



**CURRITUCK COUNTY  
NORTH CAROLINA**

June 15, 2020  
Minutes – Regular Meeting of the Board of Commissioners

**WORK SESSION**

**1. 4:00 PM Currituck Station**

The Board of Commissioners attended a 4 PM work session in the Sanderlin Auditorium of the Cooperative Education Center, 120 Community Way, Barco, North Carolina. The work session was joined by Mel Price and Peter Johnson of Work Program Architects (WPA), the firm who developed the pattern book for Currituck Station, a 3,000 acre planned, mixed-use development and employment center located in Moyock.

Ms. Price began with a brief company overview and presented the zoning sectors that make Currituck Station and the process used by WPA to develop the design standards Pattern Book. Using a powerpoint, Ms. Price described the look of building designs, public spaces and transportation elements within Currituck Station. Ms. Price responded to questions related to development of mega-site planned communities.

Donna Voliva, Assistant Planning Director, briefly reviewed the text amendment language associated with Currituck Station, initiated and developed based on citizen feedback derived from the 2012 Moyock Small Area Plan. The feasibility study, market analysis, land identification uses and development outside of Currituck Station were reviewed.

Staff reported the purpose of the mega-site, potential commercial and residential development, and rezoning processes for the area. The Work Session concluded at 5:04 PM.

**6:00 PM CALL TO ORDER**

The Currituck County Board of Commissioners met at 6:00 PM for a regular meeting in the Sanderlin Auditorium at the Currituck Cooperative Extension Center, 120 Community Way, Barco, North Carolina.

Attendee Name	Title	Status	Arrived
Bob White	Chairman	Present	
Mike H. Payment	Vice Chairman	Present	
Paul M. Beaumont	Commissioner	Present	
J. Owen Etheridge	Commissioner	Present	
Mary "Kitty" Etheridge	Commissioner	Present	
Selina S. Jarvis	Commissioner	Present	
Kevin E. McCord	Commissioner	Present	

Chairman White called the meeting to order.

**A) Moment of Silence & Pledge of Allegiance**

Commissioner Paul Beaumont offered the Invocation and led the Pledge of Allegiance.

**B) Approval of Agenda**

Chairman White amended the agenda to add a Closed Session pursuant to G.S. 143-318.11(a)(3) to preserve the attorney-client privilege in the matters entitled Currituck County v. Letendre and Currituck County v. Costa and Paradise Homes. Two budget amendments were added to Consent Agenda to provide insurance funds for Lower Currituck Volunteer Fire Department.

Commissioner Payment moved for approval of the agenda. Commissioner Jarvis seconded. The motion carried and the agenda was approved as amended.

Approved agenda:

**Work Session**

4:00 PM Currituck Station

**6:00 PM Call to Order**

A) Moment of Silence & Pledge of Allegiance

B) Approval of Agenda

**Public Comment**

*Please limit comments to matters other than those appearing on this agenda as a Public Hearing. Public comments are limited to 3 minutes.*

**Commissioner's Report**

**County Manager's**

**Report**

**Administrative**

**Reports**

A) Maritime Museum Exhibits Presentation

B) Stormwater Drainage Analysis for 500-Year Storm  
Event

**Public Hearings**

- A) **Public Hearing and Possible Action on Annual Budget for the Fiscal Year Ending June 30, 2021.** Required Public Hearing for possible adoption following presentation of the proposed budget at the June 1, 2020, Board of Commissioners Meeting.
- B) **Public Hearing & Possible Action to Authorize an Economic Development Incentive for The Cotton Gin, Jarvisburg, NC**
- C) **PB 20-09 Boswood Estates, Phase 1 & 2: Request for a preliminary plat/use permit for a 14 lot traditional development located at the 3800 block of Caratoke Highway, Maple, Parcel ID 0060000053B0000, Crawford Township.**
- D) **PB 20-07 Currituck County Alternative Water Supply for Fire Flow Text Amendment:** Request to amend the Unified Development Ordinance Chapter 6, Section 6.2.4 to allow use of water shuttling as an alternative means of meeting fire flow water supply requirements for lands not serviced by the county water system and to amend references in Chapters 4, 5, and 6 from "Fire Marshal" to "Fire Code Official".
- E) **PB 19-25 Currituck County - Currituck Station:** A request to amend the Unified Development Ordinance, Chapter 1. General Provisions, Chapter 2. Administration, Chapter 3. Zoning Districts, Chapter 4. Use Standards, Chapter 5. Development Standards, Chapter 6. Subdivision Infrastructure Standards, and Chapter 10. Definitions and Measurements for the purpose of implementing the Moyock Mega Site master plan (Currituck Station) and establishing the Planned Development - Currituck Station district and regulations.

**New Business**

- A) **Consideration of Facility Use-Naval Contractor "Jump" Operations at Currituck County Airport**
- B) **Consent Agenda**
1. Approval Of Minutes for June 1, 2020
  2. Independent Mailing Systems Lease Agreement-Mail Processing Equipment
  3. Amended-Budget Amendments

**Special Meeting of the Tourism Development Authority**

Public Hearing and Possible Adoption of the Tourism Development Authority Budget for Fiscal Year Ending June 30, 2021

**Adjourn****Special Meeting of the Ocean Sands Water & Sewer District Board**

Public Hearing and Possible Adoption of the Ocean Sands Water & Sewer District Budget for Fiscal Year Ending June 30, 2021

**Adjourn****Closed Session**

Amended-Closed Session Pursuant to G.S. 143-318.11(a)(3) to consult with the County Attorney and preserve the attorney-client privilege in the matters entitled Currituck County v. Letendre; and Currituck County v. Cossa and Paradise Homes

**Adjourn**

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Mike H. Payment, Vice Chairman
<b>SECONDER:</b>	Selina S. Jarvis, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

**PUBLIC COMMENT**

***Please limit comments to matters other than those appearing on this agenda as a Public Hearing. Public comments are limited to 3 minutes.***

Chairman White opened the Public Comment period.

Andy Deel, of Deel Engineering, and Dave Klebitz, Engineer with Bissell Professional Group, each commented on the drainage study presentation item on the meeting agenda and said the study would not resolve the county's issues with stormwater. Both suggested a more comprehensive study and analysis for stormwater management in the County.

No others wished to speak and the Public Comment period was closed.

**COMMISSIONER'S REPORT**

Chairman White discussed the Board's decision to move forward with the annual Independence Day celebration and its importance. He announced the Board's earlier work session to discuss Currituck Station and encouraged the public to attend the Board's work sessions.

Commissioner Payment reiterated the importance of celebrating the July 4th holiday and as Commissioner serving on the Albemarle Regional Health Services Board, had discussed

the event with them. He provided an update on Covid cases in the County and encouraged everyone to be careful and practice recommended safety protocols. Commissioner Payment encouraged citizens to get involved with their local volunteer fire departments.

Commissioner Mary Etheridge announced the distribution dates for Knotts Island and Corolla citizens to receive gift cards through Operation Love Thy Neighbor and thanked all who donated to the program.

Commissioner Beaumont reported his attendance at Moyock Volunteer Fire Department's reveal of a new fire apparatus. He said he attended the Currituck County High School graduation ceremony and congratulated the graduates. He acknowledged Principal Matney and the Board of Education for putting together the ceremony during the pandemic.

Commissioner McCord also attended the reveal of the new fire apparatus at Moyock Volunteer Fire Department. He discussed public attendance at the County's upcoming July 4th fireworks and recognized the Board of Education for holding the graduation ceremony. He encouraged everyone to stay safe on the roads with the increase in weekend traffic.

Commissioner J. Owen Etheridge also addressed the County's July 4th celebration as a reminder of the freedoms we have as citizens. He discussed the High School graduation ceremony and appreciated hearing the student addresses and the opportunity provided to them to reclaim some normalcy during this time.

Commissioner Jarvis also discussed the gift card distribution through the Operation Love Thy Neighbor program. She thanked all who donated and encouraged people to continue to do so in the hope of having another distribution during the summer. Commissioner Jarvis quoted the Declaration of Independence and spoke of holding to its principals to overcome the challenges we are facing.

## **COUNTY MANAGER'S REPORT**

County Manager, Ben Stikeleather, discussed the County's decision to cancel youth baseball, softball and T-ball in response to citizen inquiries received by the county. He explained the cancellations were due to a lack of interest, as there were not enough participants registered to form leagues.

## **ADMINISTRATIVE REPORTS**

### **A. Maritime Museum Exhibits Presentation**

Michelle Perry, County Engineer, gave Commissioners an update on the construction progress for the Maritime Museum in Corolla. Ms. Perry introduced Mark Tolliver of Riggs Ward Design, exhibit designer for the Maritime Museum. Mr. Tolliver showed photos and renderings while describing the interior layout, display areas, graphics, exhibits, and interactive components planned for the museum. Following the presentation, Rodney Kite, who serves on the Historic Boat Building Committee, thanked Commissioners for their support of the project, and said the entire County will be proud when it is completed.

### **B. Stormwater Drainage Analysis for 500-Year Storm Event**

County Manager, Ben Stikeleather, summarized discussion at the Board's February retreat related to stormwater and, in response to flooding concerns in the County, the Board had asked for an analysis of the impacts to developers if stormwater pond holding capacity for subdivisions was increased to accommodate a 500-year storm.

Kim Hamby, Engineer with Timmons Group, performed the analysis and presented findings. Ms. Hamby reviewed the data and methods used to develop the analysis and used two subdivisions in the county for modeling. Existing stormwater design plans were reviewed and compared with the findings based on the 500-year storm. Results showed an increased pond size of 2.5 times on average and a 15% lot reduction. Ms. Hamby responded to questions from Commissioners regarding the analysis and results, and Eric Weatherly, County Engineer, provided additional information related to culverts, release rates and ditch maintenance. A summary of findings was presented.

Following presentation, Commissioners considered holding a work session to discuss the issue further. In lieu of a work session, Commissioners directed staff to set up a meeting of stakeholders to provide an opportunity for Commissioners to hear ideas and concerns from the development community. Mr. Stikeleather said he would try to put together a meeting in July.

Discussion concluded and a recess was called at 7:31 PM. The meeting reconvened at 7:44 PM.

## **PUBLIC HEARINGS**

### **A. Public Hearing and Possible Action on Annual Budget for the Fiscal Year Ending June 30, 2021.**

Chairman White opened the Public Hearing to receive comment on the County's annual budget. No one was signed up nor wished to speak and the Public Hearing was closed.

Commissioner Beaumont said Currituck County school representatives will be addressing the Board of Commissioners at the Monday, June 22 Commissioner meeting, and suggested holding off on voting on the county budget so questions related to the school budget can be answered.

County Manager, Ben Stikeleather, said an email was sent to Commissioners to provide additional information on the salary study and Sandra Hill, Finance Director, confirmed minimal impacts to Finance if the Board waited an additional week for adoption of the budget.

Commissioner Beaumont clarified that discussion could take place, but his motion is to not take action on the budget until after the schools presentation at the next meeting. Commissioner J. Owen Etheridge seconded the motion and the motion carried.

Following the vote to delay action, Mr. Stikeleather provided a breakdown for Commissioners on costs to implement the salary study and a Cost of Living increase, and the effects based on a timeline of when the increases occur, and reviewed compression movements and clarified costs related to advanced fire and paramedic

positions. The total cost of salary study is \$811,192.00, cost of living increase is \$564,943.00, resulting in a total cost of \$1,392,358.00. Mr. Stikeleather said there is a savings of \$68,580.00 if the Cost of Living and Salary Study are implemented at the same time.

Mr. Stikeleather provided an explanation of the current pay schedule for fire employees. He said he believes the fire employees do need an increase but recommends a conservative approach with the budget and the unknowns this year. He acknowledged an uptick in vacation rentals on the beach and suggested looking at Occupancy Tax assist with determining where the County sits financially. He recommended assessing finances in September, and if the Board is inclined to move forward with increases, implementing both the cost of living and salary study for all employees. Mr. Stikeleather said selecting departments to receive raises makes it difficult to maintain employee moral.

Commissioner Mary Etheridge expressed concerns with spending from fund balance. She noted rising Covid-19 cases and said the Board needs to wait and see what happens before authorizing increases.

Commissioner McCord said the money will be there, as the beach is packed and the Board should do it now, and not wait.

Commissioner Beaumont agreed the beach is busy, and suggested the Board of Education's spending of fund balance cannot be compared with County spending.

Chairman White believes Covid-19 impacts on sales tax will be significant and cautioned Commissioners about spending out of fund balance and to wait until September.

Commissioner Jarvis said waiting will allow the Board to really fix the problem, to implement the whole salary study and the cost of living, and would allow them to have discussion if the county has the financial means.

Commissioner J. Owen Etheridge suggested making the increase retroactive if the funds are there in September. The County Manager said it would be difficult and doesn't recommend it. Mr. Stikeleather responded to questions related to fire employees, and said the County has listened to their concerns and rectified several issues raised. Commissioner Payment suggested looking at their duties to see if they are being asked to do too much. The County Manager said the call data can be analyzed.

Following discussion Commissioners agreed to come back for a review of funds in September for implementation of an increase October 1. A look-back period to July 1 will be considered.

**Move to Delay Acting on Budget until June 22, 2020 Commissioners Meeting**

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Paul M. Beaumont, Commissioner
<b>SECONDER:</b>	J. Owen Etheridge, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

### **B. Public Hearing & Possible Action to Authorize an Economic Development Incentive for The Cotton Gin, Jarvisburg, NC**

Larry Lombardi, Economic Development Director, spoke to Commissioners on behalf of the Cotton Gin in support of approval of a Resolution that would provide assistance with clearing the property following a fire that destroyed the building. Mr. Lombardi said the business has operated in the county for many decades and the Economic Development Incentive would help with costs to rebuild, thus bringing jobs back to the county. Ben Stikeleather, County Manager, discussed with Commissioners other incentives provided to local businesses, and reviewed the criteria, purpose and benefits resulting from Economic Development incentives.

Following discussion, Chairman White opened the Public Hearing. No one was signed up nor wished to speak and the Public Hearing was closed.

Commissioner Jarvis moved to authorize the Economic Development Incentive for the Cotton Gin in the amount of \$16,365.14. Commissioner Payment seconded the motion. The motion carried.

### **RESOLUTION AUTHORIZING INCENTIVES CONTRACT WITH THE COTTON GIN, INC.**

WHEREAS, Section 158-7.1 of the General Statutes of North Carolina authorizes a county to undertake an economic development project by extending assistance to a company in order to cause the company to locate or expand its operations within the county; and

WHEREAS, the Currituck County Board of Commissioners held a public hearing on June 15, 2020 to receive public comments regarding Currituck County, (the "County"), participation in an economic development project with The Cotton Gin, Inc. by providing The Cotton Gin, Inc. with a portion of the cost required for clearing property in preparation for construction of a new facility; and

WHEREAS, The Cotton Gin, Inc. will make an investment in the county in an approximate amount of between \$2,000,000 and \$3,000,000 to construct a new facility and acquire equipment necessary for its retail operations and return 34 jobs to the community workforce; and

WHEREAS, as proposed The Cotton Gin, Inc.'s investment in the county will stimulate, diversify and help stabilize the local economy, promote business in the

county, increase tourism on the County’s mainland and result in the return of jobs in the County; and

WHEREAS, the Currituck County Board of Commissioners will adopt an amendment to the County’s budget ordinance appropriating the funds necessary for the project;

NOW, THEREFORE, BE IT RESOLVED by the Currituck County Board of Commissioners that:

Section 1. The County is authorized to expend up to \$16,365.14 from the County’s General Fund for the partial cost required for The Cotton Gin, Inc.’s clearance of its property in preparation for construction of its new facility.

Section 2. This resolution and expenditure of funds are contingent on the execution of an incentives contract between the County and The Cotton Gin, Inc. outlining its investment in the county.

Section 3. The Chairman of the Board of Commissioners is authorized to execute the incentives contract and any other documents necessary to the project on behalf of the County.

Section 4. This resolution is effective upon adoption.

Adopted this 15<sup>th</sup> day of June, 2020.

\_\_\_\_\_  
Bob White, Chairman  
Board of Commissioners

ATTEST:

\_\_\_\_\_  
Leeann Walton, Clerk to the Board

(COUNTY SEAL)

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Selina S. Jarvis, Commissioner
<b>SECONDER:</b>	Mike H. Payment, Vice Chairman
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

**C. PB 20-09 Boswood Estates, Phase 1 & 2:**

APPLICATION SUMMARY	
<b>Property Owner:</b> G. Dodson Mathias 400 Avinger Lane, Villa 609 Davidson, NC 28039	<b>Applicant:</b> Boswood Estates, LLC PO Box 100 Currituck, NC 27929
<b>Case Number:</b> PB 20-09	<b>Application Type:</b> Preliminary Plat/Use Permit
<b>Parcel Identification Number:</b> 0060000053B0000	<b>Existing Use:</b> Active Agricultural/Wetlands
<b>Land Use Plan Classification:</b> Full Service/Conservation	<b>Parcel Size (Acres):</b> 26.94 acres total 20.49 acres (excludes 6.45 ac CAMA wetlands)
<b>Maple/Barco Small Area Plan Classification:</b> Mixed Use	<b>Zoning:</b> General Business (GB)
<b>Number of Units:</b> 14 residential lots (2 phases)	<b>Project Density:</b> 0.68 dwelling unit/acre
<b>Required Open Space:</b> 8.08 acres (30%)	<b>Provided Open Space:</b> 11.05 acres (41%)

SURROUNDING PARCELS		
	LAND USE	ZONING
NORTH	LOW DENSITY RESIDENTIAL	SFM
SOUTH	RESIDENTIAL/AGRICULTURAL	GB
EAST	COINJOCK BAY	N/A
WEST	LOW DENSITY RESIDENTIAL	GB

Application Summary

1. The applicant, Boswood Estates, LLC, is requesting preliminary plat approval of a 2 phase 14-lot residential subdivision.
2. The proposed development is a Type II traditional subdivision.
3. The base zoning of the property is General Business and the minimum lot size for a traditional residential subdivision is 40,000 square feet.
4. The property contains 10.66 acres of US Army Corps of Engineers jurisdictional wetlands (preliminary jurisdictional determination) and approximately 6.45 acres of coastal wetlands. The wetlands and riparian buffer will be located in open space. The applicant indicates 9.51 acres of wetlands will be dedicated to a non-profit and the remaining 1.54 acres of open space will be dedicated to the subdivision association.
5. The property contains approximately 772 linear feet of shoreline along the Coinjock Bay. The applicant is not proposing public water access to the abutting public trust waters since the total number of lots is less than 20.
6. The proposed streets are designed to be 20' in pavement width with a roadside swale within a 50' right of way. A 5' wide sidewalk is proposed between the roadside swale and the street trees. The sidewalk will not be located in the street right of way but on private property and within a 25' drainage, landscape, utility and pedestrian easement.

7. Interconnectivity is proposed to the southern property line and adjacent to active agricultural lands with a residential use (previously known as The Palmer Inn, bed and breakfast).
8. The Soil Survey of Currituck County, North Carolina identifies the proposed residential lots predominately State fine sandy loam (StA) soils. A small area near the wetlands is identified as Tomotley fine sandy loam (To) soils. The Soil Survey indicates State (StA) soils are generally sited for most urban and recreational uses; wetness is the main limitation.
9. The entrance road for the proposed development is approximately 380' south of the existing Major's Island Road, a private unpaved road. The minimum intersection spacing for a local street intersecting a major arterial street is 1,000 feet. Due to the lot width of this site and the existing private streets, the applicant is unable to meet the minimum intersection spacing of the UDO. The 10<sup>th</sup> edition of the *ITE Trip Generation Manual* states a single family dwelling generates 10 trips per day, and the proposed 14 lot development will generate a total of 140 vehicles per day. Upon advise of David B. Otts, NCDOT District Engineer, the placement of the proposed entrance road that will generate 140 vehicles per day and is approximately 380' south of the existing Major's Island Road (private), will maintain a satisfactory level of access control on Caratoke Highway. The total number of lots will not require a deceleration lane.
10. A community meeting was not required with this application due to the number of lots proposed.

INFRASTRUCTURE	
<b>WATER</b>	PROPOSED PUBLIC WATER SUPPLY
<b>SEWER</b>	ON-SITE SEPTIC
<b>TRANSPORTATION</b>	PEDESTRIAN: SIDEWALKS ON BOTH SIDES OF THE STREET CONNECTIVITY SCORE: MINIMUM = 1.4 PROPOSED = 1.5
<b>STORMWATER/DRAINAGE</b>	PROPERTY LINE VEGETATIVE SWALES WILL CONVEY RUNOFF TO A SWALE/DITCH NEAR THE REAR OF EACH LOT AND THEN TO THE DRAINAGE OUTLET EXISTING INTERNAL FARM DITCHES WILL BE FILLED AND REDIRECTED TO SHALLOW GRASS SWALES
<b>LIGHTING</b>	NO STREET LIGHTING PROPOSED
<b>LANDSCAPING</b>	A 25' STREETScape WILL BE PROVIDED ALONG CARATOKE HIGHWAY. A PERIMETER BUFFER WILL BE PROVIDED ALONG THE PROPERTIES ZONED SFM. THE APPLICANT SELECTED A 10' PERIMETER BUFFER WIDTH. STREET TREES WILL BE PROVIDED.
<b>COMPATIBILITY</b>	A 50' FARMLAND BUFFER WILL BE PROVIDED ALONG THE AGRICULTURAL USE AREA ON THE SOUTHERN PROPERTY LINE.
<b>RECREATION AND PARK AREA DEDICATION</b>	THE 0.36 ACRE PROPOSED DEDICATION IS CONSIDERED TOO SMALL TO PROVIDE ADEQUATE PARK OR RECREATION AREA AND PAYMENT IN LIEU OF THE DEDICATION SHALL BE PROVIDED.
<b>RIPARIAN BUFFERS</b>	A 30' RIPARIAN BUFFER WILL BE PROVIDED ADJACENT TO ALL WETLAND BOUNDARIES. THE BUFFER IS LOCATED IN OPEN SPACE.

**ADEQUATE PUBLIC FACILITIES – SCHOOLS<sup>1</sup>**

School	2019-2020 2020-2021 Actual Capacity <sup>2</sup>	2021-2022 Actual Capacity <sup>3</sup>	Committed Capacity <sup>3</sup>	Proposed Capacity Changes
				Number of Students
Moyock Elementary	109%	115%	122%	4 students
Shawboro Elementary	87%	90%		
Central Elementary	77%	85%		
Griggs Elementary	57%	59%	96%	
Jarvisburg Elementary	88%	95%		
Knotts Island Elementary	36%	38%	38%	
Moyock Middle	94%	83%	96%	1 student
Currituck Middle	70%			
Currituck High	84%	85%	103%	2 students
JP Knapp Early College	88%			

<sup>1</sup>Does not include minor subdivisions, exempt subdivisions, and subdivisions approved prior to the adoption of the adequate public facilities ordinance (October 1994)

<sup>2</sup>Capacity percentages are based on 2019-2020 and 2020-2021 school year classroom standards and January 2020 ADM

<sup>3</sup>Capacity percentages are based on the 2021-2022 school year classroom standards and January 2020 ADM

## RECOMMENDATIONS

### TECHNICAL REVIEW COMMITTEE

**The Technical Review Committee recommends adoption of the use permit and approval of the preliminary plat subject to the following conditions of approval:**

1. The application complies with all applicable review standards of the UDO provided the following items are addressed:
  - a. The applicant selected Option 2 for the perimeter buffer. The width of the buffer is 10 feet and is near overhead power lines and drainage improvements. Staff is concerned that this area may not be sufficient width to accommodate the existing conditions and the proposed improvements, including landscaping. It is recommended that a typical detail of the existing conditions and proposed improvements be submitted to ensure compliance of the UDO and minimize the potential effects on surrounding lands.
2. The applicant demonstrates the proposed use will meet the use permit review standards of the UDO.
3. The conditions of approval necessary to ensure compliance with the review standards of the UDO and to prevent or minimize adverse effects of the development application on surrounding lands include:
  - a. Open space shall be contiguous and shall not contain private walkways or boardwalks.
  - b. A 25' drainage easement shall be provided along all conveyance systems that drain more than 5 acres. The easement shall include the conveyance system and an additional 25' from the top of embankment. This easement will extend onto private lots. In an effort provide awareness and avoid damage to potential private improvements, no septic system or

structure shall be located the easement. A note shall be added to the final plat.

- c. The applicant selected Option 2 for the perimeter buffer. The width of the buffer is 10 feet and is near overhead power lines and drainage improvements. Staff is concerned that this area may not be sufficient width to accommodate the existing conditions and the proposed improvements, including landscaping. It is recommended that a typical detail of the existing conditions and proposed improvements be submitted to ensure compliance of the UDO and minimize the potential effects on surrounding lands.
- d. No parking signs shall be placed at along the street at intersections and the entrance (approximately 4-5 signs).

## USE PERMIT REVIEW STANDARDS

A use permit shall be approved on a finding that the applicant demonstrates the proposed use will meet the below requirements. It is staff's opinion that the evidence in the record, prepared in absence of testimony presented at a public hearing, supports the preliminary staff findings

THE USE WILL NOT ENDANGER THE PUBLIC HEALTH OR SAFETY.

### Preliminary Applicant Findings:

1. **The use will be in accordance with county health and safety standards and those recommended by the Albemarle Regional Health Services in regards to on site wastewater systems.**

THE USE WILL NOT INJURE THE VALUE OF ADJOINING OR ABUTTING LANDS AND WILL BE IN HARMONY WITH THE AREA IN WHICH IT IS LOCATED.

### Preliminary Applicant Findings:

1. **Lands to the north have been developed for single family homes as well as the properties across the highway.**
2. **Lot sizes proposed are in keeping with what is adjacent.**
3. **Wetlands will be preserved.**

THE USE WILL BE IN CONFORMITY WITH THE LAND USE PLAN OR OTHER OFFICIALLY ADOPTED PLANS.

### Preliminary Staff Findings:

1. **The 2006 Land Use Plan classifies this site as Full Service and Conservation land use classification in the Barco-Coinjock-Airport subarea.**
2. **The area intended for residential lots is predominately in the Full Service land use classification. The Full Service area contemplates a residential density (base) to be 2 units per acre.**
3. **The policy emphasis for Barco-Coinjock-Airport subarea indicates residential development densities should be limited to 1-2 units per acre in areas where on-site wastewater is proposed and other county services are may be limited. The proposed development density is 0.68 units per acre.**
4. **The proposed use is in keeping with the policies of the plan, some of which are:**

POLICY ES2: NON-COASTAL WETLANDS, including FRESHWATER SWAMPS, AND INLAND, NON-TIDAL WETLANDS, shall be conserved for the important role they play in absorbing floodwaters, filtering pollutants from stormwater runoff, recharging the ground water table, and providing critical habitat for many plant and animal species. Currituck County supports the

efforts of the U.S. Army Corps of Engineers in protecting such wetlands through the Section 4042 permit program of the Clean Water Act, as well as Section 4013 water quality certifications by the State of North Carolina.

**POLICY ES3:** COASTAL WETLANDS shall be conserved for the valuable functions they perform in protecting water quality and in providing critical habitat for the propagation and survival of important plant and animal species. CAMA use standards and policies for coastal wetlands shall be supported. Uses approved for location in a coastal wetland must be water dependent (i.e. utility easements, bridges, docks, and piers) and be developed so as to minimize adverse impacts.

**POLICY HN1:** Currituck County shall encourage development to occur at densities appropriate for the location. LOCATION AND DENSITY FACTORS shall include whether the development is within an environmentally suitable area, the type and capacity of sewage treatment available to the site, the adequacy of transportation facilities providing access to the site, and the proximity of the site to existing and planned urban services. For example, projects falling within the Full Services areas of the Future Land Use Map would be permitted a higher density because of the availability of infrastructure as well as similarity to the existing development pattern. Such projects could be developed at a density of two (2) or more dwelling units per acre. Projects within areas designated as Limited Service would be permitted a density of one (1) to one and one half (1.5) units per acre depending upon the surrounding development pattern and availability of resources. Projects within areas designated as Rural or Conservation by the Future Land Use Plan would be permitted a much lower density of 1 dwelling unit per 3 acres because of the lack of infrastructure in the area, the existing low density development pattern, and presence of environmentally sensitive natural areas.

**POLICY WQ5:** Development that preserves the NATURAL FEATURES OF THE SITE, including existing topography and significant existing vegetation, shall be encouraged. If COASTAL AND NON-COASTAL WETLANDS are considered part of a lot's acreage for the purpose of determining minimum lot size or development density, Low Impact Development techniques or appropriate buffers shall be integrated into the development. Open space developments shall be encouraged to REDUCE IMPERVIOUS SURFACE AREAS associated with new development and redevelopment.

**5. The Maple-Barco Small Area Plan, an official adopted plan, classifies the site as Mixed Use. The mixed use designation is characterized by a diverse mix of land uses, including residential.**

**6. The proposed use is in keeping with policies in the Maple-Barco Small Area Plan some of which include:**

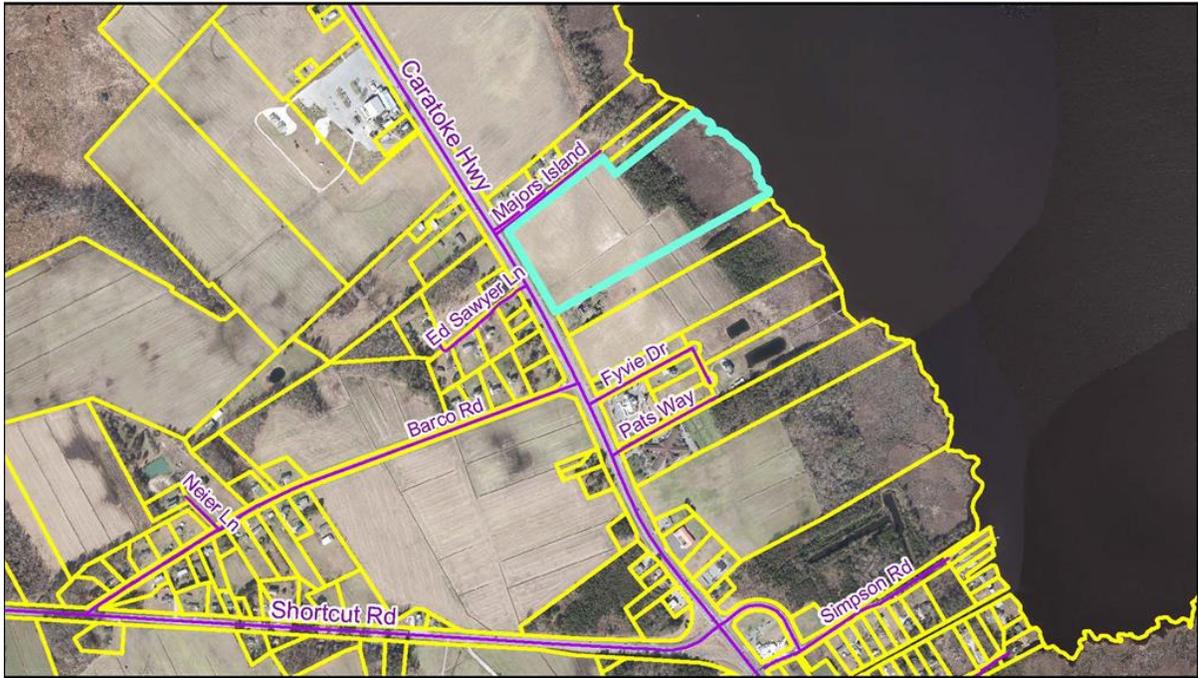
**LU6: Encourage buffers for uses that are developing adjacent to environmentally sensitive areas.**

**LU9: Evaluate development proposals using the future land use map and policies for the Maple – Barco study area to determine the desired density, character of growth, and level of services appropriate for the study area.**

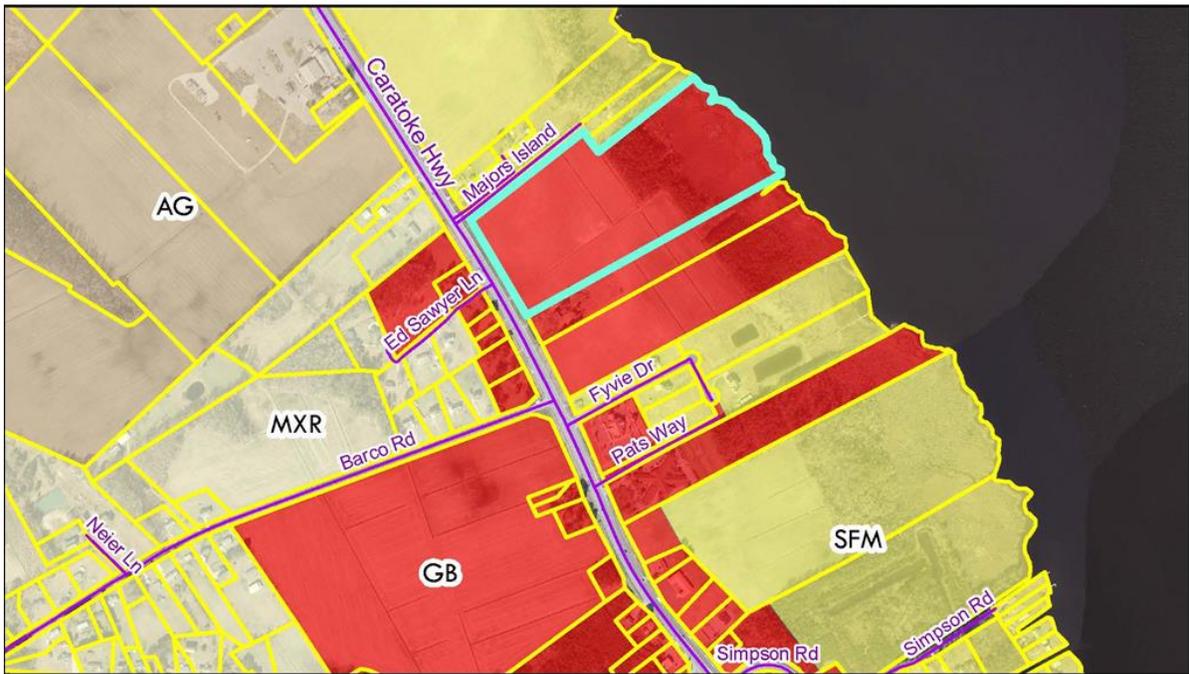
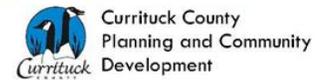
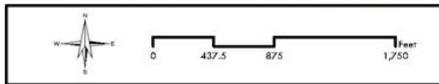
THE USE WILL NOT EXCEED THE COUNTY'S ABILITY TO PROVIDE ADEQUATE PUBLIC FACILITIES, INCLUDING, BUT NOT LIMITED TO: SCHOOLS, FIRE AND RESCUE, LAW ENFORCEMENT, AND OTHER COUNTY FACILITIES. APPLICABLE STATE STANDARDS AND GUIDELINES SHALL BE FOLLOWED FOR DETERMINING WHEN PUBLIC FACILITIES ARE ADEQUATE.

**Preliminary Staff Findings:**

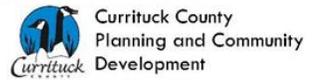
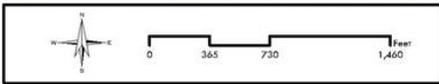
1. The proposed subdivision contains 14 residential lots.
2. Based on the Student Generation Rate study prepared by Tischler and Associates, Inc. (2004), the proposed subdivision will generate the following students:
  - a. 4 elementary school students;
  - b. 1 middle school student; and,
  - c. 2 high school students
3. According to Currituck County Schools, the proposed subdivision is located in the following school districts:
  - a. Central Elementary
    - i. 77% 2019-2021 actual capacity based on January 2020 ADM
    - ii. 85% 2021-2022 actual capacity based on January 2020 ADM
  - b. Currituck Middle School, and
    - i. 70% 2019-2021 actual capacity based on January 2020 ADM
  - c. Currituck High School.
    - i. 84% 2019-2021 actual capacity based on January 2020 ADM



PB 20-09 Boswood Estates  
Preliminary Plat/Use Permit  
Aerial

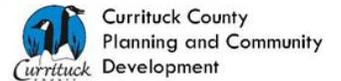
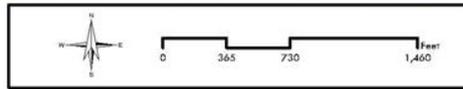


PB 20-09 Boswood Estates  
Preliminary Plat/Use Permit  
Zoning

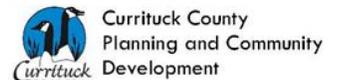
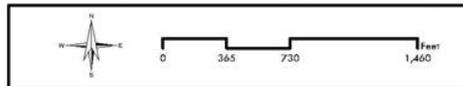




PB 20-09 Boswood Estates  
Preliminary Plat/Use Permit  
2006 Land Use Plan Classifications



PB 20-09 Boswood Estates  
Preliminary Plat/Use Permit  
Maple Barco SAP



Parties were sworn in and Donna Voliva, Assistant Planning Director, reviewed the

application for the Board of Commissioners. Using a powerpoint, Ms. Voliva displayed overhead maps to display the location, zoning and land use of the site and surrounding areas. The subdivision plat was reviewed. Intended construction phasing, infrastructure, minimum lot size and open space were presented, as were staff concerns with the buffering. Ms. Voliva responded to questions related to the proposed open space and fire apparatus turnaround. The Technical Review Committee (TRC) recommended conditions for approval, use permit review standards, supporting policies and adequate public facilities were presented.

Hood Ellis, Attorney for the applicant, introduced Engineer Jason Mizelle of Timmons Group, who testified to support approval of the request. Mr. Mizelle reviewed stormwater runoff at the site, addressed the fire truck turnaround, and discussed the minimal density and student impacts based on the development's site location in the county. He said the phasing plan addresses any issues with market fluctuations, but the hope is to build out. Connectivity and pedestrian access were reviewed.

Commissioners had no questions and Chairman White opened the Public Hearing.

Mr. Joe Robinson of Maple asked for a clarification from Mr. Mizelle related to ditching.

David Majors, adjoining property owner, wanted assurances that there would be no issues with his existing right of way or any excursion onto his property. Mr. Mizelle described the vegetative buffer they are required to install and confirmed it would be inside the property line. He said they may need to replace a culvert at his property that is not draining properly. Mr. Majors was concerned about damage to his water line. He asked that a copy of the subdivision plat be provided to him.

There were no other speakers and the Public Hearing was closed.

Commissioner Beaumont moved to approve PB 20-09, Boswood Estates, with the TRC conditions of approval because the applicant has demonstrated the proposed use meets the Use Permit Review Standards of the Unified Development Ordinance (UDO). The TRC conditions of approval consisted of:

- Open space shall be contiguous and shall not contain private walkways or boardwalks
- A 25' drainage easement shall be provided along all conveyance systems that drain more than 5 acres. The easement shall include the conveyance system and an additional 25' from the top of embankment. The easement will extend onto private lots. No septic system or structure shall be located in the easement and a note shall be added to the final plat.
- The width of the perimeter buffer will be 10 feet and is near overhead power lines and drainage improvements. The developer will work with County staff to ensure the species of trees selected are conducive to be grown underneath power lines. A typical perimeter buffer detail of the existing conditions and proposed improvements shall be submitted to ensure compliance of the UDO and minimize the potential effects to surrounding lands.
- No parking signs shall be placed along the street at intersections and the entrance, approximately 4 to 5 signs.

The use will not endanger the public safety or health. The use will be in accordance with county health and safety standards and those recommended by the Albemarle Regional Health Services in regards to onsite wastewater systems.

The use will not injure the value of adjoining or abutting lands and will be in harmony with the area in which it is located. Lands to the north have been developed for single family homes as well as the properties across the highway. Lot sizes proposed are in keeping with what is adjacent and the wetlands will be preserved.

The use will be in conformity with the Land Use Plan (LUP) or other officially adopted plans. The 2006 LUP classifies this site as Full Service and Conservation land use classification in the Barco-Coinjock-Airport subarea. The area intended for residential lots is predominantly in the Full Service land use classification. The Full Service area contemplates a residential density base to be 2 units per acre. The policy emphasis for the Barco-Coinjock-Airport subarea indicates residential development densities should be limited to 1-2 units per acre in areas where on-site wastewater is proposed and other county services are or may be limited. The proposed development density is 0.68 units per acre.

The proposed use is in keeping with the policies of the plan, some of which are:

Policy ES2-Non-coastal wetlands, including freshwater swamps and inland, non-tidal wetlands shall be conserved for the important role they play in absorbing floodwaters, filtering pollutants from stormwater runoff, recharging the ground water table, and providing critical habitat for many plant and animal species.

Policy ES3-Coastal wetlands shall be conserved for the valuable functions they perform in protecting water quality and in providing critical habitat for the propagation and survival of important plant and animal species. CAMA use standards and policies for coastal wetlands shall be supported.

Policy WQ5-Development that preserves the natural features of the site, including existing topography and significant existing vegetation, shall be encouraged.

The Maple-Barco Small Area Plan (SAP), an official adopted plan, classifies the site as Mixed Use. The Mixed Use designation is characterized by a diverse mix of land uses, including residential. The proposed use is in keeping with policies in the Maple-Barco Small Area Plan some of which include:

LU6-Encourage buffers for uses that are developing adjacent to environmentally sensitive areas.

LU9-Evaluate development proposals using the future land use map and policies for the Maple-Barco study area to determine the desired density, character of growth, and level or services appropriate for the study area.

The use will not exceed the county's ability to provide adequate public facilities.

- The proposed subdivision contains 14 residential lots
- Based on the Student Generation Rate study prepared by Tischler and Associates in 2004 the proposed subdivision will generate 4 elementary school students to attend Central Elementary, 1 middle school student and 2 high school students, both to attend Currituck Middle and/or Currituck High School.
- According to Currituck County Schools, the occupancy rates of January Average Daily Membership (ADM) are:

Central Elementary  
 77% 2019-2021 actual capacity based on January 2020 ADM  
 85% 2021-2022 actual capacity based on January 2020 ADM

Currituck Middle School, and  
 70% 2019-2021 actual capacity based on January 2020 ADM

Currituck High School  
 84% 2019-2021 actual capacity based on January 2020 ADM

The motion was seconded by Commissioner Payment. The motion carried.

At 9:11 PM, Chairman White called a brief recess. The meeting reconvened at 9:24 PM.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Paul M. Beaumont, Commissioner
<b>SECONDER:</b>	Mike H. Payment, Vice Chairman
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

**D. PB 20-07 Currituck County Alternative Water Supply for Fire Flow Text Amendment:**

To: Planning Board

From: Planning Staff

Date: May 26, 2020

Subject: PB 20-07 Currituck County Text Amendment  
 Alternative Water Supply for Fire Flow

The Board of Commissioners directed staff to prepare a text amendment to allow water shuttling as an alternative means to meet required fire flow standards for properties not served by county water.

This text amendment to the Unified Development Ordinance (UDO) allows use of water shuttling as a means of meeting fire flow water supply requirements for lands not serviced by the county water system and revises references in UDO Chapters 4, 5, & 6 from “Fire Marshal” to “Fire Code Official” to be consistent with the North Carolina Fire Code.

### Text Amendment Review Standards

The advisability of amending the text of the UDO is a matter committed to the legislative discretion of the Board of Commissioners and is not controlled by any one factor. In determining whether to adopt or deny the proposed text amendment, the Board of Commissioners may weigh the relevance of and consider whether and the extent to which the proposed text amendment:

1. Is consistent with the goals, objectives, and policies of the Land Use Plan and other applicable county-adopted plans;
2. Is not in conflict with any provision of this Ordinance or the County Code of Ordinances;
3. Is required by changed conditions;
4. Addresses a demonstrated community need;
5. Is consistent with the purpose and intent of the zoning districts in this Ordinance, or would improve compatibility among uses and ensure efficient development within the county;
6. Would result in a logical and orderly development pattern; and
7. Would not result in significantly adverse impacts on the natural environment, including but not limited to water, air, noise, stormwater management, wildlife, vegetation, wetlands, and the natural functioning of the environment.

### Staff Recommendation

Staff recommends approval of the request as submitted and suggests the following Statement of Consistency and Reasonableness:

The requested zoning text amendment is consistent with the goals, objectives, and policies of the 2006 Land Use Plan including:

- POLICY PS2: Currituck County shall support and encourage the development and improvement of FIRE FIGHTING SERVICES that enhance the security and safety of life and property, while resulting in the added benefit of lower property insurance rates. The need for additional fire stations or improvements to existing fire stations shall be examined annually to keep pace with the growth of the area.
- POLICY WS3: Currituck County endorses UTILITIES EXTENSION POLICIES that focus water and sewer services (1) within existing developed areas and in nearby targeted growth areas identified as Full Service and Limited Service areas, (2) where development densities would make the provision of all public services more efficient, (3) where the land is particularly well suited for development and (4) away from environmentally sensitive areas, such as areas with extensive wetlands or the northern beaches of the Outer Banks.
- POLICY WS4: Currituck County endorses utilities extension policies that avoid those parts of the county best suited for agriculture and to PROTECT FARMLAND FROM DEVELOPMENT PRESSURES brought about by such utilities. Exceptions to this policy may include extensions for major economic development initiatives, and extensions to address imminent public health problems or related environmental hazards.

The request is reasonable and in the public interest because:

- It is consistent with the 2006 Land Use Plan, and it is not in conflict with the provisions of the UDO.
- It continues to allow limited development without the requirement for extension of county water lines in farmland and rural areas.

STAFF REPORT PB20-07 CURRITUCK COUNTY  
TEXT AMENDMENT ALTERNATIVE WATER SUPPLY

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FOR FIRE FLOW PLANNING BOARD MAY 28, 2020
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Amendment to the Unified Development Ordinance Chapter 4. Use Standards, Chapter 5. Development Standards and Chapter 6. Subdivision and Infrastructure Standards.

BE IT ORDAINED by the Board of Commissioners of the County of Currituck, North Carolina that the Unified Development Ordinance of the County of Currituck be amended as follows:

**Item 1:** That Chapter 6: Subdivision and Infrastructure Standards is amended by adding the following underlined language, deleting the struck-through language and numbering accordingly:

### **6.2.1. 6.2.2. 6.2.3. Fire Protection Standards**

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#### **General Provisions**

##### **Fire Lanes**

Where streets or rights-of-way provide insufficient access for firefighting, unobstructed fire lanes with a minimum width complying with the current adopted version of the North Carolina State Fire Code shall be provided. In no instance shall this standard waive the requirement for primary drive aisles constructed in accordance with Section 5.6.8, Primary Drive Aisles, when required by this Ordinance.

##### **Fire Hydrants Required**

All development serviced by the county water supply system shall include a system of fire hydrants sufficient to provide adequate fire protection for the buildings located or intended to be located within the development. Fire hydrants shall be located in a manner that ensures hydrants are spaced a maximum of 1,000 linear feet apart and every portion of lot frontage is within 500 linear feet of a hydrant. The Fire Code Official ~~Marshal~~ may authorize or require a deviation from this standard if, in the opinion of the Fire Code Official ~~Marshal~~, another arrangement ~~more~~ satisfactorily complies with the intent or standards in this Ordinance.

##### **Fire Hydrant Location**

Unless an alternative placement is specified by the State Building Code or the Planning Director, in consultation with the Fire Code Official ~~Marshal~~, fire hydrants shall be

placed six feet behind the curb or within ten feet of the pavement edge of a street without curbing.

#### **Required Hose Connections**

Unless otherwise specified, all fire hydrants shall have the following hose connections:

Two two-and-one-half-inch hose connections at least 21½ inches above ground level; and

One four-and-one-half-inch connection.

All hose connections shall be sized in accordance with national standards.

#### **Water Service Main Size**

Water mains serving fire hydrants shall be at least eight inches in diameter.

#### **Water Supply Source Location**

Water supply sources shall be clearly marked for location purposes with a marker of suitable size and reflective characteristics for daylight, nighttime, and inclement weather operations.

#### **Water Supply for Fire Protection when not Serviced by County Water Supply System**

Development not serviced by the county water system shall provide a supply of water for fire-fighting purposes in accordance with the following standards:

##### **Allowable Sources**

The developer may provide the required water supply from:

~~fire~~ Fire ponds, canals, wells, cisterns, above ground storage tanks, or water lines (where a community water supply system is installed), or;

Fire Department mobile water supply approved by the Fire Code Official;

~~or a~~ Any combination of the above these features; or

An alternative means approved by the Fire Code Official.

##### **Location**

Water supply facilities shall be within 2,500 feet of every anticipated building in a development.

Water supply facilities may be located on or off-site, however the developer shall demonstrate a

sufficient legal interest in off-site facilities to ensure they will remain available to serve the development.

Water supply sources shall be so located so that fire-fighting vehicles have ready access to such sources at all times.

