAPTER 7 OF THE CURRITUCK COUNTY UNIFIED  
DEVELOPMENT ORDINANCE.

| Sheet Number | Sheet Title                                    |
|--------------|--|
| 1            | COVER SHEET, DEVELOPMENT NOTES & SITE LOCATION |
| 2            | EXISTING CONDITIONS & SITE FEATURES PLAN       |
| 3            | P.U.D. OVERVIEW & SUMMARY OF USES              |
| 4            | MIXED USE DEVELOPMENT LAYOUT & CONCEPTUAL PLAN |
| 5            | DEVELOPMENT USE OVERVIEW PLAN                  |
| 6            | DEVELOPMENT OPEN SPACE PLAN                    |
| 7            | MIXED USE DEVELOPMENT DUPLEX LOTS PLAN         |
| A403         | RESTAURANT ELEVATIONS                          |
| A404         | RESTAURANT ELEVATIONS                          |
| A403         | CLUBHOUSE ELEVATIONS                           |
| A404         | CLUBHOUSE ELEVATIONS                           |
| A403         | RETAIL ELEVATION                               |
| A404         | RETAIL ELEVATIONS                              |
| A403         | SINGLE STORY DUPLEX ELEVATIONS                 |
| A404         | SINGLE STORY DUPLEX ELEVATIONS                 |
| A403         | TWO STORY DUPLEX ELEVATIONS                    |
| A404         | TWO STORY DUPLEX ELEVATIONS                    |

| LEGEND |                                 |
|--------|---------------------------------|
|        | ROADWAY CENTERLINE              |
|        | RIGHT-OF-WAY                    |
|        | PROPERTY BOUNDARY               |
|        | ADJOINING PROPERTY LINE         |
|        | EXISTING DITCH CENTERLINE       |
|        | EXISTING/PROP DITCH TOP OF BANK |
|        | EXISTING WETLANDS               |
|        | EXISTING 404 BOUNDARY           |
|        | 30' UNDISTURBED BUFFER (COUNTY) |
|        | EXISTING GRADE CONTOUR          |
|        | PROPOSED GRADE CONTOUR          |
|        | EXISTING CULVERT                |
|        | PROPOSED CULVERT                |
|        | PROPOSED SIDEWALK               |

Bissell Professional Group  
3512 North Coxston Highway  
Kitty Hawk, North Carolina 27949  
(252) 261-2026  
Fax: (252) 261-1760

**BISSELL**  
PROFESSIONAL GROUP  
Engineers, Planners, Surveyors  
and Environmental Specialists

COVER SHEET, DEVELOPMENT  
NOTES & SITE LOCATION

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COROLLA BOAT CLUB  
NORTH CAROLINA  
POPLAR BRANCH TOWNSHIP CURRITUCK COUNTY

PRELIMINARY ZERO LOT LINE PLAN

| NO. | DATE     | DESCRIPTION       | BY  |
|-----|----------|-------------------|-----|
| 1   | 8/22/21  | ISSUE FOR PERMITS | BPG |
| 2   | 8/22/21  | UPDATE PERMITS    | BPG |
| 3   | 9/22/21  | ISSUE COMMENTS    | BPG |
| 4   | 9/27/21  | DUPLEX PLAN       | BPG |
| 5   | 11/23/21 | ISSUE COMMENTS    | BPG |
| 6   | 12/03/21 | ISSUE COMMENTS    | BPG |

**PRELIMINARY  
FOR REVIEW  
PURPOSES ONLY**

|             |           |           |     |
|-------------|-----------|-----------|-----|
| DATE:       | 6         | SCALE:    | N/A |
| DESIGNED:   | BPG       | CHECKED:  | MSB |
| DRAWN:      | KFW       | APPROVED: | BPG |
| SHEET:      | 1         | OF        | 7   |
| CAD FILE:   | 459600AS2 |           |     |
| PROJECT NO: | 4596      |           |     |





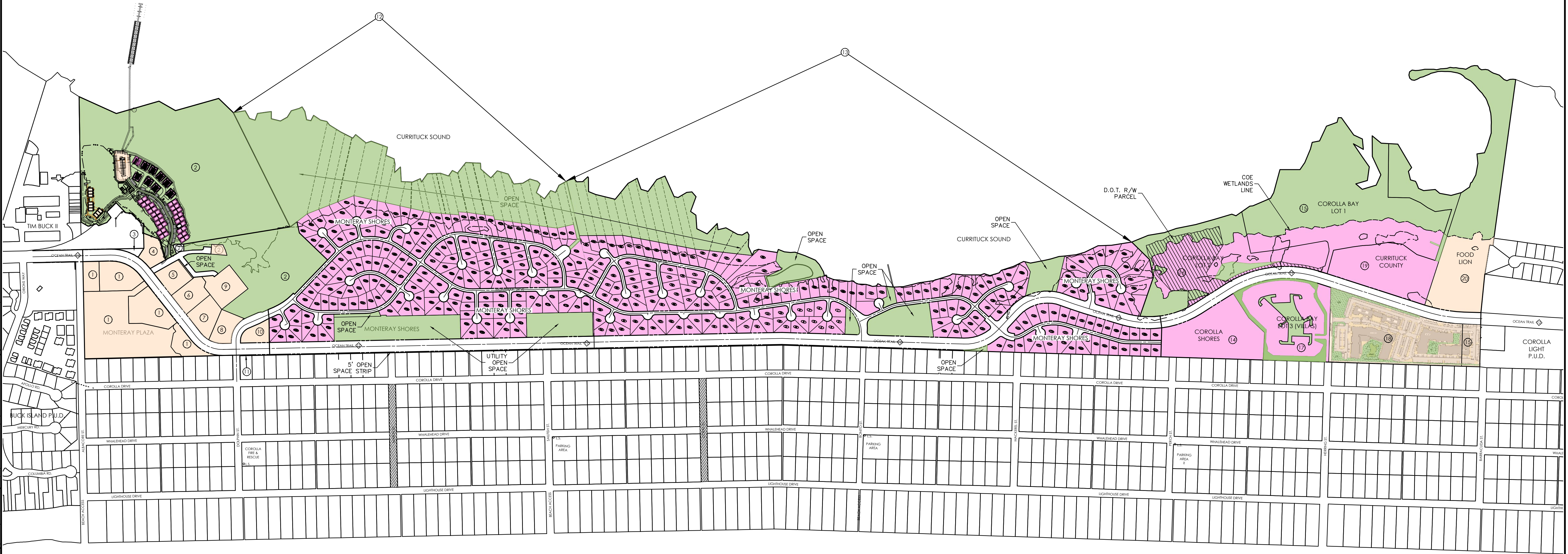


| Map Reference | Designation                      | Total Area        | CAMA Wetlands | Net Area for Development | Commercial | Residential      | Open Space            | References  | Notes; Comments   |
|---------------|----------------------------------|-------------------|---------------|--------------------------|------------|------------------|-----------------------|---|---|
| 1.            | South of Phase 2: Monterey Plaza | 69,630            | 9.0(5)        | 60.63                    | 21,736     | 52 Units         | 46,858                | "P.C. D., SL. 322"  | Original Commercial Tract   |
| 2.            | *Parcels 7, 8, 9, & 10*          | 48,721            | 9.0           | 39,721                   | 12,785     | -                | 46,744                | "P.C.K., SL. 49-51"   | Acreage includes 0.549AC R/W for Malia Drive, (R/W Ac. On plot is incorrect); parcel 10 commercial area from 11/20/09 and 1/22/14 A.S.P.'s; CAMA area unconfirmed.(2021 A.S.P.) Parcel 9 area corrected based on Parcel 5 condo Plat. |
|               | • Parcel 7                       | 0.220 (1)         | -             | 0.220                    | 0.220      | -                | -                     | -   | -   |
|               | • Parcel 8                       | 0.119 (1)         | -             | 0.119                    | -          | -                | -                     | -   | -   |
|               | • Parcel 9                       | 11,639 (1)        | -             | 11,639                   | -          | -                | 11,639                | -   | Parcel 10 commercial per last several A.S.P.'s.   |
|               | • Parcel 10                      | 36,194 (1)        | 9.15          | 27,044                   | 1,208      | 36               | 30,976                | -   | -   |
| 3.            | NCDDT Residential                | 0.165             | -             | 0.165                    | -          | -                | -                     | "D.B. 984, P. 113"  | Now mini-golf parcel  |
| 4.            | Church Parcel 6                  | 0.918             | -             | 0.918                    | 0.918      | 1 Unit           | -                     | "P.C. G, SL. 205"   | -   |
| 5.            | Commercial Lot 4                 | 0.976             | -             | 0.976                    | 0.976      | 1 Unit           | -                     | "P.C. G, SL. 90"  | -   |
| 6.            | Commercial Parcel 3              | 1.114             | -             | 1.114                    | 1.114      | -                | -                     | "P.C. G, SL. 176"   | -   |
| 7.            | Commercial Area 1                | 1.056             | -             | 1.056                    | -          | 2 Units          | -                     | "P.C. E, SL. 136"   | -   |
| 8.            | Commercial Area 2                | 0.995             | -             | 0.995                    | -          | -                | -                     | "P.C. E, SL. 137"   | -   |
| 9.            | Monterey Pines (Parcel 5)        | 2.093             | -             | 2.093                    | 1,712      | 48 Units         | 0.045 (UTILITY STRIP) | "P.C. G, SL. 135";<br>"P.C. I, SL. 133"   | Common areas & amenity areas are not credited as open space; Caroline Court R/W acreage overlaps 0.047 Ac. with Malia Dr. R/W (corrected in table); Parcel 5 area also corrected per Condo Plat.                                      |
| 10.           | Bank Parcel                      | 0.778             | -             | 0.778                    | 0.778      | -                | -                     | "P.C. D, LS. 158"   | -   |
| 11.           | 5' Buffer (across from bank)     | 0.029             | -             | 0.029                    | -          | -                | -                     | -   | Not shown on separate plat  |
| 12.           | Phase 2:                         | 93,008            | 9.4           | 83,608                   | -          | 184 Lots         | 36,542                | "P.C. D, SL. 20-27";<br>"P.C. D, SL. 54-61"   | 34.04 Ac. Open space on plot excludes 2.525 Ac. Well site (was originally included in PH. 1 open space calc.)   |
| 13.           | Phase 1:                         | 103,843           | 3.39          | 100,453                  | 200 Lots   | 19,294           | 31,699                | "P.C. B, SL. 366-371; 377"  | 43.15 Ac. Open space on plot; Ac. Shown in table is net of 16.08 Ac. WW site included in "South of Phase 2" in table) and 2.30 Ac. Well (actually 2.525 Ac. Included in Phase 2). Other open space areas (9 parcels) total 19,294 Ac. |
|               | Phase 3:                         | 88,828            | 3,875         | 84,953                   | 14,46      | 218              | 31,699                | "1.35 Ac. Disappeared from Corolla Bay when recombination plat was filed (P.C. J, SL. 126-127); and 0.1 Ac. Difference in Food Lion plats. 218 residential allocated to Phase 3 on A. S. P. dated approved on 3/15/20 and on 1/22/14 A. S. P. | -   |
| 14.           | Corolla Shores                   | 7.24              | -             | 7.24                     | -          | 19 Lots          | -                     | "P.C. N, SL. 133"   | Excludes any reduction due to NCDOT taking.   |
| 15.           | Corolla Bay 1-4                  | 57,418            | 3,875         | 53,543                   | -          | -                | -                     | "P.C. I, SL. 32-35";<br>"P.C. J, SL. 126-127"   | *CAMA wetlands shown on P.C.Q., SL. 9-11  |
|               | • Section1 (Lot 1)               | 20,590* (2)       | 3,875         | 16,715                   | -          | 30 Lots          | 9,511                 | "P.C. J, SL. 198-200"   | 0.179 Ac. of land area was lost in recombination of Lots 1 & 2 (did not supersede)  |
|               | • 1A (Lot 1)                     | Included in Lot 1 | -             | -                        | -          | 6 Lots           | -                     | "P.C. P., SL. 71"   | 0.018 Ac. Open space from Section 1 omitted from plat   |
| 16.           | Section2 (Lot 2)                 | 12,634* (2)       | - (3)         | -                        | -          | 10+24=34 Lots    | 3,504                 | "P.C. Q, SL. 9-11"  | Acreage includes NCDOT R/W acquired (6.47Ac.) and 1.16 Ac. Lost compared to P.C. J, SL. 126-127   |
|               | Section2A (future dev parcel)    | Included in Lot 2 | - (3)         | -                        | -          | 2 Lots           | -                     | Approved but not yet platted  | 2 Lots from the 26 lots approved within section 2 that were reallocated to Lot 2 prior to DOT taking  |
| 17.           | Lot 3 (Villas)                   | 12,848 *(2)       | - (3)         | -                        | 3,007      | 83 Units (57+26) | 3,818                 | "P.C. L, SL. 173-175";<br>"P.C. L, SL. 171-179";<br>"P.C. M, SL. 160-163"   | Commercial for Lots 3 & 4 shown as 8.36 Ac. on approved plans. Computes as 8.95 Ac.   |
|               | • Lot 3 (Residential)(included)  | - (3)             | -             | -                        | -          | -                | -                     | -   | -   |
| 18.           | Lot 4 (Undev.)                   | 10.00 *(2)        | - (3)         | -                        | 5,353      | 127              | 2,602                 | "P.C. I, SL. 32-35"   | Open space and residential allocation per amended sketch plans for P.U.D. (3/15/2010 & 1/22/2014);  |
| 19.           | Currituck County                 | 10.07             | - (3)         | 10.07                    | -          | -                | 2,809                 | "P.C. H, SL. 221"   | Open space per amended sketch plans for P.U.D. (3/15/2010 & 1/22/2014)  |
| 20.           | Corolla Shores V (Food Lion)     | 14.1              | - (3)         | 14.1                     | 6.1        | -                | 8                     | "P.C. G, SL. 356"   | Commercial & open space allocations are from 1/22/2014 ASP (recorded plat shows 14.0 Ac. +/-, A.S.P. shows 14.1 Ac.; 14.1 used in development calcs.  |
| TOTALS:       |                                  | 355,309           | 25,815        | 329,494                  | 36,222     | 711              | 128,888               |   | 711 total does not consider any loss of lots in Corolla Shores due to N.C.D.O.T. taking.  |