### **Capacity**

A sufficient volume of water shall be available at all times to supply the needed fire flow for the proposed structures based upon guidance from the Insurance Services Office and existing fire-fighting capacity.

Water mains serving a community water supply system shall be sized to allow the future installation of fire hydrants should the development be connected to the county water supply system.

### **Configuration**

Water supply sources shall be provided with the necessary equipment and connections (e.g., dry hydrants in ponds) to ensure that fire-fighting equipment can draw water in a safe and efficient manner, as determined by the Fire Code Official ~~Marshal~~.

Except within the SFR district, a hard-surfaced roadway shall be provided to the water source as well as a hard-surfaced turnaround area of sufficient dimensions to facilitate access by fire-fighting vehicles.

### **Maintenance Required**

The developer, or any successor in interest, shall be responsible for ensuring that all water supply sources, access roadways, and other facilities or equipment required by these standards, are maintained.

**Item 2:** That Chapter 4. Use Standards, Chapter 5. Development Standards and Chapter 6. Subdivision and Infrastructure Standards are amended by striking through all references to Fire Marshal and replacing with Fire Code Official.

**Item 3:** Statement of Consistency and Reasonableness:

The requested text amendment is consistent with the goals, policies, and objectives of the 2006 Land Use Plan including:

- POLICY PP2: Currituck County shall continue to implement a policy of ADEQUATE PUBLIC FACILITIES, sufficient to support associated growth and development. Such facilities may include but not be limited to water supply, school capacity, park and open space needs, firefighting capability, and law enforcement.
- POLICY WS3: Currituck County endorses UTILITIES EXTENSION POLICIES that focus water and sewer services (1) within existing developed areas and in nearby targeted growth areas identified as Full Service and Limited Service areas, (2) where development densities would make the provision of all public services more efficient, (3) where the land is particularly well suited for development and (4) away from environmentally sensitive areas, such as areas with extensive wetlands or the northern beaches of the Outer Banks.
- POLICY WS4: Currituck County endorses utilities extension policies that avoid those parts of the county best suited for agriculture and to PROTECT FARMLAND FROM DEVELOPMENT PRESSURES brought about by such utilities. Exceptions to this policy may include extensions for major economic development initiatives, and extensions to address imminent public health problems or related environmental hazards.

The request is reasonable and in the public interest because:

- It is consistent with the 2006 Land Use Plan, and it is not in conflict with the provisions of the UDO.
- It continues to allow limited development without the requirement for extension of county water lines in farmland and rural areas.

**Item 4:** The provisions of this Ordinance are severable and if any of its provisions or any sentence, clause, or paragraph or the application thereof to any person or circumstance shall be held unconstitutional or violative of the Laws of the State of North Carolina by any court of competent jurisdiction, the decision of such court shall not affect or impair any of the remaining provisions which can be given effect without the invalid provision or application.

**Item 5:** This ordinance amendment shall be in effect from and after the \_\_\_\_\_ day of \_\_\_\_\_, 2020.

Jennie Turner, Planner, presented the text amendment request, originally discussed by Commissioners at their annual Board Retreat. Ms. Turner reviewed the proposed text amendment language that provides alternatives for water supply to meet fireflow standards, to include Water Shuttling as an allowable water source for fire response, modifies the term reference for a Fire Code Official.

Following review, Ms. Turner responded to questions related to certification for water shuttling operations. Ms. Turner said forms and processes would be developed for approvals of water shuttling operations for the various fire departments.

Ms. Turner reported discussion by the Planning Board over concerns with recertification requirements resulting in the Board's recommendation to strike water shuttling, item B,

from the text amendment language. Ms. Turner said staff supports approval of the text amendment and consistency statements were reviewed.

Chairman White opened the Public Hearing.

Ryland Poyner, Chief of Crawford Township Volunteer Fire Department, voiced concerns with the shifting of responsibility from the developer to the volunteer fire departments. He described joint efforts necessary to get certified for water shuttling, with no guarantees that shuttling certifications will be maintained.

Commissioners clarified that shuttling is used when county water systems are not available.

Ms. Turner clarified the number of lots that could potentially be served by water shuttling if county water is not available and Bill Newns, Chief Inspector, described the trigger for fire flow requirements and an adequate water supply. He said shuttling should not be a main source for fighting fire. Deputy Chief Poulin of Lower Currituck Volunteer Fire Department commented on water shuttling in response to the recent fire at the Cotton Gin.

Pond maintenance, pond inspections and sprinkler systems requirements for residential homes were discussed.

No others wished to speak and the Public Hearing was closed.

Commissioner Beaumont moved to approve PB 20-07 because the request is consistent with the goals, objectives and policies of the 2006 Land Use Plan, including Policy WS4. The request is reasonable and in the public interest. Commissioner McCord seconded the motion. Commissioner McCord rescinded his second and Commissioner Beaumont amended his motion to add clarifying language for approval of the text amendment language as proposed by staff. Commissioner McCord seconded the motion as amended. The motion carried.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Paul M. Beaumont, Commissioner
<b>SECONDER:</b>	Kevin E. McCord, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

#### **E. PB 19-25 Currituck County - Currituck Station:**

To: Board of Commissioners

From: Planning Staff

Date: January 28, 2020

Subject: PB 19-25 Currituck County - Currituck Station:

### **Background**

This text amendment is presented on behalf of Currituck County to implement a long and in depth planning process for a specified area in Moyock known as Currituck Station (previously Moyock Mega Site). In 2012, the Board of Commissioners recognized the steady growth Moyock was experiencing and engaged staff to begin efforts to address growth and development in a comprehensive manner. The planning process began in 2012 with the Moyock Small Area Plan that was later adopted by the Board of Commissioners in 2014. The Moyock Small Area Plan identified an employment activity center for the area identified as Currituck Station. The employment center was intended to have a concentration of uses including commercial, industrial, and residential. The county later adopted a market feasibility study for the employment activity center that identified the market demands for Currituck Station. The master plan was completed in 2017. The proposed text amendment implements the small area plan, market feasibility study and the master plan for Currituck Station.

In summary, the text amendment establishes a new zoning district, Planned Development - Currituck Station District (PD-CS) and associated sub-districts, for lands recognized on the Moyock Mega Site master plan (now Currituck Station) that balances residential, commercial, industrial, and advanced manufacturing land uses. Included with the text amendment is the Currituck Station Pattern Book that establishes the intended character for the district. The pattern book utilizes historical architecture as the foundation to guide development in the district and establish a local identity through building design, massing and external treatments. Although the pattern book is intended to be used in conjunction with the UDO regulations, it is a guide and will not be a regulatory document.

### **Text Amendment Review Standards**

The advisability of amending the text of the UDO is a matter committed to the legislative discretion of the Board of Commissioners and is not controlled by any one factor. In determining whether to adopt or deny the proposed text amendment, the Board of Commissioners may weigh the relevance of and consider whether and the extent to which the proposed text amendment:

1. Is consistent with the goals, objectives, and policies of the Land Use Plan and other applicable county-adopted plans;
2. Is not in conflict with any provision of this Ordinance or the County Code of Ordinances;
3. Is required by changed conditions;
4. Addresses a demonstrated community need;
5. Is consistent with the purpose and intent of the zoning districts in this Ordinance, or would improve compatibility among uses and ensure efficient development within the county;
6. Would result in a logical and orderly development pattern; and
7. Would not result in significantly adverse impacts on the natural environment, including but not limited to water, air, noise, stormwater management, wildlife, vegetation, wetlands, and the natural functioning of the environment.

### **Staff Recommendation**

Staff recommends approval of this request subject to the staff suggested Statement of Consistency and Reasonableness listed in the staff report.

1. Is consistent with the goals, objectives, and policies of the Land Use Plan and other applicable county-adopted plans;

- a. This request is consistent with the goals, objectives, and policies of the Land Use Plan, Moyock Small Area Plan, and the Moyock Mega Site Master Plan. Please reference:
  - LUP policies AG3, HN3, CD2, CD8, WS3, and CW1.
  - MSAP policies CC1, CC2, CC3, ST1, BI2, and Actions FLU2A, CC 2A, CC 3B, BI 3B
  - Moyock Mega Site Master Plan Figure ES-1
2. Is not in conflict with any provision of this Ordinance or the County Code of Ordinances;
  - a. The request is in harmony with the UDO and the County Code of Ordinances.
3. Is required by changed conditions;
  - a. The Moyock Small Area Plan, adopted in 2014, identified this area as an employment activity area.
  - b. The 2016 Feasibility Study served as the guide for potential land use demands.
  - c. The master plan development process was designed to produce a market driven plan responsive to projected demand for a mix of land uses specific to the local market and formed by regional influences.
4. Addresses a demonstrated community need;
  - a. It is intended to establish a long-term vision for a mixed use development for approximately 3,500 acres of land that is strategically positioned to serve as a connective center between Virginia and North Carolina.
5. Is consistent with the purpose and intent of the zoning districts in this Ordinance, or would improve compatibility among uses and ensure efficient development within the county;
  - a. The proposed text amendment establishes the district that implements the master plan for the project area.
6. Would result in a logical and orderly development pattern; and
  - a. The standards are developed to provide a mix of uses and densities needed to sustain the mixed use development.
7. Would not result in significantly adverse impacts on the natural environment, including but not limited to water, air, noise, stormwater management, wildlife, vegetation, wetlands, and the natural functioning of the environment.
  - a. It should have no adverse impacts on the natural environment.

#### **Planning Board Recommendation - January 14, 2020**

Mr. Bass motioned to approve the PB-25 Currituck County's request to amend the Unified Development Ordinance, Chapter 1. General Provisions, Chapter 2. Administration, Chapter 3. Zoning Districts Chapter 4. Use Standards, Chapter 5. Development Standards, Chapter 6. Subdivision Infrastructure Standards, and Chapter 10. Definitions and Measurements for the purpose of implementing the Moyock Mega Site master plan (Currituck Station) and establishing the Planned Development - Currituck Station district and regulations with the inclusion of the following staff recommendations:

- Provide a transition from Center Station to Newtown on the south side that does not split property lines
  - Option 1 - Shift the sub-district line - Charter sub-district to include land (now Newtown) to Lazy Corner Road.
  - ~~Option 2 - Modify the use table for Newtown sub-district~~
  - ~~Option 3 - Make no change at this time and update/amend at rezoning.~~
- Pattern book corrections and images for sub-districts
- Remove the suggested materials for each sub-district
- Provide traditional architecture or building form elevations for Center Station and Charter sub-districts

Mr. Thomas seconded the motion and the motion carried unanimously.

RESULT: RECOMMENDED APPROVAL [UNANIMOUS] Next: 2/3/2020 6:00 PM

AYES: C. Shay Ballance, Chairman, Garry Owens, Vice Chairman, K. Bryan Bass, Board Member, David Doll, Board Member, Juanita S Krause, Board Member, J. Timothy Thomas, Board Member

ABSENT: Anamarie Hilgendorf, Board Member

Donna Voliva, Assistant Planning Director, provided a timeline of earlier discussion, public hearings, presentations, and work sessions for consideration of the Currituck Station text amendment. Ms. Voliva reviewed staff comments expressed by stakeholders at earlier meetings related to areas concerns with overlapping districts and recommended corrections and modifications to the pattern book. Elements of the pattern book and zoning sub-areas were displayed on the overhead during review.

Ms. Voliva presented the supporting policies and consistency statements to support staff's recommendation for approval. She responded to questions from Commissioners related to zoning, particularly related to planned developments.

Chairman White opened the Public Hearing. There were no speakers and the Public Hearing was closed.

Commissioner J. Owen Etheridge moved to approve PB 19-25, Currituck Station, because the request is consistent with the Land Use Plan (LUP), Moyock Small Area Plan (MSAP), and Moyock Mega Site Master Plan, and include option three of the staff comments as the option to be utilized, which is to make no change at Newtown Road overlapping district and amend at rezoning).

- LUP Policy AG3 encourages the county to direct new development to targeted growth areas near existing development identified as full service areas.
- LUP Policy HN3 encourages the county to provide for mixed use developments to promote self-supporting community centers served by centralized water and sewer contemplated for the full service areas.
- LUP Policy CD8 encourages mixed-use developments that are properly planned from the outset, and allows for a compatible mixture of residential and non-residential uses with a pedestrian scale and design.
- LUP Policy CW1 recognizes small area plans and allows for incorporation into the LUP as needed for citizen initiated amendments or county led planning efforts for changing demographic, economic or environmental conditions.
- MSAP supplements the LUP to more specifically address the needs and issue of the study area and establishes a new focus for growth and development.
- MSAP, adopted in 2014, identified this area as an employment activity area.
- MSAP Policy CC1 encourages and fosters development that is compatible with rural atmosphere, transition areas, and a small town, main street feel that is consistent with the vision, policies and future land use plan.
- MSAP Policy CC2 encourages non-residential and mixed use development that enhances the community appearance, promotes human scale and creates a unique sense of place including common themed building materials, forms and site amenities.

- MSAP Policy ST1 promotes establishing an area dedicated to community serving businesses that foster a small town, main street feel.
- MSAP Policy BI2 encourages a well-planned mixed use development with a range of intensities and diverse housing types and carefully located to areas supported by the future land use map and adequately served by infrastructure and county services.
- MSAP Action FLU2A explores establishment of a community center district and associated sub-districts that implement the vision and policies of the plan creating development standards specific to the Moyock study area.
- MSAP Action CC2A is to develop regulations and incentives for non-residential and mixed use development that establish design standards specific to each activity center in the plan.
- MSAP Action CC3B is to amend the UDO to create regulations that enhance public investment into entryways.
- MSAP Action BI3B is to develop regulations or incentives that require large scale residential development that utilizes centralized sewer to include a supporting non-residential component and interconnection to existing businesses.
- Moyock Mega Site Master Plan and Figure ES-1.

And the request is reasonable and in the public interest because:

- The Moyock Small Area Plan adopted in 2014 identified this area as an employment activity area.
- The 2016 Feasibility Study served as the guide for potential land use demands.
- Addresses a demonstrated community need because the master plan development process was designed to produce a market driven plan responsive to projected demand for a mix of land uses specific to the local market and formed by regional influences.
- Addresses a demonstrated community need by implementing the MSAP and Moyock Mega Site Master Plan, a long-term vision for a mixed use development for approximately 3,500 acres of land that is strategically positioned to serve as a connective center between the Commonwealth of Virginia and the State of North Carolina, resulting in logical and orderly development patterns.

Commissioner Payment seconded the motion. The motion carried.

(Clerk's Note: The Currituck Station Text Amendment can be accessed via the June 15, 2020 agenda packet or full minutes posted on the Currituck County website: <http://currituckcountync.iqm2.com/Citizens/Calendar.aspx>)

Before moving to the next item, County Manager, Ben Stikeleather, reported on live streaming issues and announced the meeting video would be posted later on YouTube.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	J. Owen Etheridge, Commissioner
<b>SECONDER:</b>	Mike H. Payment, Vice Chairman
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

## **NEW BUSINESS**

### **A. Consideration of Facility Use-Naval Contractor "Jump" Operations at Currituck County Airport**

Airport Manager, William Nelson, presented Commissioners with a request from the Navy Seals who wish to use the Currituck County Airport for jump operations. He reviewed the operation and the pros and cons of allowing jump operations to take place. Mr. Nelson said fuel sales would increase, but jump operations can inhibit jet traffic and he does not believe the airport has adequate space to accommodate the request because of the limited airport operations that would result.

Johnny Riddle, active duty Naval Special Development Group, spoke to the Board in support of the request. He described the jump operations and expressed the need initially for twelve consecutive days in July. Commissioners and staff discussed finding an alternate jump location, with shuttling back to the airport for takeoffs and landing. Mr. Riddle said they would need access Monday through Friday and would want to use the airport long term. Aircraft used and operational processes were described.

After discussion of the negative impacts to the airport and airport traffic, Commissioners were not comfortable and chose not to approve jump operations at the airport.

Chairman White called a brief recess at 10:41 PM. The meeting reconvened at 10:48 PM.

### **B) Consent Agenda**

Commissioner Mary Etheridge moved for approval of the Consent Agenda. The motion was seconded by Commissioner Jarvis. The motion carried.

Following the vote, Chairman White recessed the regular meeting of the Board of Commissioners to hold a Special Meeting of the Tourism Development Authority.

**RESULT:**           **APPROVED [UNANIMOUS]**  
**MOVER:**           Mary "Kitty" Etheridge, Commissioner  
**SECONDER:**       Selina S. Jarvis, Commissioner  
**AYES:**             Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

**1) Approval Of Minutes for June 1, 2020**

**1. Minutes for June 1, 2020**

**2. Independent Mailing Systems Lease Agreement-Mail Processing Equipment**

**3. Amended-Budget Amendments**

		Debit	Credit
<u>Account Number</u>	<u>Account Description</u>	Decrease Revenue or Increase Expense	Increase Revenue or Decrease Expense
12541-554005	Insurance - Lower Currituck VFD	\$ 46,275	
12541-588000	Contingency		\$ 2,588
12390-499900	Appropriated Fund Balance		\$ 43,687
		<u>\$ 46,275</u>	<u>\$ 46,275</u>
<b>Explanation:</b>	Fire Services (12541) - Increase appropriations for insurance renewal for Lower Currituck VFD. Renewal date is June 16 and funds were appropriated for July 1 in the next fiscal year.		
<b>Net Budget Effect:</b>	Fire Services Fund (12) - Increased by \$43,687.		

		Debit	Credit
<u>Account Number</u>	<u>Account Description</u>	<u>Decrease Revenue or Increase Expense</u>	<u>Increase Revenue or Decrease Expense</u>
30850-506000	Insurance Expense	\$ 58,000	
30380-481000	Investment Earnings		\$ 24,000
30390-499900	Fund Balance Appropriated		\$ 34,000
		<u>\$ 58,000</u>	<u>\$ 58,000</u>
<b>Explanation:</b>	Post-employment Benefits (30850) - Increase appropriations for retiree insurance expense due to increase in number of retirees this fiscal year.		
<b>Net Budget Effect:</b>	Post-employment Benefits Fund (30) - Increased by \$58,000		

**SPECIAL MEETING OF THE TOURISM DEVELOPMENT AUTHORITY**

The Currituck County Board of Commissioners held a Special Meeting during a recess of the 6:00 PM regular meeting of the Board of Commissioners to sit as the Tourism Development Authority. The meeting was held in the Sanderlin Auditorium at Currituck County Cooperative Education Center, 120 Community Way, Barco, North Carolina, for the purpose of holding a Public Hearing and possibly taking action to adopt the TDA annual budget for Fiscal Year 2020-2021.

The meeting was called to order at 10:48 PM.

**Public Hearing and Possible Adoption of the Tourism Development Authority Budget for Fiscal Year Ending June 30, 2021**

Chairman White opened the Public Hearing. There were no speakers and the Public Hearing was closed.

Chairman White moved to take no action and bring the item back to the June 22, 2020, meeting of the TDA for possible adoption. Commissioner Payment seconded the motion and the motion carried.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Bob White, Chairman
<b>SECONDER:</b>	Mike H. Payment, Vice Chairman
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

**ADJOURN**

There was no further business and Commissioner McCord moved to adjourn. Commissioner Jarvis seconded the motion. The motion carried and the meeting of the Tourism Development Authority adjourned at 10:49 PM.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Kevin E. McCord, Commissioner
<b>SECONDER:</b>	Selina S. Jarvis, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

### **SPECIAL MEETING OF THE OCEAN SANDS WATER & SEWER DISTRICT BOARD**

The Currituck County Board of Commissioners held a Special Meeting during a recess of the 6:00 PM regular meeting of the Board of Commissioners to sit as the Ocean Sands Water and Sewer District Board. The meeting was held in the Sanderlin Auditorium at Currituck County Cooperative Education Center, 120 Community Way, Barco, North Carolina, for the purpose of holding a Public Hearing and possibly taking action to adopt the Ocean Sands Water and Sewer District annual budget for Fiscal Year 2020-2021.

The meeting was called to order at 10:49 PM.

#### **Public Hearing and Possible Adoption of the Ocean Sands Water & Sewer District Budget for Fiscal Year Ending June 30, 2021**

Chairman White opened the Public Hearing. There were no speakers and the Public Hearing was closed.

Chairman White moved to take no action and bring the item back for consideration and possible adoption at the June 22, 2020 meeting of the Ocean Sands Water & Sewer District Board. The motion was seconded by Commissioner McCord. The motion carried.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Bob White, Chairman
<b>SECONDER:</b>	Kevin E. McCord, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

### **ADJOURN**

There was no further business and Commissioner Mary Etheridge moved to adjourn. The motion was seconded by Commissioner Jarvis. The motion carried and the meeting of the Ocean Sands Water and Sewer District Board adjourned at 10:51 PM.

### **CLOSED SESSION**

Following the Special Meetings of the Board of Commissioners, Chairman White reconvened the regular meeting of the Board of Commissioners at 10:51 PM.

- 1. Amended-Closed Session Pursuant to G.S. 143-318.11(a)(3) to consult with the County Attorney and preserve the attorney-client privilege in the matters entitled Currituck County v. Letendre; and Currituck County v. Cossa and Paradise Homes**

Chairman White moved the Board into Closed Session pursuant to G.S. 143-318.11(a)(3) to preserve the attorney-client privilege in the matters entitled Currituck County v. Letendre; and Currituck County v. Cossa and Paradise Homes.

## **ADJOURN**

### **Motion to Adjourn Meeting**

The Board of Commissioners returned from Closed Session and had no further business. Commissioner Payment moved to adjourn. The motion was seconded by Commissioner Beaumont. The motion carried and regular meeting of the Board of Commissioners adjourned at 10:57 PM.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Mike H. Payment, Vice Chairman
<b>SECONDER:</b>	Paul M. Beaumont, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

# STORMWATER DRAINAGE ANALYSIS FOR CURRITUCK COUNTY

JUNE 5, 2020



PREPARED BY:



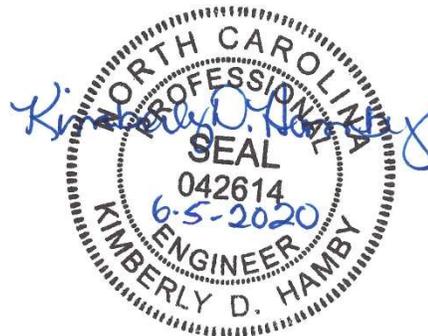
1805 West City Drive, Unit E

Elizabeth City, NC 27909

252.621.5030

License No. C-1652

[www.timmons.com](http://www.timmons.com)



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

## Stormwater Drainage Analysis for Currituck County

The purpose of this study is to analyze the impact of a potential change to the Currituck County Stormwater Manual. The Manual currently requires that residential subdivisions provide stormwater attenuation that limits the post development peak runoff during a 10-year, 24-hour statistical storm to the same level that would be produced by the property during a 2-year, 24-hour storm if the site were undeveloped and wooded. Due to ongoing flooding problems in the area, the County is considering increasing the requirement to require limiting a 12-inch, 24-hour storm to the same 2-year peak runoff rate. Based on the NOAA Atlas 14, a 12-inch, 24-hour storm in the Moyock area would be nearly equivalent to a 500-year statistical storm.

Timmons Group has prepared stormwater analyses on two subdivisions to help illustrate how the proposed change could impact future residential development design. Both subdivisions were designed and permitted under the current Manual requirements. The County Engineer provided Timmons Group with digital copies of the approved design plans and drainage narratives for both developments. The following information describes our methods and limitations and reports the results.

### Froggy Meadows Subdivision

Froggy Meadows is an 18-lot residential subdivision in Moyock that was designed by Quible & Associates, PC of Powells Point, NC in 2019. Minimum lot size is 34,000 sf. The site is located on the east side of Puddin Ridge Road adjacent to the southern edge of the Quail Run subdivision. Prior to development, the majority of the site drained by sheet flow from south to north to the existing ditch between the site and Quail Run. The ditch is approximately 4' deep and drains under Puddin Ridge Road and into the upper reach of Moyock Run. A small area drains into the NCDOT ditch along the right-of-way of Puddin Ridge Road.

The project was studied as three drainage areas:

Drainage Area DA-1 is the portion of the site that drained to the roadside ditch in the pre-development condition. Runoff was designed to drain to an infiltration basin located near the entrance.

Drainage Areas DA-2 & DA-4 have been studied as a single drainage area that represents the western portion of the site that drains to the ditch adjacent to Quail Run. Drainage Area DA-2 is the portion on the south side of the subdivision road that drains to Pond-2. Pond-2 is connected directly to Pond-4 on the north side of the road. The outfall structure in Pond #4 controls the discharge from Drainage Areas DA-2 and DA-4 allowing them to be treated as a single drainage area.

Drainage Area DA-3 is at the west end of the site. Pond #3 receives the runoff from this drainage area and discharges into the same ditch as Pond-2 & 4.

The drainage analysis for this project was developed using Hydraflow Hydrographs Extension for Autodesk Civil 3D 2019 by Autodesk, Inc. v2020. Timmons Group utilized the same software for analysis. Data provided in the report titled "Stormwater Narrative – Froggy Meadows Subdivision" dated 5/23/2019 was used to replicate the model.

Pre-development peak runoff rates for the 2-yr Storm based on the referenced Stormwater Narrative:

DA-1 (Entrance) = 2.214 cfs

DA-3 (Rear-Side) = 5.951 cfs

DA-2 & DA-4 (Combined) = 23.31 cfs

Total Pre-development Peak Discharge = 31.475 cfs

### Hydrograph Return Period Recap

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2020

Hyd. No.	Hydrograph type (origin)	Inflow hyd(s)	Peak Outflow (cfs)								Hydrograph Description
			1-yr	2-yr	3-yr	5-yr	10-yr	25-yr	50-yr	100-yr	
7	SCS Runoff	---	---	9.249	---	---	15.20	18.33	21.71	25.43	Froggy Meadows - DA 3 - Rear Side - <i>POST</i>
8	SCS Runoff	---	---	5.951	---	---	11.54	14.62	18.00	21.75	Froggy Meadows - DA 3 - Rear Side - <i>PRE</i>
9	SCS Runoff	---	---	3.209	---	---	5.373	6.515	7.747	9.101	Froggy Meadows - DA 1 - Entrance - <i>POST</i>
10	SCS Runoff	---	---	2.214	---	---	4.274	5.406	6.646	8.021	Froggy Meadows - DA 1 - Entrance - <i>PRE</i>
11	Reservoir(i)	5, 3	---	7.129	---	---	18.12	31.79	46.84	62.06	DA 2 & 4 Pond Routing
12	Reservoir	7	---	0.388	---	---	5.332	8.551	12.74	17.51	DA 3 Pond Routing
13	Combine	4, 6,	---	23.31	---	---	45.15	57.19	70.38	85.10	DA 2 & DA 4 Pre Const Combined
21	Combine	3, 5,	---	34.90	---	---	56.68	68.12	80.47	94.03	DA 2 & DA 4 Post Const

Post-development runoff rates from the 10-year storm peak at 47.3658 cfs. The runoff is routed through ponds to control the runoff rates. Post-development peak discharge rates from the ponds for the 10-year storm based on the referenced Stormwater Narrative:

DA-1: The design engineer did not model this basin in Hydraflow Hydrographs, but used calculations to determine that there is no discharge from DA-1 during a 10-yr storm due to the use of infiltration sized to handle the 10-year storm.

Pond-3 Routing = 5.332 cfs

Pond 2 & 4 Routing = 18.12 cfs

Total Post-development Peak Discharge = 23.452 cfs

The result is an overall reduction in peak flow of 8.02 cfs.

11	Reservoir(i)	5, 3	---	7.129	---	---	18.12	31.79	46.84	62.06	DA 2 & 4 Pond Routing
12	Reservoir	7	---	0.388	---	---	5.332	8.551	12.74	17.51	DA 3 Pond Routing

Timmons Group prepared a Hydraflow Hydrograph stormwater model using the data available in the Stormwater Narrative dated 5/23/2019. For Drainage Areas 1 & 3, we were able to perfectly match the runoff rates from the drainage areas. For Drainage Areas 2 & 4, we were lacking some data necessary to match the referenced narrative. The design engineer entered data for the two drainage areas separately, but only published the data and results for the combined drainage area. To calibrate the model, we increased the Curve Number of the combined area from the published 87 to 88.5. This slightly increased the runoff rate to bring the model into a reasonable tolerance while showing a minor increase to be conservative.

DA-2 & DA-4: Design Results = 56.68 cfs

Timmons Results = 57.31 cfs

These runoff rates were then routed through the design ponds.

We developed an infiltration basin for DA 1 that does result in the runoff from the 10-yr storm infiltrates into the soil and does not discharge.

We developed the pond for DA 3 based on stage-storage tables published in the narrative. Our results varied slightly but are within an acceptable tolerance.

Pond-3: Design Discharge = 5.332 cfs

Timmons Discharge = 5.364 cfs

For Pond-2 & 4 combined, we again faced the difficulty of not having data that provided a breakdown for the two drainage areas. In the Narrative, the two ponds were treated as one because they are connected by piping and have a single outlet control structure located in Pond 4. Since we did not have all of the data to separate the two, some variation in results is to be expected and is acceptable. Although the runoff into the pond slightly exceeded the design, the peak flow out of the pond shows a minimal reduced result.

Pond-2 & 4: Design Discharge = 18.12 cfs

Timmons Discharge = 17.33 cfs

Once the 10-year model was at an acceptable calibration tolerance, we prepared a new model to address the 12-inch, 24-hour rainfall as requested by Currituck County. Please note that in Hydraflow Hydrographs, the program does not allow us to name the storm. We set up the storm named 100-yr to meet the rates of a 12-inch storm for this area. We used the increased volume of rainfall to approximate the increase needed in pond areas and began trial runs increasing or decreasing pond sizes as necessary until the results were similar to those of the 10-year storm in the design model, thus insuring that the 12-year discharge would remain below the 2-year pre-development rates.

For Pond 1, the bottom area at elevation 6.80 was increased from 8,455 sf to 15,849 sf and the area at the top of bank was increased from 10,664 sf to 20,000 sf. The maximum water surface elevation in the basin increased from 7.62' to 7.81'.

For Pond 3, the water surface area was increased from 17,645 sf to 49,640sf – an increase of 181%. The maximum water surface elevation in the basin increased from 7.84' to 7.85'.

For Pond-2 & 4 combined, the water surface area was increase from 49,948 sf to 124,468 sf – an increase of 149%. The maximum water surface elevation in the basin increased from 8.14' to 8.68'.

These changes increase the total area of excavation from 106,023 sf (2.43 ac) to 220,650 sf (5.06 ac). This increase will eliminate at least 2 of the lots in the development and will threaten a third.

(Printouts from the Hydraflow Hydrographs models and a map showing impact are attached in the appendix of this report.)

#### Countryside Estates Subdivision

Countryside Estates Subdivision is a 62-lot residential subdivision in Moyock that was designed by MSA, PC of Virginia Beach, VA in 2016 & 2017. Minimum lot size is 20,000 sf. The site is located on the west side of US Hwy 168 adjacent to the south side of the North Point subdivision. The site drains to a ditch located along the southern property line by sheet flow and by existing ditches that run north to south through the property. The ditches provide the outlet from the ditch between North Point and the Countryside Estates subdivision to the ditch along the site's southern property line. The ditch along the southern property line drains easterly, crosses under Hwy 168 and ultimately drains to Northwest River.

A drainage model was performed to verify that Countryside Estates could be constructed and not negatively impact the North Point subdivision. This model was separate from the subdivision model and is not considered in the Timmons analysis; however, the area draining through the project is included in the model for the subdivision. The subdivision drainage model was performed using a 2002 version of a program call Interconnected Channel and Pond Routing Model. Based on the data provided in the published Stormwater Calculations, we decided to use PCSWMM 2019 that we use for dynamic stormwater modeling.

The design analysis for the pre-development conditions applied the 2-year, 24-hour storm to the project site but applied the 10-year, 24-hour storm to the developed area to the north. This allows the reduced pre-development flow to be applied to the proposed development only. The majority of the subdivision and off-site runoff drains to the ditch along the south side of the property. The results of their analysis show a peak discharge off site in the pre-development state under these conditions to be 51.2 cfs and a peak discharge of 48.9 cfs for post-development. For the small portion of the site that discharges into the Hwy 168 right-of-way, the results showed a pre-development peak of 0.3 cfs and a post development peak of 0.8 cfs. The slight increase into the r/w is acceptable as the total of the two discharge points does provide a reduction.

In the post-developed model, there are 7 major drainage areas. Drainage areas Bypass 1 and Bypass 2 represent the land north of the development that drains through the site in a piped system that is named with a G series in the model. The G series pipes drain to the ditch along the southern property line. DA 1 is at the west end of the site and drains to Pond 1, DA 2 drains to Pond 2 and DA 3 drains to Pond 3. The three ponds are interconnected with B and D series pipes. Pond B discharges to the outlet ditch via the E series pipes and is equipped with a long weir along the south edge to allow it to overflow into the ditch if it ever exceeds elevation 11.5. DA 5 is at the entrance and drains to Pond 5 which overflows to the pipe Series F to Pond 4. DA 4 drains to Pond 4 which discharges through Series F pipes to the outfall ditch along the southern property line. Below are the results as published in the document titled "Erosion & Sediment Control Plan for Countryside Estates" dated 8/19/16.

Outfall-1 (Southern Outfall):

Pre-Dev - Onsite 2-yr, 24-hr Peak Runoff (Forested Condition) + Offsite 10-yr, 24-hr Peak Runoff (Developed Conditions) =  $Q_{2-Pre} + Q_{10-Pre (Offsite)} = 51.2$  CFS

Post-Dev – Onsite and Offsite 10-yr, 24-hr Peak Runoff (Developed Conditions) =  $Q_{10-Post} = 48.9$  CFS

Outfall-2 (Northeastern Outfall):

Pre-Dev - Onsite 2-yr, 24-hr Peak Runoff (Forested Condition) =  $Q_{2-Pre} = 0.3$  CFS

Post-Dev – Onsite 10-yr, 24-hr Peak Runoff (Developed Conditions) =  $Q_{10-Post} = 0.8$  CFS

Entire Project:

Pre-Dev - Onsite 2-yr, 24-hr Peak Runoff (Forested Condition) + Offsite 10-yr, 24-hr Peak Runoff (Developed Conditions) =  $Q_{2-Pre} + Q_{10-Pre (Offsite)} = 51.5$  CFS

Post-Dev – Onsite and Offsite 10-yr, 24-hr Peak Runoff (Developed Conditions) =  $Q_{10-Post} = 49.7$  CFS

To set up our PCSWMM Stormwater Model for this project, we entered data for all of the drainage areas, pipes and ponds based on the data in the Stormwater Calculations for the statistical 10-year, 24-hour storm at a total rainfall depth of 6 inches. We hoped that our results would be similar to those in the published report; however, our model showed a peak flow that was about 25% higher than the flows previously published. There are always variations in results when using different programs and even different versions of the same program. We did attempt to calibrate our model by making minor changes to different parts of the data. We were able to achieve a similar result at the outfall, but it required that we make substantial reductions in Curve Numbers for the drainage areas to reduce runoff and substantial increases in the N values for pipes to slow the discharge through the pipes. The changes required to reduce the flow to replicate the design flows were such that they would compromise the accuracy of the model. Therefore, for Country Meadows we had to base the results on a comparison of the 10-year model we created and the 12-inch model. Since the original design limited the 10-year storm to the appropriate levels, the comparison we used produces acceptable results for showing the increased pond area needed to restrict peak flow from the subdivision from a 12-inch storm.

The point of comparison in the above referenced report is the discharge out of a storage called G8-H1-DITCH that represents the large ditch along the southern property line that stores stormwater before discharging to the outfall through two 30" pipes (H1-H2). The peak discharge from this location was calculated by the design engineer to be 48.87 cfs. Our model indicates flow rates in the 30" pipes. Our peak flow rate in H1-H2 is 63.21 cfs. Since this flow rate includes runoff coming from off-site, we compared the peak flows in the pipes downstream from the pond discharges before they connect with the off-site stormwater passing through the subdivision. The 10-year results of our model are 25.53 cfs from Pond 3 (Conduit E2-E3) and 12.9 cfs from Pond 4 (Conduit F16-F17) for a total of 38.43 cfs.

We used our 10-year storm model as a basis for a 12-inch model where we increased the rainfall and began adjusting pond sizes. We found that we could not increase the ponds enough to hold back the 12-inch runoff without also adding control to the pond discharge. By design and in the 10-year models, the ponds discharge through 36" culverts. To limit the 12-inch storm to less than the peak we calculated in the 10-year model, the discharge pipe out of Pond 3 was reduced to an 18" culvert and the discharge pipe out of Pond 4 was reduced to a 15" culvert. These pipe sizes reduced the flow more than necessary, but one pipe size up was not sufficient to limit the flow. Adding this control to the ponds did cause a water surface elevation rise, but the rise is still contained within the banks of the ponds. The charts below show the changes in pond sizes and water surface elevations.

POND SURFACE AREA

Pond #	10-yr surface area(sf)	12-inch surface area (sf)	% increase in size
1	44301	106,959	141%
2	29664	81,958	176%
3	51052	111,691	119%
4	45259	101,571	124%
5	2047	4500	120%

POND PEAK WATER SURFACE ELEVATION

Pond #	10-yr Peak Water Surface Elevation	12-inch Peak Water Surface Elevation	Increase in elevation
1	10.69	11.29	0.60'
2	10.64	11.26	0.62'
3	10.35	11.1	0.52'
4	10.58	11.15	0.57'
5	11.67	12.63	0.96'

The increased pond sizes in this subdivision appears to reduce the number of lots by at least 10. Reconfiguration of the subdivision would be required to determine the actual number of lots that would be lost.

Summary

Based on the results we have for Countryside Estates and Froggy Meadows, if the Manual is revised to require 12-inch storm peak runoff rates to the limits of a 2-year storm from wooded land, detention pond sizes will increase by 2 or 3 times or more. The impacts to each potential development will vary depending upon soil types, seasonal high water tables and conditions at the available drainage outlets. These factors, along with lot size and coverage, will vary the impact to the number of lots, but this study indicates that lot yields will drop by approximately 15%.

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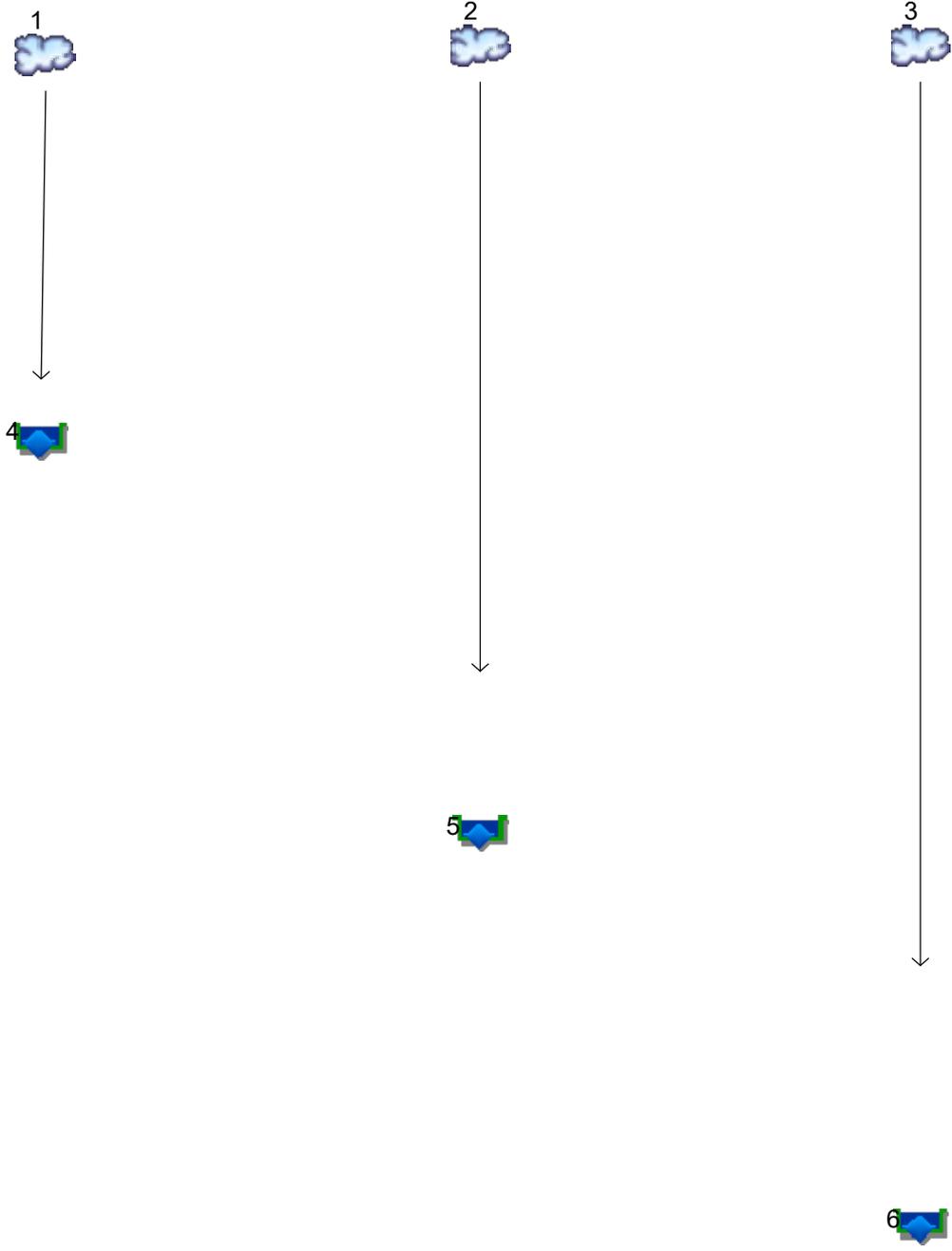
# Appendix A

## Hydraflow Reports for Froggy Meadows

Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# Watershed Model Schematic

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# Hydrograph Summary Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

Hyd. No.	Hydrograph type (origin)	Peak flow (cfs)	Time interval (min)	Time to Peak (min)	Hyd. volume (cuft)	Inflow hyd(s)	Maximum elevation (ft)	Total strge used (cuft)	Hydrograph Description
1	SCS Runoff	5.373	2	724	16,602	----	----	----	DA 1
2	SCS Runoff	15.20	2	736	77,498	----	----	----	DA 3
3	SCS Runoff	57.31	2	734	279,986	----	----	----	DA 2 & 4
4	Reservoir	0.000	2	648	0	1	7.62	7,517	Pond 1 Rte
5	Reservoir	5.364	2	764	76,158	2	7.84	90,410	Pond 3 Rte
6	Reservoir	17.33	2	764	271,214	3	8.14	275,396	Pond 2 & 4 Rte
Hydraflow.gpw					Return Period: 10 Year			Friday, 06 / 5 / 2020	

# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

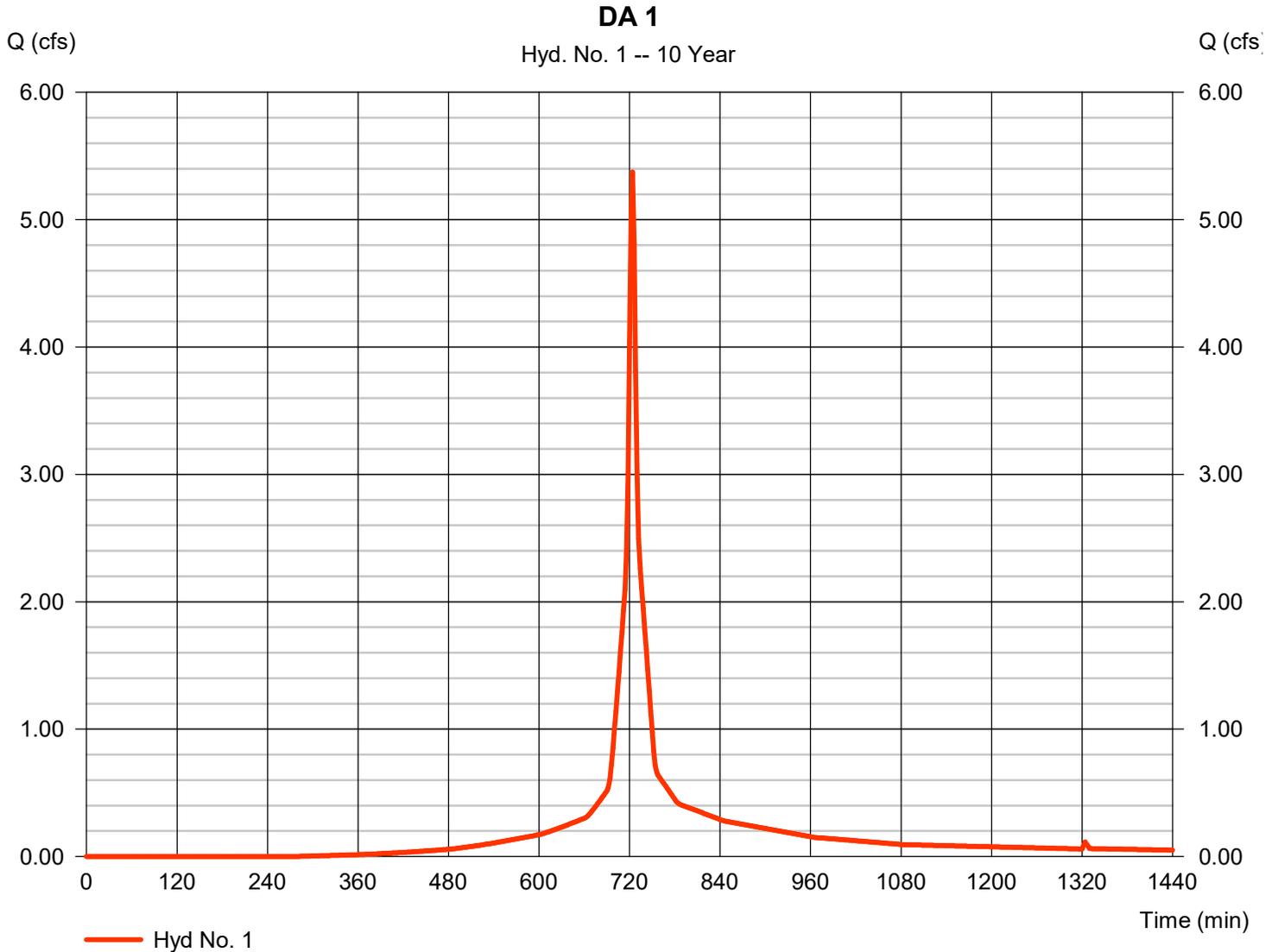
Friday, 06 / 5 / 2020

## Hyd. No. 1

DA 1

Hydrograph type	= SCS Runoff	Peak discharge	= 5.373 cfs
Storm frequency	= 10 yrs	Time to peak	= 724 min
Time interval	= 2 min	Hyd. volume	= 16,602 cuft
Drainage area	= 1.080 ac	Curve number	= 87*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= User	Time of conc. (Tc)	= 6.00 min
Total precip.	= 6.00 in	Distribution	= Type III
Storm duration	= 24 hrs	Shape factor	= 484

\* Composite (Area/CN) =  $[(0.850 \times 80) + (0.230 \times 98)] / 1.080$



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# Hydrograph Report

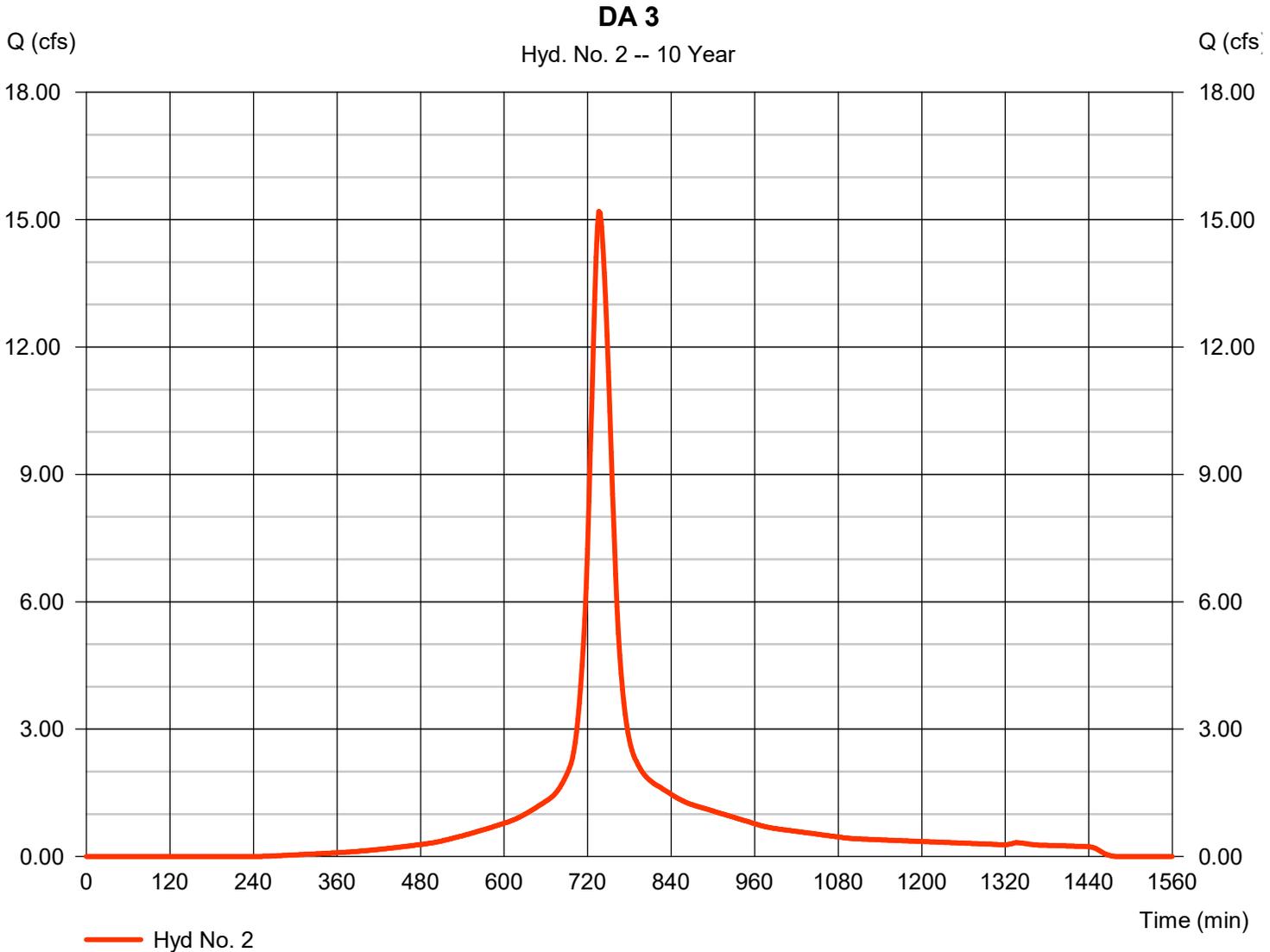
Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

Friday, 06 / 5 / 2020

## Hyd. No. 2

DA 3

Hydrograph type	= SCS Runoff	Peak discharge	= 15.20 cfs
Storm frequency	= 10 yrs	Time to peak	= 736 min
Time interval	= 2 min	Hyd. volume	= 77,498 cuft
Drainage area	= 4.580 ac	Curve number	= 89
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= User	Time of conc. (Tc)	= 26.40 min
Total precip.	= 6.00 in	Distribution	= Type III
Storm duration	= 24 hrs	Shape factor	= 484



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

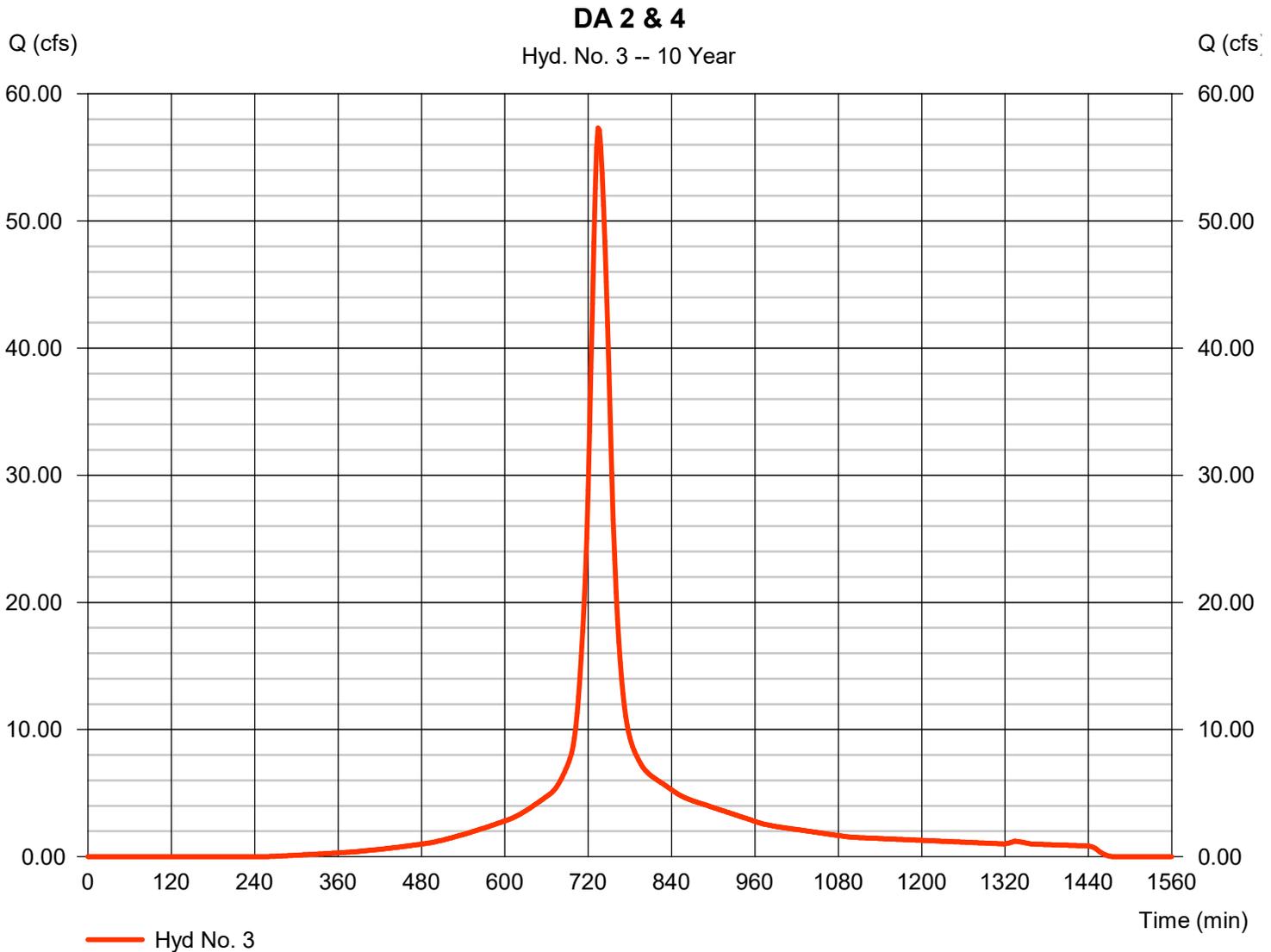
# Hydrograph Report

## Hyd. No. 3

DA 2 & 4

Hydrograph type = SCS Runoff  
Storm frequency = 10 yrs  
Time interval = 2 min  
Drainage area = 16.190 ac  
Basin Slope = 0.0 %  
Tc method = User  
Total precip. = 6.00 in  
Storm duration = 24 hrs

Peak discharge = 57.31 cfs  
Time to peak = 734 min  
Hyd. volume = 279,986 cuft  
Curve number = 88.5  
Hydraulic length = 0 ft  
Time of conc. (Tc) = 22.00 min  
Distribution = Type III  
Shape factor = 484



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

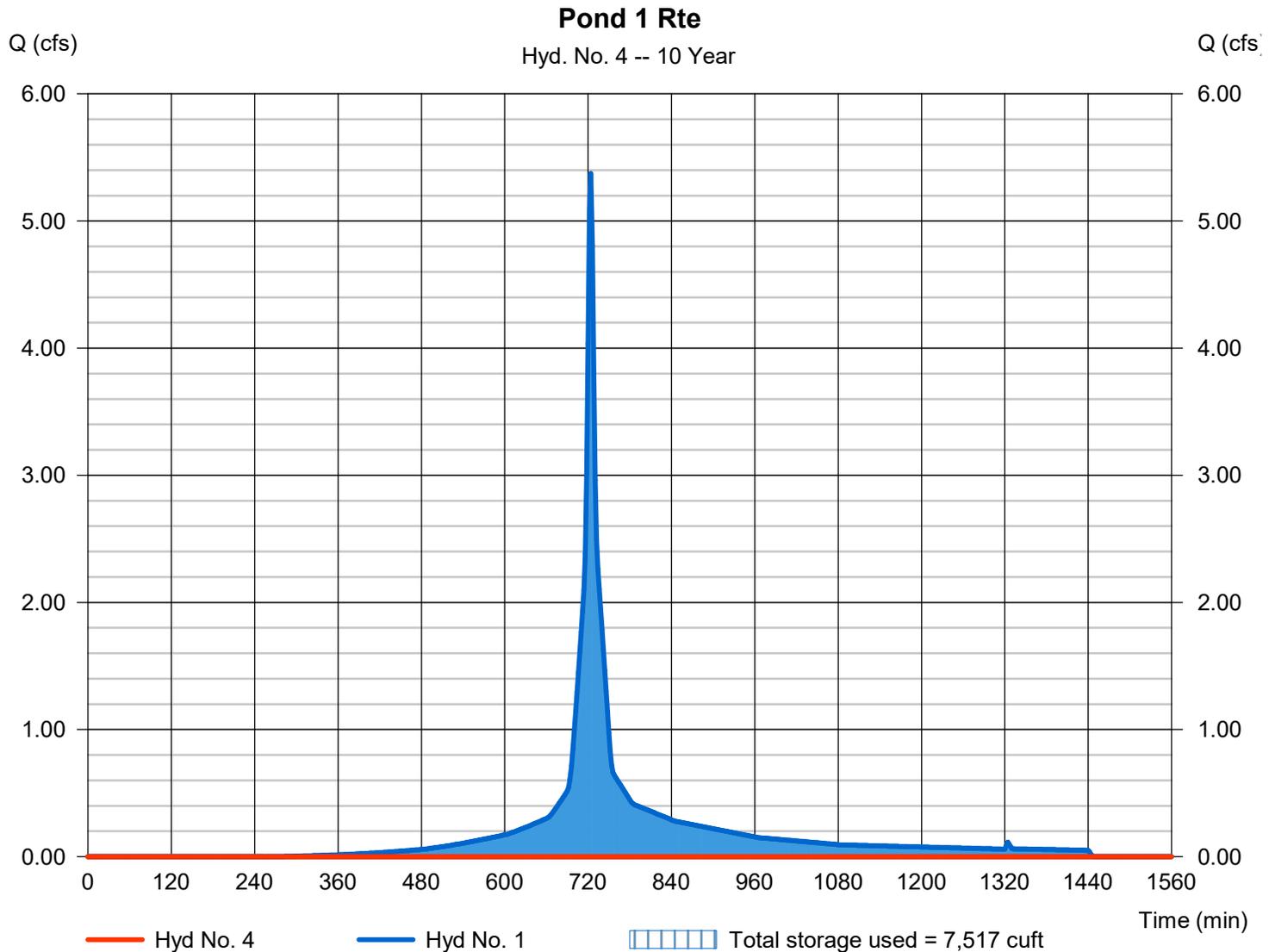
# Hydrograph Report

## Hyd. No. 4

Pond 1 Rte

Hydrograph type	= Reservoir	Peak discharge	= 0.000 cfs
Storm frequency	= 10 yrs	Time to peak	= 648 min
Time interval	= 2 min	Hyd. volume	= 0 cuft
Inflow hyd. No.	= 1 - DA 1	Max. Elevation	= 7.62 ft
Reservoir name	= Infiltration Basin	Max. Storage	= 7,517 cuft

Storage Indication method used. Exfiltration extracted from Outflow.



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

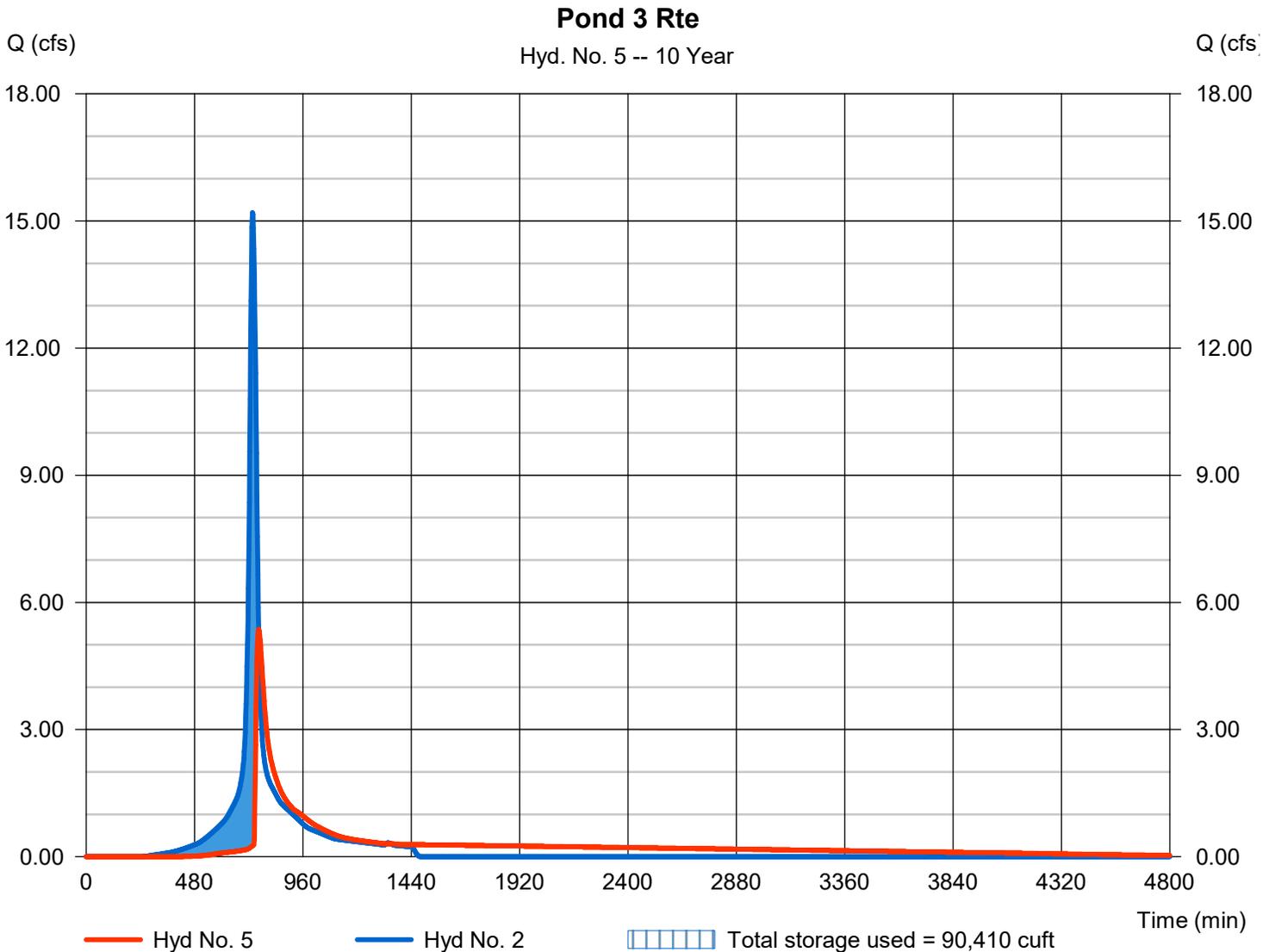
Friday, 06 / 5 / 2020

## Hyd. No. 5

Pond 3 Rte

Hydrograph type	= Reservoir	Peak discharge	= 5.364 cfs
Storm frequency	= 10 yrs	Time to peak	= 764 min
Time interval	= 2 min	Hyd. volume	= 76,158 cuft
Inflow hyd. No.	= 2 - DA 3	Max. Elevation	= 7.84 ft
Reservoir name	= Wet Basin 3	Max. Storage	= 90,410 cuft

Storage Indication method used. Wet pond routing start elevation = 5.80 ft.



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

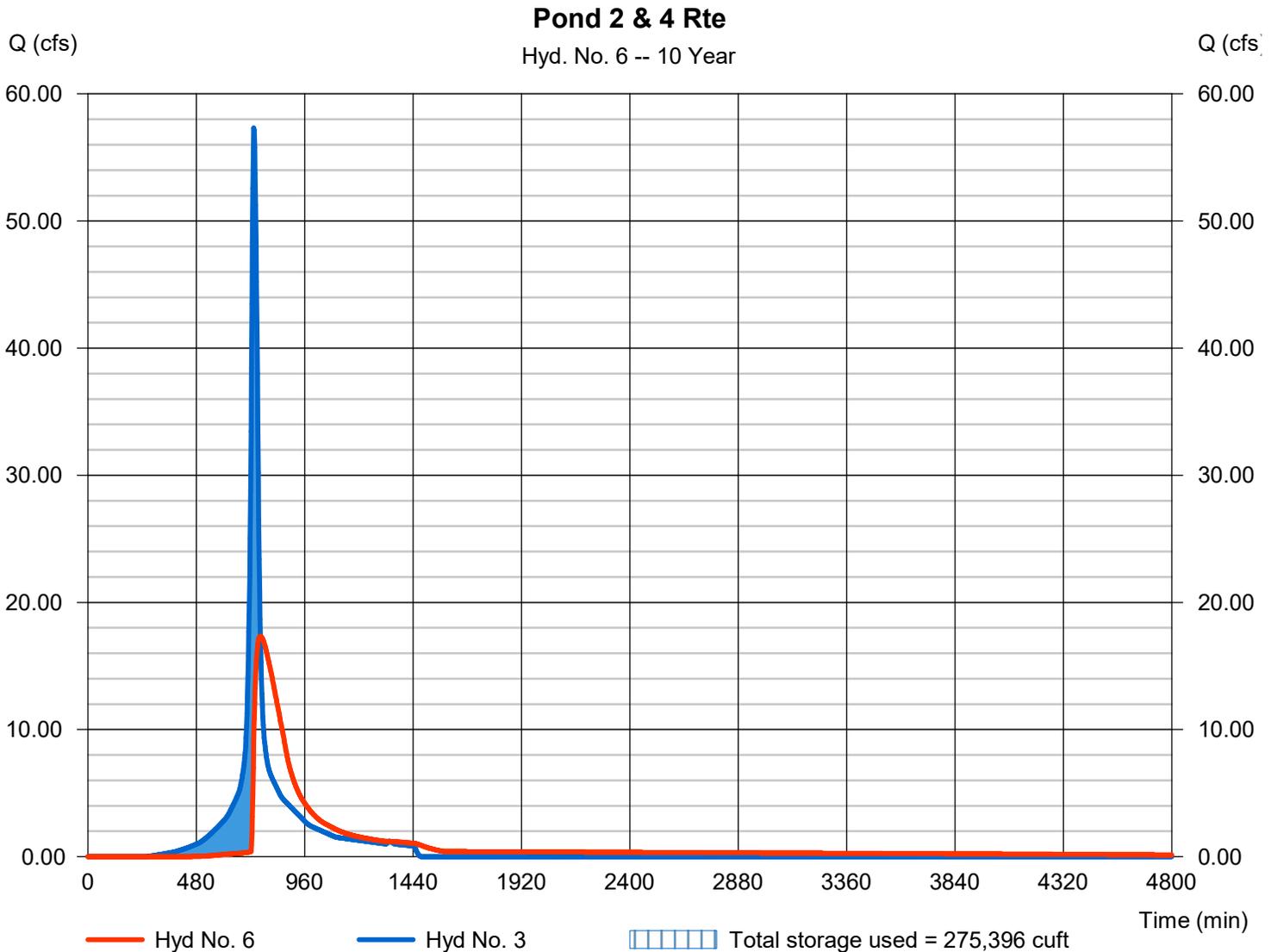
Friday, 06 / 5 / 2020

## Hyd. No. 6

Pond 2 & 4 Rte

Hydrograph type	= Reservoir	Peak discharge	= 17.33 cfs
Storm frequency	= 10 yrs	Time to peak	= 764 min
Time interval	= 2 min	Hyd. volume	= 271,214 cuft
Inflow hyd. No.	= 3 - DA 2 & 4	Max. Elevation	= 8.14 ft
Reservoir name	= Wet Basin 2 & 4	Max. Storage	= 275,396 cuft

Storage Indication method used. Wet pond routing start elevation = 5.80 ft.



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# Hydraflow Rainfall Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

Friday, 06 / 5 / 2020

Return Period (Yrs)	Intensity-Duration-Frequency Equation Coefficients (FHA)			
	B	D	E	(N/A)
1	0.0000	0.0000	0.0000	-----
2	69.8703	13.1000	0.8658	-----
3	0.0000	0.0000	0.0000	-----
5	79.2597	14.6000	0.8369	-----
10	88.2351	15.5000	0.8279	-----
25	102.6072	16.5000	0.8217	-----
50	114.8193	17.2000	0.8199	-----
100	127.1596	17.8000	0.8186	-----

File name: SampleFHA.idf

$$\text{Intensity} = B / (T_c + D)^E$$

Return Period (Yrs)	Intensity Values (in/hr)											
	5 min	10	15	20	25	30	35	40	45	50	55	60
1	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
2	5.69	4.61	3.89	3.38	2.99	2.69	2.44	2.24	2.07	1.93	1.81	1.70
3	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
5	6.57	5.43	4.65	4.08	3.65	3.30	3.02	2.79	2.59	2.42	2.27	2.15
10	7.24	6.04	5.21	4.59	4.12	3.74	3.43	3.17	2.95	2.77	2.60	2.46
25	8.25	6.95	6.03	5.34	4.80	4.38	4.02	3.73	3.48	3.26	3.07	2.91
50	9.04	7.65	6.66	5.92	5.34	4.87	4.49	4.16	3.88	3.65	3.44	3.25
100	9.83	8.36	7.30	6.50	5.87	5.36	4.94	4.59	4.29	4.03	3.80	3.60

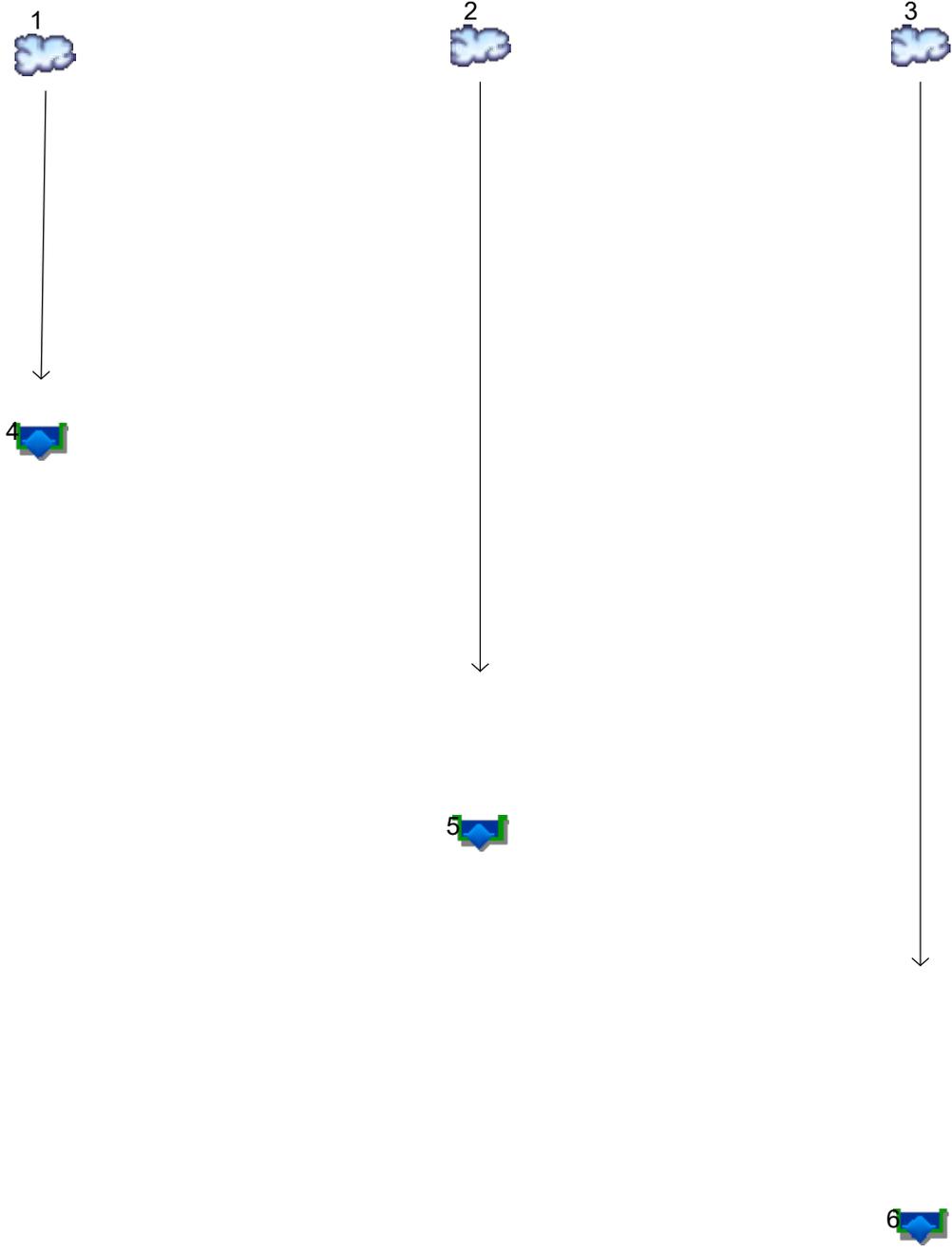
T<sub>c</sub> = time in minutes. Values may exceed 60.

Precip. file name: Sample.pcp

Storm Distribution	Rainfall Precipitation Table (in)							
	1-yr	2-yr	3-yr	5-yr	10-yr	25-yr	50-yr	100-yr
SCS 24-hour	0.00	3.74	0.00	3.30	6.00	5.77	6.80	12.00
SCS 6-Hr	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Huff-1st	0.00	0.00	0.00	2.75	0.00	5.38	6.50	0.00
Huff-2nd	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Huff-3rd	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Huff-4th	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Huff-Indy	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Custom	0.00	0.00	0.00	2.80	0.00	5.25	6.00	0.00

# Watershed Model Schematic

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# Hydrograph Summary Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

Hyd. No.	Hydrograph type (origin)	Peak flow (cfs)	Time interval (min)	Time to Peak (min)	Hyd. volume (cuft)	Inflow hyd(s)	Maximum elevation (ft)	Total strge used (cuft)	Hydrograph Description
1	SCS Runoff	11.77	2	724	38,136	----	----	----	DA 1
2	SCS Runoff	32.75	2	736	174,040	----	----	----	DA 3
3	SCS Runoff	124.16	2	734	632,298	----	----	----	DA 2 & 4
4	Reservoir	0.000	2	610	0	1	7.81	17,479	Pond 1 Rte
5	Reservoir	5.580	2	780	140,448	2	7.85	165,992	Pond 3 Rte
6	Reservoir	20.42	2	778	556,900	3	8.68	553,556	Pond 2 & 4 Rte
Hydraflow -12in.gpw					Return Period: 100 Year			Friday, 06 / 5 / 2020	

# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

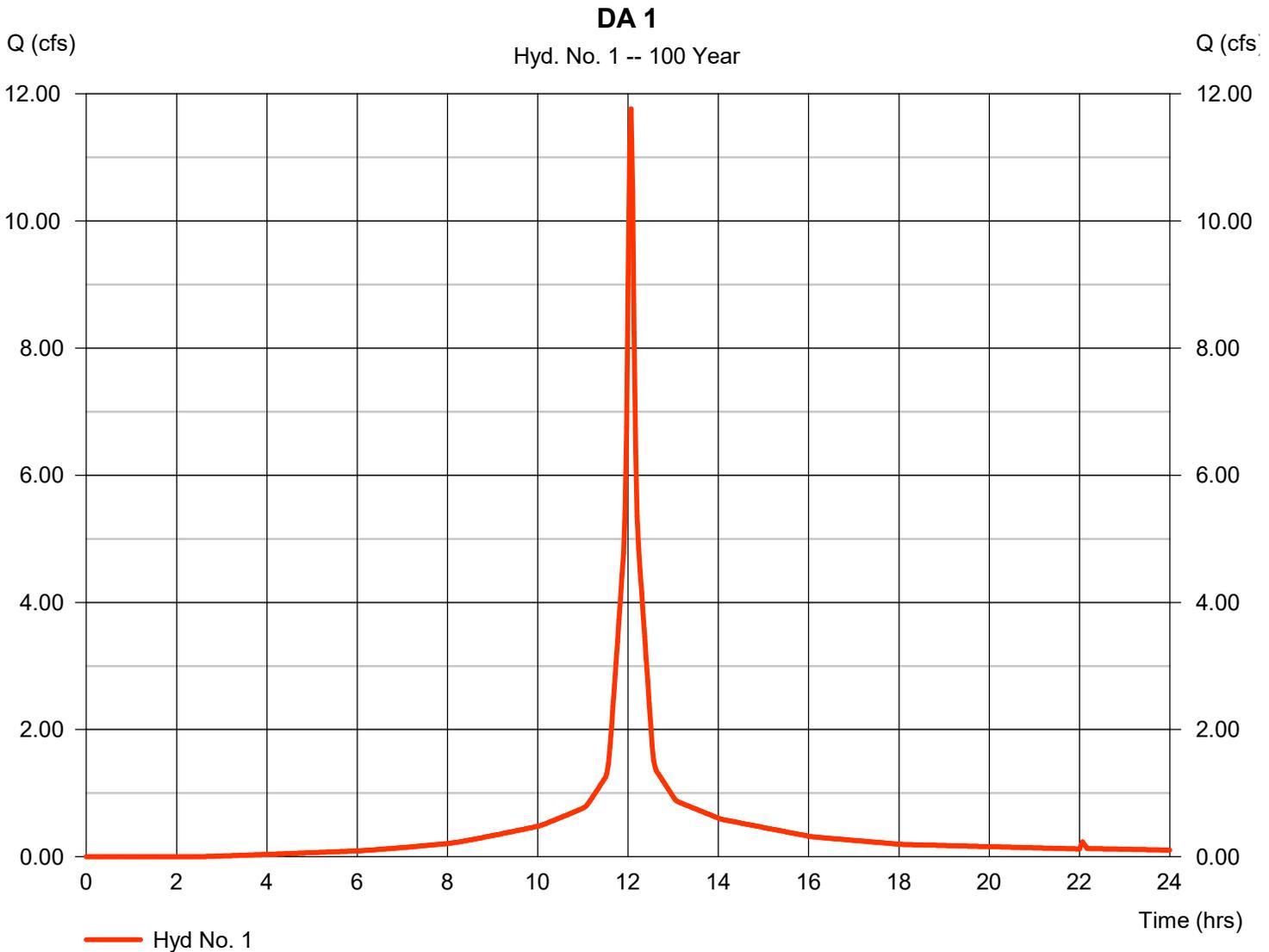
Friday, 06 / 5 / 2020

## Hyd. No. 1

DA 1

Hydrograph type	= SCS Runoff	Peak discharge	= 11.77 cfs
Storm frequency	= 100 yrs	Time to peak	= 12.07 hrs
Time interval	= 2 min	Hyd. volume	= 38,136 cuft
Drainage area	= 1.080 ac	Curve number	= 87*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= User	Time of conc. (Tc)	= 6.00 min
Total precip.	= 12.00 in	Distribution	= Type III
Storm duration	= 24 hrs	Shape factor	= 484

\* Composite (Area/CN) =  $[(0.850 \times 80) + (0.230 \times 98)] / 1.080$



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# Hydrograph Report

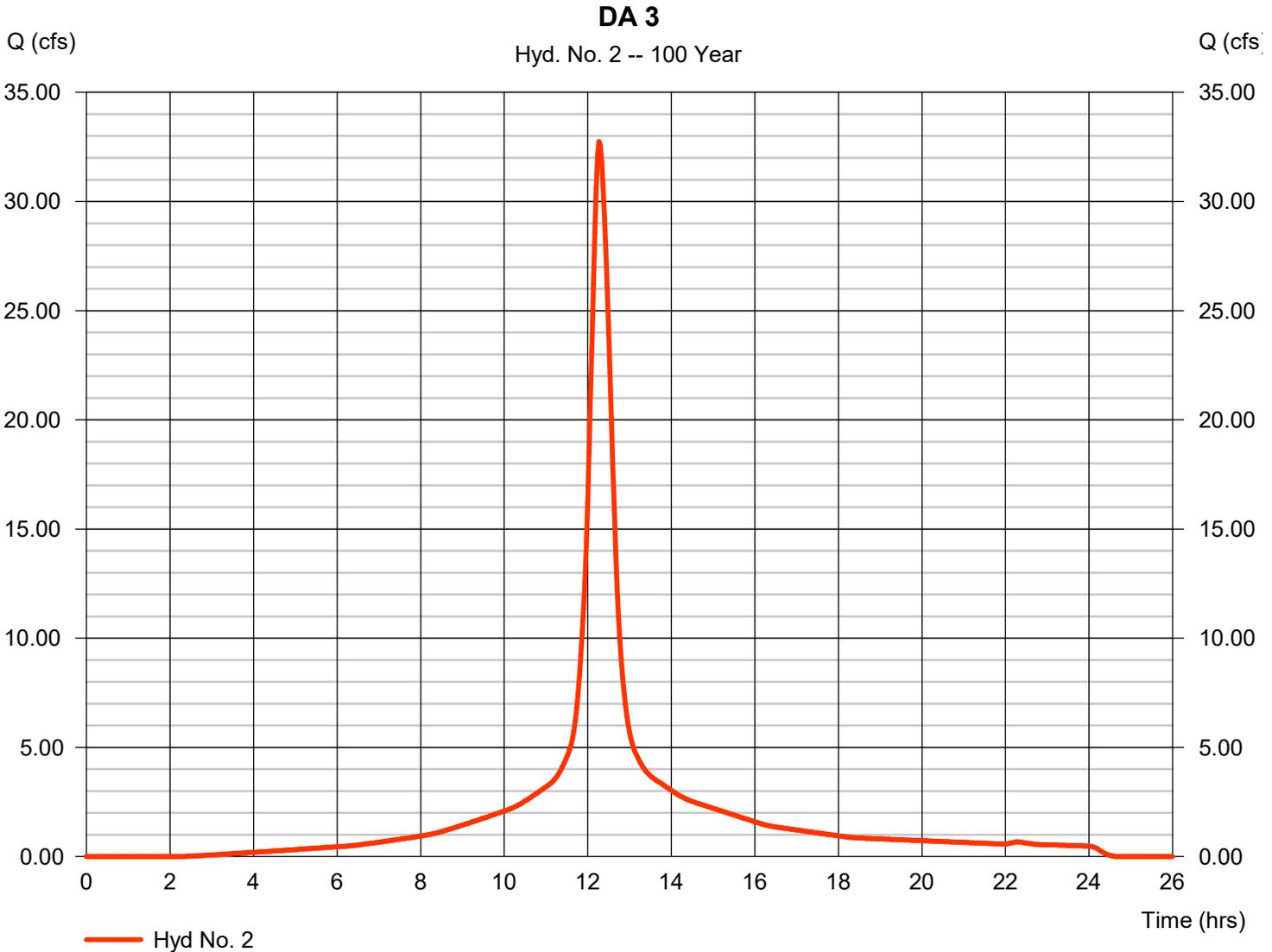
Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

Friday, 06 / 5 / 2020

## Hyd. No. 2

DA 3

Hydrograph type	= SCS Runoff	Peak discharge	= 32.75 cfs
Storm frequency	= 100 yrs	Time to peak	= 12.27 hrs
Time interval	= 2 min	Hyd. volume	= 174,040 cuft
Drainage area	= 4.580 ac	Curve number	= 89
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= User	Time of conc. (Tc)	= 26.40 min
Total precip.	= 12.00 in	Distribution	= Type III
Storm duration	= 24 hrs	Shape factor	= 484



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# Hydrograph Report

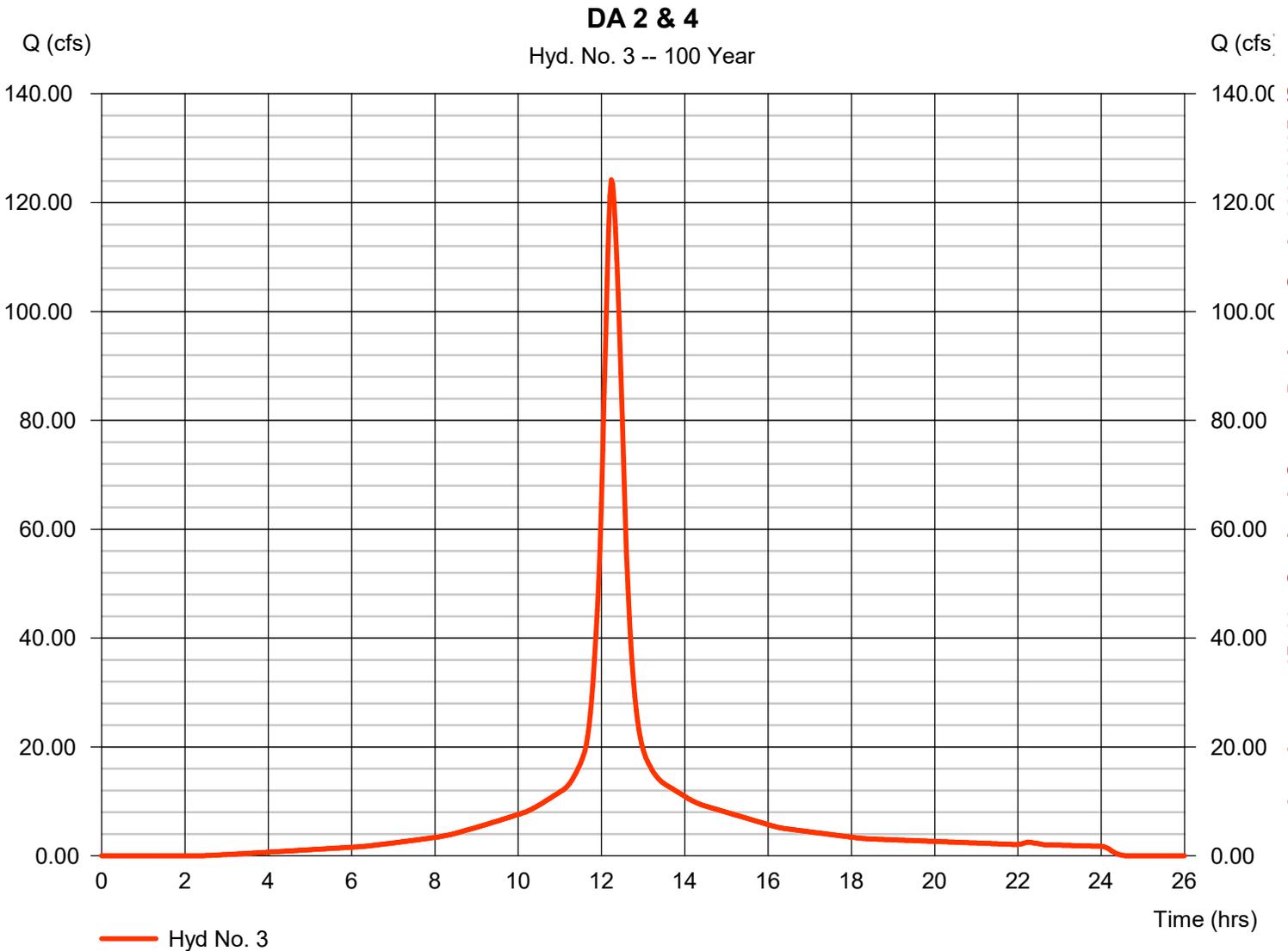
Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

Friday, 06 / 5 / 2020

## Hyd. No. 3

DA 2 & 4

Hydrograph type	= SCS Runoff	Peak discharge	= 124.16 cfs
Storm frequency	= 100 yrs	Time to peak	= 12.23 hrs
Time interval	= 2 min	Hyd. volume	= 632,298 cuft
Drainage area	= 16.190 ac	Curve number	= 88.5
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= User	Time of conc. (Tc)	= 22.00 min
Total precip.	= 12.00 in	Distribution	= Type III
Storm duration	= 24 hrs	Shape factor	= 484



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

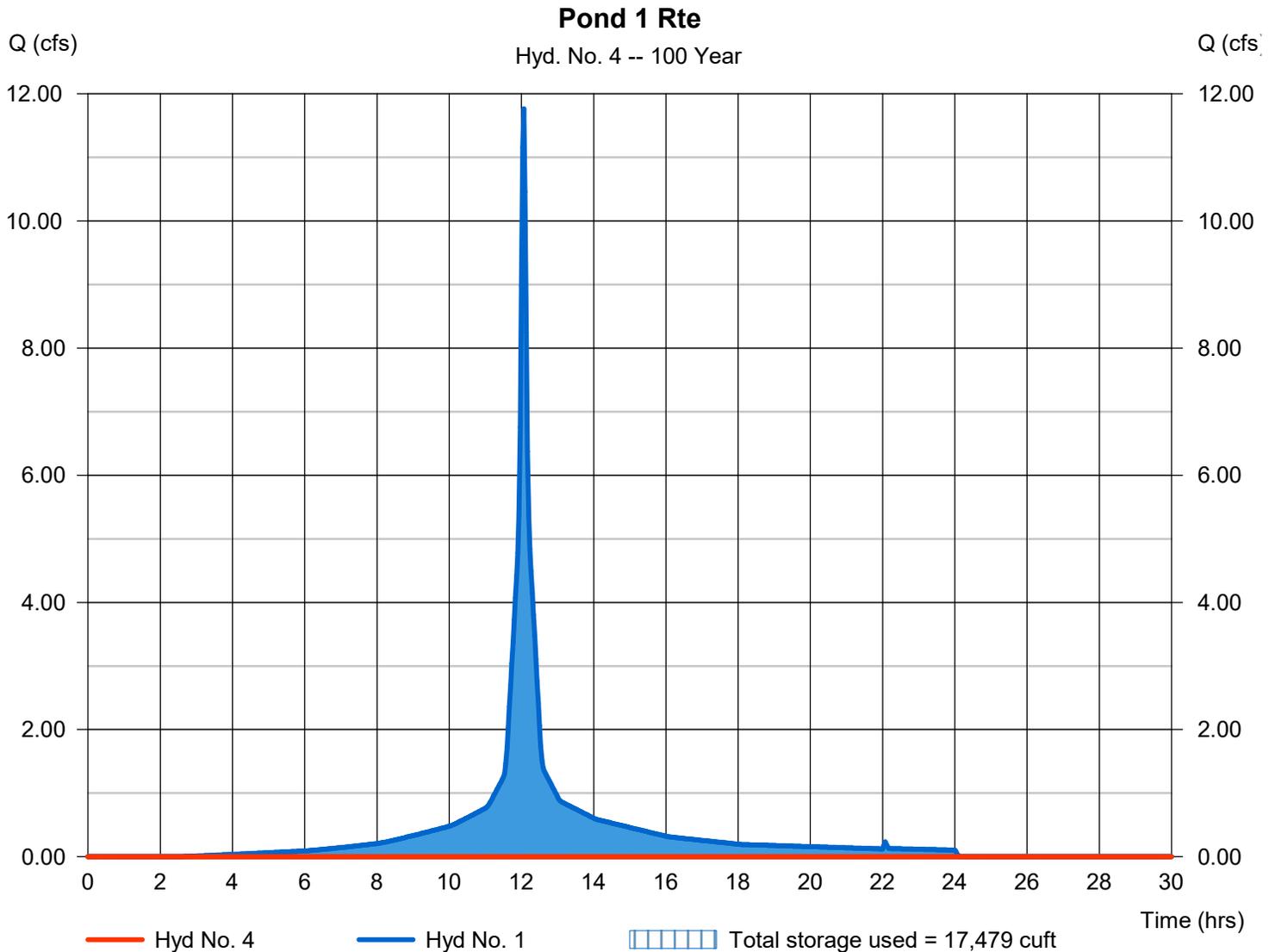
Friday, 06 / 5 / 2020

## Hyd. No. 4

Pond 1 Rte

Hydrograph type	= Reservoir	Peak discharge	= 0.000 cfs
Storm frequency	= 100 yrs	Time to peak	= 10.17 hrs
Time interval	= 2 min	Hyd. volume	= 0 cuft
Inflow hyd. No.	= 1 - DA 1	Max. Elevation	= 7.81 ft
Reservoir name	= Infiltration Basin	Max. Storage	= 17,479 cuft

Storage Indication method used. Exfiltration extracted from Outflow.



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

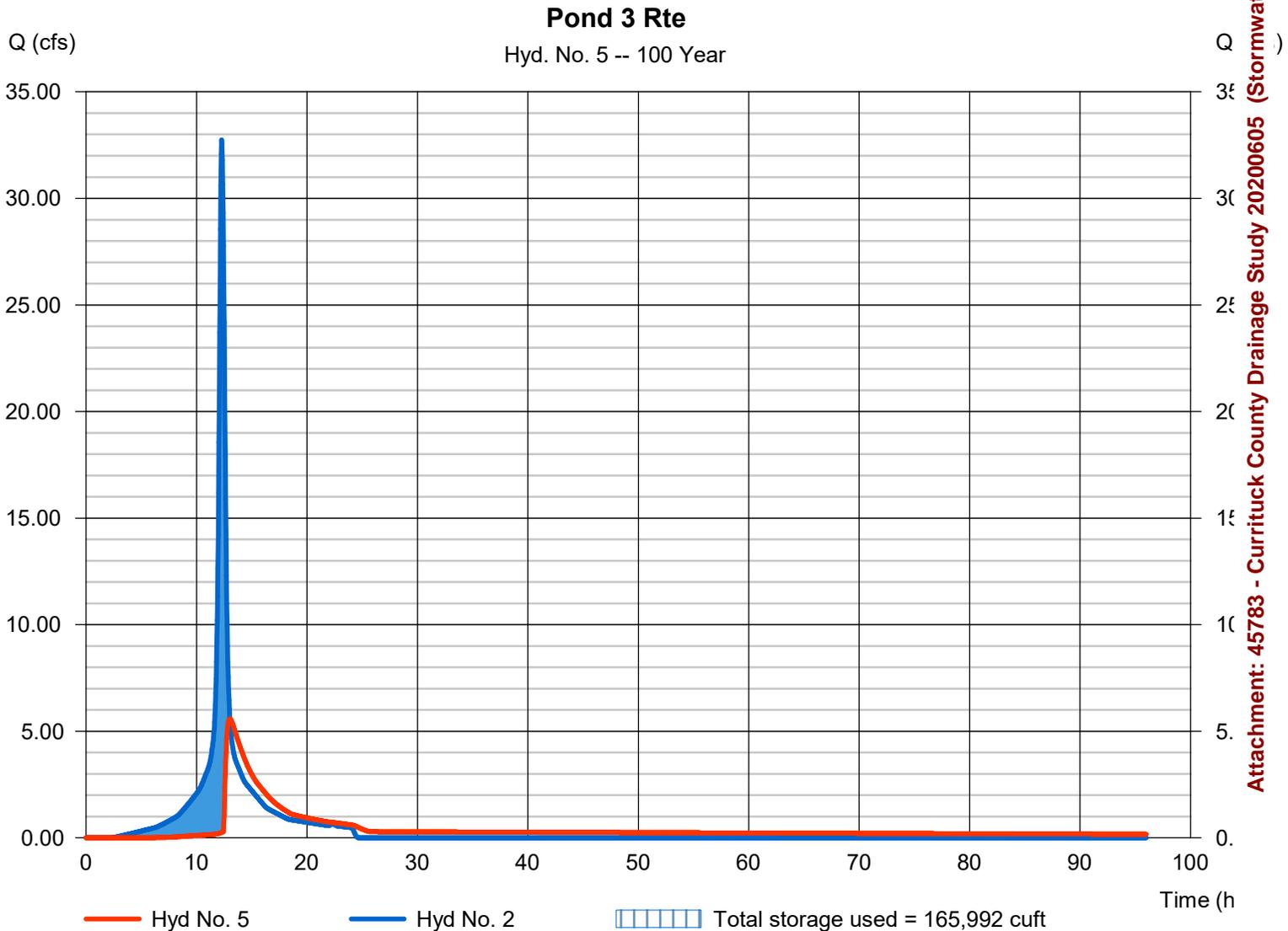
Friday, 06 / 5 / 2020

## Hyd. No. 5

Pond 3 Rte

Hydrograph type	= Reservoir	Peak discharge	= 5.580 cfs
Storm frequency	= 100 yrs	Time to peak	= 13.00 hrs
Time interval	= 2 min	Hyd. volume	= 140,448 cuft
Inflow hyd. No.	= 2 - DA 3	Max. Elevation	= 7.85 ft
Reservoir name	= Wet Basin 3	Max. Storage	= 165,992 cuft

Storage Indication method used. Wet pond routing start elevation = 5.80 ft.



Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

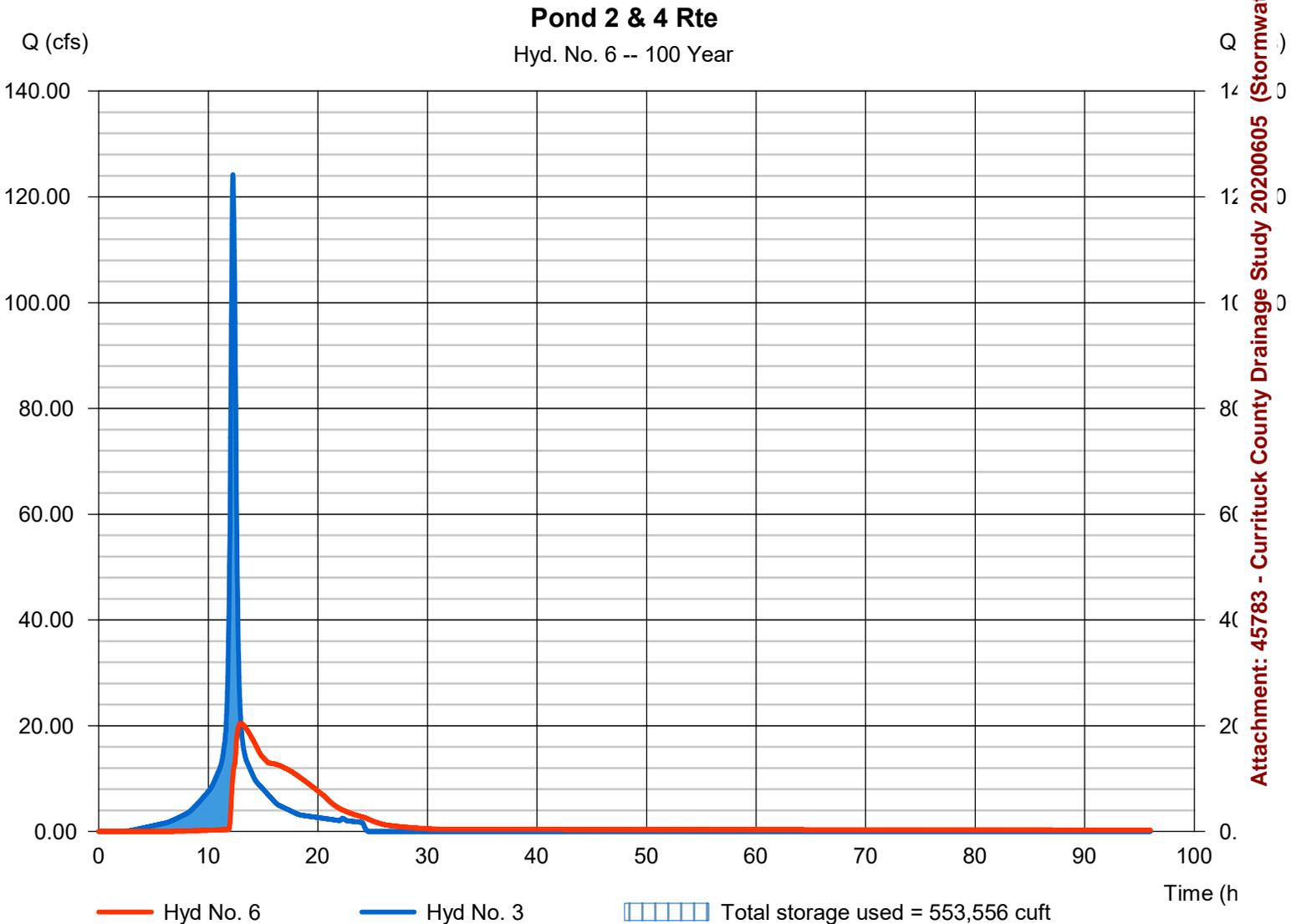
Friday, 06 / 5 / 2020

## Hyd. No. 6

Pond 2 & 4 Rte

Hydrograph type	= Reservoir	Peak discharge	= 20.42 cfs
Storm frequency	= 100 yrs	Time to peak	= 12.97 hrs
Time interval	= 2 min	Hyd. volume	= 556,900 cuft
Inflow hyd. No.	= 3 - DA 2 & 4	Max. Elevation	= 8.68 ft
Reservoir name	= Wet Basin 2 & 4	Max. Storage	= 553,556 cuft

Storage Indication method used. Wet pond routing start elevation = 5.80 ft.



# Hydraflow Rainfall Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2019

Friday, 06 / 5 / 2020

Return Period (Yrs)	Intensity-Duration-Frequency Equation Coefficients (FHA)			
	B	D	E	(N/A)
1	0.0000	0.0000	0.0000	-----
2	69.8703	13.1000	0.8658	-----
3	0.0000	0.0000	0.0000	-----
5	79.2597	14.6000	0.8369	-----
10	88.2351	15.5000	0.8279	-----
25	102.6072	16.5000	0.8217	-----
50	114.8193	17.2000	0.8199	-----
100	127.1596	17.8000	0.8186	-----

File name: SampleFHA.idf

$$\text{Intensity} = B / (T_c + D)^E$$

Return Period (Yrs)	Intensity Values (in/hr)											
	5 min	10	15	20	25	30	35	40	45	50	55	60
1	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
2	5.69	4.61	3.89	3.38	2.99	2.69	2.44	2.24	2.07	1.93	1.81	1.70
3	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
5	6.57	5.43	4.65	4.08	3.65	3.30	3.02	2.79	2.59	2.42	2.27	2.15
10	7.24	6.04	5.21	4.59	4.12	3.74	3.43	3.17	2.95	2.77	2.60	2.46
25	8.25	6.95	6.03	5.34	4.80	4.38	4.02	3.73	3.48	3.26	3.07	2.91
50	9.04	7.65	6.66	5.92	5.34	4.87	4.49	4.16	3.88	3.65	3.44	3.25
100	9.83	8.36	7.30	6.50	5.87	5.36	4.94	4.59	4.29	4.03	3.80	3.60

T<sub>c</sub> = time in minutes. Values may exceed 60.

Precip. file name: Sample.pcp

Storm Distribution	Rainfall Precipitation Table (in)							
	1-yr	2-yr	3-yr	5-yr	10-yr	25-yr	50-yr	100-yr
SCS 24-hour	0.00	3.74	0.00	3.30	6.00	5.77	6.80	12.00
SCS 6-Hr	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Huff-1st	0.00	0.00	0.00	2.75	0.00	5.38	6.50	0.00
Huff-2nd	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Huff-3rd	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Huff-4th	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Huff-Indy	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Custom	0.00	0.00	0.00	2.80	0.00	5.25	6.00	0.00

---

# Appendix B

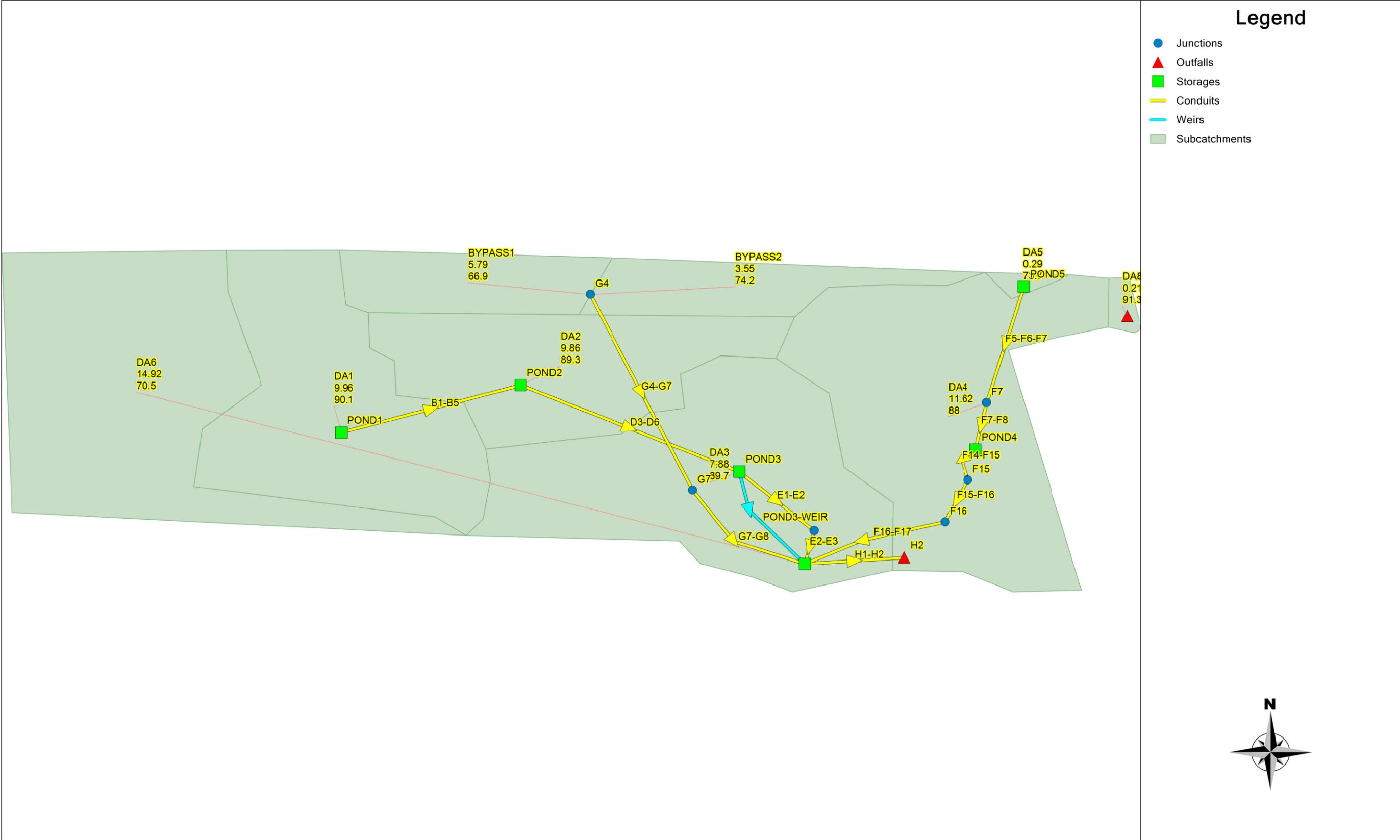
## PCSWMM Reports for Countryside Estates

Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)



**TIMMONS GROUP**  
YOUR VISION ACHIEVED THROUGH OURS.

ENGINEERING | DESIGN | TECHNOLOGY



**PROJECT:** Countryside Estates  
**SUBJECT:** PC SWMM Input Data and Results  
**JOB NO.:** 45783                      **DATE:** 6/5/2020  
**COMP. BY:** KDH                      **CHECKED BY:** KDH



**POST-DEVELOPMENT CONDITIONS: 10 YR 24 HR STORM**

**SUBCATCHMENT DATA/RESULTS**

Name	Rain Gage	Area (ac)	Flow Length (ft)	Slope (%)	Dstore Perv (in)	Curve Number	Peak Runoff (cfs)
BYPASS1	SCS_Type_III_6in	5.79	1599.997	0.5	0.99	66.9	19.24
BYPASS2	SCS_Type_III_6in	3.55	600	0.5	0.7	74.2	13.51
DA1	SCS_Type_III_6in	9.96	200	0.5	0.22	90.1	47.68
DA2	SCS_Type_III_6in	9.86	685	0.5	0.24	89.3	46.8
DA3	SCS_Type_III_6in	7.88	750.001	0.5	0.23	89.7	37.56
DA4	SCS_Type_III_6in	11.62	697	0.5	0.27	88	54.34
DA5	SCS_Type_III_6in	0.29	200	0.5	0.71	73.7	1.09
DA6	SCS_Type_III_6in	14.92	1624.999	0.5	0.84	70.5	53.12
DA8	SCS_Type_III_6in	0.21	100	0.5	0.19	91.3	1.02

**CONDUIT DATA/RESULTS**

Name	Inlet Node	Outlet Node	Length (ft)	Roughness	Inlet Elev. (ft)	Outlet Elev. (ft)	Cross-Section	Geom1 (ft)	Slope (ft/ft)	Max.  Flow
B1-B5	POND1	POND2	380	0.012	3.2	2	CIRCULAR	3	0.00316	8.06
D3-D6	POND2	POND3	706	0.012	7.03	3.5	CIRCULAR	3	0.005	15.27
E1-E2	POND3	E2	13	0.012	8.8	7.71	CIRCULAR	3	0.08414	25.59
E2-E3	E2	G8-H1-DITC	25	0.012	7.71	7.62	CIRCULAR	3	0.0036	25.53
F14-F15	POND4	F15	12	0.012	8.6	8.3	CIRCULAR	3	0.02501	12.84
F15-F16	F15	F16	30	0.012	8.3	8.24	CIRCULAR	3	0.002	12.85
F16-F17	F16	G8-H1-DITC	318	0.012	8.24	7.57	CIRCULAR	3	0.00211	12.9
F5-F6-F7	POND5	F7	129	0.012	8.99	8.98	CIRCULAR	1.25	8.00E-05	6.68
F7-F8	F7	POND4	49	0.012	6.2	6	CIRCULAR	2.5	0.00408	49.46
G4-G7	G4	G7	500	0.012	8.87	8.37	CIRCULAR	2.5	0.001	27.44

G7-G8	G7	G8-H1-DITCH	259	0.012	8.37	8.1	CIRCULAR	2.5	0.00104	27.45
H1-H2	G8-H1-DITCH	H2	25	0.012	7.55	7.5	CIRCULAR	3	0.002	63.21

**STORAGE DATA/RESULTS**

Name	Invert Elev. (ft)	Rim Elev. (ft)	Depth (ft)	Initial Depth (ft)	Storage Curve	Curve Name	Max. HGL (ft)	Max. Volume (1000 ft <sup>3</sup> )
POND1	1	12.1	11.1	7.8	TABULAR	POND1	10.69	233.362
POND5	6.3	14	7.7	0	TABULAR	POND5	11.67	5.925
POND2	1	12.1	11.1	7.8	TABULAR	POND2	10.64	181.308
POND3	1	12	11	7.8	TABULAR	POND3	10.35	289.331
POND4	1.5	12	10.5	7.1	TABULAR	POND4	10.58	261.215
G8-H1-DITCH	7.55	12	4.45	0	TABULAR	G8-H1-DITCH	10.39	11.259

**JUNCTION DATA/RESULTS**

Name	Invert Elev. (ft)	Rim Elev. (ft)	Depth (ft)	Initial Depth (ft)	Ponded Area (ft <sup>2</sup> )	Max. HGL (ft)	Contributing Area (ac)
E2	7.71	12.05	4.34	0	0	10.63	27.7
F15	8.3	12.1	3.8	0	0	10.53	11.91
F16	8.24	12.4	4.16	0	0	10.47	11.91
F7	6.2	11.42	5.22	2.4	0	12.23	11.91
G4	8.87	14.1	5.23	0	406850	14.1	9.34
G7	8.24	12.22	3.98	0	0	12.22	9.34

**POST-DEVELOPMENT CONDITIONS: 12 INCH 24 HR STORM**

**SUBCATCHMENT DATA/RESULTS**

Name	Rain Gage	Area (ac)	Flow Length (ft)	Slope (%)	Dstore Perv (in)	Curve Number	Peak Runoff (cfs)
BYPASS1	SCS_Type_III_12in	5.79	1599.997	0.5	0.99	66.9	48.63
BYPASS2	SCS_Type_III_12in	3.55	600	0.5	0.7	74.2	32.05
DA1	SCS_Type_III_12in	9.96	200	0.5	0.22	90.1	99.34
DA2	SCS_Type_III_12in	9.86	685	0.5	0.24	89.3	98.04
DA3	SCS_Type_III_12in	7.88	750.001	0.5	0.23	89.7	78.47
DA4	SCS_Type_III_12in	11.62	697	0.5	0.27	88	114.91
DA5	SCS_Type_III_12in	0.29	200	0.5	0.71	73.7	2.61
DA6	SCS_Type_III_12in	14.92	1624.999	0.5	0.84	70.5	130.13
DA8	SCS_Type_III_12in	0.21	100	0.5	0.19	91.3	2.1

**CONDUIT DATA/RESULTS**

Name	Inlet Node	Outlet Node	Length (ft)	Roughness	Inlet Elev. (ft)	Outlet Elev. (ft)	Cross-Section	Geom1 (ft)	Slope (ft/ft)	Max.  Flow  (cfs)
B1-B5	POND1	POND2	380	0.012	3.2	2	CIRCULAR	3	0.00316	5.65
D3-D6	POND2	POND3	706	0.012	7.03	3.5	CIRCULAR	3	0.005	10.72
E1-E2	POND3	E2	13	0.012	8.8	7.71	CIRCULAR	1.5	0.08414	16.06
E2-E3	E2	G8-H1-DITCH	25	0.012	7.71	7.62	CIRCULAR	3	0.0036	16.06
F14-F15	POND4	F15	12	0.012	8.6	8.3	CIRCULAR	1.25	0.02501	10.06
F15-F16	F15	F16	30	0.012	8.3	8.24	CIRCULAR	3	0.002	10.06
F16-F17	F16	G8-H1-DITCH	318	0.012	8.24	7.57	CIRCULAR	3	0.00211	10.09
F5-F6-F7	POND5	F7	129	0.012	8.99	8.98	CIRCULAR	1.25	8.00E-05	8.64
F7-F8	F7	POND4	49	0.012	6.2	6	CIRCULAR	2.5	0.00408	62.82
G4-G7	G4	G7	500	0.012	8.87	8.37	CIRCULAR	2.5	0.001	27.38
G7-G8	G7	G8-H1-DITCH	259	0.012	8.37	8.1	CIRCULAR	2.5	0.00104	27.38
H1-H2	G8-H1-DITCH	H2	25	0.012	7.55	7.5	CIRCULAR	3	0.002	103.32

**STORAGE DATA/RESULTS**

Name	Invert Elev. (ft)	Rim Elev. (ft)	Depth (ft)	Initial Depth (ft)	Storage Curve	Curve Name	Max. HGL (ft)	Max. Volume (1000 ft <sup>3</sup> )	Contributing Area (ac)
POND1	1	12.1	11.1	7.8	TABULAR	POND1	11.29	440.361	9.96
POND5	6.3	14	7.7	0	TABULAR	POND5	12.63	18.976	0.29
POND2	1	12.1	11.1	7.8	TABULAR	POND2	11.26	348.295	19.82
POND3	1	12	11	7.8	TABULAR	POND3	11.1	498.274	27.7
POND4	1.5	12	10.5	7.1	TABULAR	POND4	11.15	459.716	11.91
G8-H1-DIT	7.55	12	4.45	0	TABULAR	G8-H1-DITCH	11.59	22.016	63.87

**JUNCTION DATA/RESULTS**

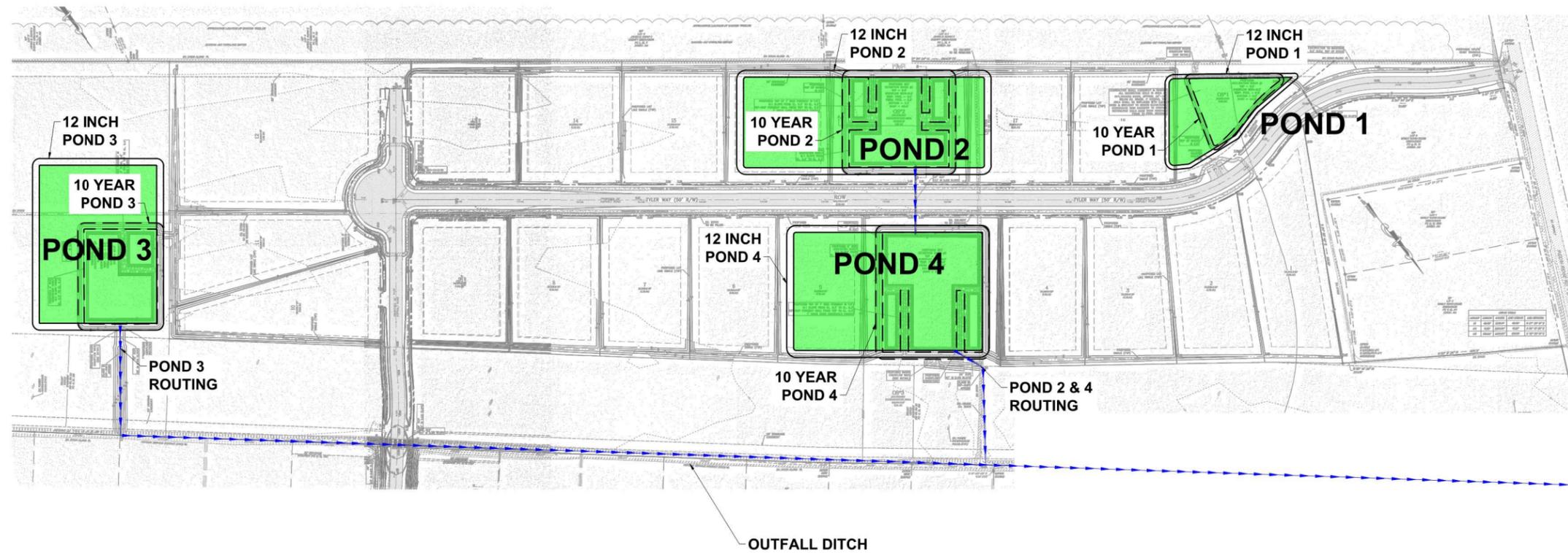
Name	Invert Elev. (ft)	Rim Elev. (ft)	Depth (ft)	Initial Depth (ft)	Ponded Area (ft <sup>2</sup> )	Max. HGL (ft)	Contributing Area (ac)
E2	7.71	12.05	4.34	0	0	11.58	27.7
F15	8.3	12.1	3.8	0	0	11.47	11.91
F16	8.24	12.4	4.16	0	0	11.51	11.91
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# Appendix C

## Exhibit Maps

Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

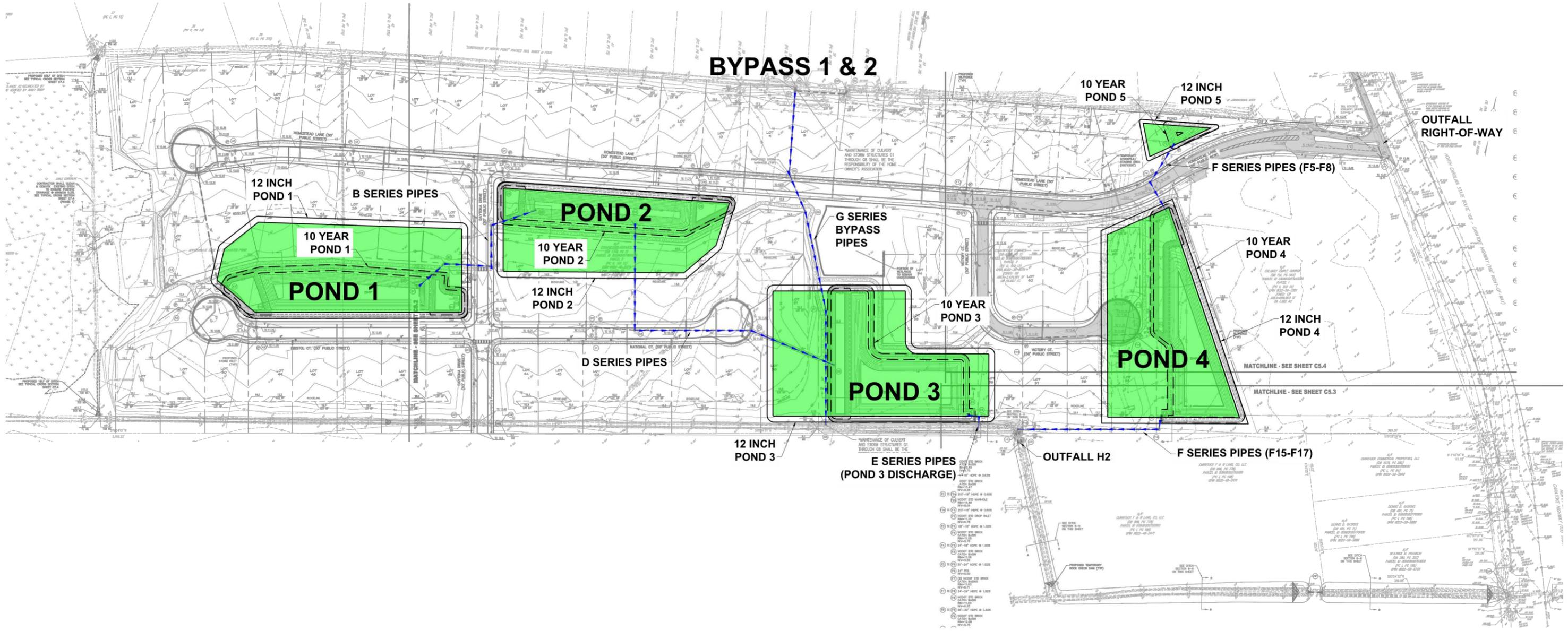


Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# FROGGY MEADOWS DRAINAGE EXHIBIT

June 05, 2020

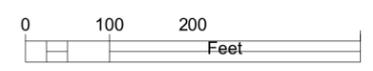




Attachment: 45783 - Currituck County Drainage Study 20200605 (Stormwater Analysis-500 Year Storm Event)

# COUNTRYSIDE ESTATES DRAINAGE EXHIBIT

June 05, 2020



**RESOLUTION AUTHORIZING INCENTIVES CONTRACT  
WITH THE COTTON GIN, INC.**

WHEREAS, Section 158-7.1 of the General Statutes of North Carolina authorizes a county to undertake an economic development project by extending assistance to a company in order to cause the company to locate or expand its operations within the county; and

WHEREAS, the Currituck County Board of Commissioners held a public hearing on June 15, 2020 to receive public comments regarding Currituck County, (the "County"), participation in an economic development project with The Cotton Gin, Inc. by providing The Cotton Gin, Inc. with a portion of the cost required for clearing property in preparation for construction of a new facility; and

WHEREAS, The Cotton Gin, Inc. will make an investment in the county in an approximate amount of between \$2,000,000 and \$3,000,000 to construct a new facility and acquire equipment necessary for its retail operations and return 34 jobs to the community workforce; and

WHEREAS, as proposed The Cotton Gin, Inc.'s investment in the county will stimulate, diversify and help stabilize the local economy, promote business in the county, increase tourism on the County's mainland and result in the return of jobs in the County; and

WHEREAS, the Currituck County Board of Commissioners will adopt an amendment to the County's budget ordinance appropriating the funds necessary for the project;

NOW, THEREFORE, BE IT RESOLVED by the Currituck County Board of Commissioners that:

Section 1. The County is authorized to expend up to \$16,365.14 from the County's General Fund for the partial cost required for The Cotton Gin, Inc.'s clearance of its property in preparation for construction of its new facility.

Section 2. This resolution and expenditure of funds are contingent on the execution of an incentives contract between the County and The Cotton Gin, Inc. outlining its investment in the county.

Section 3. The Chairman of the Board of Commissioners is authorized to execute the incentives contract and any other documents necessary to the project on behalf of the County.

Section 4. This resolution is effective upon adoption.

Adopted this 15<sup>th</sup> day of June, 2020.

\_\_\_\_\_  
Bob White, Chairman  
Board of Commissioners

ATTEST:

\_\_\_\_\_  
Leeann Walton, Clerk to the Board

(COUNTY SEAL)

Attachment: The Cotton Gin Resolution Authorizing Incentive Contract 061520 (Cotton Gin-Economic Development Incentive)



**STAFF REPORT**  
**PB 20-09 BOSWOOD ESTATES**  
**PRELIMINARY PLAT/USE PERMIT**  
**BOARD OF COMMISSIONERS**  
**JUNE 15, 2020**

### APPLICATION SUMMARY

<b>Property Owner:</b> G. Dodson Mathias 400 Avinger Lane, Villa 609 Davidson, NC 28039	<b>Applicant:</b> Boswood Estates, LLC PO Box 100 Currituck, NC 27929
<b>Case Number:</b> PB 20-09	<b>Application Type:</b> Preliminary Plat/Use Permit
<b>Parcel Identification Number:</b> 0060000053B0000	<b>Existing Use:</b> Active Agricultural/Wetlands
<b>Land Use Plan Classification:</b> Full Service/Conservation	<b>Parcel Size (Acres):</b> 26.94 acres total 20.49 acres (excludes 6.45 ac CAMA wetlands)
<b>Maple/Barco Small Area Plan Classification:</b> Mixed Use	<b>Zoning:</b> General Business (GB)
<b>Number of Units:</b> 14 residential lots (2 phases)	<b>Project Density:</b> 0.68 dwelling unit/acre
<b>Required Open Space:</b> 8.08 acres (30%)	<b>Provided Open Space:</b> 11.05 acres (41%)

### SURROUNDING PARCELS

	Land Use	Zoning
North	Low density residential	SFM
South	Residential/Agricultural	GB
East	Coinjock Bay	N/A
West	Low density residential	GB

### STAFF ANALYSIS

#### Application Summary

1. The applicant, Boswood Estates, LLC, is requesting preliminary plat approval of a 2 phase 14-lot residential subdivision.
2. The proposed development is a Type II traditional subdivision.
3. The base zoning of the property is General Business and the minimum lot size for a traditional residential subdivision is 40,000 square feet.
4. The property contains 10.66 acres of US Army Corps of Engineers jurisdictional wetlands (preliminary jurisdictional determination) and approximately 6.45 acres of coastal wetlands. The wetlands and riparian buffer will be located in open space. The applicant indicates 9.51 acres of wetlands will be dedicated to a non-profit and the remaining 1.54 acres of open space will be dedicated to the subdivision association.
5. The property contains approximately 772 linear feet of shoreline along the Coinjock Bay. The applicant is not proposing public water access to the abutting public trust waters since the total number of lots is less than 20.

6. The proposed streets are designed to be 20' in pavement width with a roadside swale within a 50' right of way. A 5' wide sidewalk is proposed between the roadside swale and the street trees. The sidewalk will not be located in the street right of way but on private property and within a 25' drainage, landscape, utility and pedestrian easement.
7. Interconnectivity is proposed to the southern property line and adjacent to active agricultural lands with a residential use (previously known as The Palmer Inn, bed and breakfast).
8. The Soil Survey of Currituck County, North Carolina identifies the proposed residential lots predominately State fine sandy loam (StA) soils. A small area near the wetlands is identified as Tomotley fine sandy loam (To) soils. The Soil Survey indicates State (StA) soils are generally sited for most urban and recreational uses; wetness is the main limitation.
9. The entrance road for the proposed development is approximately 380' south of the existing Major's Island Road, a private unpaved road. The minimum intersection spacing for a local street intersecting a major arterial street is 1,000 feet. Due to the lot width of this site and the existing private streets, the applicant is unable to meet the minimum intersection spacing of the UDO. The 10<sup>th</sup> edition of the *ITE Trip Generation Manual* states a single family dwelling generates 10 trips per day, and the proposed 14 lot development will generate a total of 140 vehicles per day. Upon advise of David B. Otts, NCDOT District Engineer, the placement of the proposed entrance road that will generate 140 vehicles per day and is approximately 380' south of the existing Major's Island Road (private), will maintain a satisfactory level of access control on Caratoke Highway. The total number of lots will not require a deceleration lane.
10. A community meeting was not required with this application due to the number of lots proposed.

Attachment: 1 Boswood Estates PP UP Staff Report (PB 20-09 Boswood Estates, Phase 1 & 2)

INFRASTRUCTURE	
<b>Water</b>	Proposed Public Water Supply
<b>Sewer</b>	On-site septic
<b>Transportation</b>	Pedestrian: Sidewalks on both sides of the street
	Connectivity Score: Minimum = 1.4 Proposed = 1.5
<b>Stormwater/Drainage</b>	Property line vegetative swales will convey runoff to a swale/ditch near the rear of each lot and then to the drainage outlet Existing internal farm ditches will be filled and redirected to shallow grass swales
<b>Lighting</b>	No street lighting proposed
<b>Landscaping</b>	A 25' streetscape will be provided along Caratoke Highway. A perimeter buffer will be provided along the properties zoned SFM. The applicant selected a 10' perimeter buffer width. Street trees will be provided.
<b>Compatibility</b>	A 50' farmland buffer will be provided along the agricultural use area on the southern property line.
<b>Recreation and Park Area Dedication</b>	The 0.36 acre proposed dedication is considered too small to provide adequate park or recreation area and payment in lieu of the dedication shall be provided.
<b>Riparian Buffers</b>	A 30' riparian buffer will be provided adjacent to all wetland boundaries. The buffer is located in open space.

<b>ADEQUATE PUBLIC FACILITIES – SCHOOLS<sup>1</sup></b>				
<b>School</b>	<b>2019-2020 2020-2021 Actual Capacity<sup>2</sup></b>	<b>2021-2022 Actual Capacity<sup>3</sup></b>	<b>Committed Capacity<sup>3</sup></b>	<b>Proposed Capacity Changes</b>
				<b>Number of Students</b>
Moyock Elementary	109%	115%	122%	4 students
Shawboro Elementary	87%	90%		
Central Elementary	77%	85%		
Griggs Elementary	57%	59%	96%	
Jarvisburg Elementary	88%	95%		
Knotts Island Elementary	36%	38%	38%	
Moyock Middle	94%	83%	96%	1 student
Currituck Middle	70%			
Currituck High	84%	85%	103%	2 students
JP Knapp Early College	88%			

<sup>1</sup>Does not include minor subdivisions, exempt subdivisions, and subdivisions approved prior to the adoption of the adequate public facilities ordinance (October 1994)

<sup>2</sup>Capacity percentages are based on 2019-2020 and 2020-2021 school year classroom standards and January 2020 ADM

<sup>3</sup>Capacity percentages are based on the 2021-2022 school year classroom standards and January 2020 ADM

**RECOMMENDATIONS**

**TECHNICAL REVIEW COMMITTEE**

The Technical Review Committee recommends adoption of the use permit and approval of the preliminary plat subject to the following conditions of approval:

1. The application complies with all applicable review standards of the UDO provided the following items are addressed:
  - a. The applicant selected Option 2 for the perimeter buffer. The width of the buffer is 10 feet and is near overhead power lines and drainage improvements. Staff is concerned that this area may not be sufficient width to accommodate the existing conditions and the proposed improvements, including landscaping. It is recommended that a typical detail of the existing conditions and proposed improvements be submitted to ensure compliance of the UDO and minimize the potential effects on surrounding lands.
2. The applicant demonstrates the proposed use will meet the use permit review standards of the UDO.
3. The conditions of approval necessary to ensure compliance with the review standards of the UDO and to prevent or minimize adverse effects of the development application on surrounding lands include:
  - a. Open space shall be contiguous and shall not contain private walkways or boardwalks.
  - b. A 25' drainage easement shall be provided along all conveyance systems that drain more than 5 acres. The easement shall include the conveyance system and an additional 25' from the top of embankment. This easement will extend onto private lots. In an effort provide awareness and avoid damage to potential private improvements, no septic system or structure shall be located the easement. A note shall be added to the final plat.

Attachment: 1 Boswood Estates PP UP Staff Report (PB 20-09 Boswood Estates, Phase 1 & 2)

- c. The applicant selected Option 2 for the perimeter buffer. The width of the buffer is 10 feet and is near overhead power lines and drainage improvements. Staff is concerned that this area may not be sufficient width to accommodate the existing conditions and the proposed improvements, including landscaping. It is recommended that a typical detail of the existing conditions and proposed improvements be submitted to ensure compliance of the UDO and minimize the potential effects on surrounding lands.
- d. No parking signs shall be placed at along the street at intersections and the entrance (approximately 4-5 signs).

## USE PERMIT REVIEW STANDARDS

A use permit shall be approved on a finding that the applicant demonstrates the proposed use will meet the below requirements. It is staff's opinion that the evidence in the record, prepared in absence of testimony presented at a public hearing, supports the preliminary staff findings

The use will not endanger the public health or safety.

Preliminary Applicant Findings:

1. The use will be in accordance with county health and safety standards and those recommended by the Albemarle Regional Health Services in regards to on site wastewater systems.

The use will not injure the value of adjoining or abutting lands and will be in harmony with the area in which it is located.

Preliminary Applicant Findings:

1. Lands to the north have been developed for single family homes as well as the properties across the highway.
2. Lot sizes proposed are in keeping with what is adjacent.
3. Wetlands will be preserved.

The use will be in conformity with the Land Use Plan or other officially adopted plans.

Preliminary Staff Findings:

1. The 2006 Land Use Plan classifies this site as Full Service and Conservation land use classification in the Barco-Coinjock-Airport subarea.
2. The area intended for residential lots is predominately in the Full Service land use classification. The Full Service area contemplates a residential density (base) to be 2 units per acre.
3. The policy emphasis for Barco-Coinjock-Airport subarea indicates residential development densities should be limited to 1-2 units per acre in areas where on-site wastewater is proposed and other county services are may be limited. The proposed development density is 0.68 units per acre.
4. The proposed use is in keeping with the policies of the plan, some of which are:  
POLICY ES2: NON-COASTAL WETLANDS, including FRESHWATER SWAMPS, AND INLAND, NON-TIDAL WETLANDS, shall be conserved for the important role they play in absorbing floodwaters, filtering pollutants from stormwater runoff, recharging the ground water table, and providing critical habitat for many plant and animal species. Currituck County supports the efforts of the U.S. Army Corps of Engineers in protecting such wetlands through the Section 4042 permit program of the Clean Water Act, as well as Section 4013 water quality certifications by the State of North Carolina.

**POLICY ES3: COASTAL WETLANDS** shall be conserved for the valuable functions they perform in protecting water quality and in providing critical habitat for the propagation and survival of important plant and animal species. CAMA use standards and policies for coastal wetlands shall be supported. Uses approved for location in a coastal wetland must be water dependent (i.e. utility easements, bridges, docks, and piers) and be developed so as to minimize adverse impacts.

**POLICY HN1:** Currituck County shall encourage development to occur at densities appropriate for the location. **LOCATION AND DENSITY FACTORS** shall include whether the development is within an environmentally suitable area, the type and capacity of sewage treatment available to the site, the adequacy of transportation facilities providing access to the site, and the proximity of the site to existing and planned urban services. For example, projects falling within the Full Services areas of the Future Land Use Map would be permitted a higher density because of the availability of infrastructure as well as similarity to the existing development pattern. Such projects could be developed at a density of two (2) or more dwelling units per acre. Projects within areas designated as Limited Service would be permitted a density of one (1) to one and one half (1.5) units per acre depending upon the surrounding development pattern and availability of resources. Projects within areas designated as Rural or Conservation by the Future Land Use Plan would be permitted a much lower density of 1 dwelling unit per 3 acres because of the lack of infrastructure in the area, the existing low density development pattern, and presence of environmentally sensitive natural areas.

**POLICY WQ5:** Development that preserves the **NATURAL FEATURES OF THE SITE**, including existing topography and significant existing vegetation, shall be encouraged. If **COASTAL AND NON-COASTAL WETLANDS** are considered part of a lot's acreage for the purpose of determining minimum lot size or development density, Low Impact Development techniques or appropriate buffers shall be integrated into the development. Open space developments shall be encouraged to **REDUCE IMPERVIOUS SURFACE AREAS** associated with new development and redevelopment.

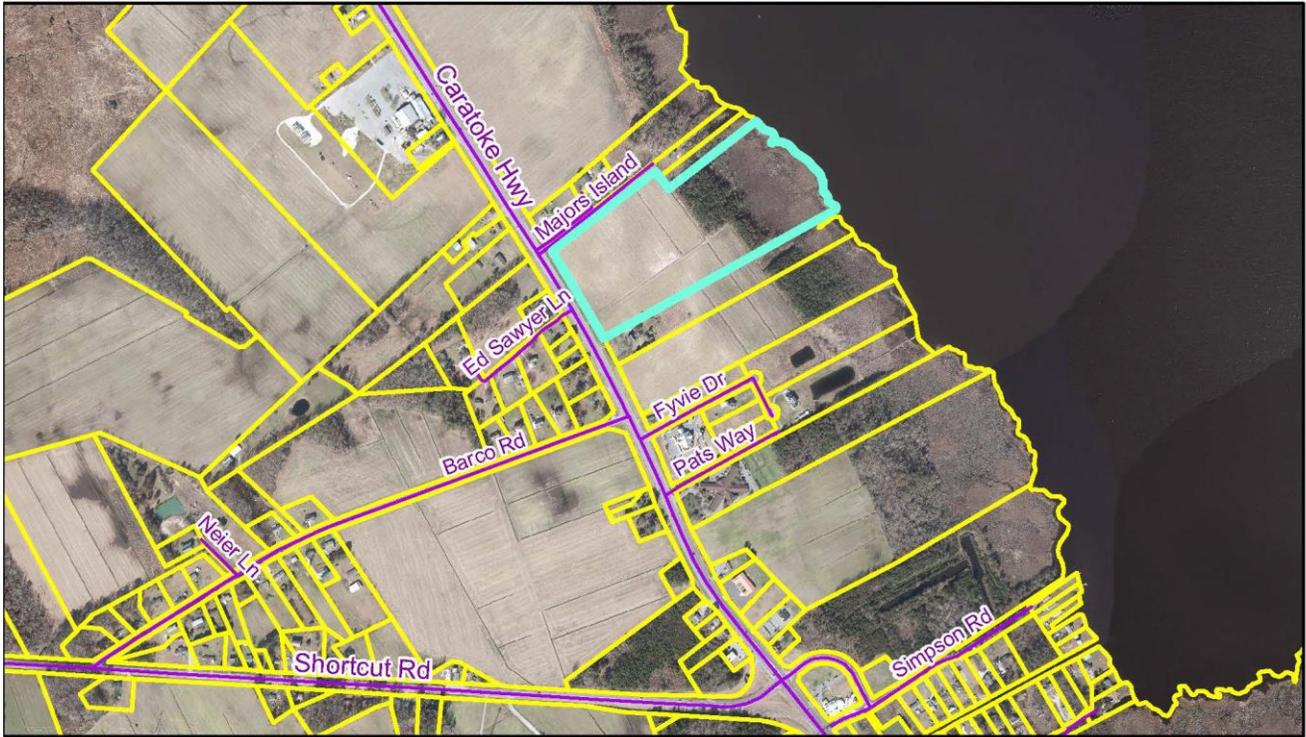
5. The Maple-Barco Small Area Plan, an official adopted plan, classifies the site as Mixed Use. The mixed use designation is characterized by a diverse mix of land uses, including residential.
6. The proposed use is in keeping with policies in the Maple-Barco Small Area Plan some of which include:
  - LU6: Encourage buffers for uses that are developing adjacent to environmentally sensitive areas.
  - LU9: Evaluate development proposals using the future land use map and policies for the Maple – Barco study area to determine the desired density, character of growth, and level or services appropriate for the study area.

The use will not exceed the county's ability to provide adequate public facilities, including, but not limited to: schools, fire and rescue, law enforcement, and other county facilities. Applicable state standards and guidelines shall be followed for determining when public facilities are adequate.

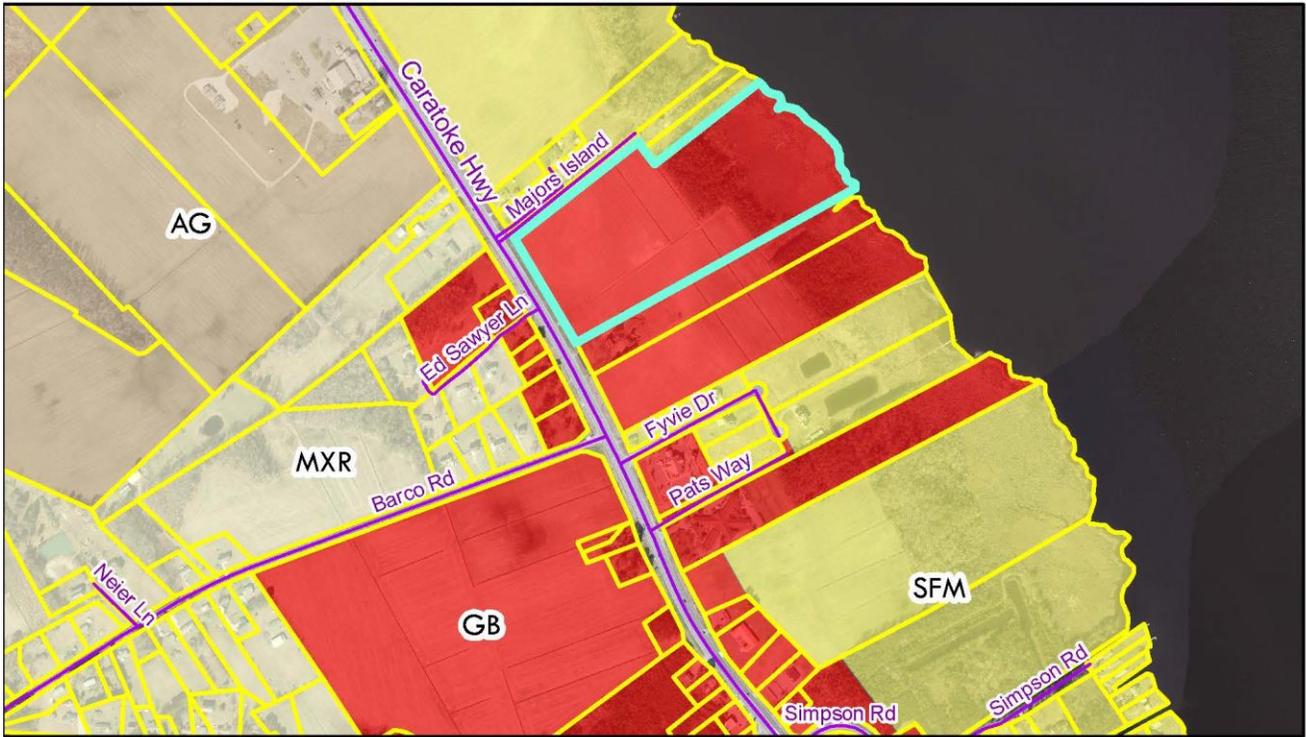
**Preliminary Staff Findings:**

1. The proposed subdivision contains 14 residential lots.
2. Based on the Student Generation Rate study prepared by Tischler and Associates, Inc. (2004), the proposed subdivision will generate the following students:
  - a. 4 elementary school students;
  - b. 1 middle school student; and,
  - c. 2 high school students

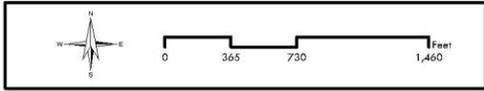
3. According to Currituck County Schools, the proposed subdivision is located in the following school districts:
  - a. Central Elementary
    - i. 77% 2019-2021 actual capacity based on January 2020 ADM
    - ii. 85% 2021-2022 actual capacity based on January 2020 ADM
  - b. Currituck Middle School, and
    - i. 70% 2019-2021 actual capacity based on January 2020 ADM
  - c. Currituck High School.
    - i. 84% 2019-2021 actual capacity based on January 2020 ADM



PB 20-09 Boswood Estates Preliminary Plat/Use Permit Aerial

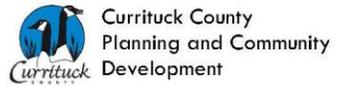
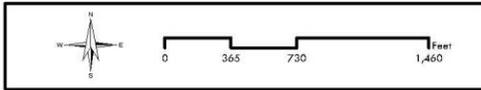


PB 20-09 Boswood Estates Preliminary Plat/Use Permit Zoning

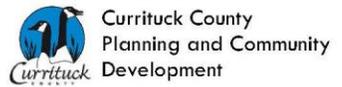
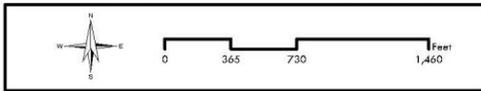




PB 20-09 Boswood Estates  
 Preliminary Plat/Use Permit  
 2006 Land Use Plan Classifications



PB 20-09 Boswood Estates  
 Preliminary Plat/Use Permit  
 Maple Barco SAP



THE APPLICATION AND RELATED MATERIALS ARE AVAILABLE ON THE COUNTY'S WEBSITE  
Board of Commissioners: [www.co.currituck.nc.us/board-of-commissioners-minutes-current.cfm](http://www.co.currituck.nc.us/board-of-commissioners-minutes-current.cfm)

Attachment: 1 Boswood Estates PP UP Staff Report (PB 20-09 Boswood Estates, Phase 1 & 2)

**INSERT**

Aerial Photography  
Zoning Map  
Land Use Plan Map

**ATTACH**

Project Application  
Site Plan  
Building Elevations, if applicable  
TRC Memo  
Community Meeting Summary  
Other Relevant Maps/Data

**Attachment: 1 Boswood Estates PP UP Staff Report (PB 20-09 Boswood Estates, Phase 1 & 2)**

- SITE DATA:**
- OWNER: G. DODSON MATHIAS  
400 AVINGER LANE, VILLA 609  
DAVIDSON, NC 28036
  - DEVELOPER: BOSWOOD ESTATES, LLC  
PO BOX 100  
CURRITUCK, NC 27929  
252-483-4300
  - SITE INFORMATION  
PIN: 006000059380000  
D.S. 211 PG. 466  
ZONING: GB (GENERAL BUSINESS)
  - TOTAL PARCEL AREA: 28.94 AC  
AREA OUTSIDE CMA JURISDICTION: 28.51 AC  
UPLAND AREA: 16.28 AC  
WETLAND AREA: 10.66 AC
  - OPEN SPACE REQUIRED: 30%  
OPEN SPACE PROVIDED: 41%  
1.54 AC BUFFERS TO BE OWNED BY HOA  
9.51 AC 404 WETLANDS TO BE DEDICATED TO NON-PROFIT
  - PROPOSED STREETS: DODSON DRIVE 50' RW 1,046 LF  
MATHIAS COURT 50' RW 150 LF
  - EXISTING USE: VACANT/AGRICULTURAL  
PROPOSED USE: RESIDENTIAL
  - TOTAL NUMBER OF LOTS = 14  
PHASE 1: LOTS 1-6  
PHASE 2: LOTS 7-14  
SMALLEST LOT = 0.91 AC  
LOT AREA = 13.22 AC  
RW AREA = 1.44 AC  
TYPE B AESTHETIC BUFFER AREA: 0.64 AC  
MAJOR ARTERIAL BUFFER AREA: 0.37 AC  
AGRICULTURAL BUFFER AREA: 0.53 AC
  - MINIMUM BUILDING SETBACKS:  
FRONT: 25' (BASED ON EASEMENT)  
SIDE: 15' (ZONING)  
SIDE (CORNER): 25' (BASED ON EASEMENT)  
REAR: 25' (ZONING)  
(30' MAJOR ARTERIAL SETBACK ALONG US HWY 168)
  - EASEMENTS:  
10' ALONG ALL REAR & SIDE PROPERTY LINES  
25' DRAINAGE, LANDSCAPE, & PEDESTRIAN EASEMENT  
ALONG RIGHT OF WAY
  - SITE IS LOCATED IN FLOOD ZONE "X", "SHADED X", & AE(4)" AS SHOWN ON F.I.R.M. PANEL 370888R00K, DATED DECEMBER 21, 2018. FLOOD ZONES SUBJECT TO CHANGE BY FEMA.
  - THE MAXIMUM BUILT UPON AREA PER LOT PER NCDEQ STORMWATER MANAGEMENT PERMIT: 24%  
THE MAXIMUM BUILT UPON AREA FOR EACH LOT IS 24%. THE ALLOTTED AMOUNT INCLUDES ANY BUILT UPON AREA CONSTRUCTED WITHIN THE LOTS PROPERTY LINES AND THAT PORTION OF THE RIGHT-OF-WAY BETWEEN THE FRONT PROPERTY LINE AND THE BACK OF CURB. BUILT UPON AREA INCLUDES, BUT IS NOT LIMITED TO, STRUCTURES, ASPHALT, GRAVEL, CONCRETE, BRICK, STONE, SLATE, OR SIMILAR MATERIAL, BUT DOES NOT INCLUDE RAISED OPEN WOOD DECKING OR THE WATER SURFACE OF SWIMMING POOLS.
  - ANY FILL BROUGHT ON SITE SHALL BE FROM AN APPROVED SITEMINE. ANY MATERIAL REMOVED FROM THE SITE SHALL BE DISPOSED OF IN A SINGLE APPROVED LOCATION.
  - DISTURBED AREAS SHALL BE MANAGED AND MAINTAINED IN ACCORDANCE WITH NCDEQ REGULATIONS.
  - EXISTING CONDITIONS DATA IS BASED UPON A TOPOGRAPHIC SITE SURVEY PERFORMED BY THE TIMMONS GROUP IN DECEMBER 2019.
  - WETLANDS SHOWN HEREON DELINEATED BY ATLANTIC ENVIRONMENTAL CONSULTANTS, LLC ON FEBRUARY 4, 2020. U.S. ARMY CORPS OF ENGINEERS APPROVED DELINEATION ONSITE FEBRUARY 4, 2020. JD LETTER ISSUED MARCH 25, 2020.
  - ELEVATION INFORMATION SHOWN HEREON IS NAVD 1988 DATUM AND DERIVED FROM LEICA RTK NETWORK.
  - ACCESS TO CORNER LOTS (10 & 14) SHALL BE FROM MATHIAS COURT.
  - NO STREET LIGHTING IS PROPOSED.
  - RECREATION & PARK AREA DEDICATION: 14 LOTS X 0.0255 AC/LOT = 0.36 AC. PAYMENT IN LIEU OF DEDICATION IS PROPOSED DUE TO SIZE.
  - STREET INTERCONNECTIVITY INDEX CALCULATION: NUMBER OF LINKS = 3  
NUMBER OF NODES = 2  
CONNECTIVITY INDEX = 3/2 = 1.50  
(FOR GB, MIN. INDEX SCORE REQ'D = 1.40)
  - ALL SIDEWALKS SHALL BE MINIMUM 4" THICK, 5' WIDE AND SHALL BE CONSTRUCTED WITHIN THE STREET RIGHT OF WAY.
  - NO DISTURBANCE IS PROPOSED WITHIN WETLANDS OR CMA AEC.
  - NO SHORELINE IMPROVEMENTS ARE PROPOSED.