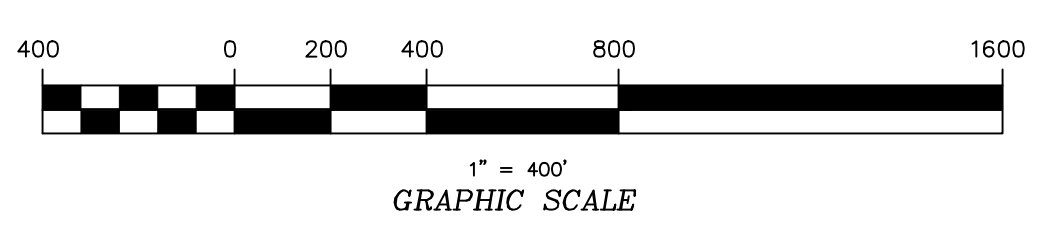
FOOTNOTES:  
 (1) Included in 48,721 Ac. Totals for Parcels 7, 8, 9 & 10.  
 (2) Included in 57,418 Ac. Totals for Corolla Bay 1-4.  
 (3) No CAMA wetland data provided on plats.  
 (4) 43.35 Ac. CAMA in 2006 A.S.P., but not identified by plat.  
 (5) Confirmed CAMA delineation is 9.15 Ac. (2022)

DATA IN TABLE IS BASED UPON LATEST APPROVED AMENDED SKETCH PLAN BY QUILBE & ASSOCIATES WITH LAST REVISION DATED 6/22/23.

|                      | Existing     | Notes                               | This Modification                            | Proposed After Modification |
|----------------------|--------------|-------------------------------------|--|-----------------------------|
| Total area in P.U.D. | 355,309 Ac.  | From Quilbe & Associates '23 A.S.P. | -  | 335,309 Ac.                 |
| CAMA Wetlands        | 43.38 Ac.    | From Quilbe & Associates '23 A.S.P. | -  | 43.38 Ac.                   |
| Net Development Area | 311,929 Ac.  | From Quilbe & Associates '23 A.S.P. | -  | 311,929 Ac.                 |
| Commercial Area      | 36,196 Ac.   | From Quilbe & Associates '23 A.S.P. | -  | 36,196 Ac.                  |
| Commercial %         | 10.195 %     | From Quilbe & Associates '23 A.S.P. | -  | 10.195 %                    |
| Residential Units    | 747          | From Quilbe & Associates '23 A.S.P. | Added 29 Units As Part Of Duplex Development | 766                         |
| Residential Density  | 2,396 DU/Ac. | PUD Residential Density <30DU/Ac.   | -  | 2,456 DU/Ac.                |
| Open Space Area      | 128,888 Ac.  | No Change in Open Space             | -  | 128,888 Ac.                 |
| Open Space %         | 36.27 %      | PUD Open Space > 35 %               | -  | 36.27 %                     |



|             |                     |
|-------------|---------------------|
| RESIDENTIAL | [Pink Shaded Box]   |
| COMMERCIAL  | [Orange Shaded Box] |
| OPEN SPACE  | [Green Shaded Box]  |



**BISSELL PROFESSIONAL GROUP**  
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**MONTEREY SHORES P.U.D.**  
 NORTH CAROLINA  
 CURRITUCK COUNTY  
 POPLAR BRANCH TOWNSHIP

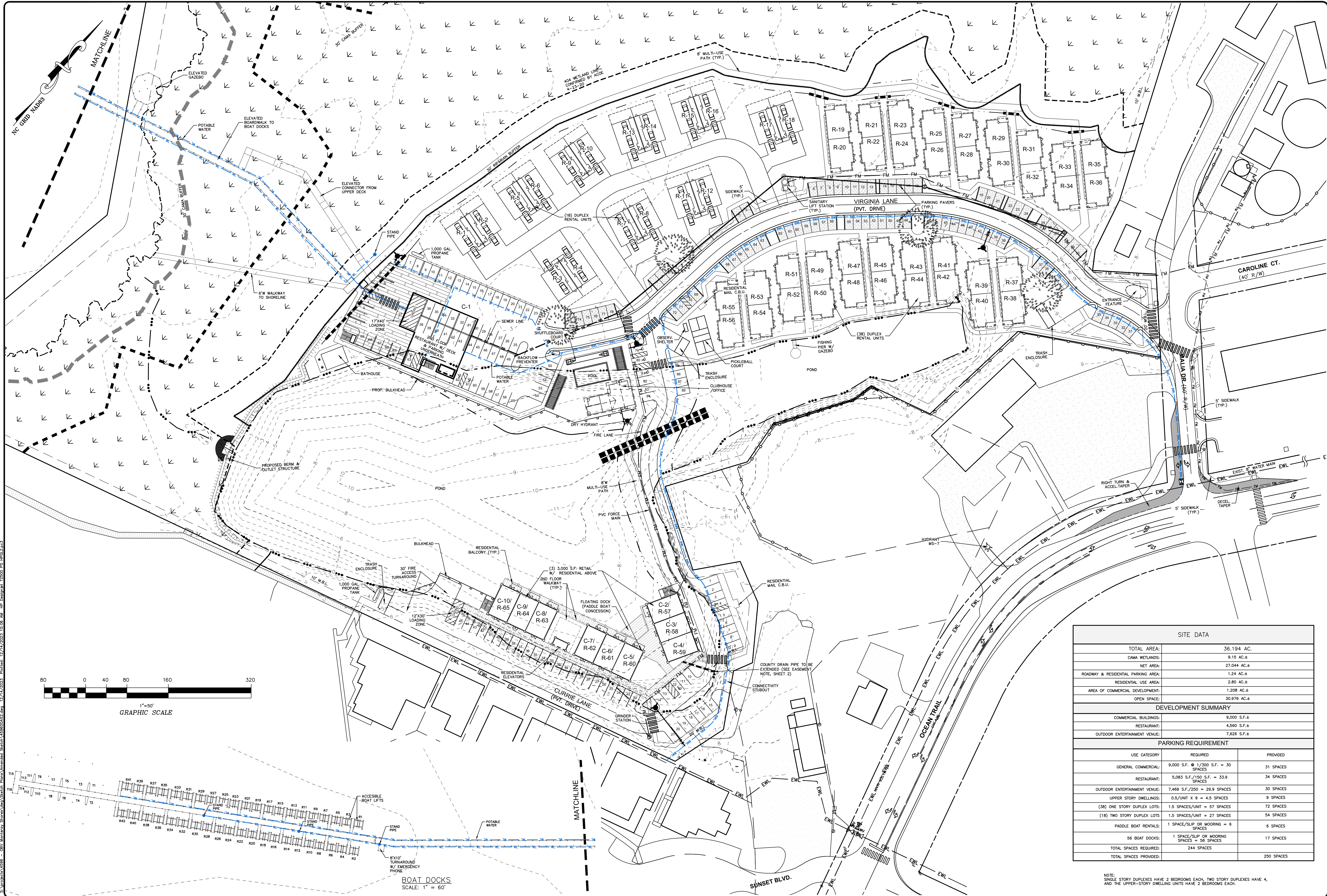
**AMENDED SKETCH PLAN**

| NO. | DATE     | DESCRIPTION                    |
|-----|----------|--------------------------------|
| 1   | 8/22/21  | PRELIMINARY                    |
| 2   | 9/21/23  | REVISED PER S.U.P.             |
| 3   | 12/14/23 | REVISED PER S.U.P.             |
| 4   | 8/22/23  | DATA UPDATED PER DOC. APPROVAL |
| 5   | 8/22/23  | DATA UPDATED PER DOC. APPROVAL |
| 6   | 8/22/23  | DATA UPDATED PER DOC. APPROVAL |
| 7   | 8/22/23  | DATA UPDATED PER DOC. APPROVAL |
| 8   | 8/22/23  | DATA UPDATED PER DOC. APPROVAL |

**PRELIMINARY**  
**DO NOT USE FOR CONSTRUCTION**

DATE: 5/27/21 SCALE: 1" = 400'  
 DESIGNED: BPG CHECKED: MSB  
 DRAWN: KFW APPROVED: BPG  
 SHEET: 3 OF 7  
 CAD FILE: 459600PUD1  
 PROJECT NO: 4596





| REVISIONS |                    |
|-----------|--------------------|
| NO.       | DESCRIPTION        |
| 1         | ISSUED FOR PERMITS |
| 2         | REVISED BOAT DOCKS |
| 3         | REVISED BOAT DOCKS |
| 4         | REVISED BOAT DOCKS |
| 5         | REVISED BOAT DOCKS |
| 6         | REVISED BOAT DOCKS |
| 7         | REVISED BOAT DOCKS |
| 8         | REVISED BOAT DOCKS |

| SITE DATA                           |             |
|-------------------------------------|-------------|
| TOTAL AREA:                         | 36.194 AC.  |
| CAMA WETLANDS:                      | 9.15 AC.±   |
| NET AREA:                           | 27.044 AC.± |
| ROADWAY & RESIDENTIAL PARKING AREA: | 1.24 AC.±   |
| RESIDENTIAL USE AREA:               | 2.80 AC.±   |
| AREA OF COMMERCIAL DEVELOPMENT:     | 1.208 AC.±  |
| OPEN SPACE:                         | 30.976 AC.± |

| DEVELOPMENT SUMMARY          |             |
|------------------------------|-------------|
| COMMERCIAL BUILDINGS:        | 9,000 S.F.± |
| RESTAURANT:                  | 4,560 S.F.± |
| OUTDOOR ENTERTAINMENT VENUE: | 7,626 S.F.± |

| PARKING REQUIREMENT          |  |            |
|------------------------------|--|------------|
| USE CATEGORY                 | REQUIRED                                   | PROVIDED   |
| GENERAL COMMERCIAL:          | 9,000 S.F. @ 1/300 S.F. = 30 SPACES        | 31 SPACES  |
| RESTAURANT:                  | 5,083 S.F./150 S.F. = 33.9 SPACES          | 34 SPACES  |
| OUTDOOR ENTERTAINMENT VENUE: | 7,469 S.F./250 = 29.9 SPACES               | 30 SPACES  |
| UPPER STORY DWELLINGS:       | 0.5/UNIT X 9 = 4.5 SPACES                  | 9 SPACES   |
| (38) ONE STORY DUPLEX LOTS:  | 1.5 SPACES/UNIT = 57 SPACES                | 72 SPACES  |
| (18) TWO STORY DUPLEX LOTS:  | 1.5 SPACES/UNIT = 27 SPACES                | 54 SPACES  |
| PADDLE BOAT RENTALS:         | 1 SPACE/SLIP OR MOORING = 6                | 6 SPACES   |
| 56 BOAT DOCKS:               | 1 SPACE/SLIP OR MOORING SPACES = 56 SPACES | 17 SPACES  |
| TOTAL SPACES REQUIRED:       | 244 SPACES                                 |            |
| TOTAL SPACES PROVIDED:       |  | 250 SPACES |

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 Kitty Hawk, North Carolina 27949  
 (252) 261-2566  
 Fax: (252) 261-7850

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**MIXED USE DEVELOPMENT  
 LAYOUT & CONCEPTUAL PLAN**

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**COROLLA BOAT CLUB**  
 POPULAR BRANCH TOWNSHIP CURRITUCK COUNTY NORTH CAROLINA

**PRELIMINARY ZERO LOT LINE PLAN**

PROJECT NO: 459600AS2  
 SHEET: 4 OF 7  
 DATE: 5-27-21  
 SCALE: 1"=50'  
 DESIGNED BY: BPG  
 CHECKED BY: MSB  
 DRAWN BY: KFW  
 APPROVED BY: BPG

**PRELIMINARY FOR REVIEW PURPOSES ONLY**

CAD FILE: 459600AS2  
 PROJECT NO: 4596

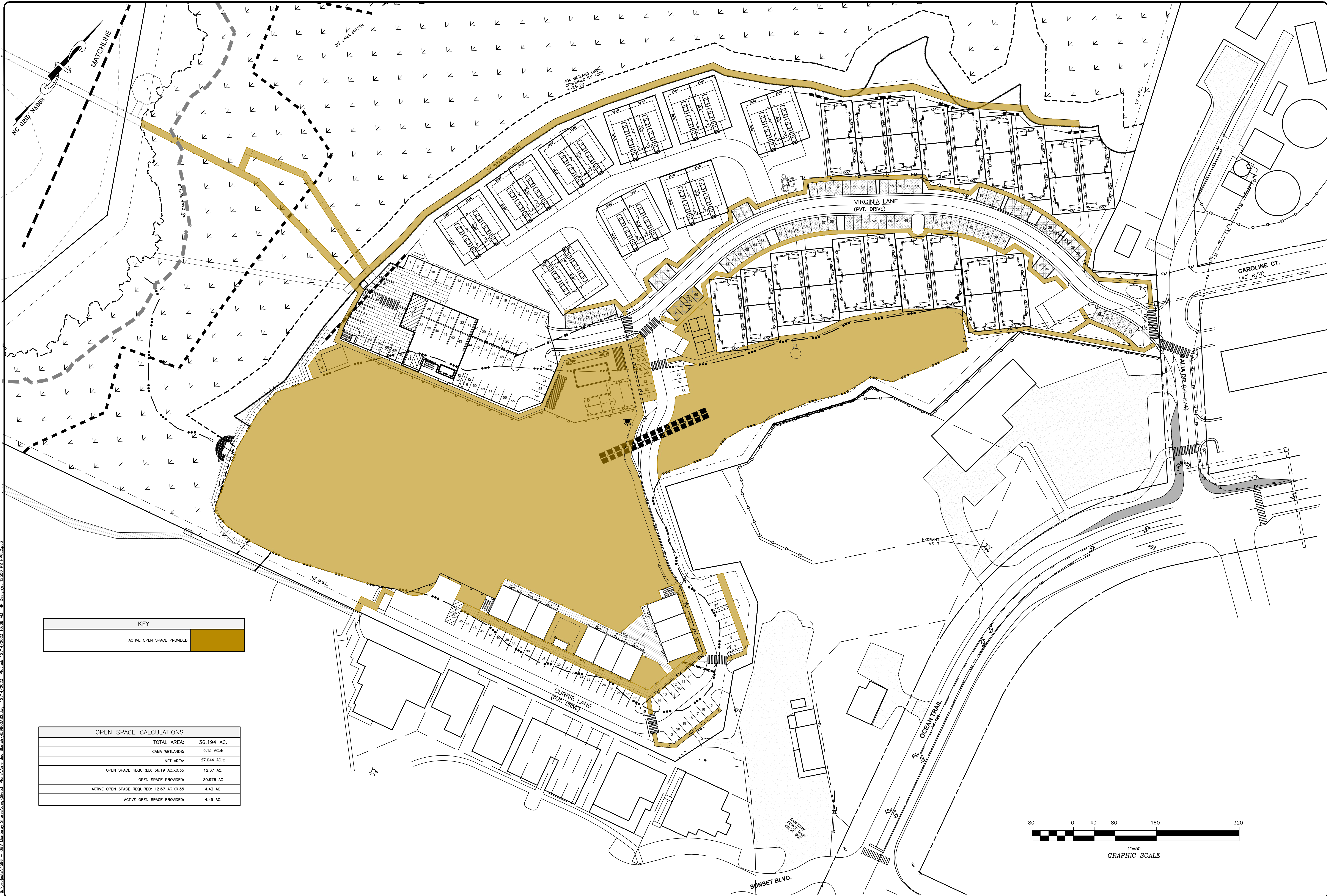
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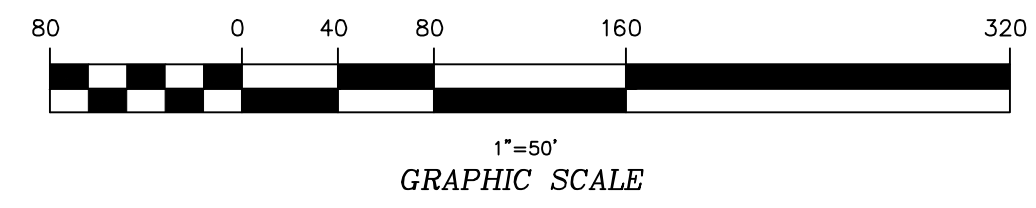


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| KEY |                             |
|-----|-----------------------------|
|     | ACTIVE OPEN SPACE PROVIDED: |

| OPEN SPACE CALCULATIONS                    |             |
|--|-------------|
| TOTAL AREA:                                | 36.194 AC.  |
| CAMA WETLANDS:                             | 9.15 AC.±   |
| NET AREA:                                  | 27.044 AC.± |
| OPEN SPACE REQUIRED: 36.19 AC.X0.35        | 12.67 AC.   |
| OPEN SPACE PROVIDED:                       | 30.978 AC.  |
| ACTIVE OPEN SPACE REQUIRED: 12.67 AC.X0.35 | 4.43 AC.    |
| ACTIVE OPEN SPACE PROVIDED:                | 4.49 AC.    |



| NO. | DATE     | DESCRIPTION        | BY  | CHKD. |
|-----|----------|--------------------|-----|-------|
| 1   | 9/21/23  | ISSUED FOR PERMITS | BPG | MSB   |
| 2   | 12/14/23 | REVISIONS          | KFW | BPG   |

**PRELIMINARY FOR REVIEW PURPOSES ONLY**

|              |           |           |        |
|--------------|-----------|-----------|--------|
| DATE:        | 9/21/23   | SCALE:    | 1"=50' |
| DESIGNED:    | BPG       | CHECKED:  | MSB    |
| DRAWN:       | KFW       | APPROVED: | BPG    |
| SHEET:       | 6 OF 7    |           |        |
| CAD FILE:    | 459600AS2 |           |        |
| PROJECT NO.: | 4596      |           |        |

PROJECT: **COROLLA BOAT CLUB** NORTH CAROLINA  
 POPULAR BRANCH TOWNSHIP CURRITUCK COUNTY

**PRELIMINARY ZERO LOT LINE PLAN**

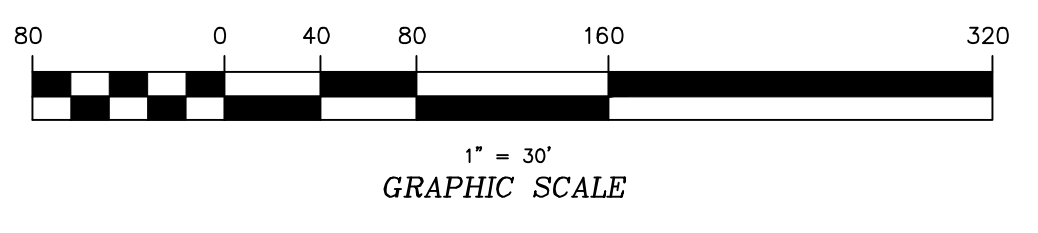
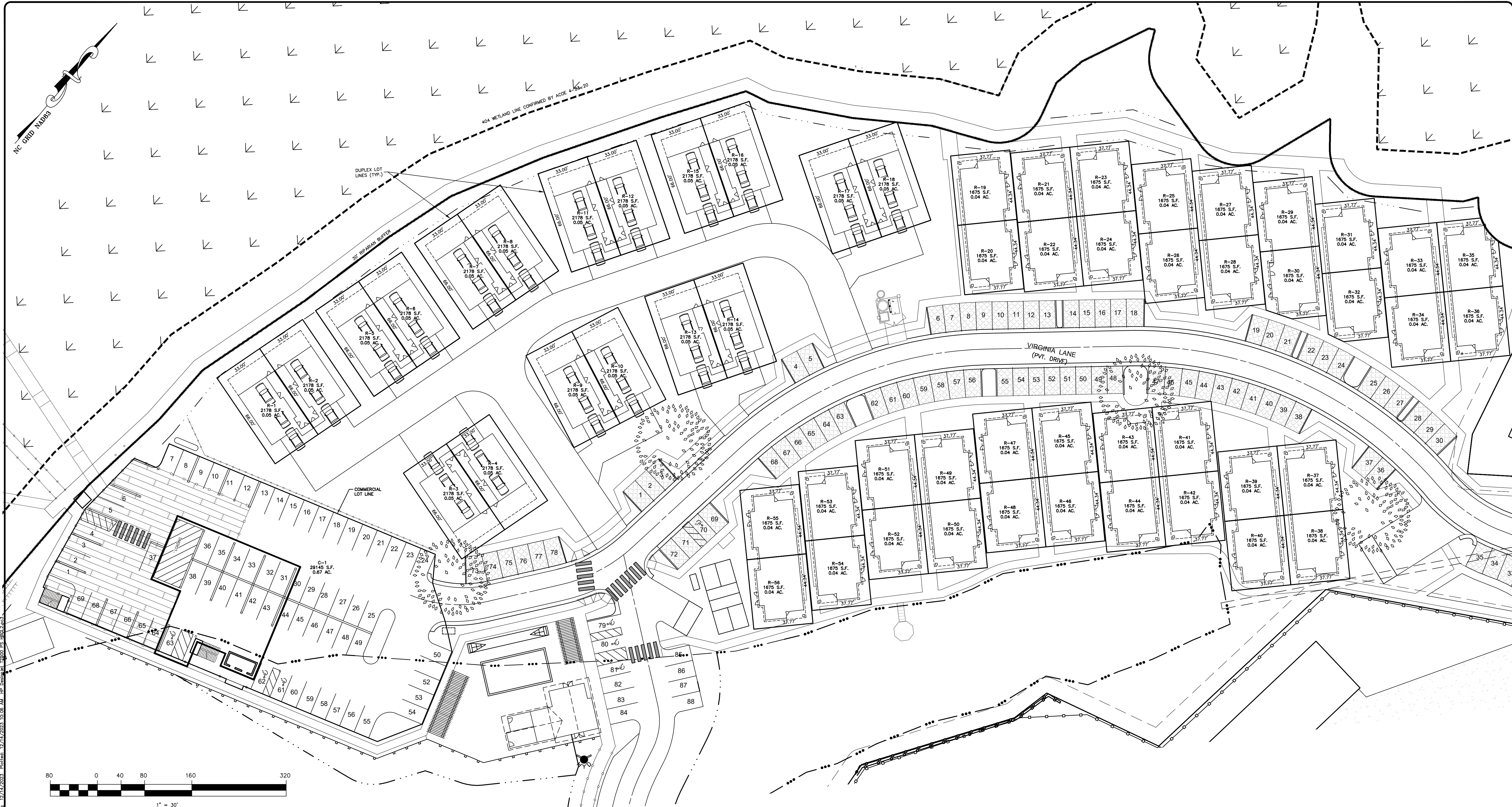
DEVELOPMENT OPEN SPACE PLAN