**APPROVAL CERTIFICATE**

I HEREBY CERTIFY THAT THE SUBDIVISION SHOWN ON THIS PLAT IS IN ALL RESPECTS IN COMPLIANCE WITH THE CURRITUCK COUNTY UNIFIED DEVELOPMENT ORDINANCE AND, THEREFORE, THIS PLAT HAS BEEN APPROVED BY THE CURRITUCK COUNTY TECHNICAL REVIEW COMMITTEE AND SIGNED BY THE ADMINISTRATOR, SUBJECT TO ITS BEING RECORDED IN THE CURRITUCK COUNTY REGISTRY WITHIN 90 DAYS OF THE DATE BELOW.

DATE \_\_\_\_\_ ADMINISTRATOR \_\_\_\_\_

**ENVIRONMENTAL CONCERN CERTIFICATE**

THIS SUBDIVISION (OR PORTIONS THEREOF) IS LOCATED WITHIN AN AREA OF ENVIRONMENTAL CONCERN.

DATE \_\_\_\_\_ LOCAL PERMIT OFFICER \_\_\_\_\_

**IMPROVEMENTS CERTIFICATE**

I HEREBY CERTIFY THAT ALL IMPROVEMENTS REQUIRED BY THE CURRITUCK COUNTY UNIFIED DEVELOPMENT ORDINANCE HAVE BEEN INSTALLED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS PREPARED BY THE TIMMONS GROUP, AND SAID IMPROVEMENTS COMPLY WITH CURRITUCK COUNTY SPECIFICATIONS.

DATE \_\_\_\_\_ REGISTERED LAND SURVEYOR/ENGINEER \_\_\_\_\_

REGISTRATION NUMBER \_\_\_\_\_

**OWNERSHIP AND DEDICATION CERTIFICATE**

I HEREBY CERTIFY THAT I AM THE OWNER OF THE PROPERTY DESCRIBED HEREON, WHICH PROPERTY IS LOCATED WITHIN THE SUBDIVISION JURISDICTION OF CURRITUCK COUNTY, THAT I HEREBY FREELY ADOPT THIS PLAT OF SUBDIVISION AND DEDICATE TO PUBLIC USE ALL AREA SHOWN ON THIS PLAT AS STREETS, UTILITIES, ALLEYS, WALKS, RECREATION AND PARKS, OPEN SPACE AND EASEMENTS, EXCEPT THOSE SPECIFICALLY INDICATED AS PRIVATE AND THAT I WILL MAINTAIN ALL SUCH AREAS UNTIL THE OFFER OF DEDICATION IS ACCEPTED BY THE APPROPRIATE PUBLIC AUTHORITY OR HOME OWNERS' ASSOCIATION. ALL PROPERTY SHOWN ON THIS PLAT AS DEDICATED FOR A PUBLIC USE SHALL BE DEEMED TO BE DEDICATED FOR ANY OTHER PUBLIC USE AUTHORIZED BY LAW WHEN SUCH USE IS APPROVED BY THE APPROPRIATE PUBLIC AUTHORITY IN THE PUBLIC INTEREST.

DATE \_\_\_\_\_ OWNER \_\_\_\_\_

I, \_\_\_\_\_, A NOTARY PUBLIC OF \_\_\_\_\_ COUNTY,

NORTH CAROLINA, DO HEREBY CERTIFY THAT \_\_\_\_\_

PERSONALLY APPEARED BEFORE ME THIS DATE AND ACKNOWLEDGED THE DUE EXECUTION OF THE FOREGOING CERTIFICATE.

WITNESS MY HAND AND OFFICIAL SEAL THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_\_

NOTARY PUBLIC \_\_\_\_\_

MY COMMISSION EXPIRES \_\_\_\_\_

**PUBLIC STREETS DIVISION OF HIGHWAY DISTRICT ENGINEER CERTIFICATE**

I HEREBY CERTIFY THAT THE PUBLIC STREETS SHOWN ON THIS PLAT ARE INTENDED FOR DEDICATION AND HAVE BEEN DESIGNED OR COMPLETED IN ACCORDANCE WITH AT LEAST THE MINIMUM SPECIFICATIONS AND STANDARDS OF THE NC DEPARTMENT OF TRANSPORTATION FOR ACCEPTANCE OF SUBDIVISION STREETS ON THE NC HIGHWAY SYSTEM FOR MAINTENANCE.

DATE \_\_\_\_\_ OWNER \_\_\_\_\_

**ADJACENT ACTIVE FARMLAND STATEMENT**

AS OF THE RECORDED DATE OF THIS PLAT, SOME PORTIONS OF THIS DEVELOPMENT ADJOIN LAND USED FOR ACTIVE AGRICULTURE PURPOSES THAT IS ANTICIPATED TO GENERATE NOISE, LIGHT, DUST, OR VIBRATION AS PART OF ITS NORMAL OPERATION.

**AIRPORT OVERLAY DISTRICT STATEMENT**

ALL OR A PORTION OF THIS PROPERTY LIES WITHIN THE AIRPORT OVERLAY DISTRICT. PERSONS ON THE PREMISES MAY BE EXPOSED TO NOISE AND OTHER EFFECTS AS MAY BE INHERENT IN AIRPORT OPERATIONS. CURRITUCK COUNTY HAS PLACED CERTAIN RESTRICTIONS ON DEVELOPMENT AND USE OF PROPERTY WITHIN THIS OVERLAY.

**EASEMENT ESTABLISHMENT STATEMENT**

A 10 FOOT EASEMENT FOR UTILITIES AND DRAINAGE ALONG REAR AND SIDE PROPERTY LINES AND A 25 FOOT EASEMENT ALONG THE FRONT PROPERTY LINE IS HEREBY ESTABLISHED.

**FLOODWAY/FLOODPLAIN STATEMENT**

USE OF LAND WITHIN A FLOODWAY OR FLOODPLAIN IS SUBSTANTIALLY RESTRICTED BY CHAPTER 7 OF THE CURRITUCK COUNTY UNIFIED DEVELOPMENT ORDINANCE.

**INTERCONNECTIVITY STATEMENT**

THIS SUBDIVISION CONTAINS A RIGHT-OF-WAY THAT IS PLATTED WITH THE INTENT OF BEING EXTENDED AND CONTINUED TO AND FROM ADJOINING PROPERTIES. ACCESS WITHIN THE RIGHT-OF-WAY FOR STREETS AND UTILITIES SHALL NOT BE RESTRICTED.

**PUBLIC DEDICATION OF RECREATION AND PARK AREA STATEMENT**

A PAYMENT-IN-LIEU OF RECREATION AND PARK AREA DEDICATION HAS BEEN PROVIDED IN ACCORDANCE WITH THE CURRITUCK COUNTY UNIFIED DEVELOPMENT ORDINANCE. PAYMENTS-IN-LIEU RECEIVED BY THE COUNTY SHALL BE USED ONLY FOR THE ACQUISITION OR DEVELOPMENT OF RECREATION AND PARK AREAS, AND OPEN SPACE SITES CONSISTENT WITH THE REQUIREMENTS OF NORTH CAROLINA GENERAL STATUTES SECTION 153A-331.

**STORMWATER STATEMENT**

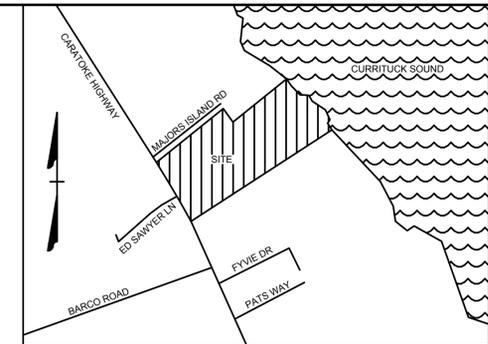
NO MORE THAN 24% OF ANY LOT SHALL BE COVERED BY IMPERVIOUS STRUCTURES AND MATERIALS, INCLUDING ASPHALT, GRAVEL, CONCRETE, BRICK, STONE, SLATE, OR SIMILAR MATERIAL, NOT INCLUDING WOOD DECKING OR THE WATER SURFACE OF SWIMMING POOLS. THIS COVENANT IS INTENDED TO ENSURE COMPLIANCE WITH THE STORMWATER PERMIT NUMBER ISSUED BY THE STATE OF NORTH CAROLINA. THE COVENANT MAY NOT BE CHANGED OR DELETED WITHOUT THE CONSENT OF THE STATE. FILLING IN OR PIPING OF ANY VEGETATIVE CONVEYANCES (DITCHES, SWALES, ETC.) ASSOCIATED WITH THIS DEVELOPMENT, EXCEPT FOR AVERAGE DRIVEWAY CROSSINGS, IS STRICTLY PROHIBITED BY ANY PERSON. THE LOT COVERAGE ALLOWANCE PROVIDED IN THE CURRITUCK COUNTY UNIFIED DEVELOPMENT ORDINANCE MAY BE DIFFERENT THAN THE NC STATE STORMWATER PERMIT. THE MOST RESTRICTIVE LOT COVERAGE SHALL APPLY.

**WETLANDS STATEMENT**

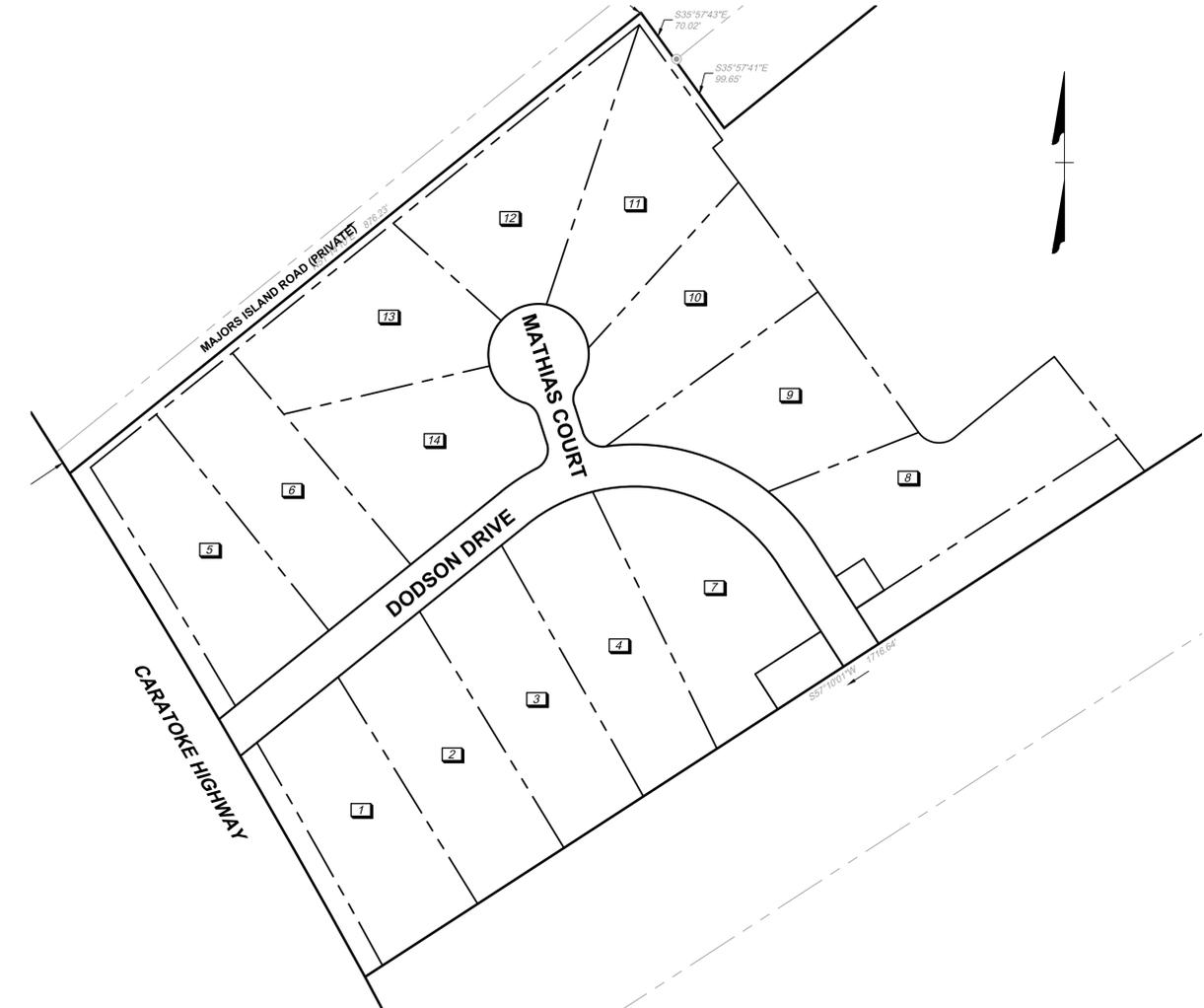
PROPERTY ON THIS PLAT MAY CONTAIN 404 WETLANDS AND MAY REQUIRE U.S. CORP OF ENGINEERS APPROVAL PRIOR TO DEVELOPMENT OF THE PROPERTY.

**PROPERTY ADDRESSES**

- LOT 1: 100 DODSON DR
- LOT 2: 102 DODSON DR
- LOT 3: 104 DODSON DR
- LOT 4: 106 DODSON DR
- LOT 5: 101 DODSON DR
- LOT 6: 103 DODSON DR
- LOT 7: 108 DODSON DR
- LOT 8: 111 DODSON DR
- LOT 9: 109 DODSON DR
- LOT 10: 100 MATHIAS CT
- LOT 11: 102 MATHIAS CT
- LOT 12: 104 MATHIAS CT
- LOT 13: 103 MATHIAS CT
- LOT 14: 101 MATHIAS CT



**VICINITY MAP**  
NO SCALE



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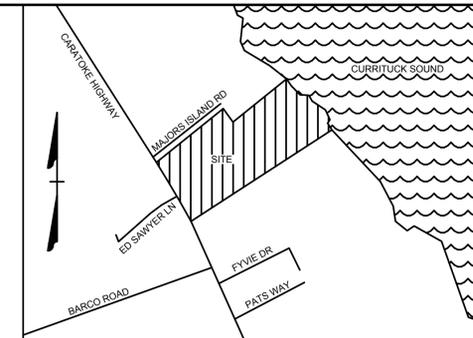
DATE 04/22/2020  
DRAWN BY JHS  
DESIGNED BY JAM  
CHECKED BY JAM  
SCALE N/A

**TIMMONS GROUP**  
NORTH CAROLINA LICENSE NO. C-1652  
BOSWOOD ESTATES  
CRAWFORD TOWNSHIP - CURRITUCK COUNTY - NORTH CAROLINA  
COVER SHEET

JOB NO. 44845  
SHEET NO. C1.0

Attachment: 2 Boswood Estates ProPlat 20200003 (PB 20-09 Boswood Estates, Phase 1 & 2)

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DATE	REVISION DESCRIPTION
08/22/2020	REVISED PER TRC COMMENTS
08/03/2020	REVISED PER TRC COMMENTS

DATE  
04/22/2020

DRAWN BY  
JHS

DESIGNED BY  
NA

CHECKED BY  
JAM

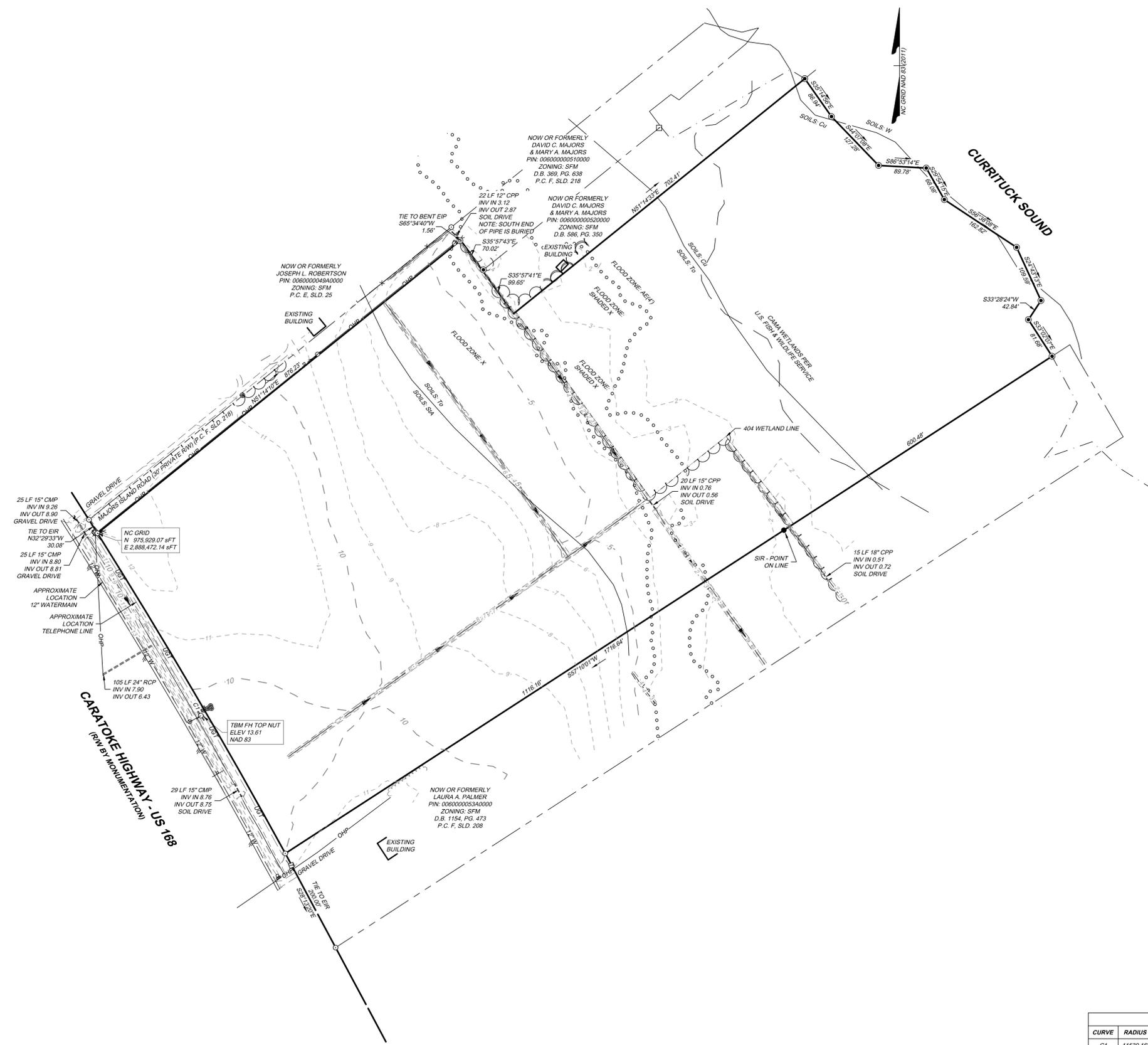
SCALE  
1" = 100'

# TIMMONS GROUP

NORTH CAROLINA LICENSE NO. C-1652  
**BOSWOOD ESTATES**  
CRAWFORD TOWNSHIP - CURRITUCK COUNTY - NORTH CAROLINA  
**EXISTING CONDITIONS**

JOB NO.  
44845  
SHEET NO.  
C2.0

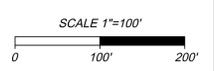
Attachment: 2 Boswood Estates ProPlan 20200603 (PB 20-09 Boswood Estates, Phase 1 & 2)



### LEGEND

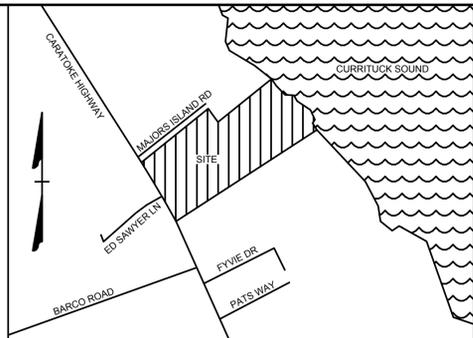
- BENCHMARK
- EXISTING IRON PIPE (EIP)
- EXISTING IRON REBAR (EIR)
- SET IRON REBAR (SIR)
- CALCULATED POINT
- EXISTING FIRE HYDRANT
- EXISTING WATER VALVE
- EXISTING WATER METER
- EXISTING SIGN / MARKER
- EXISTING SIGN / MARKER
- EXISTING UTILITY POLE
- EXISTING GUY WIRE
- EXISTING TELEPHONE PEDESTAL
- EXISTING MAILBOX
- EXISTING BOLLARD
- PROPERTY BOUNDARY LINES
- PROPERTY EXTENSION LINES
- RIGHT OF WAY LINES
- EXISTING EDGE OF PAVEMENT
- EXISTING TELEPHONE LINE
- 
- EXISTING OVERHEAD POWER
- EXISTING CENTER OF DITCH
- EXISTING TOP OF BANK
- EXISTING STORM PIPE
- EXISTING APPROXIMATE SHORELINE
- EXISTING FENCE
- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- FEMA FLOOD ZONE
- 404 WETLAND LINE
- SOILS LINE
- CAMA LINE
- SHRUB LINE
- TREELINE

CURVE TABLE						
CURVE	RADIUS	LENGTH	TANGENT	DELTA	CHORD BEARING	CHORD
C1	11539.16'	695.78'	347.99'	3°27'17"	N30°31'05\"/>	695.67'



S:\10944445 - Boswood Estates - Mathias Property - Barco - NC\DWG\Sheet\CD\44845 - C1.0 - EXIST COND.dwg | Printed on 05/20/2020 4:08 PM | by John Sawyer

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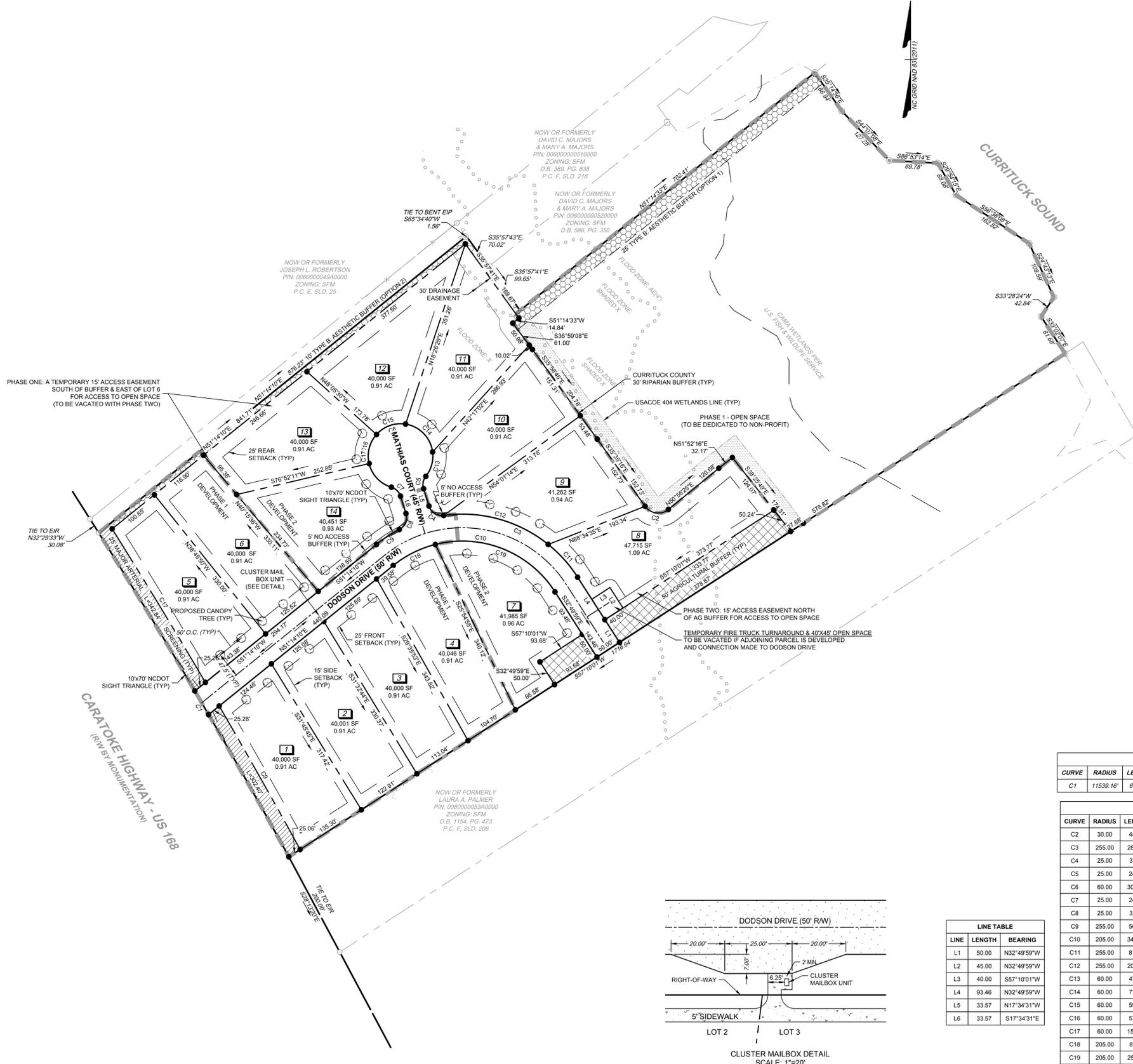
DATE  
04/22/2020  
DRAWN BY  
JHS  
DESIGNED BY  
JAM  
CHECKED BY  
JAM  
SCALE  
1" = 100'

# TIMMONS GROUP

NORTH CAROLINA LICENSE NO. C-1652  
BOSWOOD ESTATES  
CRAWFORD TOWNSHIP - CURRITUCK COUNTY - NORTH CAROLINA  
PRELIMINARY PLAT & BUFFERING PLAN

JOB NO.  
44845  
SHEET NO.  
C3.0

Attachment: 2 Boswood Estates ProPlat 20200003 (PB 20-09 Boswood Estates, Phase 1 & 2)



**BUFFERING REQUIREMENTS**

**TYPE B, OPTION 1: 702 LF PROPOSED (25' WIDE)\***

CANOPY TREES: 8 ACI PER 100 LF = 56 ACI REQUIRED  
UNDERSTORY TREES: 10 ACI PER 100 LF = 70 ACI REQUIRED  
SHRUBS: 15 SHRUBS PER 100 LF = 105 SHRUBS REQUIRED

**TYPE B, OPTION 2: 1020 LF PROPOSED (10' WIDE)**

CANOPY TREES: 2 ACI PER 100 LF = 20 ACI REQUIRED  
UNDERSTORY TREES: 14 ACI PER 100 LF = 143 ACI REQUIRED  
SHRUBS: 20 SHRUBS PER 100 LF = 204 SHRUBS REQUIRED

**MAJOR ARTERIAL SCREENING: 645 LF (25' WIDE)**

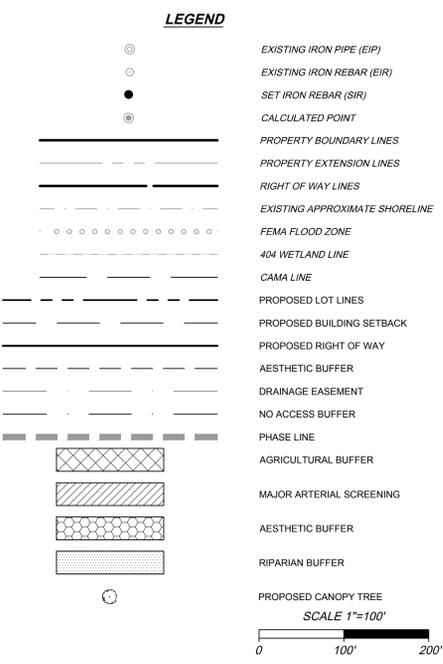
CANOPY TREES: 6 ACI PER 100 LF = 39 ACI REQUIRED  
UNDERSTORY TREES: 3 ACI PER 100 LF = 19 ACI REQUIRED  
SHRUBS: 5 SHRUBS PER 100 LF = 32 SHRUBS REQUIRED

**AGRICULTURAL BUFFER: 472 LF (50' WIDE)**

14 TREES (HARDWOOD/EVERGREEN MIX) PER 100 LF = 19 TREES REQUIRED

ACI = AGGREGATE CALIPER INCHES

\*AREA OF PROPOSED TYPE B, OPTION 1 BUFFER IS LOCATED IN WETLANDS.



**CURVE TABLE**

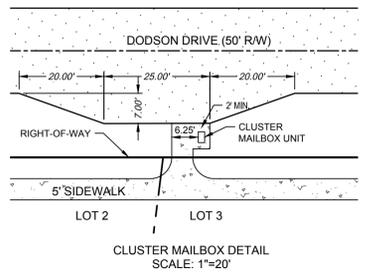
CURVE	RADIUS	LENGTH	TANGENT	DELTA	CHORD BEARING	CHORD
C1	11539.16'	695.78'	347.99'	3°27'17"	N30°31'05"W	695.67'

**CURVE TABLE**

CURVE	RADIUS	LENGTH	TANGENT	DELTA	CHORD BEARING	CHORD
C2	30.00	48.94	31.88	93°28'20"	S82°19'25"E	43.69
C3	255.00	289.18	162.38	64°58'30"	N65°19'14"W	273.93
C4	25.00	35.01	21.06	80°13'59"	S57°41'30"E	32.22
C5	25.00	24.45	13.30	56°01'32"	S10°26'15"W	23.48
C6	60.00	305.84	40.43	292°03'04"	S72°25'29"W	67.06
C7	25.00	24.45	13.30	56°01'32"	N45°35'17"W	23.48
C8	25.00	35.01	21.06	80°13'59"	N22°32'28"E	32.22
C9	255.00	50.83	25.50	11°25'18"	S66°56'49"W	50.75
C10	205.00	343.23	227.40	95°55'51"	N80°47'55"W	304.52
C11	255.00	81.99	41.35	18°25'19"	N42°02'39"W	81.64
C12	255.00	207.19	109.70	46°33'11"	N74°31'54"W	201.54
C13	60.00	47.97	25.35	45°48'18"	N15°32'52"E	46.70
C14	60.00	77.26	45.03	73°46'47"	N44°14'41"W	72.03
C15	60.00	59.85	32.55	56°57'26"	S70°23'13"W	57.22
C16	60.00	57.64	31.26	55°02'19"	S14°23'20"W	55.45
C17	60.00	152.92	196.41	146°01'32"	S0°35'17"E	114.76
C18	205.00	88.11	44.75	24°37'35"	S63°32'57"W	87.43
C19	205.00	255.12	147.05	71°18'16"	N68°29'07"W	238.97

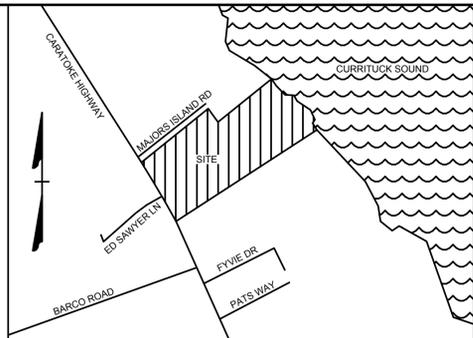
**LINE TABLE**

LINE	LENGTH	BEARING
L1	50.00	N32°49'59"W
L2	45.00	N32°49'59"W
L3	40.00	S57°10'01"W
L4	93.46	N32°49'59"W
L5	33.57	N17°34'31"W
L6	33.57	S17°34'31"E



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DATE	REVISION DESCRIPTION
08/22/2020	REVISED PER TRC COMMENTS
08/03/2020	REVISED PER TRC COMMENTS

DATE  
04/22/2020  
DRAWN BY  
JHS  
DESIGNED BY  
JAM  
CHECKED BY  
JAM  
SCALE  
1" = 50'

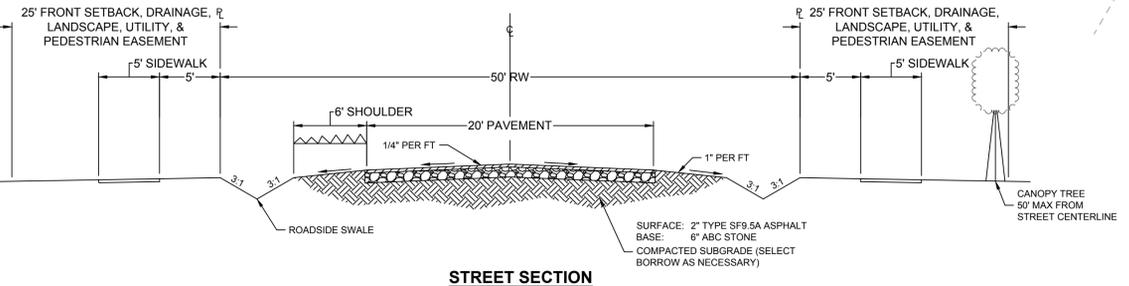
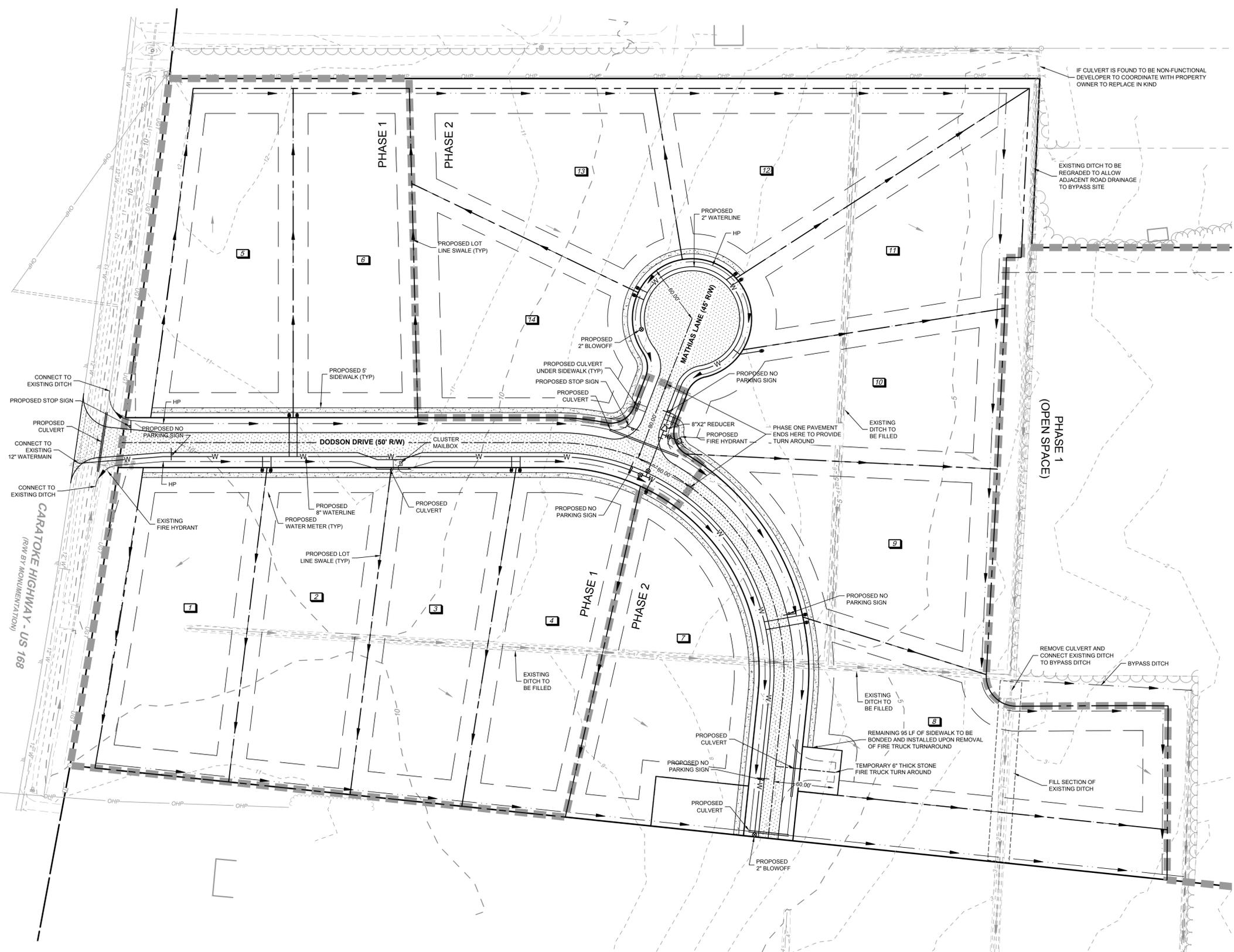
# TIMMONS GROUP

NORTH CAROLINA LICENSE NO. C-1652

**BOSWOOD ESTATES**  
CRAWFORD TOWNSHIP - CURRITUCK COUNTY - NORTH CAROLINA  
**PRELIMINARY PLAT - STORMWATER MANAGEMENT & UTILITIES**

Attachment: 2 Boswood Estates PrePlat 20200603 (PB 20-09 Boswood Estates, Phase 1 & 2)

JOB NO.  
44845  
SHEET NO.  
C4.0



**LEGEND**

	BENCHMARK
	EXISTING IRON PIPE (EIP)
	EXISTING IRON REBAR (EIR)
	SET IRON REBAR (SIR)
	CALCULATED POINT
	EXISTING FIRE HYDRANT
	EXISTING WATER VALVE
	EXISTING WATER METER
	EXISTING SIGN / MARKER
	EXISTING UTILITY POLE
	EXISTING GUY WIRE
	EXISTING TELEPHONE PEDESTAL
	EXISTING MAILBOX
	EXISTING BOLLARD
	PROPERTY BOUNDARY LINES
	PROPERTY EXTENSION LINES
	RIGHT OF WAY LINES
	EXISTING EDGE OF PAVEMENT
	EXISTING TELEPHONE LINE
	EXISTING WATERLINE
	EXISTING OVERHEAD POWER
	EXISTING CENTER OF DITCH
	EXISTING TOP OF BANK
	EXISTING STORM PIPE
	EXISTING APPROXIMATE SHORELINE
	EXISTING FENCE
	EXISTING MAJOR CONTOUR
	EXISTING MINOR CONTOUR
	SHRUB LINE
	TREE LINE
	PROPOSED WATER METER
	PROPOSED FIRE HYDRANT
	PROPOSED LOT LINES
	PROPOSED RIGHT OF WAY
	PROPOSED BUILDING SETBACK & DRAINAGE EASEMENT
	DRAINAGE EASEMENT
	PHASE LINE
	PROPOSED WATERLINE
	PROPOSED DITCH/SWALE
	PROPOSED DRAINAGE DIRECTION
	PROPOSED CONCRETE
	PROPOSED ASPHALT

SCALE 1"=50'  
0 50' 100'

S:\10944845 - Boswood Estates - Mathias Property - Barco, NC\DWG\Sheet\CD\44845\_C3.0 - PRELIM PLAT.dwg | Plotted on 06/23/2020 4:10 PM | by John Sawyer



## Currituck County

Department of Planning and Community Development  
 153 Courthouse Road, Suite 110  
 Currituck, North Carolina 27929  
 252-232-3055  
 FAX 252-232-3026

### MEMORANDUM

**To:** Jason Mizelle, Timmons Group  
**From:** Planning Staff  
**Date:** May 13, 2020  
**Subject:** Boswood Estates, Preliminary Plat/Use Permit TRC Comments

The following comments were received for the May 13, 2020 TRC meeting. In order to be scheduled for the June 15, 2020 (tentative) Board of Commissioners meeting, please address all comments and resubmit a corrected plan by 3:00 p.m. on May 26, 2020. TRC comments are valid for six months from the date of the TRC meeting.

#### **Planning, Donna Voliva 252-232-6032**

Reviewed

1. Identify the recreation and park area dedication on the preliminary plat. It is anticipated that a payment in lieu of the dedication will be accepted due to the size of the area to be dedicated.
2. Are there any ditches on the property that drain five acres or more?
3. The CBU (cluster mailbox unit) shall be designed to not back into the street. The NCDOT guidance documents suggest installation of turnout that allows for vehicle pull off, parking (parallel), and placement of the CBU. Consider placing the CBU on the right side of the entrance. This could be accessed when property owners and USPS enter the development. Although there are 14 lots proposed, the CBU is designed so that access must be made when exiting the development. This design appears to encourage u-turns.
4. The minimum building setbacks indicate an increased setback along the front property line. Is this intended to provide additional space between the dwelling and the sidewalk and will this be enforced by restrictive covenants?
5. Crosswalks shall be provided on Mathias Lane (Court).
6. Sidewalks and utilities shall be designed to extend to property lines (farmland buffer).
7. Identify open space on the preliminary plat.
8. Provide proposed landscape plan for major arterial screening, farmland buffer, and perimeter screening.
9. There are several existing ditches proposed to be filled. What impacts will this have on the surrounding drainage patterns?
10. Albemarle Regional Bicycle Plan suggests an extended shoulder along Caratoke Highway.
11. Are street lights proposed?
12. Are there any improvements proposed in the wetlands? Private improvements such as walkways are not allowed in common open space.
13. What is the intended ownership of the open space?
14. How will access be provided to the conservation area with Phase 1?

#### **Currituck County Building and Fire Inspections (Bill News, 252-232-6023)**

Approval with corrections

1. No parking signage shall be spaced at intersections and entrance (approximately 4-5 signs).

2. Recommend that turn around for fire department is located at the end towards AG buffer and is of material that will allow it to be removed if/when road connected/extended in the future. This way when the road is connected the owner of that lot with current plan raise issue.

**Currituck County GIS (Harry Lee, 252-232-4039)**

Reviewed

1. Since the proposed Mathias Lane is a cul-de-sac, can you please change the name to Mathias Court?
2. Address assignment:

Lot 1: 100 Dodson Dr  
 Lot 2: 102 Dodson Dr  
 Lot 3: 104 Dodson Dr  
 Lot 4: 106 Dodson Dr  
 Lot 5: 101 Dodson Dr  
 Lot 6: 103 Dodson Dr  
 Lot 7: 108 Dodson Dr  
 Lot 8: 111 Dodson Dr  
 Lot 9: 109 Dodson Dr  
 Lot 10: 100 Mathias Ct  
 Lot 11: 102 Matthias Ct  
 Lot 12: 104 Matthias Ct  
 Lot 13: 103 Mathias Ct  
 Lot 14: 101 Mathias Ct

**Currituck County Parks and Recreation (Jason Weeks, 252-232-3007)**

No comment

**Currituck County Public Utilities, Water (Will Rumsey and Dave Spence, 252-232-2769)**

Reviewed

**Currituck County Economic Development (Larry Lombardi, 252-232-6015)**

Reviewed

1. No comment

**Currituck Soil and Stormwater (Dylan Lloyd, 252-232-3360)**

Approval with corrections

1. Replace 22' LF 12" CCP just north of the lot line between lots 11 and 12, ensure drainage from adjacent properties to the north into regarded ditch. Comments (pg. 2) say south end of existing pipe is buried – upon site visit, appeared non-functional.
2. Lot line between 14 and 13 show uphill drainage – will this create grading issues?
3. Since the 6.43 acres within CAMA setback is to be undisturbed, no CAMA permit is required – are there any drainage features that need to be improved within this area since flow lines show new ditches flowing in this direction?



**NCDOT (David Otts, 252-331-4737)**

Reviewed

1. The CBU parking spaces as shown can become a maintenance issue along the edge of pavement.
2. The fire truck turnaround shall be evaluated to ensure NCDOT maintenance of this area if the streets are intended to be turned over to NCDOT.