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MIXED USE DEVELOPMENT  
DUPLEX LOTS PLAN

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COROLLA BOAT CLUB  
PRELIMINARY ZERO LOT LINE PLAN

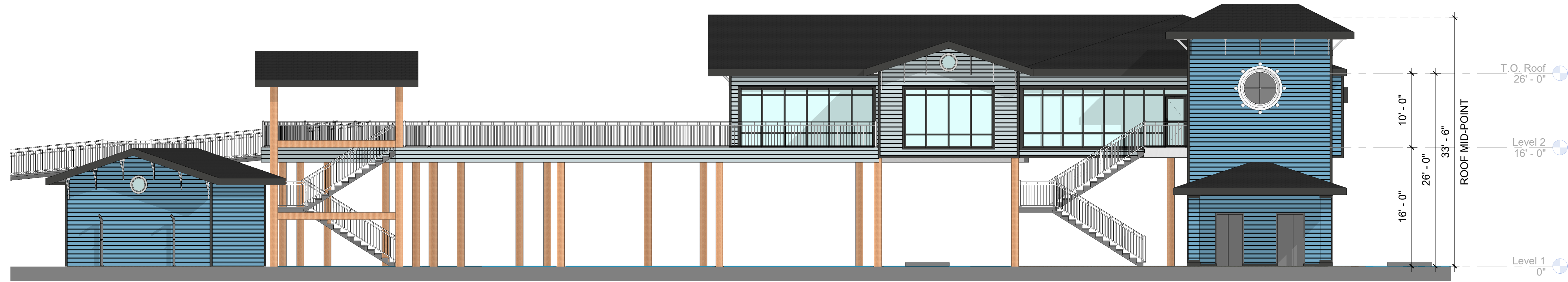
POPULAR BRANCH TOWNSHIP CURRITUCK COUNTY NORTH CAROLINA

| NO. | DATE     | DESCRIPTION | BY | CHK |
|-----|----------|-------------|----|-----|
| 1   | 5/27/21  | ASBUILT     |    |     |
| 2   | 12/14/23 | REVISIONS   |    |     |

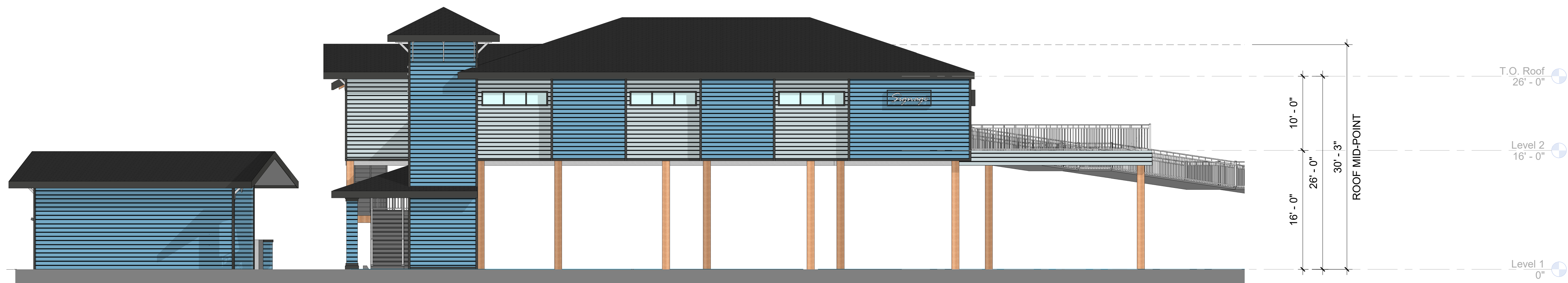
**PRELIMINARY FOR REVIEW PURPOSES ONLY**

|                     |               |
|---------------------|---------------|
| DATE: 5-27-21       | SCALE: 1"=30' |
| DESIGNED: BPG       | CHECKED: MSB  |
| DRAWN: KFW          | APPROVED: BPG |
| SHEET: 7            | OF 7          |
| CAD FILE: 459600AS2 |               |
| PROJECT NO: 4596    |               |





1 North Elevation  
A403 1/8" = 1'-0"



2 East Elevation  
A403 1/8" = 1'-0"

| No. | DESCRIPTION | DATE |
|-----|-------------|------|
|-----|-------------|------|

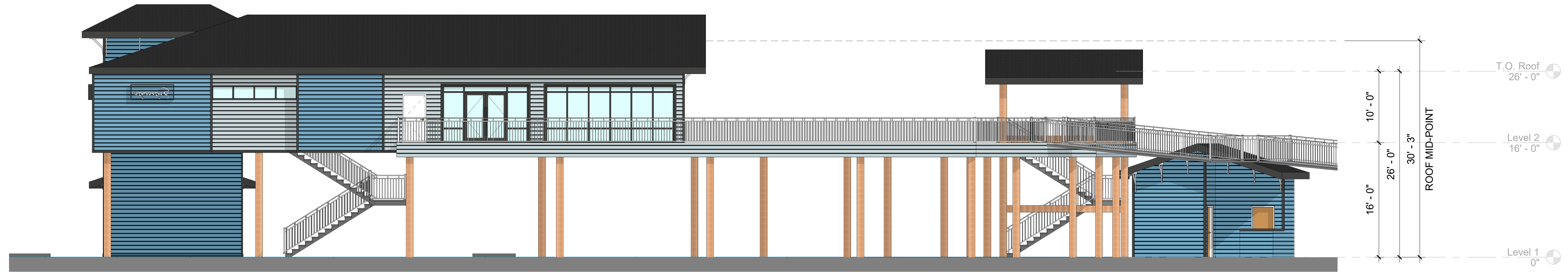
**Restaurant  
Currituck County, NC**

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|   |
|---|
| DRAWN<br>PC                             |
| PROTOTYPE VERSION                       |
| ISSUE DATE<br>11/16/2023                |
| SCALE<br>1/8" = 1'-0"                   |
| JOB NUMBER<br>23-026                    |
| PROJECT STATUS<br>SD                    |
| SHEET NAME<br>Color Building Elevations |
| LOCATION / INN CODE                     |

SHEET NUMBER  
**A403**



1 South Elevation  
A404 1/8" = 1'-0"



2 West Elevation  
A404 1/8" = 1'-0"

| No. | DESCRIPTION | DATE |
|-----|-------------|------|
|-----|-------------|------|

**Restaurant  
Currituck County, NC**

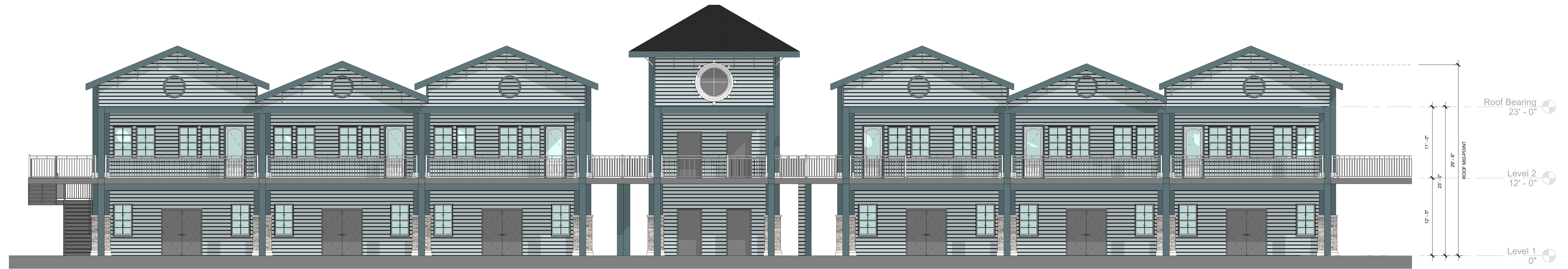
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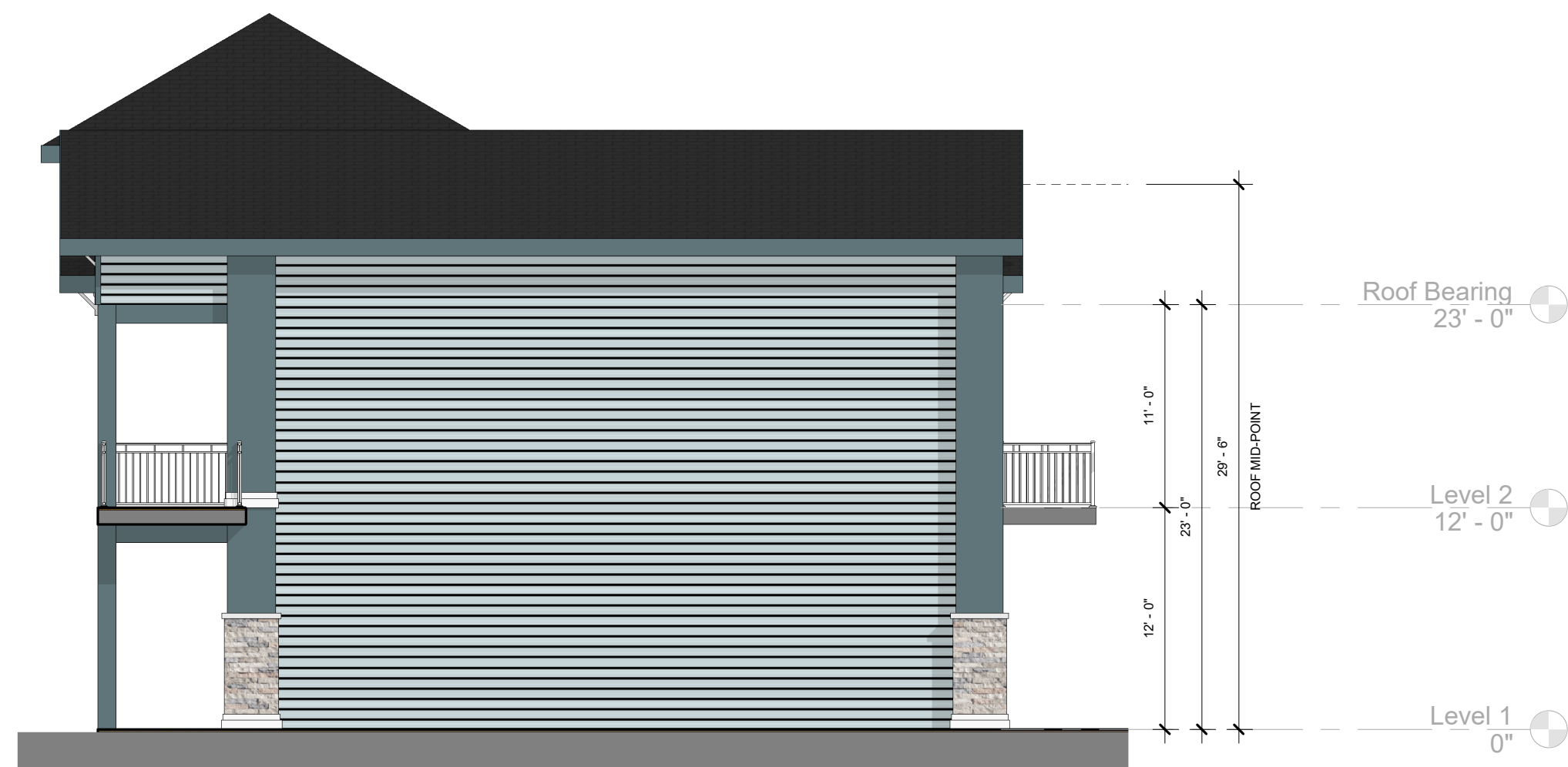
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| PROTOTYPE VERSION   |                           |
| ISSUE DATE          | 11/16/2023                |
| SCALE               | 1/8" = 1'-0"              |
| JOB NUMBER          | 23-026                    |
| PROJECT STATUS      | SD                        |
| SHEET NAME          | Color Building Elevations |
| LOCATION / INN CODE |                           |

SHEET NUMBER  
**A404**

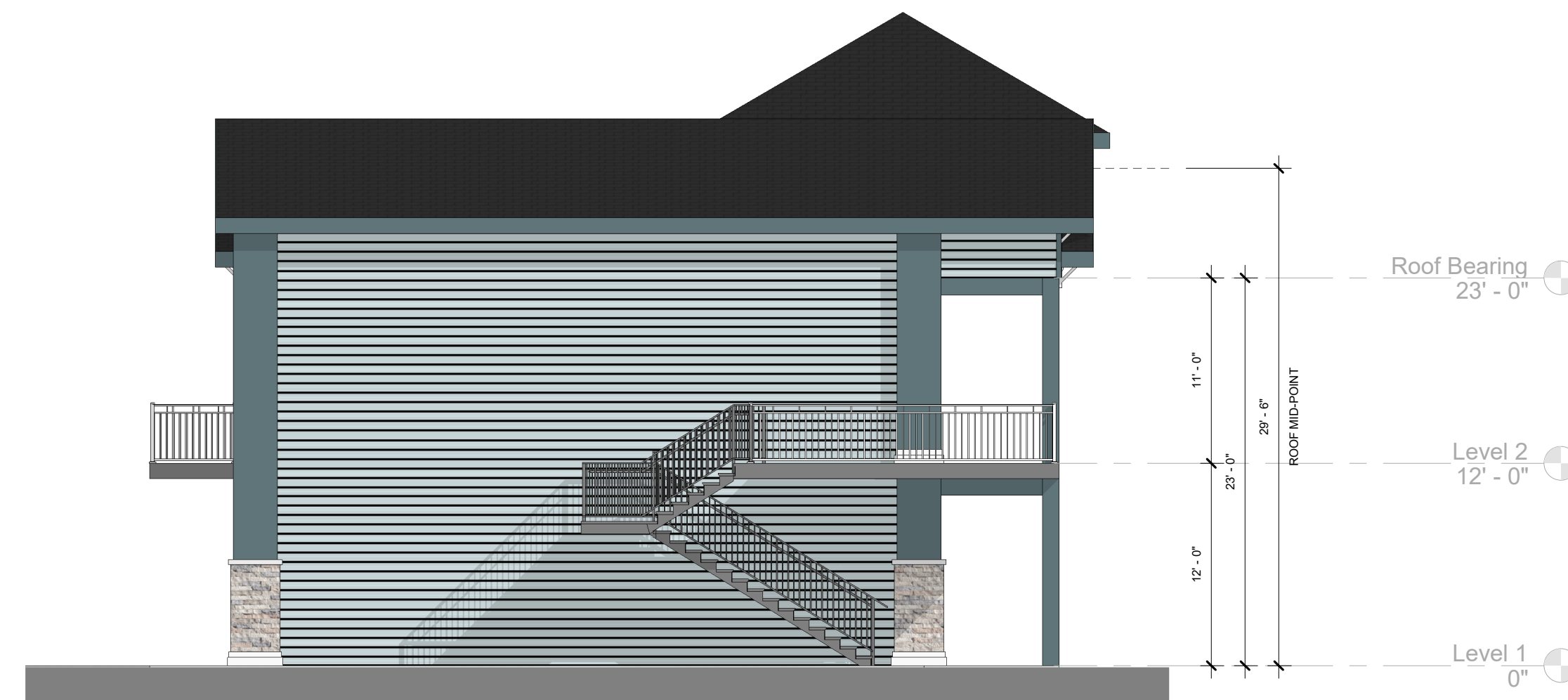




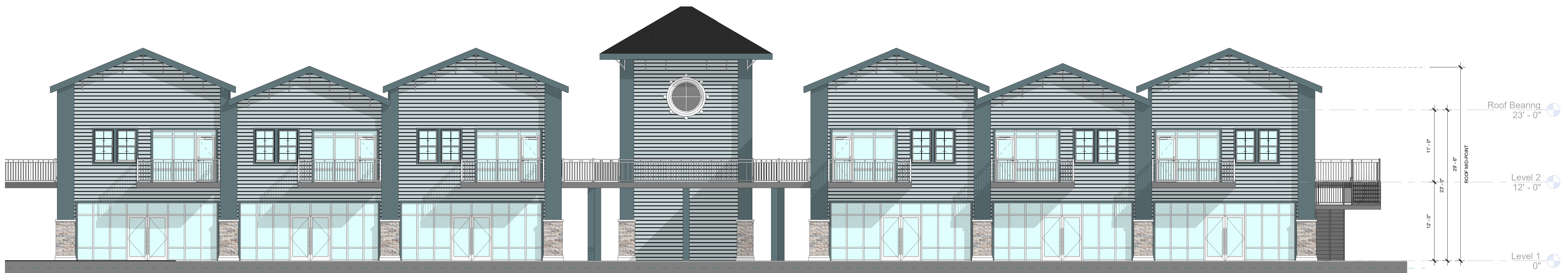
1 North Elevation  
A403 1/8" = 1'-0"



2 East Elevation  
A403 1/8" = 1'-0"



3 West Elevation  
A403 1/8" = 1'-0"



4 South Elevation  
A403 1/8" = 1'-0"

| No. | DESCRIPTION | DATE |
|-----|-------------|------|
|-----|-------------|------|

Retail and Apartments  
Currituck County, NC

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**dllw**  
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542 Douglas Avenue  
Dunedin, FL 34698  
727.736.6000  
www.dlwarchitects.com  
established 1981

DRAWN AS  
PROTOTYPE VERSION

ISSUE DATE  
12/06/2023

SCALE  
1/8" = 1'-0"