**NC Division of Coastal Management (Charlan Owens , 252-264-3901)**

Reviewed

1. Clarify whether or not any disturbance is proposed within an Area of Environmental Concern (AEC).
2. Clarify whether or not subdivision or individual property owners will be alled to install shoreline access improvements.
3. For permitting assistance, please contact Robert Corbet, DCM Field Representative. He can be reached at 252-264-3901 and at Robert.coret@ncdenr.gov.

**Albemarle Regional Health Services (Joe Hobbs, 252-232-6603)**

Reviewed

**The following items are necessary for resubmittal:**

- 3 - full size copies of revised plans.
- 1- 8.5"x11" copy of all revised plans.
- 1- PDF digital copy of all revised documents and plans.

A use permit hearing is an evidentiary hearing where the Board of Commissioners must make a Quasi-Judicial Decision.

- An evidentiary hearing will be held for the Board of Commissioners to gather competent, material and substantial evidence to establish the facts of the case.

- All testimony is made under oath.
- The applicant or opposing parties shall establish written findings of fact and conclusions of law.
- Parties with standing may participate fully in the evidentiary hearing, including presenting evidence, cross-examining witnesses, objecting to evidence, and making legal arguments.
- Non-parties may present competent, material, and substantial evidence that is not repetitive.

#### Typical Use Permit (Quasi-Judicial) Hearing at BOC

- Swearing in of witnesses/speakers
- Presentation by County Staff on Application
- Required Presentation by Applicant or Authorized Agent (20-25 minutes)
  - Applicant to Present Findings of Fact
- Public Comment Period (typically 3 minutes each)
- Applicant Rebuttal (typically 5 minutes)



# Major Subdivision Application

OFFICIAL USE ONLY:

Case Number: \_\_\_\_\_  
Date Filed: \_\_\_\_\_  
Gate Keeper: \_\_\_\_\_  
Amount Paid: \_\_\_\_\_

### Contact Information

APPLICANT:

Name: Boswood Estates, LLC  
Address: P.O. Box 100  
Currituck, NC 27929  
Telephone: 252.453-4300  
E-Mail Address: \_\_\_\_\_

PROPERTY OWNER:

Name: G. Dodson Mathias  
Address: 400 Avinger Lane, Villa 609  
Davidson, NC 28036  
Telephone: \_\_\_\_\_  
E-Mail Address: \_\_\_\_\_

LEGAL RELATIONSHIP OF APPLICANT TO PROPERTY OWNER: Contract Purchaser

### Request

Physical Street Address: 3800 Block, Caratoke Highway, Barco, NC 27917

Parcel Identification Number(s): 0060000053B0000

Subdivision Name: Boswood Estates

Number of Lots or Units: 14

Phase: I & II

TYPE OF SUBMITTAL

- Conservation and Development Plan
- Amended Sketch Plan/Use Permit
- Preliminary Plat (or amended)
  - Type I OR  Type II
- Construction Drawings (or amended)
- Final Plat (or amended)

TYPE OF SUBDIVISION

- Traditional Development
- Conservation Subdivision
- Planned Unit Development
- Planned Development

I hereby authorize county officials to enter my property for purposes of determining compliance with all applicable standards. All information submitted and required as part of this process shall become public record.

X   
Property Owner(s)/Applicant\*

4/3/20  
Date

**\*NOTE: Form must be signed by the owner(s) of record, contract purchaser(s), or other person(s) having a recognized property interest. If there are multiple property owners/applicants a signature is required for each.**

### Community Meeting, if applicable

Date Meeting Held: \_\_\_\_\_ Meeting Location: \_\_\_\_\_

Attachment: 4 Boswood Estates App 20may13 (PB 20-09 Boswood Estates, Phase 1 & 2)

Use Permit Review Standards, if applicable

PUD Amended Sketch Plan/Use Permit, Type II Preliminary Plat

Purpose of Use Permit and Project Narrative (please provide on additional paper if needed):
Boswood Estates proposes the division of 26.94 acres into a 14-lot Traditional Subdivision with open space set aside to preserve the wetlands and marsh along Currituck Sound. The open space exceeds the UDO requirements and storm water management will be provided in accordance with the Currituck storm water manual.

The applicant shall provide a response to the each one of the following issues. The Board of Commissioners must provide specific findings of fact based on the evidence submitted. All findings shall be made in the affirmative for the Board of Commissioners to issue the use permit.

- A. The use will not endanger the public health or safety. The us will be in accordance with County health & safety standards and those recommended by the Albemarle Regional Health Services in regards to on site wastewater systems. The project should not adversely affect health & safety.
B. The use will not injure the value of adjoining or abutting lands and will be in harmony with the area in which it is located. Lands to the north have been developed for single-family homes as well as the properties across the highway. Lot sizes proposed are in keeping with what is adjacent. Wetlands will be preserved. The use will not injure the value of adjacent properties and is in harmony with the surrounding area.
C. The use will be in conformity with the Land Use Plan or other officially adopted plan. The Land Use Plan shows the land proposed for development as Full Service and the wetlands as Conservation. Although density is not specified in the GB zone, the density proposed is 0.5 units per acre. The low density is in keeping with the neighboring areas and is less than the 2 units per acre suggested in the Land Use Plan.
D. The use will not exceed the county's ability to provide adequate public facilities, including, but not limited to, schools, fire and rescue, law enforcement, and other county facilities. Applicable state standards and guidelines shall be followed for determining when public facilities are adequate. The proposed development is within the school districts that have capacity. County water is available and on the project side of the highway. The proposed 14 lots should not impede the County's ability to provide service.

I, the undersigned, do certify that all of the information presented in this application is accurate to the best of my knowledge, information, and belief. Further, I hereby authorize county officials to enter my property for purposes of determining zoning compliance. All information submitted and required as part of this application process shall become public record.

X [Signature]
Property Owner(s)/Applicant\*

[Signature] 4/3/20
Date

\*NOTE: Form must be signed by the owner(s) of record, contract purchaser(s), or other person(s) having a recognized property interest. If there are multiple property owners/applicants a signature is required for each.

Attachment: 4 Boswood Estates App 20may13 (PB 20-09 Boswood Estates, Phase 1 & 2)

**BOSWOOD ESTATES**  
CRAWFORD TOWNSHIP, CURRITUCK COUNTY, NORTH CAROLINA

**PRELIMINARY STORMWATER  
MANAGEMENT NARRATIVE**

APRIL 20, 2020

**PREPARED BY:**



**TIMMONS GROUP**  
YOUR VISION ACHIEVED THROUGH OURS.

1805 West City Drive, Unit E  
Elizabeth City, NC 27909  
252.621.5030  
License No. C-1652  
[www.timmons.com](http://www.timmons.com)

Attachment: 5 Boswood Estates Preliminary Stormwater Narrative (PB 20-09 Boswood Estates, Phase 1 & 2)

## BOSWOOD ESTATES – 14-LOT SUBDIVISION

### Site Information

Boswood Estates is a 26.94-acre parcel located adjacent to the Currituck Sound located in Barco, Crawford Township. The site contains 16.28 acres of uplands and 10.66 acres of combined CAMA and U.S. Army Corps of Engineers jurisdictional wetlands. The upland area is currently cultivated and consist of existing drainage ditches that captures the over land runoff from the property and further conveys that runoff downstream to the Currituck Sound.

The topography of the site is gently sloping from South to North from the Highway to the Woods/Wetlands and out to the Currituck Sound. Elevations range from as high as 12 foot MSL at the highway to 3-4 foot MSL at the wetlands.

Soils within the site consist primarily of State (StA) fine sandy loam in the front of the site and Tomotley (To) fine sandy loam at the rear of the upland area and into the wooded portion of the site containing the 404 jurisdictional wetlands. The soils within the CAMA wetland portion of the site are primarily Currituck (Cu) mucky peat. The upland soils are well drained from the current agricultural ditches.

### Proposed Development

The proposed development will utilize vegetative swales to convey runoff from the proposed lots along side property lines, to swales adjacent to the rear property lines of each lot. From there the runoff will continue to be directed through vegetated swales to the existing outlet ditch to the east. The existing outlet ditch directly outlets to the Currituck Sound with no downstream conflicts. There is very minimal off-site runoff from the adjacent private access road to the north of the property which will be allowed to continue draining through the eastern most ditch to the wetlands. This ditch will be regraded to improve the flow from the access road culvert and provide relief for the current condition, while bypassing the proposed development.

The southwest side of the property along Caratoke Highway is located along a ridge that separates the subject sites runoff from that of the NCDOT highway. Runoff from the highway and the adjacent roadside ditch is directed via a cross culvert under the highway south opposite the subject property. Other than minor runoff from within the State's right of way for road connection, no additional runoff is intended to be conveyed into that drainage area.

### Preliminary Analysis

The proposed development will convert current constantly disturbed agricultural lands and steep sloped ditches to maintained grass lawns and vegetated grass swales with shallow grades to allow for reduced runoff rates and better treatment of runoff. Is the intention of this development to utilize the NCDEQ's Low Density Stormwater Permit option to ensure that the proposed development maintains a low impact design. The direct outlet to the Currituck Sound and lack of substantial off-site runoff into the property, makes the site applicable for the Alternative Stormwater Runoff Storage as depicted in the County's Stormwater Manual. It is anticipated that any rise in stormwater runoff to the water surface will be below 0.01 feet. HEC-RAS models for the site will be provided during the Permitting & Construction document phase.

**U.S. ARMY CORPS OF ENGINEERS  
WILMINGTON DISTRICT**

Action Id. SAW-2020-00073 County: Currituck County U.S.G.S. Quad: Barco

**NOTIFICATION OF JURISDICTIONAL DETERMINATION**

**Property Owner/Applicant:** Dodson Mathias  
**Address:** 400 Avinger Lane  
Villa 609  
Davidson, NC, 28036

Size (acres) 29.64  
Nearest Waterway Currituck Sound  
USGS HUC 03010205

Nearest Town Barco  
River Basin Albemarle-Chowan  
Coordinates Latitude: 36.394244  
Longitude: -75.978563

**Location description:** Property is located at 3860 Caratoke Highway, 0.5 miles north of Shortcut Road and Caratoke Highway intersection, near the town of Barco, Currituck County, North Carolina. Property can be identified by Parcel Index Number 0060000053A0000 and is currently in both agricultural and forested abutting the Currituck Sound.

**Indicate Which of the Following Apply:**

**A. Preliminary Determination**

- There are waters, including wetlands, on the above described property, that may be subject to Section 404 of the Clean Water Act (CWA)(33 USC § 1344) and/or Section 10 of the Rivers and Harbors Act (RHA) (33 USC § 403). The waters, including wetlands, have been delineated, and the delineation has been verified by the Corps to be sufficiently accurate and reliable. Therefore this preliminary jurisdiction determination may be used in the permit evaluation process, including determining compensatory mitigation. For purposes of computation of impacts, compensatory mitigation requirements, and other resource protection measures, a permit decision made on the basis of a preliminary JD will treat all waters and wetlands that would be affected in any way by the permitted activity on the site as if they are jurisdictional waters of the U.S. This preliminary determination is not an appealable action under the Regulatory Program Administrative Appeal Process (Reference 33 CFR Part 331). However, you may request an approved JD, which is an appealable action, by contacting the Corps district for further instruction.
- There are wetlands on the above described property, that may be subject to Section 404 of the Clean Water Act (CWA)(33 USC § 1344) and/or Section 10 of the Rivers and Harbors Act (RHA) (33 USC § 403). However, since the waters, including wetlands, have not been properly delineated, this preliminary jurisdiction determination may not be used in the permit evaluation process. Without a verified wetland delineation, this preliminary determination is merely an effective presumption of CWA/RHA jurisdiction over all of the waters, including wetlands, at the project area, which is not sufficiently accurate and reliable to support an enforceable permit decision. We recommend that you have the waters of the U.S. on your property delineated. As the Corps may not be able to accomplish this wetland delineation in a timely manner, you may wish to obtain a consultant to conduct a delineation that can be verified by the Corps.

**B. Approved Determination**

- There are Navigable Waters of the United States within the above described property subject to the permit requirements of Section 10 of the Rivers and Harbors Act (RHA) (33 USC § 403) and Section 404 of the Clean Water Act (CWA)(33 USC § 1344). Unless there is a change in law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.
- There are waters of the U.S., including wetlands, on the above described project area subject to the permit requirements of Section 404 of the Clean Water Act (CWA) (33 USC § 1344). Unless there is a change in the law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.
- We recommend you have the waters of the U.S. on your property delineated. As the Corps may not be able to accomplish this wetland delineation in a timely manner, you may wish to obtain a consultant to conduct a delineation that can be verified by the Corps.

SAW-2020-00073

- The waters of the U.S., including wetlands, on your project area have been delineated and the delineation has been verified by the Corps. We strongly suggest you have this delineation surveyed. Upon completion, this survey should be reviewed and verified by the Corps. Once verified, this survey will provide an accurate depiction of all areas subject to CWA jurisdiction on your property which, provided there is no change in the law or our published regulations, may be relied upon for a period not to exceed five years.
- The waters of the U.S., including wetlands, have been delineated and surveyed and are accurately depicted on the plat signed by the Corps Regulatory Official identified below on \_\_\_\_\_. Unless there is a change in the law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.
- There are no waters of the U.S., to include wetlands, present on the above described project area which are subject to the permit requirements of Section 404 of the Clean Water Act (33 USC 1344). Unless there is a change in the law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.
- The property is located in one of the 20 Coastal Counties subject to regulation under the Coastal Area Management Act (CAMA). You should contact the Division of Coastal Management in Elizabeth City, NC, at (252) 264-3901 to determine their requirements.

Placement of dredged or fill material within waters of the US, including wetlands, without a Department of the Army permit may constitute a violation of Section 301 of the Clean Water Act (33 USC § 1311). Placement of dredged or fill material, construction or placement of structures, or work within navigable waters of the United States without a Department of the Army permit may constitute a violation of Sections 9 and/or 10 of the Rivers and Harbors Act (33 USC § 401 and/or 403). If you have any questions regarding this determination and/or the Corps regulatory program, please contact Krystynka Stygar at (910) 251-4619 or Krystynka.B.Stygar@usace.army.mil.

**C. Basis For Determination: This property exhibits wetland criteria as described in the 1987 Corps Wetland Delineation Manual and the Atlantic and Gulf Coast Regional Supplement. The Section 404 wetlands are directly abutting the Currituck Sound a Traditional Navigable Water.**

**D. Remarks:**

**E. Attention USDA Program Participants**

This delineation/determination has been conducted to identify the limits of Corps' Clean Water Act jurisdiction for the particular site identified in this request. The delineation/determination may not be valid for the wetland conservation provisions of the Food Security Act of 1985. If you or your tenant are USDA Program participants, or anticipate participation in USDA programs, you should request a certified wetland determination from the local office of the Natural Resources Conservation Service, prior to starting work.

**F. Appeals Information for Approved Jurisdiction Determinations (as indicated in Section B. above)**

If you object to this determination, you may request an administrative appeal under Corps regulations at 33 CFR Part 331. Enclosed you will find a Notification of Appeal Process (NAP) fact sheet and Request for Appeal (RFA) form. If you request to appeal this determination you must submit a completed RFA form to the following address:

US Army Corps of Engineers  
South Atlantic Division  
Attn: Philip Shannin, Appeal Review Officer  
60 Forsyth Street SW, Room 10M15  
Atlanta, Georgia 30303-8801

In order for an RFA to be accepted by the Corps, the Corps must determine that it is complete, that it meets the criteria for appeal under 33 CFR part 331.5, and that it has been received by the Division Office within 60 days of the date of the NAP. Should you decide to submit an RFA form, it must be received at the above address by NA.

**It is not necessary to submit an RFA form to the Division Office if you do not object to the determination in this correspondence.**

Corps Regulatory Official: \_\_\_\_\_

*Krystynka Stygar*

SAW-2020-00073

Date: **March 25, 2020** Expiration Date: NA

The Wilmington District is committed to providing the highest level of support to the public. To help us ensure we continue to do so, please complete our Customer Satisfaction Survey, located online at [http://corpsmapu.usace.army.mil/cm\\_apex/f?p=136:4:0](http://corpsmapu.usace.army.mil/cm_apex/f?p=136:4:0).

Copy Furnished

Doug Dorman  
PO Box 3266  
Kitty Hawk, NC 27949

dougdaec@gmail.com

## NOTIFICATION OF ADMINISTRATIVE APPEAL OPTIONS AND PROCESS AND REQUEST FOR APPEAL

Applicant: **Dodson Mathias**File Number: **SAW-2020-00073**Date: **March 25, 2020**

Attached is:

See Section below

<input type="checkbox"/>	INITIAL PROFFERED PERMIT (Standard Permit or Letter of permission)	A
<input type="checkbox"/>	PROFFERED PERMIT (Standard Permit or Letter of permission)	B
<input type="checkbox"/>	PERMIT DENIAL	C
<input type="checkbox"/>	APPROVED JURISDICTIONAL DETERMINATION	D
<input checked="" type="checkbox"/>	PRELIMINARY JURISDICTIONAL DETERMINATION	E

SECTION I - The following identifies your rights and options regarding an administrative appeal of the above decision.

Additional information may be found at <http://www.usace.army.mil/Missions/CivilWorks/RegulatoryProgramandPermits.aspx> or Corps regulations at 33 CFR Part 331.

**A: INITIAL PROFFERED PERMIT:** You may accept or object to the permit.

- **ACCEPT:** If you received a Standard Permit, you may sign the permit document and return it to the district engineer for final authorization. If you received a Letter of Permission (LOP), you may accept the LOP and your work is authorized. Your signature on the Standard Permit or acceptance of the LOP means that you accept the permit in its entirety, and waive all rights to appeal the permit, including its terms and conditions, and approved jurisdictional determinations associated with the permit.
- **OBJECT:** If you object to the permit (Standard or LOP) because of certain terms and conditions therein, you may request that the permit be modified accordingly. You must complete Section II of this form and return the form to the district engineer. Your objections must be received by the district engineer within 60 days of the date of this notice, or you will forfeit your right to appeal the permit in the future. Upon receipt of your letter, the district engineer will evaluate your objections and may: (a) modify the permit to address all of your concerns, (b) modify the permit to address some of your objections, or (c) not modify the permit having determined that the permit should be issued as previously written. After evaluating your objections, the district engineer will send you a proffered permit for your reconsideration, as indicated in Section B below.

**B: PROFFERED PERMIT:** You may accept or appeal the permit

- **ACCEPT:** If you received a Standard Permit, you may sign the permit document and return it to the district engineer for final authorization. If you received a Letter of Permission (LOP), you may accept the LOP and your work is authorized. Your signature on the Standard Permit or acceptance of the LOP means that you accept the permit in its entirety, and waive all rights to appeal the permit, including its terms and conditions, and approved jurisdictional determinations associated with the permit.
- **APPEAL:** If you choose to decline the proffered permit (Standard or LOP) because of certain terms and conditions therein, you may appeal the declined permit under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the division engineer. This form must be received by the division engineer within 60 days of the date of this notice.

**C: PERMIT DENIAL:** You may appeal the denial of a permit under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the division engineer. This form must be received by the division engineer within 60 days of the date of this notice.

**D: APPROVED JURISDICTIONAL DETERMINATION:** You may accept or appeal the approved JD or provide new information.

- **ACCEPT:** You do not need to notify the Corps to accept an approved JD. Failure to notify the Corps within 60 days of the date of this notice means that you accept the approved JD in its entirety, and waive all rights to appeal the approved JD.
- **APPEAL:** If you disagree with the approved JD, you may appeal the approved JD under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the division engineer. This form must be received by the division engineer within 60 days of the date of this notice.

SAW-2020-00073

E: PRELIMINARY JURISDICTIONAL DETERMINATION: You do not need to respond to the Corps regarding the preliminary JD. The Preliminary JD is not appealable. If you wish, you may request an approved JD (which may be appealed), by contacting the Corps district for further instruction. Also you may provide new information for further consideration by the Corps to reevaluate the JD.

**SECTION II - REQUEST FOR APPEAL or OBJECTIONS TO AN INITIAL PROFFERED PERMIT**

REASONS FOR APPEAL OR OBJECTIONS: (Describe your reasons for appealing the decision or your objections to an initial proffered permit in clear concise statements. You may attach additional information to this form to clarify where your reasons or objections are addressed in the administrative record.)

ADDITIONAL INFORMATION: The appeal is limited to a review of the administrative record, the Corps memorandum for the record of the appeal conference or meeting, and any supplemental information that the review officer has determined is needed to clarify the administrative record. Neither the appellant nor the Corps may add new information or analyses to the record. However, you may provide additional information to clarify the location of information that is already in the administrative record.

**POINT OF CONTACT FOR QUESTIONS OR INFORMATION:**

If you have questions regarding this decision and/or the appeal process you may contact:  
**District Engineer, Wilmington Regulatory Division,**  
**Attn: Krystynka Stygar**  
**2407 West 5<sup>th</sup> Street**  
**Washington, NC 27889**

If you only have questions regarding the appeal process you may also contact:  
 Mr. Philip Shannin, Administrative Appeal Review Officer  
 CESAD-PDO  
 U.S. Army Corps of Engineers, South Atlantic Division  
 60 Forsyth Street, Room 10M15  
 Atlanta, Georgia 30303-8801  
 Phone: (404) 562-5137

RIGHT OF ENTRY: Your signature below grants the right of entry to Corps of Engineers personnel, and any government consultants, to conduct investigations of the project site during the course of the appeal process. You will be provided a 15 day notice of any site investigation, and will have the opportunity to participate in all site investigations.

_____ Signature of appellant or agent.	Date:	Telephone number:
---	-------	-------------------

**For appeals on Initial Proffered Permits send this form to:**

**District Engineer, Wilmington Regulatory Division, Attn: Krystynka Stygar, 2407 West 5<sup>th</sup> Street, Washington, NC 27889**

**For Permit denials, Proffered Permits and approved Jurisdictional Determinations send this form to:**

**Division Engineer, Commander, U.S. Army Engineer Division, South Atlantic, Attn: Mr. Philip Shannin, Administrative Appeal Officer, CESAD-PDO, 60 Forsyth Street, Room 10M15, Atlanta, Georgia 30303-8801 Phone: (404) 562-5137**

Attachment: 6 Boswood Estates USACE Wetlands (PB 20-09 Boswood Estates, Phase 1 & 2)

I CERTIFY THAT THIS MAP WAS DRAWN UNDER MY SUPERVISION FROM AN ACTUAL SURVEY MADE UNDER MY SUPERVISION FROM DEED DESCRIPTION RECORDED IN BOOK 211, PAGE 466; THAT THE BOUNDARIES NOT SURVEYED ARE INDICATED AS DRAWN; THAT THE RATIO OF PRECISION OR POSITIONAL ACCURACY IS 1:10,000±; AND THAT THIS MAP MEETS THE REQUIREMENTS OF THE STANDARDS OF PRACTICE FOR LAND SURVEYING IN NORTH CAROLINA 21 NCAC 56. 1600. THAT THIS MAP WAS PREPARED IN ACCORDANCE WITH BOARD RULES.

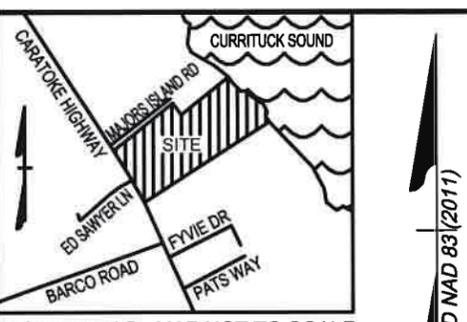
WITNESS MY ORIGINAL SIGNATURE, REGISTRATION NUMBER, AND SEAL, THIS 9TH DAY OF MARCH, 2020.

*Jason A. Mizelle*  
 JASON A. MIZELLE, PLS L-4719



THIS CERTIFIES THAT THIS COPY OF THIS PLAT IDENTIFIES ALL AREAS OF WATERS OF THE UNITED STATES REGULATED PURSUANT TO SECTION 404 OF THE CLEAN WATER ACT AS DETERMINED BY THE UNDERSIGNED ON THIS DATE. UNLESS THERE IS CHANGE IN THE LAW OR OUR PUBLISHED REGULATIONS, THIS DETERMINATION OF SECTION 404 JURISDICTION MAY BE RELIED UPON FOR A PERIOD NOT TO EXCEED FIVE YEARS FROM THIS DATE. THE UNDERSIGNED COMPLETED THIS DETERMINATION UTILIZING THE APPROPRIATE REGIONAL SUPPLEMENT TO THE 1987 U.S. ARMY CORPS OF ENGINEERS WETLANDS DELINEATION MANUAL.

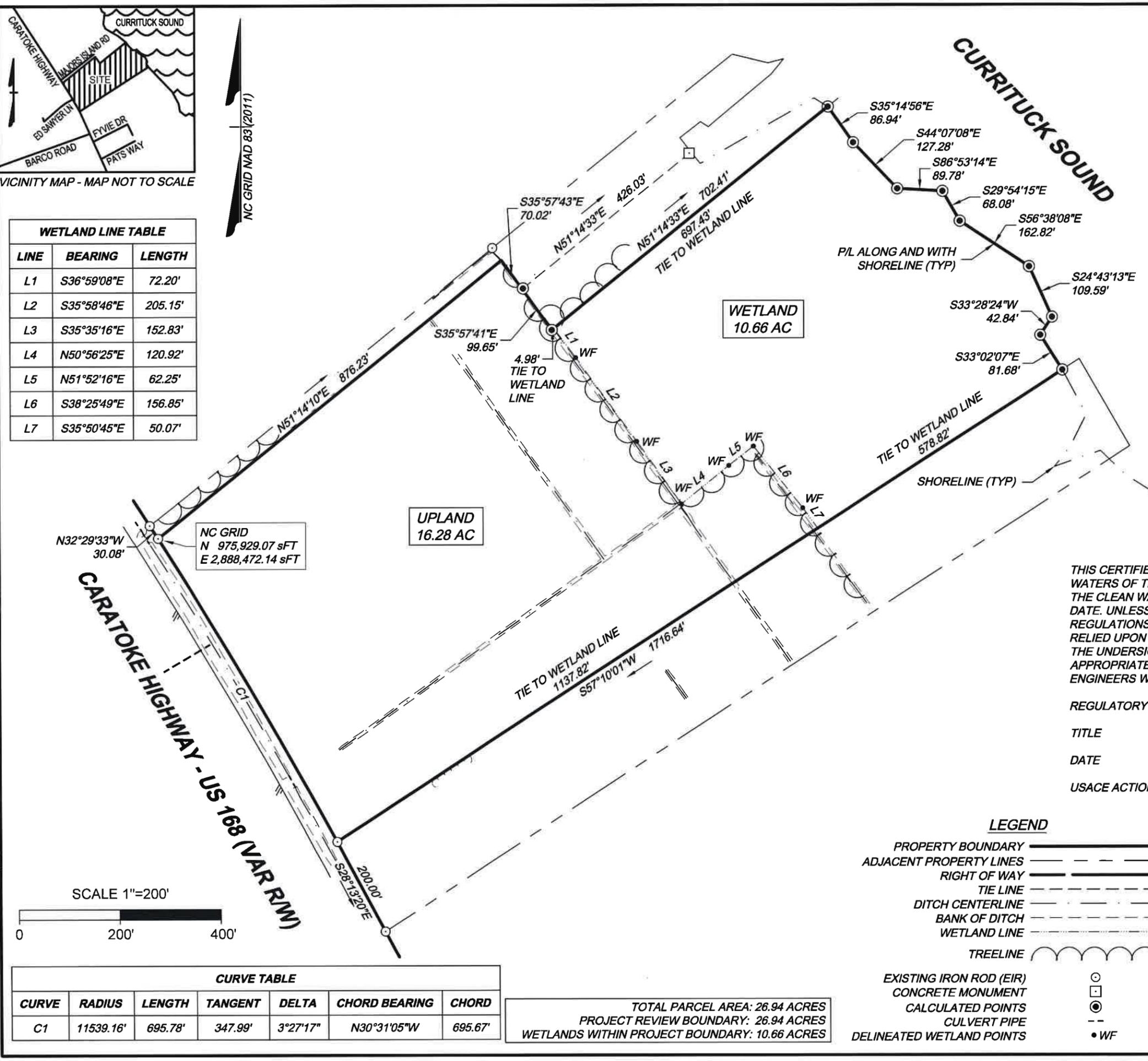
REGULATORY OFFICIAL \_\_\_\_\_  
 TITLE \_\_\_\_\_  
 DATE \_\_\_\_\_  
 USACE ACTION ID NO.: \_\_\_\_\_



VICINITY MAP - MAP NOT TO SCALE

WETLAND LINE TABLE		
LINE	BEARING	LENGTH
L1	S36°59'08"E	72.20'
L2	S35°58'46"E	205.15'
L3	S35°35'16"E	152.83'
L4	N50°56'25"E	120.92'
L5	N51°52'16"E	62.25'
L6	S38°25'49"E	156.85'
L7	S35°50'45"E	50.07'

S:\10944845 - Boswood Estates - Mathias Property - Barco, NCD\DWG\4845\404 wetlands.dwg | Plotted on 3/9/2020 1:39 PM | by John Sawyer



CURVE TABLE						
CURVE	RADIUS	LENGTH	TANGENT	DELTA	CHORD BEARING	CHORD
C1	11539.16'	695.78'	347.99'	3°27'17"	N30°31'05"W	695.67'

TOTAL PARCEL AREA: 26.94 ACRES  
 PROJECT REVIEW BOUNDARY: 26.94 ACRES  
 WETLANDS WITHIN PROJECT BOUNDARY: 10.66 ACRES

- LEGEND**
- PROPERTY BOUNDARY ————
  - ADJACENT PROPERTY LINES - - - -
  - RIGHT OF WAY ————
  - TIE LINE - - - -
  - DITCH CENTERLINE ————
  - BANK OF DITCH - - - -
  - WETLAND LINE - - - -
  - TREELINE
  - EXISTING IRON ROD (EIR)
  - CONCRETE MONUMENT
  - CALCULATED POINTS
  - CULVERT PIPE
  - DELINEATED WETLAND POINTS

**WETLAND SURVEY FOR  
 G. DODSON MATHIAS  
 PROPERTY DESCRIBED IN  
 DEED BOOK 211, PAGE 466**

Crawford Township	Currituck County, NC
DATE: March 9, 2020	SCALE: 1" = 200'
SHEET 1 OF 1	J.N.: 44845
DRAWN BY: J. Sawyer	CHECK BY: J. Mizelle
REVISIONS:	



## Currituck County

Planning and Community Development Department  
*Planning Division*  
 153 Courthouse Road, Suite 110  
 Currituck, North Carolina, 27929  
 252-232-3055 FAX 252-232-3026

To: Planning Board

From: Planning Staff

Date: May 26, 2020

Subject: PB 20-07 Currituck County Text Amendment  
 Alternative Water Supply for Fire Flow

The Board of Commissioners directed staff to prepare a text amendment to allow water shuttling as an alternative means to meet required fire flow standards for properties not served by county water.

This text amendment to the Unified Development Ordinance (UDO) allows use of water shuttling as a means of meeting fire flow water supply requirements for lands not serviced by the county water system and revises references in UDO Chapters 4, 5, & 6 from "Fire Marshal" to "Fire Code Official" to be consistent with the North Carolina Fire Code.

### Text Amendment Review Standards

The advisability of amending the text of the UDO is a matter committed to the legislative discretion of the Board of Commissioners and is not controlled by any one factor. In determining whether to adopt or deny the proposed text amendment, the Board of Commissioners may weigh the relevance of and consider whether and the extent to which the proposed text amendment:

1. Is consistent with the goals, objectives, and policies of the Land Use Plan and other applicable county-adopted plans;
2. Is not in conflict with any provision of this Ordinance or the County Code of Ordinances;
3. Is required by changed conditions;
4. Addresses a demonstrated community need;
5. Is consistent with the purpose and intent of the zoning districts in this Ordinance, or would improve compatibility among uses and ensure efficient development within the county;
6. Would result in a logical and orderly development pattern; and
7. Would not result in significantly adverse impacts on the natural environment, including but not limited to water, air, noise, stormwater management, wildlife, vegetation, wetlands, and the natural functioning of the environment.

PB20-07 Currituck County – Text Amendment  
 Alternative Water Supply for Fire Flow

### Staff Recommendation

Staff recommends approval of the request as submitted and suggests the following Statement of Consistency and Reasonableness:

The requested zoning text amendment is consistent with the goals, objectives, and policies of the 2006 Land Use Plan including:

- POLICY PS2: Currituck County shall support and encourage the development and improvement of FIRE FIGHTING SERVICES that enhance the security and safety of life and property, while resulting in the added benefit of lower property insurance rates. The need for additional fire stations or improvements to existing fire stations shall be examined annually to keep pace with the growth of the area.
- POLICY WS3: Currituck County endorses UTILITIES EXTENSION POLICIES that focus water and sewer services (1) within existing developed areas and in nearby targeted growth areas identified as Full Service and Limited Service areas, (2) where development densities would make the provision of all public services more efficient, (3) where the land is particularly well suited for development and (4) away from environmentally sensitive areas, such as areas with extensive wetlands or the northern beaches of the Outer Banks.
- POLICY WS4: Currituck County endorses utilities extension policies that avoid those parts of the county best suited for agriculture and to PROTECT FARMLAND FROM DEVELOPMENT PRESSURES brought about by such utilities. Exceptions to this policy may include extensions for major economic development initiatives, and extensions to address imminent public health problems or related environmental hazards.

The request is reasonable and in the public interest because:

- It is consistent with the 2006 Land Use Plan, and it is not in conflict with the provisions of the UDO.
- It continues to allow limited development without the requirement for extension of county water lines in farmland and rural areas.



**STAFF REPORT  
PB20-07 CURRITUCK COUNTY  
TEXT AMENDMENT  
ALTERNATIVE WATER SUPPLY  
FOR FIRE FLOW  
PLANNING BOARD  
MAY 28, 2020**

Amendment to the Unified Development Ordinance Chapter 4. Use Standards, Chapter 5. Development Standards and Chapter 6. Subdivision and Infrastructure Standards.

BE IT ORDAINED by the Board of Commissioners of the County of Currituck, North Carolina that the Unified Development Ordinance of the County of Currituck be amended as follows:

**Item 1:** That Chapter 6: Subdivision and Infrastructure Standards is amended by adding the following underlined language, deleting the struck-through language and numbering accordingly:

#### **6.2.4. Fire Protection Standards**

##### **A. General Provisions**

##### **(1) Fire Lanes**

Where streets or rights-of-way provide insufficient access for firefighting, unobstructed fire lanes with a minimum width complying with the current adopted version of the North Carolina State Fire Code shall be provided. In no instance shall this standard waive the requirement for primary drive aisles constructed in accordance with Section 5.6.8, Primary Drive Aisles, when required by this Ordinance.

##### **(2) Fire Hydrants Required**

All development serviced by the county water supply system shall include a system of fire hydrants sufficient to provide adequate fire protection for the buildings located or intended to be located within the development. Fire hydrants shall be located in a manner that ensures hydrants are spaced a maximum of 1,000 linear feet apart and every portion of lot frontage is within 500 linear feet of a hydrant. The Fire Code Official ~~Marshal~~ may authorize or require a deviation from this standard if, in the opinion of the Fire Code Official ~~Marshal~~, another arrangement ~~more~~ satisfactorily complies with the intent or standards in this Ordinance.

##### **(3) Fire Hydrant Location**

PB20-07 Currituck County – Text Amendment  
Alternative Water Supply for Fire Flow

Unless an alternative placement is specified by the State Building Code or the Planning Director, in consultation with the Fire Code Official ~~Marshal~~, fire hydrants shall be placed six feet behind the curb or within ten feet of the pavement edge of a street without curbing.

**(4) Required Hose Connections**

Unless otherwise specified, all fire hydrants shall have the following hose connections:

- (a) Two two-and-one-half-inch hose connections at least 21½ inches above ground level; and
- (b) One four-and-one-half-inch connection.

All hose connections shall be sized in accordance with national standards.

**(5) Water Service Main Size**

Water mains serving fire hydrants shall be at least eight inches in diameter.

**(6) Water Supply Source Location**

Water supply sources shall be clearly marked for location purposes with a marker of suitable size and reflective characteristics for daylight, nighttime, and inclement weather operations.

**B. Water Supply for Fire Protection when not Serviced by County Water Supply System**

Development not serviced by the county water system shall provide a supply of water for fire-fighting purposes in accordance with the following standards:

**(1) Allowable Sources**

The developer may provide the required water supply from:

- (a) ~~fire~~ Fire ponds, canals, wells, cisterns, above ground storage tanks, or water lines (where a community water supply system is installed); ~~or~~;
- (b) Fire Department mobile water supply approved by the Fire Code Official;
- (c) ~~or a~~ Any combination of the above these features; or
- (d) An alternative means approved by the Fire Code Official.

**(2) Location**

- (a) Water supply facilities shall be within 2,500 feet of every anticipated building in a development.
- (b) Water supply facilities may be located on or off-site, however the developer shall demonstrate a sufficient legal interest in off-site facilities to ensure they will remain available to serve the development.

- (c) Water supply sources shall be so located so that fire-fighting vehicles have ready access to such sources at all times.

**(3) Capacity**

- (a) A sufficient volume of water shall be available at all times to supply the needed fire flow for the proposed structures based upon guidance from the Insurance Services Office and existing fire-fighting capacity.
- (b) Water mains serving a community water supply system shall be sized to allow the future installation of fire hydrants should the development be connected to the county water supply system.

**(4) Configuration**

- (a) Water supply sources shall be provided with the necessary equipment and connections (e.g., dry hydrants in ponds) to ensure that fire-fighting equipment can draw water in a safe and efficient manner, as determined by the Fire Code Official Marshal.
- (b) Except within the SFR district, a hard-surfaced roadway shall be provided to the water source as well as a hard-surfaced turnaround area of sufficient dimensions to facilitate access by fire-fighting vehicles.

**(5) Maintenance Required**

The developer, or any successor in interest, shall be responsible for ensuring that all water supply sources, access roadways, and other facilities or equipment required by these standards, are maintained.

**Item 2:** That Chapter 4. Use Standards, Chapter 5. Development Standards and Chapter 6. Subdivision and Infrastructure Standards are amended by striking through all references to Fire Marshal and replacing with Fire Code Official.

**Item 3:** Statement of Consistency and Reasonableness:

The requested text amendment is consistent with the goals, policies, and objectives of the 2006 Land Use Plan including:

- POLICY PP2: Currituck County shall continue to implement a policy of ADEQUATE PUBLIC FACILITIES, sufficient to support associated growth and development. Such facilities may include but not be limited to water supply, school capacity, park and open space needs, firefighting capability, and law enforcement.
- POLICY WS3: Currituck County endorses UTILITIES EXTENSION POLICIES that focus water and sewer services (1) within existing developed areas and in nearby targeted growth areas identified as Full Service and Limited Service areas, (2) where development densities would make the provision of all public services more efficient, (3)

where the land is particularly well suited for development and (4) away from environmentally sensitive areas, such as areas with extensive wetlands or the northern beaches of the Outer Banks.

- **POLICY WS4:** Currituck County endorses utilities extension policies that avoid those parts of the county best suited for agriculture and to PROTECT FARMLAND FROM DEVELOPMENT PRESSURES brought about by such utilities. Exceptions to this policy may include extensions for major economic development initiatives, and extensions to address imminent public health problems or related environmental hazards.

The request is reasonable and in the public interest because:

- It is consistent with the 2006 Land Use Plan, and it is not in conflict with the provisions of the UDO.
- It continues to allow limited development without the requirement for extension of county water lines in farmland and rural areas.

**Item 4:** The provisions of this Ordinance are severable and if any of its provisions or any sentence, clause, or paragraph or the application thereof to any person or circumstance shall be held unconstitutional or violative of the Laws of the State of North Carolina by any court of competent jurisdiction, the decision of such court shall not affect or impair any of the remaining provisions which can be given effect without the invalid provision or application.

**Item 5:** This ordinance amendment shall be in effect from and after the \_\_\_\_\_ day of \_\_\_\_\_, 2020.

\_\_\_\_\_  
Board of Commissioners' Chairman  
Attest:

\_\_\_\_\_  
Leeann Walton  
Clerk to the Board

DATE ADOPTED: \_\_\_\_\_  
MOTION TO ADOPT BY COMMISSIONER: \_\_\_\_\_  
SECONDED BY COMMISSIONER: \_\_\_\_\_  
VOTE: \_\_\_\_\_AYES \_\_\_\_\_NAYS  
.....

PLANNING BOARD DATE: \_\_\_\_\_  
PLANNING BOARD RECOMMENDATION: \_\_\_\_\_  
VOTE: \_\_\_\_\_AYES \_\_\_\_\_NAYS

ADVERTISEMENT DATE OF PUBLIC HEARING: \_\_\_\_\_  
BOARD OF COMMISSIONERS PUBLIC HEARING: \_\_\_\_\_  
BOARD OF COMMISSIONERS ACTION: \_\_\_\_\_  
POSTED IN UNIFIED DEVELOPMENT ORDINANCE: \_\_\_\_\_

AMENDMENT NUMBER: \_\_\_\_\_

PB20-07 Currituck County – Text Amendment  
Alternative Water Supply for Fire Flow




**Currituck County**

Planning and Community Development Department  
 Planning and Zoning Division  
 153 Courthouse Road Suite 110  
 Currituck NC 27929  
 252-232-3055 Fax 252-232302

To: Board of Commissioners  
 From: Planning Staff  
 Date: January 28, 2020  
 Subject: PB 19-25 Currituck County – Currituck Station:

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**Background**

This text amendment is presented on behalf of Currituck County to implement a long and in depth planning process for a specified area in Moyock known as Currituck Station (previously Moyock Mega Site). In 2012, the Board of Commissioners recognized the steady growth Moyock was experiencing and engaged staff to begin efforts to address growth and development in a comprehensive manner. The planning process began in 2012 with the Moyock Small Area Plan that was later adopted by the Board of Commissioners in 2014. The Moyock Small Area Plan identified an employment activity center for the area identified as Currituck Station. The employment center was intended to have a concentration of uses including commercial, industrial, and residential. The county later adopted a market feasibility study for the employment activity center that identified the market demands for Currituck Station. The master plan was completed in 2017. The proposed text amendment implements the small area plan, market feasibility study and the master plan for Currituck Station.

In summary, the text amendment establishes a new zoning district, Planned Development – Currituck Station District (PD-CS) and associated sub-districts, for lands recognized on the Moyock Mega Site master plan (now Currituck Station) that balances residential, commercial, industrial, and advanced manufacturing land uses. Included with the text amendment is the Currituck Station Pattern Book that establishes the intended character for the district. The pattern book utilizes historical architecture as the foundation to guide development in the district and establish a local identity through building design, massing and external treatments. Although the pattern book is intended to be used in conjunction with the UDO regulations, it is a guide and will not be a regulatory document.

**Text Amendment Review Standards**

The advisability of amending the text of the UDO is a matter committed to the legislative discretion of the Board of Commissioners and is not controlled by any one factor. In determining whether to adopt or deny the proposed text amendment, the Board of Commissioners may weigh the relevance of and consider whether and the extent to which the proposed text amendment:

1. Is consistent with the goals, objectives, and policies of the Land Use Plan and other applicable county-adopted plans;

2. Is not in conflict with any provision of this Ordinance or the County Code of Ordinances;
3. Is required by changed conditions;
4. Addresses a demonstrated community need;
5. Is consistent with the purpose and intent of the zoning districts in this Ordinance, or would improve compatibility among uses and ensure efficient development within the county;
6. Would result in a logical and orderly development pattern; and
7. Would not result in significantly adverse impacts on the natural environment, including but not limited to water, air, noise, stormwater management, wildlife, vegetation, wetlands, and the natural functioning of the environment.

### Staff Recommendation

Staff recommends approval of this request subject to the staff suggested Statement of Consistency and Reasonableness listed in the staff report.

1. Is consistent with the goals, objectives, and policies of the Land Use Plan and other applicable county-adopted plans;
  - a. This request is consistent with the goals, objectives, and policies of the Land Use Plan, Moyock Small Area Plan, and the Moyock Mega Site Master Plan. Please reference:
    - LUP policies AG3, HN3, CD2, CD8, WS3, and CW1.
    - MSAP policies CC1, CC2, CC3, ST1, BI2, and Actions FLU2A, CC 2A, CC 3B, BI 3B
    - Moyock Mega Site Master Plan Figure ES-1
2. Is not in conflict with any provision of this Ordinance or the County Code of Ordinances;
  - a. The request is in harmony with the UDO and the County Code of Ordinances.
3. Is required by changed conditions;
  - a. The Moyock Small Area Plan, adopted in 2014, identified this area as an employment activity area.
  - b. The 2016 Feasibility Study served as the guide for potential land use demands.
  - c. The master plan development process was designed to produce a market driven plan responsive to projected demand for a mix of land uses specific to the local market and formed by regional influences.
4. Addresses a demonstrated community need;
  - a. It is intended to establish a long-term vision for a mixed use development for approximately 3,500 acres of land that is strategically positioned to serve as a connective center between Virginia and North Carolina.
5. Is consistent with the purpose and intent of the zoning districts in this Ordinance, or would improve compatibility among uses and ensure efficient development within the county;
  - a. The proposed text amendment establishes the district that implements the master plan for the project area.
6. Would result in a logical and orderly development pattern; and
  - a. The standards are developed to provide a mix of uses and densities needed to sustain the mixed use development.
7. Would not result in significantly adverse impacts on the natural environment, including but not limited to water, air, noise, stormwater management, wildlife, vegetation, wetlands, and the natural functioning of the environment.
  - a. It should have no adverse impacts on the natural environment.

### Planning Board Recommendation – January 14, 2020

Mr. Bass motioned to approve the PB-25 Currituck County's request to amend the Unified Development Ordinance, Chapter 1. General Provisions, Chapter 2. Administration, Chapter 3. Zoning Districts Chapter 4. Use Standards, Chapter 5. Development Standards, Chapter 6. Subdivision Infrastructure Standards, and Chapter 10. Definitions and Measurements for the purpose of implementing the Moyock Mega Site master plan (Currituck Station) and establishing the Planned Development - Currituck Station district and regulations with the inclusion of the following staff recommendations:

- Provide a transition from Center Station to Newtown on the south side that does not split property lines
  - Option 1 - Shift the sub-district line - Charter sub-district to include land (now Newtown) to Lazy Corner Road.
  - ~~Option 2 – Modify the use table for Newtown sub-district~~
  - ~~Option 3 – Make no change at this time and update/amend at rezoning.~~
- Pattern book corrections and images for sub-districts
- Remove the suggested materials for each sub-district
- Provide traditional architecture or building form elevations for Center Station and Charter sub-districts

Mr. Thomas seconded the motion and the motion carried unanimously.

RESULT: RECOMMENDED APPROVAL [UNANIMOUS] Next: 2/3/2020 6:00 PM

AYES: C. Shay Ballance, Chairman, Garry Owens, Vice Chairman, K. Bryan Bass, Board Member, David Doll, Board Member, Juanita S Krause, Board Member, J. Timothy Thomas, Board Member

ABSENT: Anamarie Hilgendorf, Board Member



PB 19-25 CURRITUCK COUNTY  
TEXT AMENDMENT  
PLANNING BOARD  
DECEMBER 10, 2019

Currituck County requests an amendment to the Unified Development Ordinance, Chapter 1. General Provisions, Chapter 2. Administration, Chapter 3. Zoning Districts, Chapter 4. Use Standards, Chapter 5. Development Standards, Chapter 6. Subdivision Infrastructure Standards, and Chapter 10. Definitions and Measurements. for the purpose of implementing the Moyock Mega Site master plan and establishing the Planned Development – Currituck Station district and regulations.

BE IT ORDAINED by the Board of Commissioners of the County of Currituck, North Carolina that the Unified Development Ordinance of the County of Currituck be amended as follows:

**Item 1:** That Chapter 1 is amended by deleting the following strikethrough language and adding the underlined language in Section 1.5.2.:

# CHAPTER 1.

## GENERAL PROVISIONS

### 1.5.2. Adopted Plans

#### B. Small Area Plans

- (1) The Board of Commissioners has adopted the following plans for specific geographic areas and corridors within the county:
  - (a) The Corolla Village Small Area Plan;
  - (b) The Maple-Barco Small Area Plan; ~~and~~
  - (c) The U.S. Highway 158 & N.C. Highway 168 Corridor Plan; and,
  - (d) The Moyock Small Area Plan
- (2) These plans include goals, objectives, policies, and actions related to, and that serve as a guide to, various aspects of development intensity and design within specific geographic areas.