JOB NUMBER  
23-026

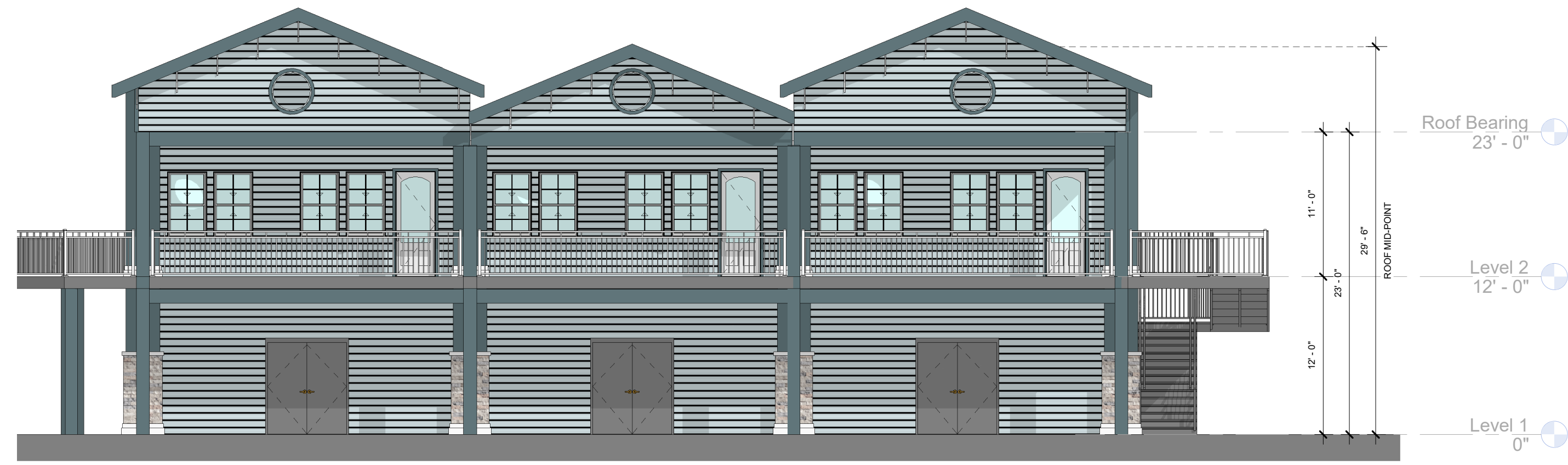
PROJECT STATUS  
SD

SHEET NAME  
Color Building Elevations

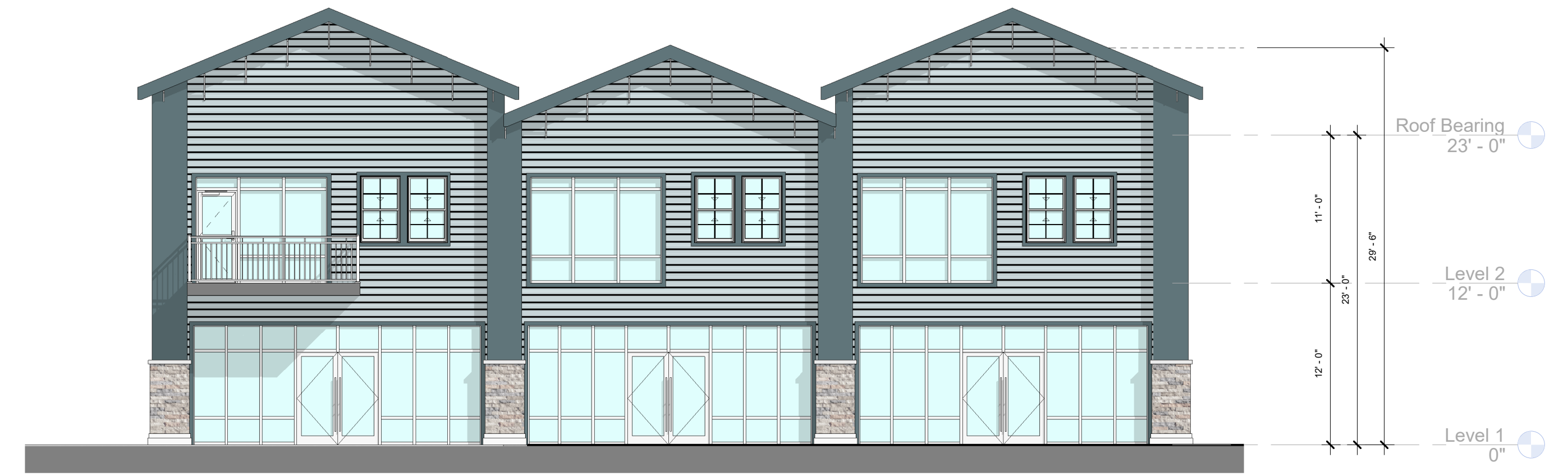
LOCATION / INN CODE

SHEET NUMBER  
**A403**

PLOT DATE: 12/13/2023 12:19:24 PM



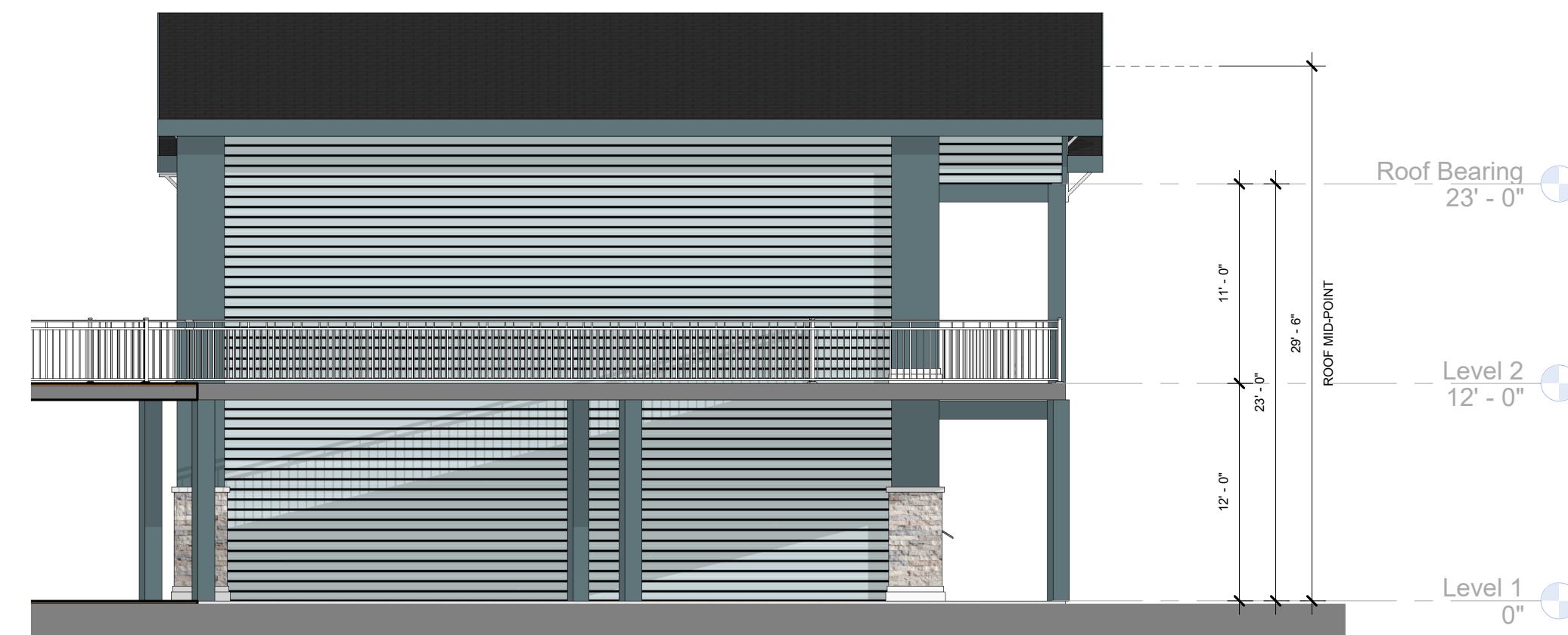
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A404  
North Elevation  
1/8" = 1'-0"



2  
A404  
South Elevation  
1/8" = 1'-0"



3  
A404  
East Elevation  
1/8" = 1'-0"

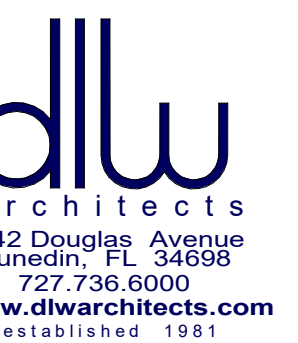


4  
A404  
West Elevation  
1/8" = 1'-0"

| No. | DESCRIPTION | DATE |
|-----|-------------|------|
|-----|-------------|------|

**Retail and Apartments  
Currituck County, NC**

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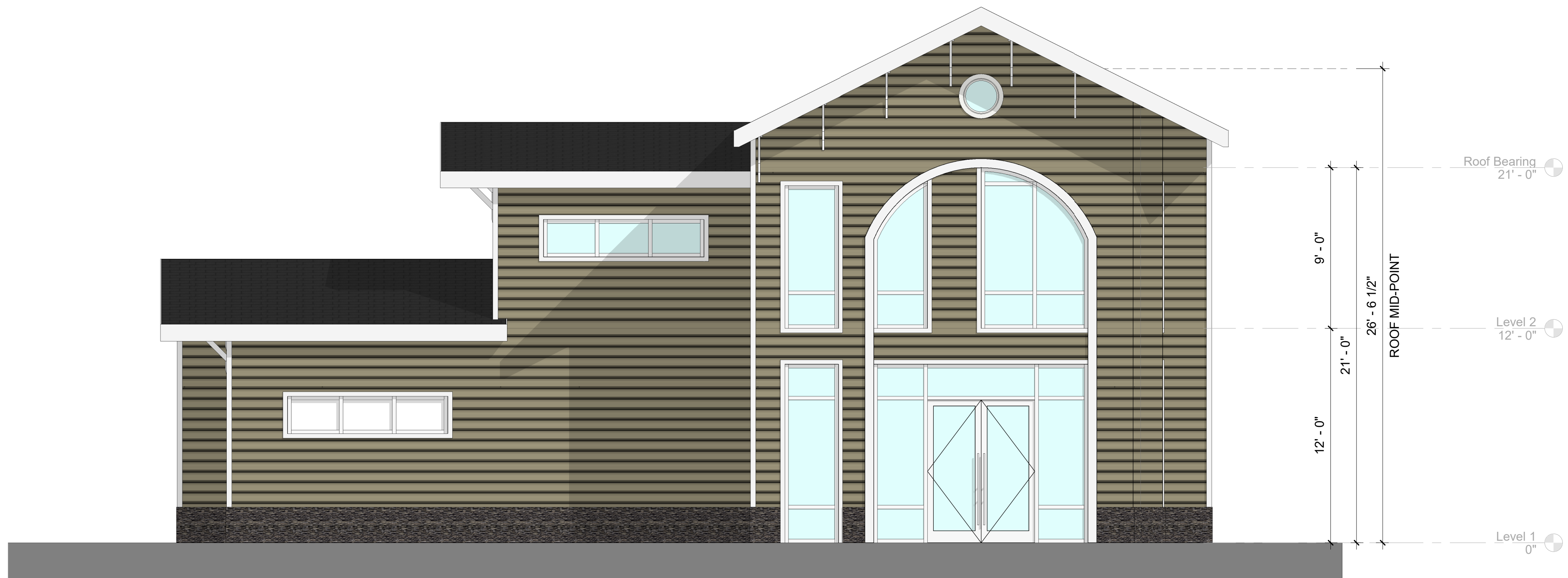
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| SCALE<br>1/8" = 1'-0"                   |
| JOB NUMBER<br>23-026                    |
| PROJECT STATUS<br>SD                    |
| SHEET NAME<br>Color Building Elevations |
| LOCATION / INN CODE                     |

SHEET NUMBER  
**A404**





1 South Elevation  
A403 1/4" = 1'-0"



2 North Elevation  
A403 1/4" = 1'-0"

| No. | DESCRIPTION | DATE |
|-----|-------------|------|
|-----|-------------|------|

**Corolla Boat Club**  
**Clubhouse**  
**Currituck County, NC**

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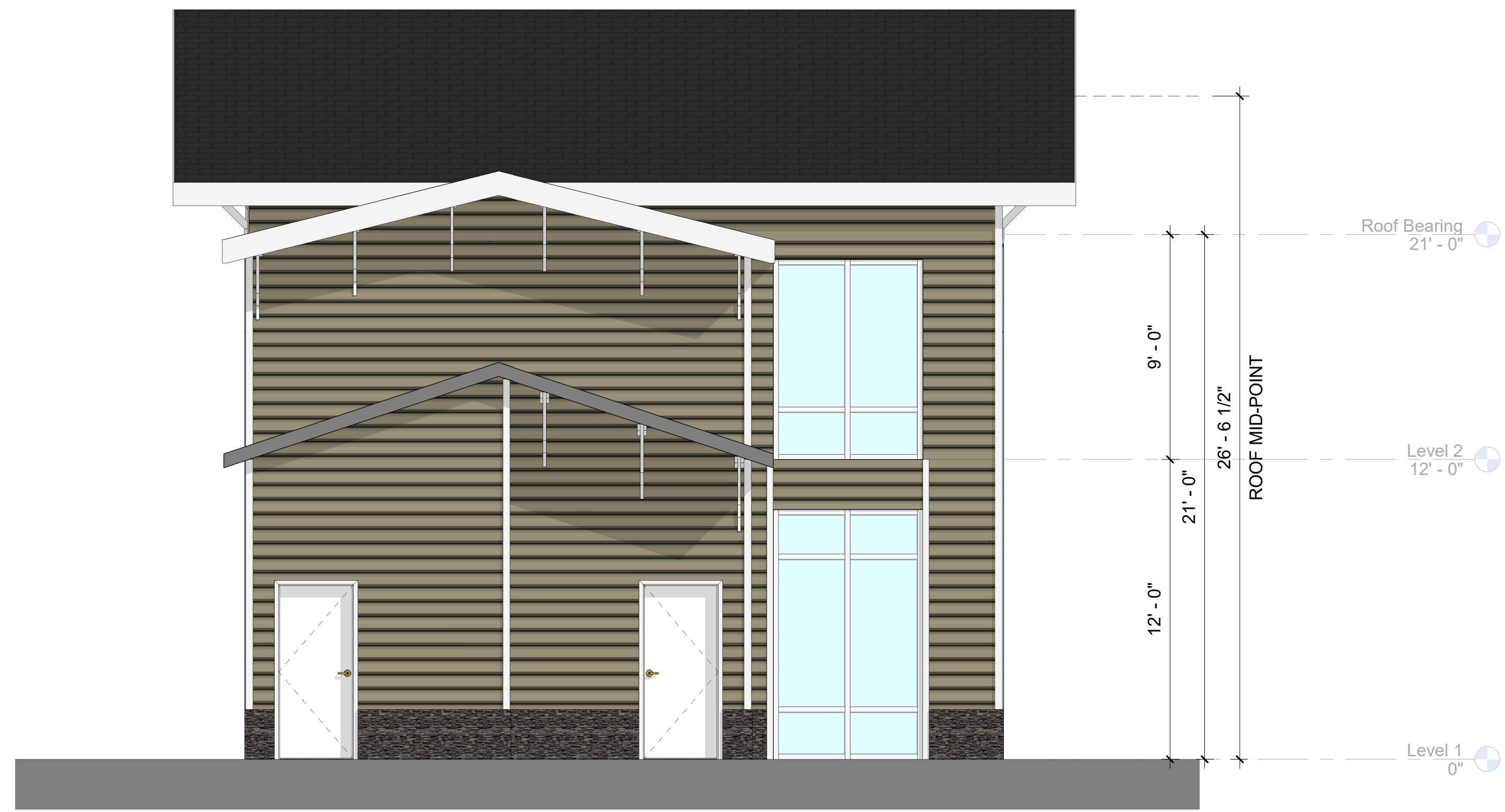
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| SCALE               | 1/4" = 1'-0"              |
| JOB NUMBER          | 23-026                    |
| PROJECT STATUS      | SD                        |
| SHEET NAME          | Color Building Elevations |
| LOCATION / INN CODE |                           |

SHEET NUMBER  
**A403**





1  
A404 **West Elevation**  
1/4" = 1'-0"



2  
A404 **East Elevation**  
1/4" = 1'-0"

| No. | DESCRIPTION | DATE |
|-----|-------------|------|
|-----|-------------|------|

**Corolla Boat Club  
Clubhouse**  
Currituck County, NC

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|---|
| DRAWN<br>PC/AS                          |
| PROTOTYPE VERSION                       |
| ISSUE DATE<br>11/16/2023                |
| SCALE<br>1/4" = 1'-0"                   |
| JOB NUMBER<br>23-026                    |
| PROJECT STATUS<br>SD                    |
| SHEET NAME<br>Color Building Elevations |
| LOCATION / INN CODE                     |
| SHEET NUMBER<br><b>A404</b>             |



1 South Elevation  
A403 1/4" = 1'-0"



2 North Elevation  
A403 1/4" = 1'-0"

PLOT DATE: 11/15/2023 3:08:12 PM

| No. | DESCRIPTION | DATE |
|-----|-------------|------|
|-----|-------------|------|

**Corolla Boat Club**  
**Multi-Story Duplex**  
Currituck County, NC

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|                     |                           |
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| DRAWN               | PC                        |
| PROTOTYPE VERSION   |                           |
| ISSUE DATE          | 11/16/2023                |
| SCALE               | 1/4" = 1'-0"              |
| JOB NUMBER          | 23-026                    |
| PROJECT STATUS      | SD                        |
| SHEET NAME          | Color Building Elevations |
| LOCATION / INN CODE |                           |

SHEET NUMBER  
**A403**





1 West Elevation  
A404 1/4" = 1'-0"



2 East Elevation  
A404 1/4" = 1'-0"

| No. | DESCRIPTION | DATE |
|-----|-------------|------|
|-----|-------------|------|

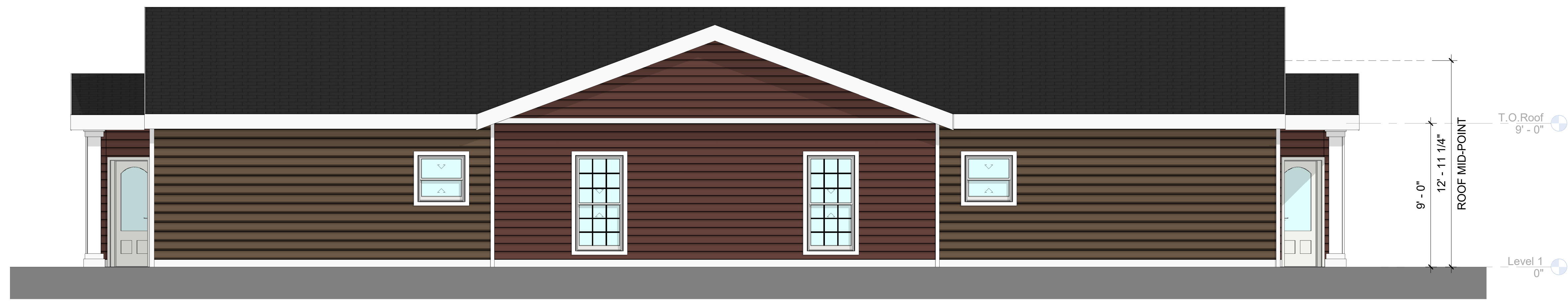
**Corolla Boat Club**  
**Multi-Story Duplex**  
Currituck County, NC

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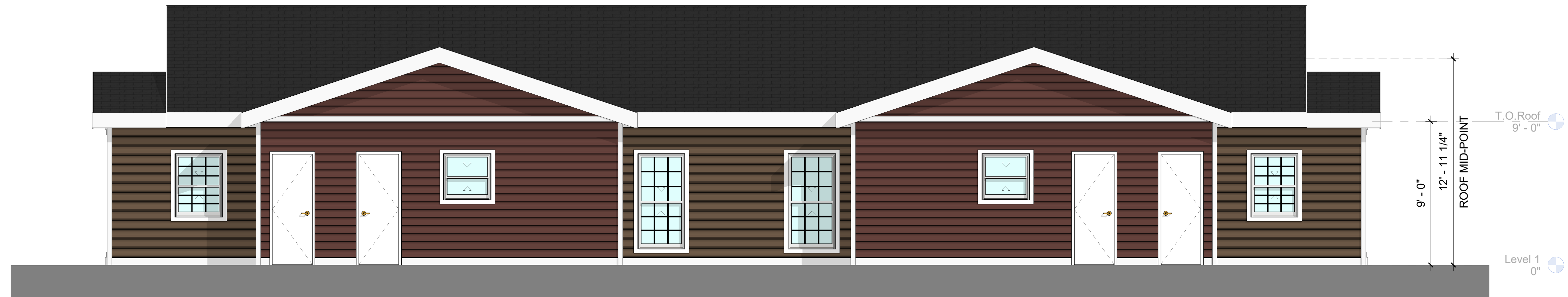


|   |
|---|
| DRAWN<br>Author                         |
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| JOB NUMBER<br>23-026                    |
| PROJECT STATUS<br>SD                    |
| SHEET NAME<br>Color Building Elevations |
| LOCATION / INN CODE                     |

SHEET NUMBER  
**A404**



1 South Elevation  
A403 1/4" = 1'-0"



2 North Elevation  
A403 1/4" = 1'-0"

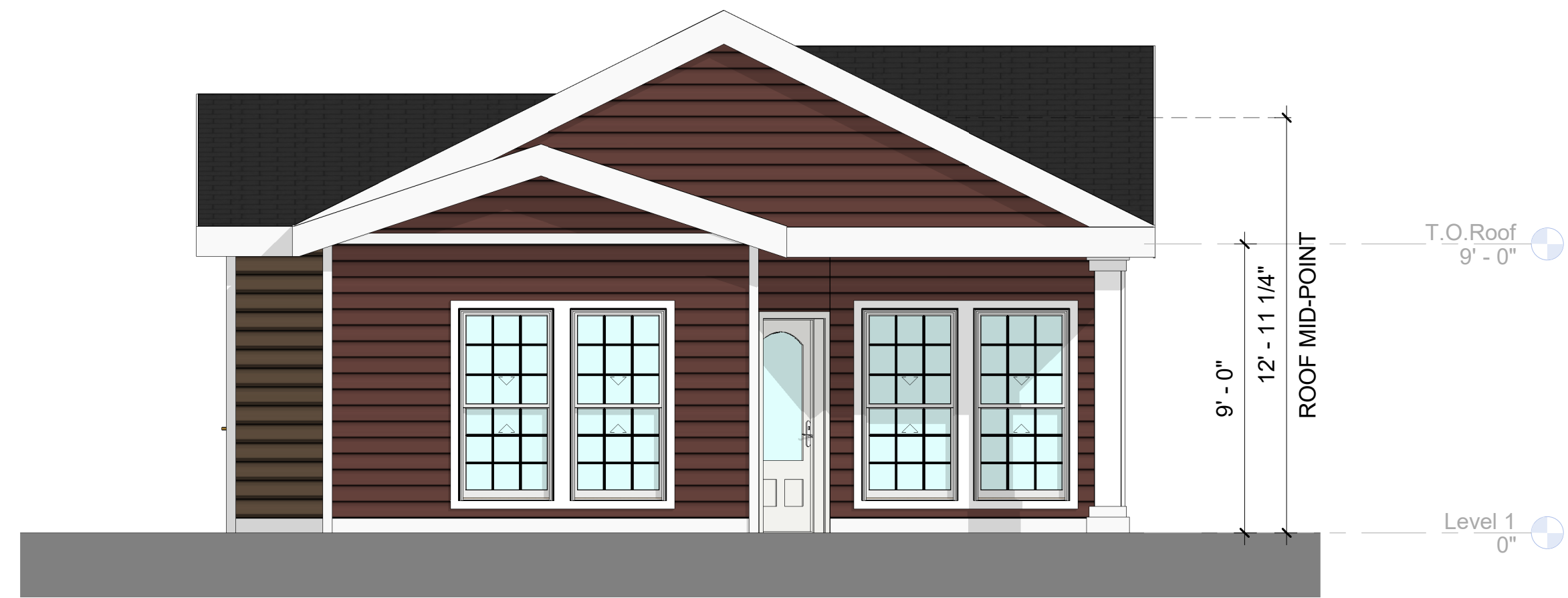
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**Corolla Boat Club**  
**Single-Story Duplex**  
**Currituck County, NC**

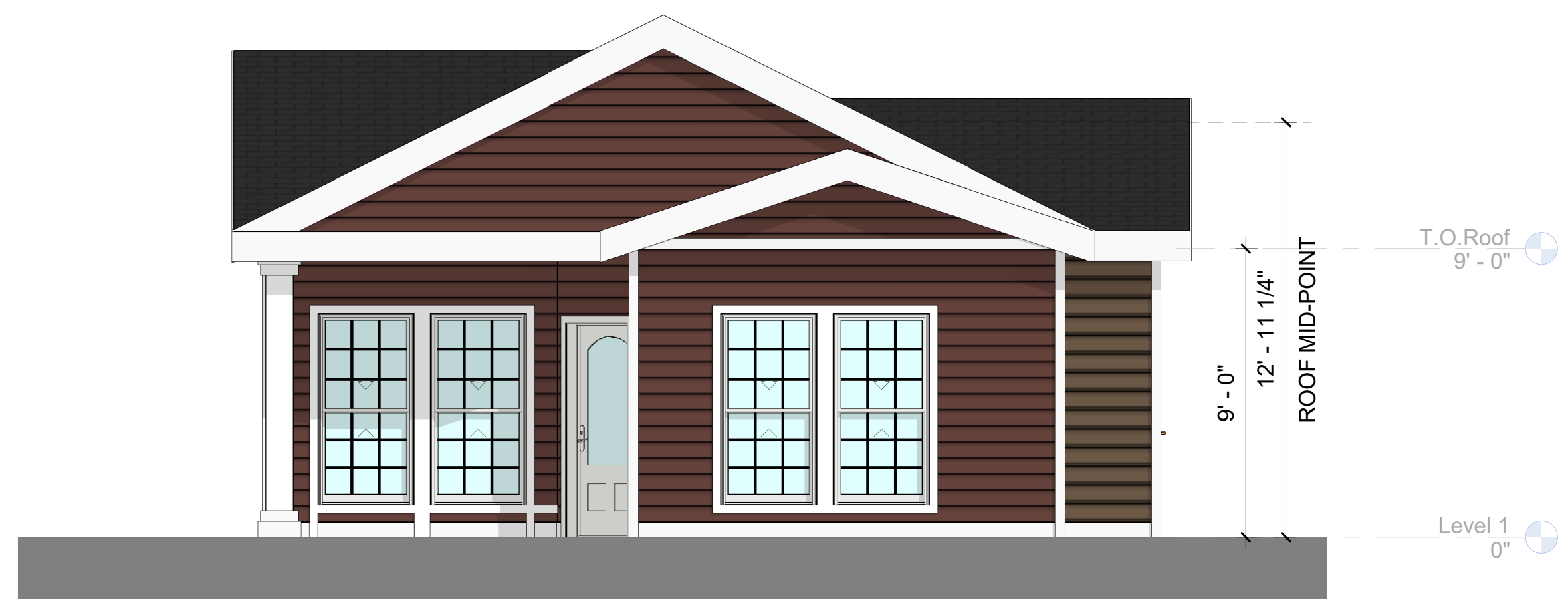
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|                     |                           |
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| DRAWN               | PC/AS                     |
| PROTOTYPE VERSION   |                           |
| ISSUE DATE          | 11/16/2023                |
| SCALE               | 1/4" = 1'-0"              |
| JOB NUMBER          | 23-026                    |
| PROJECT STATUS      | SD                        |
| SHEET NAME          | Color Building Elevations |
| LOCATION / INN CODE |                           |
| SHEET NUMBER        | <b>A403</b>               |



1  
A404 **West Elevation**  
1/4" = 1'-0"



2  
A404 **East Elevation**  
1/4" = 1'-0"

PLOT DATE: 11/15/2023 2:43:15 PM

| No. | DESCRIPTION | DATE |
|-----|-------------|------|
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**Corolla Boat Club**  
**Single-Story Duplex**  
**Currituck County, NC**

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|---------------------|---------------------------|
| DRAWN               | PC/AS                     |
| PROTOTYPE VERSION   |                           |
| ISSUE DATE          | 11/16/2023                |
| SCALE               | 1/4" = 1'-0"              |
| JOB NUMBER          | 23-026                    |
| PROJECT STATUS      | SD                        |
| SHEET NAME          | Color Building Elevations |
| LOCATION / INN CODE |                           |
| SHEET NUMBER        | <b>A404</b>               |





To: Currituck County Planning & Zoning  
153 Courthouse Rd, Suite 110  
Currituck, NC 27929

Date: December 14, 2023

Memorandum

Project #: 34755.00

From: Andrew Topp, PE, PTOE

Re: Corolla Boat Club Parking Demand Assessment – Corolla, NC

## Parking Demand

Monterey Shores includes the construction of a residential and commercial development in Corolla, North Carolina. **Table 1** summarizes the proposed mix of uses within the development and the required parking per the Currituck County Unified Development Ordinance (UDO).