#### C. Functional Plans and Documents

The county has adopted functional plans, documents, and regulations relating to future development (e.g. Administrative Manual), provision of public infrastructure and services (e.g., the Currituck County Stormwater Manual), economic development (e.g. Currituck County Moyock Mega Site Master Plan), and tourism. The county will continue to adopt and amend these types of functional plans, documents and regulations. These plans, documents, and regulations include goals, objectives, policies, and actions related to the form

and timing of the county's growth and development as well as to the location and design of public infrastructure.

**Item 2:** That Chapter 2 is amended by deleting the following strikethrough language and adding the underlined language:

# CHAPTER 2.

## ADMINISTRATION

### 2.4.5. Planned Development

#### A. Purpose

A planned development is a development that is planned and developed under unified control in accordance with more flexible standards and procedures that are conducive to creating more mixed-use, pedestrian-oriented, and otherwise higher quality development than could be achieved through general use (base) zoning district regulations. The purpose of this section is to provide a uniform means for amending the Official Zoning Map to establish a Planned Development (PD) zoning district.

#### B. Scope

A planned development district is established by amendment of the Official Zoning Map to rezone land to a ~~Planned Development~~ PD zoning district classification that is defined by a PD master plan and a PD terms and conditions document.

#### C. Planned Development Procedure

##### (1) Pre-Application Conference

Applicable (see Section 2.3.2).

##### (2) Community Meeting

Applicable (see Section 2.3.3).

##### (3) Application Submittal and Acceptance

(a) Applicable (see Section 2.3.4). Planned development applications may not be initiated by anyone other than the landowner(s) of the land subject to the application.

(b) The application shall include a master plan depicting the general configuration and relationship of the principal elements of the proposed development, including uses, general building types, density/intensity, resource protection, pedestrian and vehicular circulation, open space, public facilities, and phasing (see Section 3.7.2.A, Planned Development Master Plan).

(c) The application shall also include a terms and conditions document specifying terms and conditions defining development parameters, providing for environmental

mitigation, and outlining how public facilities will be provided to serve the planned development.

- (d) To ensure unified control, the application shall also include a copy of the title to all land that is part of the proposed PD zoning district classification.

**(4) Staff Review and Action**

Applicable (see Section 2.3.5). The Technical Review Committee shall review the application, prepare a staff report, and provide a recommendation in accordance with Section 2.3.5.B, Staff Report and Recommendation, and Section 2.4.5.D, Planned Development Review Standards.

**(5) Public Hearing Scheduling and Public Notification**

Applicable (see Section 2.3.6).

**(6) Public Hearing Procedures**

Applicable (see Section 2.3.8).

**(7) Advisory Body Review and Recommendation**

Applicable (see Section 2.3.9). The Planning Board, following a public meeting, shall make a recommendation on the application in accordance with Section 2.3.9, Advisory Body Review and Recommendation, and Section 2.4.5.D, Planned Development Review Standards.

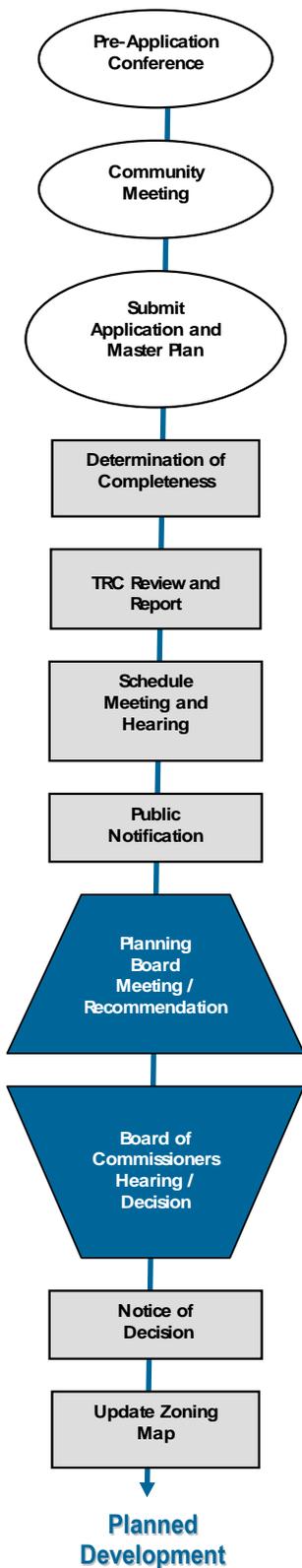
**(8) Decision-Making Body Review and Decision**

(a) Applicable (see Section 2.3.10). The Board of Commissioners, following a legislative public hearing (Section 2.3.8.B), shall decide the application in accordance with Section 2.3.10, Decision-Making Body Review and Decision, and Section 2.4.5.D, Planned Development Review Standards. The decision shall be one of the following:

- (i) Approval of the planned development subject to the PD master plan and PD terms and conditions in the application;
- (ii) Approval of the planned development subject to additional or revised conditions related to the PD master plan or PD terms and conditions;
- (iii) Denial of the planned development; or
- (iv) Remand of the planned development application back to the Planning Board for further consideration.

(b) As part of the decision, the Board of Commissioners shall adopt a written statement of consistency and reasonableness that:

- (i) Describes whether the decision is consistent with all county-adopted plans that are applicable; and



Attachment: 2- Currituck Station Draft UDO 12-3-19 (PB 19-25 Currituck County - Currituck Station)

- (ii) Explains why the decision is reasonable and in the public interest.

#### **D. Planned Development Review Standards**

The advisability of establishing a planned development is a matter committed to the legislative discretion of the Board of Commissioners and is not controlled by any one factor. In determining whether to adopt or deny a planned development, the Board of Commissioners shall consider the standards in Section 2.4.3.C, Zoning Map Amendment Standards, and the standards for the proposed type of PD district in Section 3.7, Planned Development Base Zoning Districts.

#### **E. Designation on the Official Zoning Map**

Designation of a PD zoning district on the Official Zoning Map shall note the ordinance number approving the PD zoning classification.

#### **F. Effect**

Lands rezoned to a PD district shall be subject to the approved PD master plan and the approved PD terms and conditions. The master plan and terms and conditions are binding on the land as an amendment to the Official Zoning Map. The applicant may apply for and obtain subsequent development permits and approvals necessary to implement the PD master plan in accordance with the appropriate procedures and standards set forth in this Ordinance. Any permits or approvals shall comply with the PD master plan and the PD terms and conditions.

#### **G. Expiration**

- (1) If no application for approval of a preliminary plat or site plan for any part of the approved PD master plan is submitted within three years after approval of the planned development, the Planning Director shall initiate a map amendment application to rezone the land back to its prior zoning classification or any other base zoning classification determined to be appropriate. Such time period shall not be extended with transfer of ownership.

#### **H. Minor Deviation**

##### **(1) General**

Subsequent plans and permits for development within an approved planned development may include minor deviations from the PD master plan or PD terms and conditions, provided the Planning Director determines such deviations are limited to changes addressing technical considerations that could not reasonably be anticipated during the PD zoning classification process or any other change that has no material effect on the character of the approved planned development or any of its approved terms or conditions. Changes in the following shall constitute minor deviations and may be approved by the Planning Director:

- (a) Driveway locations;
- (b) Structure floor plan revisions;

- (c) Minor shifts in building size or location that do not result in any substantive changes or impacts to the site elements or surrounding lands; and
- (d) Facility design modifications for amenities and the like.

**(2) Material Changes are Amendments**

Changes that materially affect the basic concept of the PD master plan or basic parameters set by the PD terms and conditions are not considered minor deviations, and shall only be changed as amendments to the PD master plan or PD terms and conditions.

**I. Amendments**

**(1) General**

If an applicant determines it is necessary to alter the concept or intent of the PD master plan or the PD terms and conditions, the PD master plan or PD terms and conditions shall be amended, extended, or modified only in accordance with the procedures and standards for its original approval.

**(2) Amendments Defined**

The following items are considered an alteration of the concept or intent of the PD master plan or PD terms and conditions and are treated as an amendment:

- (a) Changes in use designations;
- (b) Density/intensity increases;
- (c) Decreases in open space;
- (d) Substantial changes in the location of streets (particularly if streets are to be deleted or access points to the development moved so traffic flows both inside and outside the development are affected);
- (e) Change in the location of any public easement; or
- (f) Change in the proportion of housing types by more than 15 percent.

**Item 3:** That Chapter 3 is amended by deleting the following strikethrough language and adding the underlined language:

# CHAPTER 3. ZONING DISTRICTS

## 3.2. BASE ZONING DISTRICTS ESTABLISHED

### 3.2.1. General

Table 3.2.1, Base Zoning Districts Established, sets out the base zoning districts established by this Ordinance. Base zoning districts are grouped into Special, Residential, Business and Mixed-Use, and Planned Development districts.

<b>TABLE 3.2.1: BASE ZONING DISTRICTS ESTABLISHED</b>	
<b>DISTRICT NAME</b>	<b>ABBREVIATION</b>
<b>SPECIAL DISTRICTS</b>	
Resource Conservation	RC
Agriculture	AG
<b>RESIDENTIAL DISTRICTS</b>	
Single-Family Residential – Mainland	SFM
Single-Family Residential – Outer Banks	SFO
Single-Family Residential – Outer Banks, Remote	SFR
Single-Family Residential – Isolated	SFI
Mixed Residential	MXR
<b>BUSINESS AND MIXED-USE DISTRICTS</b>	
General Business	GB
Limited Business	LB
Community Center	CC
Village Center	VC
Light Industrial	LI
Heavy Industrial	HI
<b>PLANNED DEVELOPMENT DISTRICTS</b>	
Planned Development – Residential	PD-R
Planned Development – Mixed	PD-M
Planned Development – Outer Banks	PD-O
<u>Planned Development – Currituck Station</u>	<u>PD-CS</u>

**A. Classification of Base Zoning Districts**

Land shall be classified or reclassified into a base zoning district only in accordance with the procedures and requirements set forth in Section 2.4.3, Zoning Map Amendment, or Section 2.4.5, Planned Development District, as appropriate.

**B. Relationship to Overlay Zoning Districts**

Regulations governing development in an overlay zoning district shall apply in addition to the regulations governing development in the underlying base zoning district. If the standards governing a base zoning district expressly conflict with those governing an overlay zoning district, the standards governing the overlay zoning district shall control.

**C. Organization of Base Zoning District Regulations**

Sections 3.3 through 3.5 set out the general purposes of each group of base zoning districts and contain subsections that set out the specific purpose, density, bulk, and dimensional standards for each individual base zoning district. These subsections have a common structure consisting of a purpose statement, applicable dimensional standards, photographs showing hypothetical preferred building forms for the district, a graphic depiction of typical street layout and lot patterns, and a hypothetical graphic depiction of the district's bulk and dimensional standards as applied to typical lot patterns and building forms. Each district includes a summary table of dimensional standards that include numbers in black circles. The black circles in the dimensional standards table correspond to the black circles depicted in the district graphics. The building form photographs and lot pattern diagrams are for illustrative purposes only, and may not be consistent with all the dimensional requirements. In these cases, the dimensional requirements in the text of this Ordinance shall control. The range of allowable uses for each base zoning district are described in Chapter 4: Use Standards, which includes Table 4.1.1A. and Table 4.1.1.B. Summary Use Table, a summary use table specifying permitted, special, and allowable uses for each of the base zoning districts and references any standards specific to individual uses.

## 3.7 PLANNED DEVELOPMENT BASE ZONING DISTRICTS

### 3.7.1 General

#### A. General Planned Development District Purposes

The purpose of Planned Development (PD) districts ~~are is to established and intended to~~ encourage innovative and efficient land planning and site design concepts that support a higher quality of life and achieve a higher quality of development, environmental sensitivity, energy efficiency, and other county goals and objectives by:

- (1) Reducing or diminishing the inflexibility or uniform design that sometimes results from strict application of zoning and development standards designed primarily for individual lots;
- (2) Allowing ~~greater~~ freedom in selecting the form and design of development by ways pedestrians and traffic circulate, location and integration of open space and civic space into the development, and design amenities; means of providing access, open space, and design amenities;
- (3) ~~Allowing greater freedom in providing~~ Encouraging a well-integrated mix of residential and nonresidential land uses in the same development, including a mix of housing types and lot sizes;
- (4) Providing for efficient use of land resulting in smaller networks of utilities and streets ~~and thereby lowering development and housing costs;~~ and
- (5) Promoting quality design and environmentally sensitive development that respects surrounding established land use character and respects and takes advantage of a site's natural and man-made features, such as trees, estuaries, shorelines, dunes, maritime forest, special flood hazard area, and historic features.
- (6) In return for flexibility, planned developments are expected to deliver communities of exceptional design, character, and quality that preserve critical environmental resources and provide superior open space amenities. Such communities incorporate creative design in the layout of buildings, open space, and circulation; assure compatibility with surrounding land uses and neighborhood character; and provide greater efficiency in the layout and provision of roads, utilities, and other infrastructure.

#### B. Intent

The PD district is intended to be used sparingly under this Ordinance, and only where the development demonstrates innovated design, character, and higher quality development.

#### C. Classification of Planned Development Zoning Districts

Land shall be classified into a planned development zoning district only in accordance with the procedures and requirements set forth in Section 2.4.5, Planned Development, and this section.

**D. Relationship to PUD or RET Overlay Districts**

Lands designated as Planned Unit Development (PUD) Overlay or Planned Adult Retirement (RET) Overlay on January 1, 2013 are subject to the standards and conditions included within the previously-adopted sketch plans and other requirements related to their approval. These developments may proceed subject to their original approvals in accordance with Section 1.8, Transitional Provisions. In the event the approval associated with a PUD or RET expires, or a modification is proposed, an applicant may seek to establish a PD district in accordance with this section and Section 2.4.5, Planned Development.

**E. Organization of Planned Development Zoning District Regulations**

Section 3.7.2, General Standards for All Planned Development Districts, sets out general standards applicable to all types of Planned Development districts. Sections 3.7.3 to ~~3.7.5~~ 3.7.6 set out the purpose statements and standards for ~~each of the three~~ specific types of Planned Development (PD) districts. These subsections have a common structure consisting of a purpose statement and applicable development standards. Some PD districts also include additional district and sub-district specific standards. Chapter 4: Use Standards, includes a summary use table specifying the allowable uses for each of the PD districts (see Table 4.1.1.B Summary Use Table) subject to an approved master plan. Uses that do not include an "MP" or "U" under a particular PD district column in Table 4.1.1.B, are prohibited within that PD district.

**3.7.2. General Standards for All Planned Development Districts**

Before approving a PD zoning district classification, the Board of Commissioners shall find that the application for the PD zoning district classification, as well as the PD master plan and the PD terms and conditions document included as part of the application, comply with the following standards:

**A. Planned Development Master Plan**

The PD master plan shall:

- ~~(1)~~ Include a statement of planning objectives and development goals for the district that is consistent with the intent and purposes of the particular PD district, the 2006 Land Use Plan, and other officially adopted plans;
- ~~(2)~~ Identify the general location of individual development areas, identified by land use(s) and/or development density or intensity;
- ~~(3)~~ Demonstrate the innovative site planning techniques that improve upon the standards in other allowable zoning districts with the purpose of enhancing the county's health, safety and welfare;
- ~~(4)~~ Depict the general configuration and relationship of the principal elements of the proposed development, including general building types;
- ~~(5)~~ Identify the development area in the PD district, and identify each individual development area the acreage, types and mix of land uses, number of residential units (by use type), nonresidential floor area (by use type), residential density, and nonresidential intensity;

- (6) Identify how the proposed land uses, residential densities, nonresidential intensity, traffic circulation and design are compatible with adjacent land uses, environmental features, and character of the surrounding area;
- (7) Identify the general location, amount, and type (whether designated for active or passive recreation) of open space consistent with the purposes of the individual PD district and the requirements of this ordinance;
- (8) Identify the location of environmentally sensitive lands, wildlife habitat, and resource protection lands, waterway corridors and ensure protection of these lands consistent with the purposes of the individual PD district and the requirements of this ordinance;
- (9) Identify the on-site pedestrian circulation system, and how it will connect to off-site pedestrian systems that are consistent with the purposes of the individual PD district, and the requirements of this ordinance;
- (10) Identify the on-site transportation circulation system, including the general location of all public and private streets with street types, existing or projected transit corridors, pedestrian, bicycle, and vehicular circulation features, and how they will connect ~~with~~ to existing and planned ~~county~~ systems;
- (11) Identify the general location of existing and proposed utilities including on-site potable water and wastewater facilities, and how they will serve the proposed development and connect to county systems;
- (12) Identify the general location of on-site stormwater management facilities, and how they will connect to ~~county~~ existing and planned systems; and
- (13) Identify the general location of all other on-site public facilities serving the development, including but not limited to parks, schools, and facilities for fire protection, police protection, EMS, and solid waste management.

## **B. Densities/Intensities**

- (1) The densities for residential development and the intensities for nonresidential development applicable in each development area of a PD district shall be as established in the master plan, and shall be consistent with the intent, purposes, and standards of the individual PD district, the 2006 Land Use Plan, other officially adopted plans, and requirements of this ordinance.
- ~~(2) Development located within a Full Service area designated by the Land Use Plan may maintain a maximum density of up to three units per acre.~~
- ~~(3) Development located within a Limited Service Area designated by the Land Use Plan may maintain a maximum density of up to one and one-half units per acre.~~

- (4) Dwelling units within a PD district may be concentrated or evenly distributed throughout the development, provided the maximum allowable density for the development as a whole is not exceeded.

### C. Dimensional Standards

The dimensional standards applicable in each development area of a PD district shall be as established in the master plan and shall be consistent with the purpose of the individual particular type of PD district. The master plan shall include at least the following types of dimensional standards:

- (1) Minimum lot area;
- (2) Minimum lot width;
- (3) Minimum and maximum setbacks;
- (4) Maximum lot coverage;
- (5) Maximum building height;
- (6) Maximum individual building size;
- (7) Floor area ratio; and
- (8) Minimum setbacks from adjoining residential development or residential zoning districts.

### D. Development Standards

All development in a PD district shall comply with the development standards of Chapter 5: Development Standards, the subdivision and infrastructure design standards of Chapter 6: Subdivision and Infrastructure Standards, and the environmental protection standards in Chapter 7: Environmental Protection, unless modified in accordance with this section.

### E. Consistency with County Plans

The PD zoning district designation, the master plan, and the terms and conditions document shall be consistent with the 2006 Land Use Plan, and any applicable functional plans and small area plans adopted by the county.

### F. Compatibility with Surrounding Areas

Development along the perimeter of a PD district shall be compatible with adjacent existing or proposed development. Where there are issues of compatibility, the master plan shall ~~provide for~~ identify transition areas at the edges of the PD district that provide for appropriate buffering and/or ensure a complementary character of uses. Determination of complementary character shall be based on densities/intensities, lot size and dimensions, building height, building mass and scale, hours of operation, exterior lighting, siting of service areas, traffic circulation, environmental features, or other aspects identified by the Board of Commissioners.

### G. Development Phasing Plan

If development in the PD district is proposed to be phased, the master plan shall include a development phasing plan that identifies the ~~general~~ sequence or phases in which the district is proposed to be developed, including how residential and nonresidential development will be timed, how infrastructure (public and private) and open space will be provided and timed, and how

development will be coordinated with the county's capital improvements program.

#### **H. Conversion Schedule**

The PD master plan may include a conversion schedule that identifies the extent to which one type of residential use may be converted to another type of residential use or one type of nonresidential use may be converted to another type of nonresidential use (i.e., residential to residential, or nonresidential to nonresidential). These conversions may occur within development areas and between development areas, as long as they occur within the same development phase, as identified by the approved development phasing plan, and are consistent with established extents of conversion set down in the conversion schedule.

#### **I. On-Site Public Facilities**

##### **(1) Design and Construction**

The PD master plan shall establish the responsibility of the developer/landowner to design and construct or install required and proposed on-site public facilities in compliance with applicable county, state, and federal regulations.

##### **(2) Dedication**

The PD master plan shall establish the responsibility of the developer/landowner to dedicate to the public the right-of-way and easements necessary for the construction or installation of required and proposed on-site public facilities in compliance with applicable county, state, and federal regulations.

##### **(3) Modifications to Street Standards**

In approving a master plan, the Board of Commissioners may approve modifications or reductions of street design standards including those for right-of-way widths, pavement widths, required materials, and turning radii, with NCDOT approval, on finding that:

- (a)** The master plan provides for adequate separation/integration of vehicular, pedestrian, and bicycle traffic;
- (b)** Access for emergency service vehicles is not substantially impaired;
- (c)** Adequate parking is provided for the uses proposed; and
- (d)** Adequate space for public utilities is provided within the street right-of-way.

#### **J. Planned Development Terms and Conditions**

The PD terms and conditions document is a required component in the establishment of a PD zoning district and shall incorporate by reference or include, but not be limited to:

- (1)** Conditions related to approval of the application for the PD zoning district classification;
- (2)** The master plan, including any density/intensity standards, dimensional standards, and development standards established in the master plan;

- (3) Conditions related to the approval of the master plan, including any conditions related to the form and design of development shown in the master plan;
- (4) The development pattern that addresses the district and sub-district character, development matrix, street types and patterns, block patterns, building form and types, architectural patterns, pedestrian configuration, signage patterns, landscaping, site amenities and open space patterns;
- (5) Provisions addressing how transportation, potable water, wastewater, stormwater management, and other infrastructure will be provided and maintained to accommodate the proposed development;
- (6) Provisions related to environmental protection and monitoring; and
- (7) Any other provisions the Board of Commissioners determines are relevant and necessary to the development of the PD in accordance with applicable standards and regulations.

#### **K. Uses**

The uses allowed in a PD district are identified in Table 4.1.1.B Summary Use Table, as allowed subject to a planned development master plan. Allowed uses shall be established in the master plan and are subject to any use regulations applicable to the PD district. Allowed uses shall be consistent with county plans, the purpose of the ~~particular type of~~ individual PD district, and subject to any additional limitations or requirements set forth in Sections 3.7.3 – ~~3.7.5~~ 3.7.6. for the ~~particular type of~~ individual PD district.

#### **L. Amendments to Approved Master Plan**

Amendments or modifications to a master plan shall be considered in accordance with the standards in Section 2.4.5.I, Amendments.

**3.7.3. Planned Development – Residential (PD-R) District**

**PD-R  
PLANNED  
DEVELOPMENT –  
RESIDENTIAL**

**A. DISTRICT PURPOSE**

The Planned Development – Residential (PD-R) District is established and intended to encourage the use of innovative and creative design to provide a mix of different residential uses in close proximity to one another on mainland Currituck County, while at the same time providing an efficient use of open space. Limited, small-scale commercial uses may be allowed in the PD-R district, primarily to serve the needs of residents in the development.

**B. DIMENSIONAL STANDARDS**

**C. DEVELOPMENT STANDARDS**

District area, minimum (acres)	50	The standards in Chapter 5: Development Standards, shall apply to all development in PD-R districts, but some of those standards may be modified as part of the master plan if consistent with the general purposes of the PD-R district and the procedures noted below.	
Gross residential density, maximum (dwelling units/acre) [1]	<del>3 - Full Service Areas</del> or <del>1.5 - Limited Service Areas</del>		
Lot area, minimum (sq ft)	To be established in the master plan	<b>Development Standard</b>	<b>Means of Modifying</b>
Lot width, minimum (ft)		Off-street parking & loading	Specify in Alternative Parking Plan (see Section 5.1.6)
Nonresidential land area, maximum (% of district total)	40	Landscaping [2]	Specify in Alternative Landscaping Plan (see Section 5.2.9)
Single housing type, maximum (% of units)	85	Tree protection	
Lot coverage, maximum (% of lot area)	To be established in the master plan	Open space set-aside [3]	30%
Nonresidential FAR, maximum (%)		Fences and walls	Specify in Security Plan (see Sections 5.3.5. and 5.4.4)
Individual building size, maximum (sq ft)		Exterior lighting	
Building height, maximum (ft)		Community form	Specify in master plan
Setbacks, minimum or maximum (ft)		Nonresidential design	
Setback from abutting residential zoning district or existing residential use (ft)		Multi-family design	
Setback from agriculture (ft)		Community compatibility [4]	Modifications prohibited
Setback from major arterial streets (ft)	Signage	Modifications prohibited	
Min. Wetland/Riparian Buffer (ft)	30	Adequate public facilities	Modifications prohibited

**NOTES:**

- [1] ~~May not exceed three units per acre in Full Service areas or one and one-half units per acre in Limited Service areas~~
- [2] Uses internal to the development shall not be required to provide perimeter buffers
- [3] The required percentage of open space set-aside shall be calculated based on the total district

**D. ENVIRONMENTAL PROTECTION STANDARDS**

The environmental protection standards in Chapter 7 of the UDO may not be modified by a planned development

[4] Community compatibility standards shall not apply to uses internal to the development

Attachment: 2- Currituck Station Draft UDO 12-3-19 (PB 19-25 Currituck County - Currituck Station)

3.7.4. Planned Development – Mixed (PD-M) District

# PD-M

## PLANNED DEVELOPMENT – MIXED

### A. DISTRICT PURPOSE

The Planned Development – Mixed (PD-M) District is established and intended to encourage the development of a mix of employment generating uses (office, research, light industrial, and limited commercial), and may allow low-to-medium density residential uses at appropriate locations on the Currituck County mainland in a planned and aesthetically pleasing way. This is done by allowing design flexibility as well as a mix of uses.

### B. DIMENSIONAL STANDARDS

### C. DEVELOPMENT STANDARDS

District area, minimum (acres)	50	The standards in Chapter 5: Development Standards, shall apply to all development in PD-M districts, but some of those standards may be modified as part of the master plan if consistent with the general purposes of the PD-M district and the procedures noted below.	
Gross residential density, maximum (dwelling units/acre) {1}	3 – Full Service Areas or 4.5 – Limited Service Areas		
Lot area, minimum (sq ft)	To be established in the master plan	<b>Development Standard</b>	<b>Means of Modifying</b>
Lot width, minimum (ft)		Off-street parking & loading	Specify in Alternative Parking Plan (see Section 5.1.6)
Residential land area, maximum (% of district total)	35	Landscaping [2] Tree protection	Specify in Alternative Landscaping Plan (see Section 5.2.9)
Lot coverage, maximum (% of lot area)	To be established in the master plan	Open space set-aside [3]	20%
Nonresidential FAR, maximum (%)		Fences and walls	Specify in Security Plan (see Sections 5.3.5 and 5.4.9 and)
Individual building size, maximum (sq ft)		Exterior lighting	
Building height, maximum (ft)		Community form	Specify in master plan
Setbacks, minimum or maximum (ft)		Nonresidential design	
Setback from abutting residential zoning district or existing residential use (ft)		Multi-family design Shopping center design	
Setback from agriculture (ft)		Community compatibility [4]	Modifications prohibited
Setback from major arterial streets (ft)		Signage	Modifications prohibited
Min. Wetland/Riparian Buffer (ft)	30	Adequate public facilities	Modifications prohibited

**NOTES:**

- [1] ~~May not exceed three units per acre in Full Service areas or one and one-half units per acre in Limited Service areas~~
- [2] Uses internal to the development shall not be required to provide perimeter buffers
- [3] The required percentage of open space set-aside shall be calculated based on the total district area

### D. ENVIRONMENTAL PROTECTION STANDARDS

The environmental protection standards in Chapter 7 of the UDO may not be modified by a planned development

[4] Neighborhood compatibility standards shall not apply to uses internal to the development

**3.7.5. Planned Development – Outer Banks (PD-O) District**

**PD-O  
PLANNED  
DEVELOPMENT –  
OUTER BANKS**

**A. DISTRICT PURPOSE**

The Planned Development – Outer Banks (PD-O) District is established and intended to provide landowner/developers with a flexible framework within which to develop a compact, mixed-use, pedestrian-oriented neighborhood development as an alternative to conventional residential development served primarily by vehicles. The PD-O district option is available for use within the portion of the outer banks served by a state-maintained highway. The district is intended to promote and maintain a beach village atmosphere that is primarily residential in character but that contains centralized nonresidential development that allows residents to meet some of their employment, shopping, and recreation needs without use of an automobile. New development shall maintain a small-scale, low-rise character with diverse housing types organized around common open space, natural resources, and facilities providing for alternative forms of transportation.

**B. DIMENSIONAL STANDARDS**

**C. DISTRICT-SPECIFIC STANDARDS**

District area, minimum (acres)	25
Gross residential density, maximum (dwelling units/acre) [1]	3 – in Full Service Areas
Lot area, minimum (sq ft)	To be established in the master plan
Lot width, minimum (sq ft)	
Nonresidential land area, maximum (% of district total)	10
Single housing type, maximum (% of units)	75 [2]
Lot coverage, maximum (% of lot area)	To be established in the Master Plan
Nonresidential FAR, maximum (%)	
Individual building size, maximum (sq ft)	
Building height, maximum (ft)	
Setbacks, minimum and maximum (ft)	
Setback from abutting residential zoning district or existing residential use (ft)	
Setback from major arterial streets, minimum (ft)	
Min. Wetland/Riparian Buffer (ft)	30

The standards in Section 3.7.5.A, Additional District-Specific Standards for the PD-O District, shall apply to all development within the PD-O District.

**D. DEVELOPMENT STANDARDS**

The standards in Chapter 5: Development Standards, shall apply to all development in PD-O districts, but some of those standards may be modified as part of the master plan if consistent with the general purposes the district and the procedures noted below.

Development Standard	Means of Modifying
Off-street parking & loading	Specify in Alternative Parking Plan (see Section 5.1.6)
Landscaping [3]	Specify in Alternative
Tree protection	Modifications prohibited
Open space set-aside [4]	30%
Fences and walls	Specify in Security Plan (see Section 5.3.5)
Exterior lighting	Modifications prohibited
Community form	Specify in master plan
Nonresidential design	
Multi-family design Shopping Center Design	
Community compatibility	Modifications prohibited
Signage	Modifications prohibited
Adequate public facilities	Modifications prohibited

**E. ENVIRONMENTAL PROTECTION STANDARDS**

The environmental protection standards in Chapter 7 of the UDO may not be modified by a planned development

**NOTES:**

- [1] May not exceed three units per acre in Full Service areas
- [2] May be exceeded only on demonstration that a less diverse mix of housing types is appropriate
- [3] Internal uses shall not be required to provide perimeter buffers
- [4] The required percentage of open space set-aside shall be calculated based on the total district area

**3.7.6. Planned Development – Currituck Station (PD-CS) District**

# PD-CS PLANNED DEVELOPMENT – CURRITUCK STATION

## A. DISTRICT PURPOSE

The Planned Development – Currituck Station (PD-CS) District is established to accommodate a high-quality mixed-use development that will foster economic development supported by a pedestrian-friendly environment that creates a sense of a unified neighborhood. Standards are intended to provide a mix of uses and densities/intensities needed to sustain a mixed-use development. The PD-CS district is proposed for lands included in the Moyock Mega Site Master Plan (and now known and referenced as Currituck Station Master Plan). The PD-CS district includes sub-district specific standards. Design emphasis is placed on achieving safe and efficient access with connected roads and pedestrian systems and visual compatibility of development within the district and surrounding areas.

## B. DISTRICT AND SUB-DISTRICTS ESTABLISHED

The Board of Commissioners shall establish individual Planned Development districts and associated sub-district designations in accordance with this section and Section 2.4.5, Planned Development, upon approval of zoning for the PD-CS district and specific sub-district designation stating the general location, attributes, policy objectives and terms and conditions for the district and sub-district. In establishing a new PD-CS district and sub-district designation, the Board of Commissioners may also establish a unique set of development standards applicable to all development in the particular sub-district that is consistent with the adopted master plan for Currituck Station (formerly known as the Moyock Mega Site Master Plan).



Sub-Districts	Development Type Summary
Center Station	Non-residential and mixed-use
Charter	Non-residential and mixed-use
Crossroads	Industrial
Cypress	Low density residential with limited neighborhood commercial
Junction	Low density residential with limited neighborhood commercial
Moyock Run	Civic and public oriented uses
Newtown	Low density residential with limited neighborhood commercial
Oak Trail	Very low density and may include environmentally sensitive areas

## C. PATTERN BOOK

The *Currituck Station Pattern Book* is incorporated herein by reference and shall be used to supplement this ordinance and provide guidance for establishing the desired character through architectural patterns, street patterns, and design of open space when developing properties in the PD-CS district and specific sub-districts.

## D. DISTRICT CRITERIA

The PD-CS district shall be a tract or combined tracts that are planned and developed as an integral unit in accordance with the Currituck Station Master Plan. The district shall maintain the balance of land uses intended for Currituck Station.

## E. INTENDED LAND USE

Intended District Area	3,000+/- acres
Residential	3,000 dwelling units
Retail	250,000 square feet
Office	300,000 square feet
Industrial	1,000,000 square feet

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## F. DEVELOPMENT STANDARDS AND MEANS OF MODIFICATION

The standards in Chapter 5: Development Standards shall apply to all development in the PD-CS district, but some of the standards may be modified as part of the master plan if consistent with the general purpose of the PD-CS district, the general purpose of the sub-districts, and the procedures noted below.

<u>Development Standard</u>	<u>Means of Modification</u>	<u>Section</u>
<u>Off-street parking and loading</u>	<u>Specify in Alternative Parking Plan</u>	<u>5.1.6.</u>
<u>Landscaping and Buffers</u>	<u>Specify in Alternative Landscaping Plan</u>	<u>5.2.9.</u>
<u>Fences and walls</u>	<u>Specify in Security Plan</u>	<u>5.3.5.</u>
<u>Exterior Lighting</u>	<u>Specify in Security Plan</u>	<u>5.4.9</u>
<u>Community Compatibility</u>	<u>Modifications prohibited</u>	
<u>Signage</u>	<u>Modifications prohibited</u>	
<u>Open space set-aside</u>	<u>Modifications prohibited</u>	
<u>Adequate public facilities</u>	<u>Modifications prohibited</u>	

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## G. Center Station Sub-District

### (1) Sub-District Intent

The Center Station sub-district is intended to be the center of the PD-CS district and is designed to support the primary economic and social components of Currituck Station by providing an integrated mix of uses in a downtown oriented setting including retail, convenience, entertainment, civic, and public uses with supporting attached and/or upper-story residential.

### (2) Sub-District Characteristics

The Center Station sub-district characteristics shall include a mix of uses developed in a downtown design pattern with a street grid, supporting pedestrian activities, on-street parallel or perpendicular parking where possible, wide sidewalks with consistent tree canopy, outdoor café seating opportunities, public spaces such as plazas or parks, and off-street parking lots located behind buildings. Development shall be oriented toward the street and provide pedestrian entrances from the street. Detached single family dwellings are not permitted in the Center Station sub-district. Development along Caratoke Highway shall provide additional attention to detail through architectural and aesthetic treatments, integrated signage, landscape and buffer enhancements, vehicle use area and building plantings, and interconnected development. Residential uses in Center Station sub-district shall not be located along Caratoke Highway.

**TABLE 3.3.1.G: INTENSITY AND DIMENSIONAL STANDARDS TABLE**

TYPE	REQUIREMENT	ADDITIONAL STANDARD
<b>DEVELOPMENT STANDARDS</b>		
Density (du/ac)	5 minimum	12 du/ac maximum if part of a mixed use building with vertical integration of uses. A maximum of 1,500 dwelling units is intended for the Center Station sub-district with a balance of nonresidential uses.
	8 maximum	
Intensity/FAR (% of lot)	0.5 minimum	
	1.5 maximum	
Open Space Set-Aside, minimum (% of development)	10%	The required percentage of open space set-aside is calculated on the development acreage; maintaining the minimum percentage of the sub-district
<b>LOT STANDARDS</b>		
Lot Area, minimum (acres)	N/A	
Lot Width	40' minimum	Modifications as part of the master plan approved by the Board of Commissioners
	200' maximum	
Lot Depth		Lot depth shall not exceed 3 times lot width
Lot Coverage (% lot area)	90% maximum	
Perimeter Fill Setback (ft.)	N/A	
Riparian Buffer	30'	
<b>SETBACKS</b>		
Front Setback (ft.)	0' minimum	75% of the front façade shall be within the setback range; the remaining 25% may exceed the maximum specified
	25' maximum	
Major Arterial Street Setback	30' minimum	
Side Setback (ft.)	10' minimum	
	50' maximum	
Corner Side Setback (ft.)	0' minimum	75% of the side corner façade shall be within the setback range; the remaining 25% may exceed the maximum specified
	40' maximum	
Rear Setback (ft.)	N/A	
Accessory Use Setback (ft.)	5' minimum	
<b>BUILDING STANDARDS</b>		
Building Height	2 stories minimum	5 stories may be permitted if more than 200' from the perimeter of the sub-district boundary and approved as part of the master plan
	4 stories maximum	

Building Stepback	15' minimum	Applied to buildings with four or more stories
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## H. Charter Sub-District

### (1) Sub-District Intent

The Charter sub-district is intended to provide a more conventional development pattern that reinforces the walkable nature of Center Station sub-district with interconnected sidewalks. The vertical mixing of residential development with office and retail, and horizontal mixing of stand-alone nonresidential development providing well-integrated uses, access and circulation and compatible design that supports the adjacent neighborhood scale residential development is encouraged. The uses in the sub-district should be designed to provide buffers and, where necessary, additional mitigation techniques to the adjacent residential and environmentally identified areas of the district.

### (2) Sub-District Characteristics

The Charter sub-district characteristics shall include neighborhood focal points or centers typically at intersections of higher intensity streets and provide a range of public, institutional and civic type uses, office park design and appearance through centralized (internal) streets, driveway connections, connected and shared parking; attached single family, multi-family, and limited detached single family.

**TABLE 3.3.1.H: INTENSITY AND DIMENSIONAL STANDARDS TABLE**

TYPE	STANDARD	ADDITIONAL STANDARD
<b>DEVELOPMENT STANDARDS</b>		
Density (du/ac)	2 maximum	4 du/acre maximum if part of a vertical mixed use building. The minimum mixed use project acreage to apply 4 du/acre shall be 5 acres.
Intensity/FAR (% of lot)	0.2 minimum 0.5 maximum	Applies to non-residential
Open Space Set-Aside, minimum (% of development)	20%	The required percentage of open space set-aside is calculated on the development acreage; maintaining the minimum percentage of the sub-district
Residential land area, maximum (% of sub-district total)	35%	
<b>LOT STANDARDS</b>		
Lot Area, minimum (square feet)	10,000	
Lot Width (ft)	100' minimum 200' maximum [1]	[1] Applies to single family residential lots
Lot Depth		Lot depth shall not exceed 3 times lot width
Lot Coverage (% lot area)	65% maximum	
Perimeter Fill Setback (ft.)	N/A	
Riparian Buffer	30'	
<b>SETBACKS</b>		
Front Setback (ft.)	25' minimum 65' maximum	50% of the front façade shall be within the setback range; the remaining 50% may exceed the maximum specified
Major Arterial Street Setback (ft)	30' minimum	
Side Setback (ft.)	10' minimum 50' maximum	
Corner Side Setback (ft.)	15' minimum	
Rear Setback (ft)	10' minimum 50' maximum	
Accessory Use Setback (ft.)	10' minimum	
<b>BUILDING STANDARDS</b>		
Building Height	3 stories maximum	
Building Stepback	N/A	

## I. Crossroads Sub-District

### (1) Sub-District Intent

The Crossroads sub-district is intended to provide industrial and job producing non-residential development. The uses in the sub-district should be designed to provide buffers and, where necessary, additional mitigation techniques to the adjacent residential and environmentally identified areas of the district.

### (2) Sub-District Characteristics

The Crossroads sub-district characteristics include industrial uses and accessory uses such as small scale offices, professional services, childcare facilities, medical and limited office or café uses. The perimeter buffers between industrial and residential and environmental uses shall be provided at no less than 100 feet. The setbacks and buffer standards may be modified based on alternative mitigation techniques approved by the Board of Commissioners and shown on the master plan. Detached single family dwellings are not permitted in the Crossroads sub-district.

**TABLE 3.3.1.I: INTENSITY AND DIMENSIONAL STANDARDS TABLE**

<u>TYPE</u>	<u>STANDARD</u>	<u>ADDITIONAL STANDARD</u>
<b><u>DEVELOPMENT STANDARDS</u></b>		
<u>Density (du/ac)</u>	<u>N/A</u>	
<u>Intensity/FAR, maximum (% of lot)</u>	<u>1.0</u>	
<u>Open Space Set-Aside, minimum (% of development)</u>	<u>20%</u>	<u>The required percentage of open space set-aside is calculated on the development acreage; maintaining the minimum percentage of the sub-district</u>
<b><u>LOT STANDARDS</u></b>		
<u>Lot Area, minimum (acres)</u>	<u>N/A</u>	
<u>Lot Width (ft.)</u>	<u>100' minimum</u>	
<u>Lot Depth</u>		<u>Lot depth shall not exceed 4 times lot width</u>
<u>Lot Coverage (% lot area)</u>	<u>65% maximum</u>	
<b><u>SETBACKS</u></b>		
<u>Front Setback, minimum (ft.)</u>	<u>50'</u>	
<u>Major Arterial Street Setback, minimum (ft.)</u>	<u>50'</u>	
<u>Side Setback, minimum (ft.)</u>	<u>25'</u>	
<u>Corner Side Setback, minimum (ft.)</u>	<u>25'</u>	
<u>Rear Setback, minimum (ft.)</u>	<u>25'</u>	
<u>Accessory Use Setback, minimum (ft.)</u>	<u>10'</u>	
<u>Perimeter Fill Setback (ft.)</u>	<u>10'</u>	
<u>Riparian Buffer</u>	<u>30'</u>	
<b><u>BUILDING STANDARDS</u></b>		
<u>Building Height, maximum</u>	<u>3 stories</u>	<u>Building heights shall be reduced to two stories when adjacent to existing residential development</u>
<u>Building Stepback</u>	<u>N/A</u>	

**J. Cypress Sub-District****(1) Sub-District Intent**

The Cypress sub-district is intended to provide areas for low-density, single family uses.

**(2) Sub-District Characteristics**

The Cypress sub-district characteristics include traditional residential neighborhood development typically including single family dwelling detached residential on individual lots. Limited single family attached housing such as mansion apartments and duplexes may be allowed. Limited non-residential uses may be provided when fronting and accessed by a collector or major arterial street. Non-residential uses shall require additional site design and development standards including but not limited to increased landscape buffers, exterior lighting, placement of service or mechanical equipment. Non-residential uses are limited to a maximum of four acres per intersection.

**TABLE 3.3.1.J: INTENSITY AND DIMENSIONAL STANDARDS TABLE**

Type	Standard	Additional Standard
<b>DEVELOPMENT STANDARDS</b>		
Density, maximum (du/ac)	2 maximum	
Intensity/FAR, maximum (% of lot)	0.2	Non-residential
Open Space Set-Aside, minimum (% of development)	35%	
Non-residential land area, maximum (acres)	4 acres per intersection	Collector and/or major arterial street
<b>LOT STANDARDS</b>		
Lot Area, minimum (square feet)	10,000 sf	
Lot Width (ft.)	40' minimum 150' maximum	
Lot Depth		Lot depth shall not exceed 3 times lot width
Lot Coverage (% lot area)	45% maximum	
Perimeter Fill Setback (ft.)	N/A	
Riparian Buffer	30'	
<b>SETBACKS</b>		
Front Setback, minimum (ft.)	20'	
Major Arterial Street Setback, minimum (ft.)	50'	
Side Setback, minimum (ft.)	10'	[1]
Corner Side Setback, minimum (ft.)	20'	[1]
Rear Setback, minimum (ft.)	25'	[1]
Accessory Use Setback, minimum (ft.)	10'	[1]
<b>BUILDING STANDARDS</b>		
Building Height, maximum	35'	
Building Stepback	N/A	
[1] The setbacks shall be multiplied by 2.0 for non-residential uses adjacent to existing residential uses. Community compatibility standards shall apply.		

## K. Junction and Newtown Sub-Districts

### (1) Sub-District Intent

The Junction and Newtown sub-districts are intended to support the district through redevelopment opportunities. Care should be given to ensure compatibility between the existing and proposed development. The Junction sub-district was not included in the Master Plan but the area may be included in the PD-CS district.

### (2) Sub-District Characteristics

The Junction and Newtown sub-district characteristics include traditional residential neighborhood development typically including single family dwelling detached residential on individual lots, but may provide for single family attached when included in a planned mixed use project. Limited nonresidential uses may be provided when fronting and accessed by a collector or major arterial street. Nonresidential uses shall require additional site design and development standards when located adjacent to single family detached development. Parking shall be located to the side and/or rear of the buildings.

**TABLE 3.3.1.K: INTENSITY AND DIMENSIONAL STANDARDS**

Type	Standard	Additional Standard
<b>DEVELOPMENT STANDARDS</b>		
Density, maximum (du/ac)	2.0	Maximum 4 du/acre if part of a mixed use project with vertical or horizontal integration of uses. Minimum project acreage shall be 5.0
Intensity/FAR, maximum (% of lot)	0.2	Applies to non-residential
Open Space Set-Aside, minimum	35%	
<b>LOT STANDARDS</b>		
Lot Area, minimum (square feet)	10,000 sf	
Lot Width (ft.)	40' minimum 200' maximum	
Lot Depth		Lot depth shall not exceed 3 times lot width
Lot Coverage (% lot area)	50% maximum	
Perimeter Fill Setback (ft.)	N/A	
Riparian Buffer	30'	
<b>SETBACKS</b>		
Front Setback, minimum (ft.)	20'	
Major Arterial Street Setback, minimum	30'	
Side Setback, minimum (ft.)	10'	[1]
Corner Side Setback, minimum (ft.)	20'	
Rear Setback, minimum (ft.)	10'	[1]
Accessory Use Setback, minimum (ft.)	5'	[1]
<b>BUILDING STANDARDS</b>		
Building Height, maximum	35'	
Building Stepback	N/A	
[1] The setbacks shall be multiplied by 2.0 for nonresidential uses adjacent to existing residential uses.		



**L. Moyock Run Sub-District****(1) Sub-District Intent**

The Moyock Run sub-district is intended to provide centralized governmental and public oriented uses including but not limited to schools, parks, utilities, transportation, regional facilities (i.e. stormwater, environmental, and mitigation areas). Residential uses are not intended for the Moyock Run sub-district.

**(2) Sub-District Characteristics**

The Moyock Run sub-district characteristics include an office park design and appearance through centralized (internal) roadways, driveway connections, connected/shared parking.

**TABLE 3.3.1.L: INTENSITY AND DIMENSIONAL STANDARDS TABLE**

<u>Type</u>	<u>Standard</u>	<u>Additional Standard</u>
<b><u>DEVELOPMENT STANDARDS</u></b>		
<u>Density, maximum (du/ac)</u>	<u>N/A</u>	
<u>Intensity/FAR, maximum (% of lot)</u>	<u>1.0</u>	<u>Non-residential uses</u>
<u>Open Space Set-Aside, minimum</u>	<u>35%</u>	
<b><u>LOT STANDARDS</u></b>		
<u>Lot Area, minimum (square feet)</u>	<u>N/A</u>	
<u>Lot Width (ft.)</u>	<u>40' minimum</u> <u>200' maximum</u>	<u>Does not apply to governmental and public uses</u>
<u>Lot Depth</u>		<u>Lot depth shall not exceed 4 times lot width</u>
<u>Lot Coverage (% lot area)</u>	<u>65% maximum</u>	
<u>Perimeter Fill Setback (ft.)</u>	<u>N/A</u>	
<u>Riparian Buffer</u>	<u>30'</u>	
<b><u>SETBACKS</u></b>		
<u>Front Setback, minimum (ft.)</u>	<u>20'</u>	
<u>Major Arterial Street Setback, minimum (ft.)</u>	<u>50'</u>	
<u>Side Setback, minimum (ft.)</u>	<u>10'</u>	
<u>Corner Side Setback, minimum (ft.)</u>	<u>10'</u>	
<u>Rear Setback, minimum (ft.)</u>	<u>10'</u>	
<u>Accessory Use Setback, minimum (ft.)</u>	<u>10'</u>	
<b><u>BUILDING STANDARDS</u></b>		
<u>Building Height, maximum</u>	<u>3 stories</u>	
<u>Building Stepback</u>	<u>N/A</u>	

**M. Oak Trail Sub-District****(1) Sub-District Intent**

The Oak Trail sub-district is intended to reflect those areas generally identified as environmentally sensitive including but not limited to native habitats, wetlands, riparian buffers, and floodplain. Low density and low impact residential uses and recreational uses may be permitted upon determination of the environmental area limits.

**(2) Sub-District Characteristics**

The Oak Trail sub-district characteristics include a transition area between more intense uses and activities by providing lower densities and intensities.