**Table 1: Monterey Shores Land Use Totals and UDO Parking Requirements**

| Land Use             | Amount   | UDO      | Spaces       |
|----------------------|----------|----------|--------------|
| Commercial           | 9,000 sf | 1/300 sf | 30           |
| Restaurant           | 5,083 sf | 1/150 sf | 33.9         |
| Entertainment Venue  | 7,469 sf | 1/250 sf | 29.9         |
| Upper story dwelling | 9 units  | 0.5/unit | 4.5          |
| One story duplex     | 38 units | 1.5/unit | 57           |
| Two story duplex     | 18 units | 1.5/unit | 27           |
| Paddle board rentals | 6 slips  | 1/slip   | 6            |
| Boat docks           | 56 slips | 1/slip   | 56           |
| <b>Total</b>         |          |          | <b>244.3</b> |

The proposed parking on site is a mix of surface spaces within a close, walkable area to their respective uses in congruence with the UDO. Parking will be shared, which allows for greater flexibility among the different uses throughout the day with additional spaces available to accommodate shoppers, residents, and guests.

To determine the hourly parking demand in a shared parking environment, the parking demand for each use per hour was estimated using the latest hourly parking distribution percentages listed in the *Institute of Traffic Engineers* manual. Since parking demand for the boat docks and paddle board rentals could not be determined, 100% occupancy was assumed to be conservative, and Land Use Code 445 was a similar use used to represent parking demand at the entertainment venue since most demand will occur in the evening and begin in the early afternoon. Since parking spaces in the driveways of the 18 duplex rental units and one space per single story units are not shared, those spots were separated.

As shown in **Table 2**, the maximum parking demand occurs in the evening at 8:00 pm, with a total parking demand of 227 spaces, reflecting a 7.7% reduction over the maximum demand per the UDO. This timeframe reflects the hour when the residential, restaurant, and entertainment venue uses are at or near their maximum occupancy with the retail at 70% occupancy while also assuming all boat dock and paddle rental spaces are 100% occupied. If boat trailers associated with the slips were on site, then they would likely be included within the surplus of residential spaces, however no sharing of those spaces were assumed to be conservative.



**Table 2: Shared Parking Demand**

| All Parking Shared Among All Uses |                                  |        |                                   |        |                                   |        |                                |        |                  |        |                      |        |                               |        |                      |        |          |        |       |           |        |       |           |        |       |           |
|-----------------------------------|----------------------------------|--------|-----------------------------------|--------|-----------------------------------|--------|--------------------------------|--------|------------------|--------|----------------------|--------|-------------------------------|--------|----------------------|--------|----------|--------|-------|-----------|--------|-------|-----------|--------|-------|-----------|
| Hour Beginning                    | 18 Duplex Rental Units (ITE 215) |        | 38 1-Story Duplex Units (ITE 215) |        | 38 1-Story Duplex Units (ITE 215) |        | 9 Multi-Family Units (ITE 220) |        | Retail (ITE 822) |        | Restaurant (ITE 931) |        | Entertainment Venue (ITE 445) |        | Marina+ Paddle Board |        | Demand   |        |       | Supply    |        |       | Surplus   |        |       |           |
|                                   | Dedicated                        |        | Dedicated                         |        | Shared                            |        | Shared                         |        | Shared           |        | Shared               |        | Shared                        |        | Shared               |        | Occupied |        |       | Occupied  |        |       | Occupied  |        |       |           |
|                                   | %                                | Spaces | %                                 | Spaces | %                                 | Spaces | %                              | Spaces | %                | Spaces | %                    | Spaces | %                             | Spaces | %                    | Spaces | %        | Spaces | Total | Dedicated | Shared | Total | Dedicated | Shared | Total | Dedicated |
| 12am-4am                          | 100%                             | 27     | 100%                              | 38     | 100%                              | 19     | 97%                            | 5      | -                | -      | -                    | -      | 3%                            | 1      | 100%                 | 62     | 151      | 65     | 86    | 250       | 65     | 185   | 99        | 0      | 99    |           |
| 5am                               | 98%                              | 26     | 98%                               | 38     | 98%                               | 18     | 100%                           | 5      | -                | -      | -                    | -      | -                             | -      | 100%                 | 62     | 149      | 64     | 85    | 250       | 65     | 185   | 101       | 1      | 100   |           |
| 6am                               | 97%                              | 26     | 97%                               | 38     | 97%                               | 17     | 96%                            | 5      | -                | -      | -                    | -      | -                             | -      | 100%                 | 62     | 148      | 64     | 84    | 250       | 65     | 185   | 102       | 1      | 101   |           |
| 7am                               | 85%                              | 23     | 85%                               | 38     | 85%                               | 10     | 85%                            | 4      | -                | -      | -                    | -      | -                             | -      | 100%                 | 62     | 137      | 61     | 76    | 250       | 65     | 185   | 113       | 4      | 109   |           |
| 8am                               | 78%                              | 21     | 78%                               | 38     | 78%                               | 6      | 67%                            | 3      | -                | -      | -                    | -      | -                             | -      | 100%                 | 62     | 130      | 59     | 71    | 250       | 65     | 185   | 120       | 6      | 114   |           |
| 9am                               | 70%                              | 19     | 70%                               | 38     | 70%                               | 2      | 54%                            | 3      | 38%              | 11     | -                    | -      | -                             | -      | 100%                 | 62     | 223      | 57     | 166   | 250       | 65     | 185   | 27        | 8      | 19    |           |
| 10am                              | 70%                              | 19     | 70%                               | 38     | 70%                               | 2      | 48%                            | 2      | 55%              | 17     | -                    | -      | -                             | -      | 100%                 | 62     | 220      | 57     | 163   | 250       | 65     | 185   | 30        | 8      | 22    |           |
| 11am                              | 63%                              | 17     | 63%                               | 36     | 63%                               | 0      | 45%                            | 2      | 66%              | 20     | 20%                  | 7      | -                             | -      | 100%                 | 62     | 180      | 53     | 127   | 250       | 65     | 185   | 70        | 12     | 58    |           |
| 12pm                              | 64%                              | 17     | 64%                               | 36     | 64%                               | 0      | 45%                            | 2      | 85%              | 26     | 40%                  | 14     | 19%                           | 6      | 100%                 | 62     | 163      | 53     | 110   | 250       | 65     | 185   | 87        | 12     | 75    |           |
| 1pm                               | 64%                              | 17     | 64%                               | 36     | 64%                               | 0      | 42%                            | 2      | 100%             | 30     | 62%                  | 21     | 26%                           | 8      | 100%                 | 62     | 176      | 53     | 123   | 250       | 65     | 185   | 74        | 12     | 62    |           |
| 2pm                               | 59%                              | 16     | 59%                               | 34     | 59%                               | 0      | 42%                            | 2      | 96%              | 29     | 69%                  | 23     | 62%                           | 19     | 100%                 | 62     | 185      | 50     | 135   | 250       | 65     | 185   | 65        | 15     | 50    |           |
| 3pm                               | 60%                              | 16     | 60%                               | 34     | 60%                               | 0      | 47%                            | 2      | 79%              | 24     | 49%                  | 17     | 62%                           | 19     | 100%                 | 62     | 174      | 50     | 124   | 250       | 65     | 185   | 76        | 15     | 61    |           |
| 4pm                               | 66%                              | 18     | 66%                               | 38     | 66%                               | 0      | 49%                            | 2      | 66%              | 20     | 66%                  | 22     | 52%                           | 16     | 100%                 | 62     | 178      | 56     | 122   | 250       | 65     | 185   | 72        | 9      | 63    |           |
| 5pm                               | 70%                              | 19     | 70%                               | 38     | 70%                               | 2      | 56%                            | 3      | 64%              | 19     | 79%                  | 27     | 78%                           | 24     | 100%                 | 62     | 194      | 57     | 137   | 250       | 65     | 185   | 56        | 8      | 48    |           |
| 6pm                               | 81%                              | 22     | 81%                               | 38     | 81%                               | 8      | 64%                            | 3      | 67%              | 20     | 85%                  | 29     | 80%                           | 25     | 100%                 | 62     | 207      | 60     | 147   | 250       | 65     | 185   | 43        | 5      | 38    |           |
| 7pm                               | 84%                              | 23     | 84%                               | 38     | 84%                               | 10     | 72%                            | 4      | 70%              | 21     | 99%                  | 34     | 100%                          | 31     | 100%                 | 62     | 223      | 61     | 162   | 250       | 65     | 185   | 27        | 4      | 23    |           |
| 8pm                               | 92%                              | 25     | 92%                               | 38     | 92%                               | 14     | 77%                            | 4      | 70%              | 21     | 99%                  | 34     | 95%                           | 29     | 100%                 | 62     | 227      | 63     | 164   | 250       | 65     | 185   | 23        | 2      | 21    |           |
| 9pm                               | 96%                              | 26     | 96%                               | 38     | 96%                               | 17     | 85%                            | 4      | 51%              | 15     | 100%                 | 34     | 95%                           | 29     | 100%                 | 62     | 225      | 64     | 161   | 250       | 65     | 185   | 25        | 1      | 24    |           |
| 10pm                              | 97%                              | 26     | 97%                               | 38     | 97%                               | 17     | 92%                            | 5      | -                | -      | 88%                  | 30     | 44%                           | 14     | 100%                 | 62     | 192      | 64     | 128   | 250       | 65     | 185   | 58        | 1      | 57    |           |
| 11pm                              | 99%                              | 27     | 99%                               | 38     | 99%                               | 18     | 95%                            | 5      | -                | -      | -                    | -      | 44%                           | 14     | 100%                 | 62     | 164      | 65     | 99    | 250       | 65     | 185   | 86        | 0      | 86    |           |

Shared parking supply excludes the dedicated residential driveway spaces with hourly supply and demand shown in **Table 2**. At the 8:00 pm peak, 21 spaces are still expected to be available within the shared parking inventory on site, with shared residential parking at or near 100% occupancy and assuming 100% occupancy of boat dock and paddle board rental spaces. With the retail, restaurant, and other shared parking available and within walking distance of one another, shared parking is expected to be available at all hours including the evening peak.

The 250 proposed spaces are more than the 244 spaces specified in the UDO. Additionally, when you consider the impact of hourly occupancy use differences, based on the shared and walkable nature of the development, the proposed spaces will adequately serve the site during its busiest hours throughout the day with a projected 21 or more surplus spaces within the shared parking inventory.



12/14/2023