**TABLE 3.3.1.M: INTENSITY AND DIMENSIONAL STANDARDS**

Type	Standard	Additional Standard
<b>DEVELOPMENT STANDARDS</b>		
Density, maximum (du/ac)	0.5	Maximum 1.0 du/ac may be allowed for a development that provides regional stormwater
Intensity/FAR, maximum (% of lot)	N/A	
Open Space Set-Aside, minimum	50%	
<b>LOT STANDARDS</b>		
Lot Area, minimum (square feet)	40,000 sf	
Lot Width (ft.)	100' minimum 200' maximum	
Lot Depth		Lot depth shall not exceed 4 times lot width
Lot Coverage (% lot area)	25% maximum	
<b>SETBACKS</b>		
Front Setback, minimum (ft.)	50'	
Major Arterial Street Setback, minimum	50'	
Side Setback, minimum (ft.)	25'	
Corner Side Setback, minimum (ft.)	25'	
Rear Setback, minimum (ft.)	50'	
Accessory Use Setback, minimum (ft.)	25'	
Perimeter Fill Setback, minimum (ft.)	10'	
Riparian Buffer	30'	
<b>BUILDING STANDARDS</b>		
Building Height, maximum	35'	
Building Stepback	N/A	N/A

## **N. District-Specific Standards for the PD-CS District**

### **(1) General Design Standards**

Development in the Currituck Station district shall meet the requirements of this ordinance. Development patterns shall be in general compliance with the *Currituck Station Pattern Book*.

### **(2) Development Center**

**(a)** A PD-CS district shall be designed with a development center (Center Station sub-district designation) intended to serve as a public gathering area for residents. A development center shall include and be served by open space resources that allow pedestrians to walk to and through the development center.

**(b)** The development center shall include on-street and off-street parking resources that allow residents to park their vehicles and walk to destinations within the district.

### **(3) Connectivity**

**(a)** The streets, driveways, alleys, and multi-modal connections within the district shall be functional, attractive, and designed to accommodate multi-modes of transportation and where possible incorporate Complete Streets elements.

**(b)** The arrangement of streets shall provide for the alignment and continuation of existing and proposed streets into adjoining lands where the adjoining lands are undeveloped and intended for future development, or where the adjoining lands are developed and include opportunities for connections.

### **(4) Use Mixing**

**(a)** A PD-CS district shall be structured to provide a mix of uses, such as residential, retail, office, employment, civic, and recreational uses. The integration of residential and nonresidential uses allows residents to meet more of their daily needs within the development.

**(b)** Civic uses such as churches, post offices, and community centers are encouraged, but not required, as part of the district's nonresidential uses.

### **(5) Open Space Design**

**(a)** The PD-CS district shall include formal open space areas for recreation and community gathering.

**(b)** Open spaces shall include pedestrian and bicycle features that allow residents and visitors to move through and around commercial and mixed-use portions of the district.

**(c)** Open space resources shall connect new developments with existing developments so that it is accessible to and usable by all persons living in the project area.

### **(6) Signage**

Signs shall be well-designed, communicate a message clearly, and shall not dominate the building or property. Signs shall be compatible

and complementary to the architectural designs of the building. Sign patterns shall be compatible with the district or sub-district development.

**(7) Building Configuration**

**(a) Location and Relationship between Buildings**

Buildings in a PD-CS Center Station sub-district shall be used to define the street edge and the distinction between the public domain of the street and the private space of individual lots. To this end, buildings shall have a fairly consistent, narrow setback alignment along the street frontage.

**(b) Relationship between Building Types**

Buildings in a PD-CS district should be built on a human scale, designed with common harmonious architecture, and landscaped to lend an intimate and personal feel to the streetscape. The intent should not be to create a uniform appearance, but rather a distinct sense of place.

**(8) Building Design**

Buildings in the PD-CS district are intended to utilize architectural patterns and features in the building design that are consistent with the Currituck vernacular as provided in the *Currituck Station Pattern Book*. Deviations to the local vernacular may be permitted, as provided in the master plan, when the development meets the requirements of Chapter 5, Development Standards and incorporates simple design elements typically known as the Currituck vernacular including but not limited to:

- (a)** Deep porches;
- (b)** Pitched roofs with overhangs;
- (c)** Shutters (fixed, operable, Bermuda);
- (d)** Roof dormers;
- (e)** Chimneys;
- (f)** Elevated buildings; and,
- (g)** Gabled masses that break up long facades

## **3.8. OVERLAY ZONING DISTRICTS**

### **3.8.1. Purpose**

Overlay zoning districts are superimposed over portions of one or more underlying base zoning districts, conditional zoning districts, or planned development districts with the intent of supplementing generally applicable development regulations with additional development regulations that address special area-specific conditions, features, or plans while maintaining the character and purposes of the underlying zoning district.

### 3.8.2. Establishment of Overlay Zoning Districts

Table 3.8.2, Overlay Zoning Districts Established, sets out the overlay zoning districts established by this Ordinance. Except where specifically provided in this Ordinance, variances from the overlay zoning district standards shall not be granted.

**TABLE 3.8.2: OVERLAY ZONING DISTRICTS ESTABLISHED**

DISTRICT NAME	ABBREVIATION
Airport Overlay	AO
Corolla Village Overlay [placeholder only]	CVO
Transportation Corridor Overlay District	TCOD

### 3.8.6. Transportation Corridor Overlay District (TCOD)

#### A. Purpose

The purpose of the Transportation Corridor Overlay District (TCOD) is to protect and promote the desired character of the transportation corridors, enhance the economic and aesthetic appeal, reduce the unnecessary visual distractions, and provide consistent and orderly development of lands adjacent to existing and proposed major transportation corridors in the county.

#### B. Boundaries of the TCOD Established

The TCOD extends 500 feet from the right-of-way boundary on either side of roadways, following identifiable boundaries whenever possible. The established TCOD boundaries include:

- (1) NC 168 (Caratoke Highway) from the North Carolina/Virginia State Line southward to the intersection of SR 1227 (South Mills Road); and,
- (2) SR 1227 (South Mills Road) from its intersection with NC 168 (Caratoke Highway) westward to the intersection of SR 1218 Northwest Backwoods Road.

#### C. Applicability

- (1) Development and use of lands within the TCOD shall be subject to the standards in this section. In the case of conflict between the standards of the TCOD and other standards in this ordinance, the overlay standards shall control.

#### D. Modifications of Otherwise Applicable Standards

Development in the TCOD shall comply with the following standards:

##### (1) Prohibited Uses

The following uses are prohibited in the TCOD:

- (a) Adult entertainment;
- (b) Outdoor storage, as a principal use;
- (c) Tattoo parlor; and
- (d) Vehicle sales and service, light and heavy;

**(2) Access Management**

A traffic impact analysis shall be required in all instances in which the proposed development area exceeds three acres or generates 60 or more peak hour vehicle trips. The traffic impact shall be prepared by a qualified professional specializing in transportation.

**Item 4:** That Chapter 4 is amended by deleting the following strikethrough language and adding the underlined language

# CHAPTER 4.

# USE STANDARDS

## 4.1. USE TABLE

### 4.1.1. Explanation of Use Table Structure

#### A. General

- (1) Table 4.1.1A. and Table 4.1.1B. Summary Use Table, lists use types and indicates whether they are allowed by right, allowed with a use permit, allowed in a conditional zoning district, allowed in a planned development zoning district, or prohibited in a zoning district. The use table also includes references to any additional requirements or regulations applicable to the specific use type.
- (2) The status of a use in a conditional zoning district shall be the same as in the parallel base zoning district unless such status is modified by conditions imposed as part of the conditional rezoning designating the conditional zoning district.

#### C. Uses Allowed Subject to a Planned Development District Classification

A “MP” in a cell of the use table indicates that the corresponding use category or use type is allowed in the corresponding planned development district, subject to compliance with the use-specific regulations set forth in the final column of the table and provided the use is included in the required list of possible use types in the planned development master plan. An “U” in the cell of the use table indicates that the corresponding use category or use type is allowed in the corresponding planned development district only upon approval of a use permit in accordance with Section 2.4.6, Use Permit, and any conditions imposed as part of the approved master plan and terms and conditions. Allowed uses are subject to other applicable regulations in this Ordinance, including those set forth in Section 3.7, Planned Development Base Zoning Districts.

#### D. Prohibited Uses

- (1) A blank cell in the use table indicates that the corresponding use category or use type is prohibited in the corresponding zoning district.

Use types with a blank cell are not allowable in a corresponding planned development district, and shall not be included in the planned development master plan.

- (2) The following activities or use types are not identified in Table 4.1.1.A and Table 4.1.1.B. but are prohibited in all zoning districts in the county.
  - (a) Use of a parked motor vehicle to buy, sell, or store goods or services, except as allowed in Section 4.3 Accessory Use Standards or Section 4.4, Temporary Use Standards;
  - (b) Use of a boat, houseboat, or other floating structure as a temporary or permanent residence (this shall not prevent the overnight occupancy of a vessel temporarily docked while in transit on navigable waters);
  - (c) Use of a travel trailer as a permanent residence or use of a travel trailer as a temporary residence;
  - (d) Operation of a principal (non-accessory) commercial use located solely on the beach strand (uses simply transporting customers to the beach from an approved off-beach location are exempted); and
  - (e) Manufactured home parks or private campgrounds as a principal use.

**E. Use-Specific Standards**

**4.1.2. Use Table**

**TABLE 4.1.1.A.: SUMMARY USE TABLE**

Z = Zoning Compliance Permit; U = Use Permit; MP = Allowed with Master Plan;  
 CZ= Allowed in a Conditional Zoning District blank cell = Prohibited

USE CATEGORY	USE TYPE	ZONING DISTRICT [NOTE: OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]															ADDITIONAL REQ. (4.2.____)		
		RC	AG	SFM	SFO	SFR	SFI	MXR	GB	LB	CC	VC	LI	HI	PD-R	PD-M		PD-O	
<b>AGRICULTURAL USE CLASSIFICATION</b>																			
Agriculture / Horticulture	All	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	M P	M P	M P	
Animal Husbandry	All		Z				Z						Z	Z			M P		1.A
Agriculture Support and Services (Directly Related)	Agri-education	Z	Z				Z		Z	Z	Z						M P		1.B.2
	Agri-entertainment	Z	Z				Z		Z	Z	Z						M P		1.B.2
	Agricultural processing		Z					U					Z	Z			M P		1.B
	Agribusiness		Z				Z		Z	Z	Z			Z			M P		1.B
	Equestrian facility		Z	Z			Z		Z		Z	Z	Z			M P	M P	M P	1.B.3
	Farmers market		Z				Z		Z	Z	Z	Z	Z			M P	M P	M P	1.B

**TABLE 4.1.1.A.: SUMMARY USE TABLE**

Z = Zoning Compliance Permit; U = Use Permit; MP = Allowed with Master Plan;  
 CZ= Allowed in a Conditional Zoning District blank cell = Prohibited

USE CATEGORY	USE TYPE	ZONING DISTRICT [NOTE: OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]															ADDITIONAL REG. (4.2.____)	
		RC	AG	SFM	SFO	SFR	SFI	MXR	GB	LB	CC	VC	LI	HI	PD-R	PD-M		PD-O
	Nursery, production		Z				Z		Z	Z			Z	Z	M P	M P		1.B.4
	Roadside market		Z						Z									1.B.5
Agriculture Support and Services (Not Directly Related)	Agricultural research facility		Z						Z	Z	Z		Z	Z		M P		1.C
	Distribution hub for agricultural products		Z						Z	Z			Z	Z		M P		1.C
	Farm machinery sales, rental, and service		Z						Z	Z	Z		Z	Z		M P		1.C
	Stockyard / Slaughterhouse		U											U				1.C
Silviculture	All	Z	Z	Z			Z	Z	Z	Z			Z	Z	M P	M P		1.C
<b>RESIDENTIAL USE CLASSIFICATION</b>																		
Household Living	Dwelling, duplex			Z/U				Z			Z	Z			M P	M P	M P	2.A.1
	Dwelling, live/work							Z	Z	Z	Z	Z			M P	M P	M P	2.A.2
	Dwelling, mansion apartment							C Z			Z	Z			M P	M P	M P	2.A.3
	Dwelling, manufactured home (class A)		Z	Z			Z	Z	Z									2.A.4
	Dwelling, manufactured home (class B)		Z	Z			Z	Z	Z									2.A.4
	Dwelling, multi-family							C Z			Z	Z			M P	M P	M P	2.A.5
	Dwelling, single-family detached		Z	Z	Z	Z	Z	Z	Z	Z	Z	Z			M P	M P	M P	
	Dwelling, townhouse							C Z			Z	Z			M P	M P	M P	2.A.5
	Dwelling, upper story							Z	Z	Z	Z	Z			M P	M P	M P	2.A.6
Group Living	Dormitory						Z	Z	U	Z	Z				M P			2.B.1
	Family care home			Z	Z	Z	Z	Z	Z	Z	Z				M P	M P	M P	2.B.2
	Rooming or boarding house						Z	Z		Z	Z				M P	M P	M P	2.B.3
<b>INSTITUTIONAL USE CLASSIFICATION</b>																		
Community Services	Community center			Z	Z	U	Z	Z	Z	Z	Z	Z			M P	M P	M P	
	Cultural facility	U		Z	Z			Z	Z	Z	Z	Z			M P	M P	M P	
	Library			Z	Z		Z	Z	Z	Z	Z	Z			M P	M P	M P	
	Museum			Z				Z	Z	Z	Z	Z			M P	M P	M P	
	Senior center							Z	Z	Z	Z	Z			M P	M P	M P	
	Youth club facility							Z	Z	Z	Z	Z			M P	M P	M P	
Day Care	Adult day care center							Z	Z	Z	Z	Z			M P	M P	M P	
	Child care center			Z	Z		Z	Z	Z	Z	Z				M P	M P	M P	3.A
College or university									Z	Z	Z	Z			M	M	M	

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**TABLE 4.1.1.A.: SUMMARY USE TABLE**

Z = Zoning Compliance Permit; U = Use Permit; MP = Allowed with Master Plan;  
 CZ= Allowed in a Conditional Zoning District blank cell = Prohibited

USE CATEGORY	USE TYPE	ZONING DISTRICT [NOTE: OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]															ADDITIONAL REG. (4.2.____)		
		RC	AG	SFM	SFO	SFR	SFI	MXR	GB	LB	CC	VC	LI	HI	PD-R	PD-M		PD-O	
															P	P	P		
	School, elementary		U	Z	Z		U	Z	Z	Z	Z	Z			M P	M P	M P		
	School, middle		U	Z	Z		U	Z	Z	Z	Z	Z			M P	M P	M P		
	School, high							Z	Z	Z	Z	Z			M P	M P	M P		
	Vocational or trade school								Z	Z	Z	Z	Z	Z		M P	M P	M P	3.B
Government Facilities	Government maintenance, storage, or distribution facility								Z	Z	Z	Z	Z	Z	M P	M P	M P		
	Government office		Z	Z	Z		Z	Z	Z	Z	Z	Z	Z		M P	M P	M P		
Health Care Facilities	Blood/tissue collection facility								Z		U	U	Z						
	Drug or alcohol treatment facility								Z	U	U	U	Z						3.C.1
	Hospital								Z	Z	Z	Z	Z			M P	M P	M P	3.C.2
	Medical treatment facility								Z	Z	Z	Z			M P	M P	M P		
Institutions	Assisted living facility							Z	Z	Z	Z	Z			M P	M P	M P		
	Auditorium, conference, and convention center								Z	Z	Z	Z				M P	M P	M P	3.D.1
	Club or lodge			U				U	Z	Z	Z	Z	Z		M P	M P	M P		
	Halfway house							U	U				U						3.D.2
	Nursing home							Z	Z	Z	Z	Z			M P	M P	M P		
	Psychiatric treatment facility							U					U						
	Religious institution		Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z		M P	M P	M P	3.D.3	
Parks and Open Areas	Arboretum or botanical garden	Z	Z	Z	Z		Z	Z	Z	Z	Z	Z			M P	M P	M P		
	Cemetery, columbaria, mausoleum							Z					Z	Z		M P			3.E
	Community garden	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z			M P	M P	M P		
	Park, public or private	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z			M P	M P	M P		
Public Safety	Correctional facility												U	U					
	Police, fire, or EMS facility		Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	M P	M P	M P		
	Security training facility		U																3.F
Transportation	Airport		U						U		U		U	U		M P	M P	M P	
	Helicopter landing facility								U		U		U	U		M P			3.G
	Passenger terminal, surface transportation			U	U				Z		Z	Z			M P	M P	M P		
Utilities	Solar array																		
	Telecommunications antenna collocation on tower or building		Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	M P	M P	M P	3.H.2	

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**TABLE 4.1.1.A.: SUMMARY USE TABLE**

Z = Zoning Compliance Permit; U = Use Permit; MP = Allowed with Master Plan;  
 CZ= Allowed in a Conditional Zoning District blank cell = Prohibited

USE CATEGORY	USE TYPE	ZONING DISTRICT [NOTE: OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]															ADDITIONAL REG. (4.2.____)	
		RC	AG	SFM	SFO	SFR	SFI	MXR	GB	LB	CC	VC	LI	HI	PD-R	PD-M		PD-O
	Telecommunications tower, freestanding		U			U			U	U			U	U				3.H.2
	Utility, major	U	U	U	U	U	U	U	U	U	U	U	U	U	MP	MP	MP	3.I
	Utility, minor	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	MP	MP	MP	3.J
	Wind energy facility, large																	
<b>COMMERCIAL USE CLASSIFICATION</b>																		
<b>Adult Entertainment</b>	All													U				4.A
<b>Animal Care</b>	Animal grooming								Z	Z	Z	Z	Z		MP	MP	MP	4.B
	Animal shelter								Z					Z		MP		4.B
	Kennel		U						Z	Z		Z	Z	Z		MP	MP	4.B
	Veterinary clinic		U						Z	Z	Z	Z	Z		MP	MP	MP	4.B
<b>Eating Establishments</b>	Dinner theater								Z	Z	Z	Z			MP	MP	MP	
	Restaurant, with indoor or outdoor seating						U	Z	Z	Z	Z	Z			MP	MP	MP	4.C
	Specialty eating establishment						Z	Z	Z	Z	Z	Z			MP	MP	MP	
<b>Offices</b>	Business and sales							Z	Z	Z	Z	Z			MP	MP	MP	4.D
	Professional services							Z	Z	Z	Z	Z			MP	MP	MP	4.D
<b>Parking, Commercial</b>	Parking lot								Z	Z	Z	Z	Z	Z	MP	MP	MP	4.E.1
	Parking structure										Z	Z			MP	MP	MP	4.E.2
<b>Recreation/ Entertainment, Indoor</b>	Fitness center							Z	Z	Z	Z	Z	Z		MP	MP	MP	
	Recreation, indoor							Z	Z	Z	Z	Z	Z		MP	MP	MP	
	Theater							Z	Z	Z	Z	Z			MP	MP	MP	
<b>Recreation / Entertainment, Outdoor</b>	Automotive Racing													U				
	Arena, amphitheater, or stadium								U		U	U			MP	MP	MP	4.F.1
	Athletic facility			Z	Z		Z	Z	Z	Z	Z	Z			MP	MP	MP	4.F.2
	Golf course			U	U			U							MP	MP	MP	
	Golf driving range							Z	Z	Z					MP	MP		
	Marinas							U	Z		Z	Z	Z		MP	MP	MP	4.F.3
	Outdoor shooting range		U											U				4.F.5
	Outdoor tour operator								U	U	Z	Z				MP	MP	4.F.6
	Recreation, outdoor							U	Z	Z	Z	Z			MP	MP	MP	4.F.4

Attachment: 2- Currituck Station Draft UDO 12-3-19 (PB 19-25 Currituck County - Currituck Station)

**TABLE 4.1.1.A.: SUMMARY USE TABLE**

Z = Zoning Compliance Permit; U = Use Permit; MP = Allowed with Master Plan;  
 CZ= Allowed in a Conditional Zoning District blank cell = Prohibited

USE CATEGORY	USE TYPE	ZONING DISTRICT [NOTE: OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]															ADDITIONAL REQ. (4.2.____)	
		RC	AG	SFM	SFO	SFR	SFI	MXR	GB	LB	CC	VC	LI	HI	PD-R	PD-M		PD-O
Retail Sales & Services	Artisan Food and Beverage Producer								Z				Z		M P	M P	M P	
	Auction House												Z			M P		
	Bar, nightclub, or cocktail lounge								Z		Z	Z			M P	M P	M P	4.G.1
	Brewery, Large												Z		M P	M P	M P	
	Convenience store							Z	Z	Z	Z	Z	Z		M P	M P	M P	
	Crematory													U				
	Distillery												Z					
	Drug store or pharmacy							Z	Z	Z	Z	Z			M P	M P	M P	
	Entertainment establishment							Z	Z	Z	Z	Z			M P	M P	M P	
	Financial institution							Z	Z	Z	Z	Z			M P	M P	M P	
	Flea market												Z	Z	M P	M P		4.G.2
	Funeral home								Z	Z	Z	Z				M P	M P	
	Grocery store								Z		Z	Z			M P	M P	M P	
	Laundromat							Z	Z	Z	Z	Z	Z		M P	M P	M P	
	Pawn shop								U				U					
	Personal services establishment							Z	Z	Z	Z	Z	Z		M P	M P	M P	
	Repair establishment								Z	Z	Z	Z	Z	Z		M P	M P	4.G.3
	Retail sales establishments								Z	Z	Z	Z			M P	M P	M P	
	Shopping center								U		Z	Z				M P	M P	4.G.4
Tattoo parlor/body piercing establishment												Z	Z				4.G.5	
Winery								Z				Z		M P	M P	M P		
Vehicle Sales and Services, Heavy	Aircraft parts, sales, and maintenance											Z	Z					
	Automotive wrecker service											Z	Z				M P	4.H.2
	Boat and marine rental, sales, and service								Z			Z	Z					4.H.3
Vehicle Sales and Services, Light	Automotive parts and installation								Z		Z	Z	Z			M P	M P	4.I.1
	Automobile repair and servicing (including painting/bodywork)								Z		Z	Z	Z			M P	M P	4.I.2
	Automobile sales or rentals								U		Z	Z	Z			M P	M P	4.I.3
	Car wash or auto detailing								Z		Z	Z	Z		M P	M P	M P	4.I.4
	Taxicab service								Z	Z	Z	Z			M P	M P	M P	4.I.5
Visitor	Bed and breakfast inn			Z	Z		Z	Z	Z	Z	Z			M	M	M	4.J.1	

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USE CATEGORY	USE TYPE	ZONING DISTRICT [NOTE: OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]															ADDITIONAL REG. (4.2.____)		
		RC	AG	SFM	SFO	SFR	SFI	MXR	GB	LB	CC	VC	LI	HI	PD-R	PD-M		PD-O	
Accommodations																P	P	P	
	Hotel or motel									Z		Z	Z			M P	M P	M P	4.J.2
	Hunting lodge		U						Z	Z	Z					M P	M P	M P	
<b>INDUSTRIAL USE CLASSIFICATION</b>																			
Extractive Industry	All uses		U										U	U					5.A
	Contractor service									Z		Z	Z	Z	Z		M P	M P	
Industrial Services	Crabshedding		Z						Z	Z			Z	Z	M P	M P			5.B.1
	Fuel oil/bottled gas distributor												Z	Z		M P			
	General industrial service and repair												Z	Z		M P			5.B.2
	Heavy equipment sales, rental, and service												Z	Z		M P			5.B.3
	Laundry, dry cleaning, and carpet cleaning plants												Z	Z		M P			5.B.4
	Manufactured home and prefabricated building sales												Z	Z					5.B.5
	Research and development									Z	Z	Z	Z	Z	Z	M P	M P	M P	
	Manufacturing and Production	Manufacturing, heavy													Z				
Manufacturing, light													Z	Z		M P			5.C.2
Warehouse and Freight Movement	Cold storage plant												Z	Z					
	Outdoor storage (as a principal use)												Z	Z					5.D.1
	Self-service storage												Z	Z		M P			5.D.2
	Truck or freight terminal												Z	Z		M P			5.D.3
	Warehouse (distribution)												Z	Z		M P			5.D.3
	Warehouse (storage)												Z	Z	M P	M P	M P		5.D.3
Waste-Related Services	Incinerator													U					5.E.1
	Landfill, land clearing and inert debris or construction debris												U	U					5.E.2
	Public convenience center/transfer station		U	U	U	U	U	U	U	U			U	U					5.E.3
	Recycling center, processing													U					5.E.4
	Recycling center, transfer												U	U					5.E.5
	Salvage and junkyard													U					5.E.6
	Waste composting		U										U	U					
Wholesale Sales	All uses												Z	Z		M P	M P		

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**TABLE 4.1.1.B: SUMMARY USE TABLE**

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USE CATEGORY	USE TYPE	PLANNED DEVELOPMENT ZONING DISTRICT [OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]											ADDITIONAL REQ. (4.2.)	
		PD-R	PD-M	PD-O	PD-CS ZONING DISTRICT SUB-DISTRICT DESIGNATIONS									
					CENTER STATION	CHARTER	CROSSROADS	CYPRESS	JUNCTION	MOYOCK RUN	NEWTOWN			OAK TRAIL
<b>AGRICULTURAL USE CLASSIFICATION</b>														
<u>Agriculture / Horticulture</u>	All	M/P	M/P	M/P									M/P	
<u>Animal Husbandry</u>	All		M/P											1.A
<u>Agriculture Support and Services (Directly Related)</u>	Agri-education		M/P											1.B.2
	Agri-entertainment		M/P											1.B.2
	Agricultural processing		M/P											1.B
	Agribusiness		M/P											1.B
	Equestrian facility	M/P	M/P	M/P										1.B.3
	Farmers market	M/P	M/P	M/P	M/P	M/P							M/P	1.B
	Nursery, production	M/P	M/P										M/P	1.B.4
	Roadside market													1.B.5
<u>Agriculture Support and Services (Not Directly Related)</u>	Agricultural research facility		M/P											1.C
	Distribution hub for agricultural products		M/P											1.C
	Farm machinery sales, rental, and service		M/P											1.C
	Stockyard / Slaughterhouse													1.C
<u>Silviculture</u>	All	M/P	M/P											1.C
<b>RESIDENTIAL USE CLASSIFICATION</b>														
<u>Household Living</u>	Dwelling, duplex	M/P	M/P	M/P	M/P	M/P		M/P	M/P			M/P		2.A.1
	Dwelling, live/work	M/P	M/P	M/P				M/P						2.A.2
	Dwelling, mansion apartment	M/P	M/P	M/P				U		U		U		2.A.3
	Dwelling, manufactured home (class A)													2.A.4
	Dwelling, manufactured home (class B)													2.A.4
	Dwelling, multi-family	M/P	M/P	M/P	U	U				U		U		2.A.5
	Dwelling, single-family detached	M/P	M/P	M/P				M/P	M/P			M/P	M/P	
	Dwelling, townhouse	M/P	M/P	M/P				U		U		U		2.A.5
	Dwelling, upper story	M/P	M/P	M/P				U						2.A.6
<u>Group Living</u>	Dormitory		M/P											2.B.1
	Family care home	M/P	M/P	M/P										2.B.2

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**TABLE 4.1.1.B: SUMMARY USE TABLE**

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USE CATEGORY	USE TYPE	PLANNED DEVELOPMENT ZONING DISTRICT [OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]												
		PD-R	PD-M	PD-O	PD-CS ZONING DISTRICT SUB-DISTRICT DESIGNATIONS							ADDITIONAL REQ. (4.2.)		
					CENTER STATION	CHARTER	CROSSROADS	CYPRESS	JUNCTION	MOYOCK RUN	NEWTOWN		OAK TRAIL	
<b>Parks and Open Areas</b>	Arboretum or botanical garden	M P	M P	M P										
	Cemetery, columbaria, mausoleum		M P											
	Community garden	M P	M P	M P								M P		
	Park, public or private	M P	M P	M P	M P	M P	M P	M P	M P	M P	M P	M P		
<b>Public Safety</b>	Correctional facility													
	Police, fire, or EMS facility	M P	M P	M P	M P	M P	M P	M P	M P	M P	M P	M P		
	Security training facility													
<b>Transportation</b>	Airport		M P	M P										
	Helicopter landing facility		M P											
	Passenger terminal, surface transportation	M P	M P	M P										
<b>Utilities</b>	Solar array													
	Telecommunications antenna collocation on tower or building	M P	M P	M P									U	
	Telecommunications tower, freestanding												U	
	Utility, major	M P	M P	M P							U			
	Utility, minor	M P	M P	M P							U			3.J
	Wind energy facility, large		M P	M P										3.K
<b>COMMERCIAL USE CLASSIFICATION</b>														
<b>Adult Entertainment</b>	All													4.A
<b>Animal Care</b>	Animal grooming	M P	M P	M P	M P	M P	M P							4.B
	Animal shelter		M P											4.B
	Kennel		M P	M P										4.B
	Veterinary clinic	M P	M P	M P	M P	M P	M P		M P		M P			4.B INDOOR NO BOARDING
<b>Eating Establishments</b>	Dinner theater	M P	M P	M P	M P	M P								
	Restaurant, with indoor or outdoor seating	M P	M P	M P	M P	M P	U	M P	M P		M P			4.C
	Specialty eating establishment	M P	M P	M P	M P	M P	U	M P	M P		M P			
<b>Offices</b>	Business and sales	M P	M P	M P	M P	M P	M P					M P		4.D
	Professional services	M P	M P	M P	M P	M P	M P					M P		4.D
<b>Parking, Commercial</b>	Parking lot	M P	M P	M P			U							4.E.1
	Parking structure	M P	M P	M P			U							4.E.2

**TABLE 4.1.1.B: SUMMARY USE TABLE**

Z = Zoning Compliance Permit; U = Use Permit; MP = Allowed with Master Plan;  
 CZ= Allowed in a Conditional Zoning District blank cell = Prohibited

USE CATEGORY	USE TYPE	PLANNED DEVELOPMENT ZONING DISTRICT [OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]												
		PD-R	PD-M	PD-O	PD-CS ZONING DISTRICT SUB-DISTRICT DESIGNATIONS							ADDITIONAL REQ. (4.2.)		
					CENTER STATION	CHARTER	CROSSROADS	CYPRESS	JUNCTION	MOYOCK RUN	NEWTOWN		OAK TRAIL	
<b>Recreation/ Entertainment, Indoor</b>	Fitness center	M P	M P	M P		M P	M P		M P					
	Recreation, indoor	M P	M P	M P		U	U							
	Theater	M P	M P	M P		M P	M P							
<b>Recreation / Entertainment, Outdoor</b>	Automotive Racing													
	Arena, amphitheater, or stadium	M P	M P	M P										4.F.1
	Athletic facility	M P	M P	M P										4.F.2
	Golf course	M P	M P	M P										
	Golf driving range	M P	M P											
	Marinas	M P	M P	M P										4.F.3
	Outdoor shooting range													4.F.5
	Outdoor tour operator		M P	M P										4.F.6
	Recreation, outdoor	M P	M P	M P										4.F.4
<b>Retail Sales &amp; Services</b>	Artisan Food and Beverage Producer	M P	M P	M P		M P	M P	M P						
	Auction House		M P											
	Bar, nightclub, or cocktail lounge	M P	M P	M P										4.G.1
	Brewery, Large	M P	M P	M P		M P	M P	M P						
	Convenience store	M P	M P	M P		U	U	U	U	U		U		
	Crematory													
	Distillery					U	U	M P						
	Drug store or pharmacy	M P	M P	M P		M P	M P							
	Entertainment establishment	M P	M P	M P										
	Financial institution	M P	M P	M P		M P	M P							
	Flea market	M P	M P											4.G.2
	Funeral home		M P	M P										
	Grocery store	M P	M P	M P		U								
	Laundromat	M P	M P	M P										
	Pawn shop													
	Personal services establishment	M P	M P	M P		M P								
	Repair establishment		M	M										4.G.3

**TABLE 4.1.1.B: SUMMARY USE TABLE**

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USE CATEGORY	USE TYPE	PLANNED DEVELOPMENT ZONING DISTRICT [OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]											
		PD-R	PD-M	PD-O	PD-CS ZONING DISTRICT SUB-DISTRICT DESIGNATIONS							ADDITIONAL REQ. (4.2.)	
					CENTER STATION	CHARTER	CROSSROADS	CYPRESS	JUNCTION	MOYOCK RUN	NEWTOWN		OAK TRAIL
			P	P									
	Retail sales establishments	M P	M P	M P	M P	M P	M P						
	Shopping center		M P	M P					J C		J C		4.G.4
	Tattoo parlor/body piercing establishment												4.G.5
	Winery	M P	M P	M P	M P	M P	J C						
<b>Vehicle Sales and Services, Heavy</b>	Aircraft parts, sales, and maintenance												
	Automotive wrecker service			M P									4.H.2
	Boat and marine rental, sales, and service		M P	M P									4.H.3
<b>Vehicle Sales and Services, Light</b>	Automotive parts and installation		M P	M P									4.I.1
	Automobile repair and servicing (including painting/bodywork)		M P	M P									4.I.2
	Automobile sales or rentals	M P	M P	M P									4.I.3
	Car wash or auto detailing	M P	M P	M P									4.I.4
	Taxicab service	M P	M P	M P					J C		J C		4.J.5
<b>Visitor Accommodations</b>	Bed and breakfast inn	M P	M P	M P	M P	M P					J C		4.J.1
	Hotel or motel	M P	M P	M P					M P		M P		
	Hunting lodge	M P	M P	M P					M P		M P		
<b>INDUSTRIAL USE CLASSIFICATION</b>													
<b>Extractive Industry</b>	All uses												
<b>Industrial Services</b>	Contractor service		M P	M P									
	Crabshedding	M P	M P										5.B.1
	Fuel oil/bottled gas distributor		M P										
	General industrial service and repair		M P										5.B.2
	Heavy equipment sales, rental, and service		M P										5.B.3
	Laundry, dry cleaning, and carpet cleaning plants		M P										5.B.4
	Manufactured home and prefabricated building sales												5.B.5
	Research and development	M P	M P	M P			M P	M P					
<b>Manufacturing and Production</b>	Manufacturing, heavy												5.C.1
	Manufacturing, light		M P				J C	M P					5.C.2

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**TABLE 4.1.1.B: SUMMARY USE TABLE**

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USE CATEGORY	USE TYPE	PLANNED DEVELOPMENT ZONING DISTRICT [OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]												
		PD-R	PD-M	PD-O	PD-CS ZONING DISTRICT SUB-DISTRICT DESIGNATIONS							ADDITIONAL REQ. (4.2.)		
					CENTER STATION	CHARTER	CROSSROADS	CYPRESS	JUNCTION	MOYOCK RUN	NEWTOWN		OAK TRAIL	
<b>Freight Movement</b>	<u>Cold storage plant</u>													
	<u>Outdoor storage (as a principal use)</u>													
	<u>Self-service storage</u>		M P											5.D.2
	<u>Truck or freight terminal</u>		M P											5.D.3
	<u>Warehouse (distribution)</u>		M P											5.D.3
	<u>Warehouse (storage)</u>	M P	M P	M P										5.D.3
<b>Waste-Related Services</b>	<u>Incinerator</u>													
	<u>Landfill, land clearing and inert debris or construction debris</u>													
	<u>Public convenience center/transfer station</u>													
	<u>Recycling center, processing</u>													
	<u>Recycling center, transfer</u>													
	<u>Salvage and junkyard</u>													
	<u>Waste composting</u>													
<b>Wholesale Sales</b>	<u>All uses</u>		M P	M P				M P						

**4.2. USE-SPECIFIC STANDARDS**

Use-specific standards are the requirements applied to individual use types regardless of the zoning district in which they are located or the review procedure by which they are approved. This section is intended to identify the use-specific standards for all principal uses identified in Table 4.1.1.A. and 4.1.1.B, Summary Use Table, as subject to “Additional Req.” These standards may be modified by other applicable requirements in this Ordinance.

**4.2.2. Residential Uses**

**(5) Dwellings, Multi-Family and Townhouse**

Multi-family and townhouse dwellings shall comply with the multi-family design standards in Section 5.7, the community compatibility standards in Section 5.10 (as appropriate), and the following standards:

- (a)** No accessory structure shall be less than ten feet from another structure.

- (b) No improved recreation area shall be located within required exterior setbacks or within 20 feet of any dwelling unit.
- (c) No individual building shall exceed a length of ~~250~~ 200 feet.
- (d) A townhouse building shall contain at least three but no more than six side-by-side dwelling units.

**4.3. ACCESSORY USE STANDARDS**

**4.3.1. Purpose**

This section authorizes the establishment of accessory uses that are incidental and customarily subordinate to principal uses. The purpose of this section is to allow a broad range of accessory uses, so long as such uses are located on the same site as the principal use, and so long as they comply with the standards set forth in this section in order to reduce potentially adverse impacts on surrounding lands.

**4.3.2. General Standards and Limitations**

**E. Table of Common Accessory Uses**

Table 4.3.2.E, Table of Common Accessory Uses, specifies common types of accessory use and the zoning district where each type may be permitted.

<b>TABLE 4.3.2.E: TABLE OF COMMON ACCESSORY USES</b>																	
P = Permitted by-right    Z= Zoning Compliance Permit    U = Use Permit    MP = Allowed with master plan    blank cell = Prohibited																	
ACCESSORY USE TYPE	ZONING DISTRICT															ADDITIONAL REQ. (4.3. )	
	RC	AG	SFM	SFO	SFR	SFI	MXR	GB	LB	CC	VC	LI	HI	PD-R	PD-M		PD-O
Accessory Dwelling Unit		Z	Z	Z	Z	Z	Z	Z	Z	P	P			MP	MP	MP	3.A
Aggregate Storage and Processing		Z															3.B
Amateur Ham Radio		P	P			P	P			P	P			MP	MP		3.C
Automated Teller Machine							Z	Z	Z	Z	Z	Z		MP	MP	MP	
Campground, Public	U	Z					Z	Z						MP			3.D
Cemetery (family or religious institution)		Z	Z			Z	Z	Z	Z					MP	MP		3.E
Child Care, Incidental		Z	Z	Z	Z	Z	Z	Z	Z	Z	Z			MP	MP	MP	3.F
Community Agriculture	P	P	P			P	P							MP			3.G
Dock, Pier, Boat House, or Boatlift	P	P	P	P	P	P	P	P	P	P	P	P	P	MP	MP	MP	
Drive-Through								Z	Z	Z	Z	Z		MP	MP	MP	3.H
Electronic Gaming Operation								Z									3.I
Excavation		Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	MP	MP	MP	3.J
Gasoline Sales								Z		Z	Z	Z		MP	MP	MP	3.K
Home Occupation		P	P	P	P	P	P	P	P	P	P			MP	MP	MP	3.L

**TABLE 4.3.2.E: TABLE OF COMMON ACCESSORY USES**

P = Permitted by-right    Z= Zoning Compliance Permit    U = Use Permit    MP = Allowed with master plan    blank cell = Prohibited

ACCESSORY USE TYPE	ZONING DISTRICT															ADDITIONAL REQ. (4.3.____)	
	RC	AG	SFM	SFO	SFR	SFI	MXR	GB	LB	CC	VC	LI	HI	PD-R	PD-M		PD-O
Housing for Poultry		P	P			P	P	P						MP	MP		3.M
Ice House								Z	Z	Z	Z	Z			MP	MP	3.N
Inoperable Vehicle		P	P	P	P	P	P	P									3.O
Land Application of Sludge or Septage		U				U							U				3.P
Outdoor Display/Sales								Z	Z	Z	Z	Z			MP	MP	3.Q
Outdoor Storage		Z						Z	Z	Z	Z	Z	Z		MP	MP	3.R
Parking of Boats or Watercraft		P	P	P	P	P	P	P	P					MP	MP	MP	3.S
Parking of Heavy Trucks, or Trailers		P						P				P	P		MP		3.T
Parking of Major Recreational Equipment		P	P	P		P	P	P						MP	MP	MP	3.T
Produce Stand		P						P	P	P	P			MP	MP	MP	3.U
Retail Sales from a Vehicle		P					P	P	P	P	P	P		MP	MP	MP	3.V
Solar Energy Equipment	P	P	P	P	P	P	P	P	P	P	P	P	P	MP	MP	MP	3.W
Stable (horses)		P	P			P		P						MP			3.X
Underground Storage Tank		P						P	P	P	P	P	P		MP		3.Y
Wind Energy Facility, Small		Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	Z	MP	MP	MP	3.Z

Attachment: 2- Currituck Station Draft UDO 12-3-19 (PB 19-25 Currituck County - Currituck Station)

**TABLE 4.3.2.E: TABLE OF COMMON ACCESSORY USES**

Z = Zoning Compliance Permit; U = Use Permit; MP = Allowed with Master Plan;  
blank cell = Prohibited

ACCESSORY USE TYPE	PLANNED DEVELOPMENT ZONING DISTRICT [OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]											ADDITIONAL REQUIREMENTS (4.3. )	
	PLANNED DEVELOPMENT			PD-CS ZONING DISTRICT									
	PD-R	PD-M	PD-O	CENTER STATION	CHARTER	CROSSROADS	CYPRESS	JUNCTION	MOYOCK RUN	NEWTOWN	OAK TRAIL		
Accessory Dwelling Unit	M P	M P	M P										3.A
Amateur Ham Radio		M P											3.C.
Automated Teller Machine	M P	M P	M P		M P	M P							
Campground, Public	M P												3.D.
Cemetery (family or religious institution)	M P	M P											3.E.
Child Care, Incidental	M P	M P	M P			M P	M P	M P	M P		M P		3.F.
Community Agriculture	M P											M P	3.G.
Dock, Pier, Boat House, or Boatlift	M P	M P	M P										
Drive-Through	M P	M P	M P		U	M P					U		3.H.
Excavation	M P	M P	M P										3.J.
Gasoline Sales	M P	M P	M P		U		U	U			U		3.K.
Home Occupation	M P	M P	M P			M P		M P	M P		M P	M P	3.L.
Housing for Poultry	M P	M P											3.M.
Ice House		M P	M P										3.N.
Outdoor Display/Sales		M P	M P		M P	M P							3.Q.
Outdoor Storage		M P	M P				M P						3.R.
Parking of Boats or Watercraft	M P	M P	M P										3.S.
Parking of Heavy Trucks or Trailers			M P										3.T.
Parking of Major Recreational Equipment	M P	M P	M P										3.T.
Produce Stand	M P	M P	M P										3.U.
Retail Sales from a Vehicle	M P	M P	M P										3.V.
Solar Energy Equipment	M P	M P	M P		U			U				U	3.W.
Stable (horses)	M P											M P	3.X.
Underground Storage Tank		M P											3.Y.
Wind Energy Facility, Small	M	M	M										3.Z.

Attachment: 2- Currituck Station Draft UDO 12-3-19 (PB 19-25 Currituck County - Currituck Station)

**TABLE 4.3.2.E: TABLE OF COMMON ACCESSORY USES**

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ACCESSORY USE TYPE	PLANNED DEVELOPMENT ZONING DISTRICT [OVERLAY OR SUB-DISTRICT REQUIREMENTS MAY FURTHER LIMIT USES]												
	PLANNED DEVELOPMENT			PD-CS ZONING DISTRICT									ADDITIONAL REQUIREMENTS (4.3. )
	PDR	PD-M	PD-O		CENTER STATION	CHARTER	CROSSROADS	CYPRESS	JUNCTION	MOYOCK RUN	NEWTOWN	OAK TRAIL	
	P	P	P										

**4.3.3. Specific Standards for Certain Accessory Uses**

**H. Drive-Through**

- (1) Drive-through facilities shall be located at least 100 feet from any detached single-family dwelling or single-family residential zoning district.
- (2) Outdoor speakers associated with a drive-through shall be at least 50 feet from any lot line.
- (3) Drive-through facilities shall not be located on the front façade of the building they serve.
- (4) Drive-through facilities shall be designed so as not to obstruct the movement of pedestrians along sidewalks, through areas intended for public use, or between the building entrance and customer parking spaces.
- (5) Canopies or other features installed over a drive through window shall maintain common roof lines and materials with the principal structure.
- (6) Any portion of the drive-through lane adjacent to and between an order box and pick-up window shall provide a landscaped planting area at least three-feet in width or a masonry wall at least 30 inches in height and utilizing exterior finishing materials compatible with the principal use.
- (7) In addition to streetscape and site landscaping, any portion of a drive-through lane located between the principal building and the street shall provide a landscaped planting area at least three feet in width.

**K. Gasoline Sales**

Gasoline sales may be permitted as an accessory use in accordance with the following standards:

- (1) **Location**

- (a) Gasoline pumps, canopies, and associated service areas are prohibited in any ~~established front yard setback, side corner setback, or major arterial setback~~ in the ~~CC~~, and ~~VC~~ districts.
- (b) If the gasoline sales use is located on a corner lot, the lot shall have an area of at least 30,000 square feet and a frontage of at least 200 feet on each street side. In all other cases, the lot shall have an area of at least 15,000 square feet and a lot width of at least 150 feet.

**(2) Circulation**

The gasoline sales use shall have no more than two vehicular access points. Access points shall be located at least 150 feet from each other and from any intersecting street right-of-ways, and at least 15 feet from any other lot line.

**Item 5:** That Chapter 5 is amended by deleting the following strikethrough language and adding the underlined language

# CHAPTER 5.

## DEVELOPMENT

## STANDARDS

### 5.1. OFF-STREET PARKING AND LOADING

#### 5.1.3. Off-Street Parking Standards

##### A. Parking Plan Required

The parking plan shall accurately designate the required parking spaces, access aisles, pedestrian connections, materials, dimensions, and driveways, loading area and circulation, and the relation of the off-street parking facilities to the development the facilities are designed to serve.

##### B. Minimum Number of Spaces Required

Unless otherwise expressly stated in this section or approved through an alternative parking plan, the minimum number of off-street parking spaces shall be provided in accordance with Table 5.1.3.C, Minimum Off-Street Parking Standards.

##### C. Maximum Number of Spaces Permitted

###### ~~(1)~~ Provision up to 175 Percent of Minimum

- (a) Commercial and Industrial uses of 5,000 square feet in area or larger listed in Table 5.1.3.C, Minimum Off-Street Parking Standards, shall not exceed 125 percent of the minimum number of parking spaces required in the table.
- (b) Through approval of an alternative parking plan in accordance with Section 5.1.6.A, Provision Over the Maximum Allowed, commercial and industrial uses may provide up to a maximum of 175 percent of the minimum number of parking spaces required in the table.
- (c) Provision of more than 175 percent of the minimum number of parking spaces for uses subject to the standards of this section shall require approval of a use permit in accordance with Section 2.4.6, Use Permit.

**D. Stacking Spaces**

**(1) Required Number of Stacking Spaces**

In addition to meeting the off-street parking standards in Table 5.1.3.C, Minimum Off-Street Parking Standards, uses with drive-through facilities and other auto-oriented uses where vehicles queue up to access a service shall provide the minimum number of stacking/standing spaces established in Table 5.1.3.D, Required Stacking Spaces, and Figure 5.1.3.D, Stacking Spaces.

**Figure 5.1.3.D, Stacking Spaces**

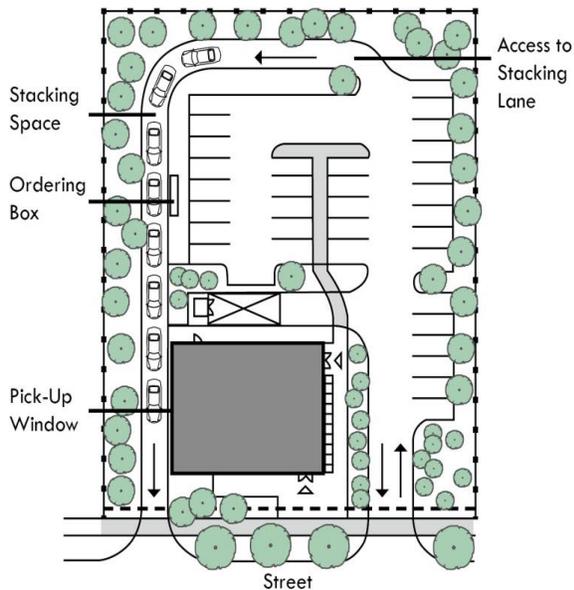


TABLE 5.1.3.D: REQUIRED STACKING SPACES		
USE OR ACTIVITY	MINIMUM NUMBER OF STACKING SPACES	MEASURED FROM
Automated teller machine (drive-up)	2	Teller window
Automobile repair and service	2 per bay	Bay entrance
Car wash	1 per bay	Bay entrance
Day care center, elementary, and middle school	2	Main building entrance
Financial institution or drug store with drive-through service	3 per lane	Agent window
Gasoline sales	1 per pump island	Each end of the outermost gas pump island
Nursing home or assisted living facility	3	Building entrance
Personal services with drive-through (e.g., laundry/dry-cleaning establishment)	3 per lane	Agent window
Restaurant, with drive-through service	3 per window/lane + 3 per order board box	

**(2) Design and Layout**

Required stacking spaces shall:

- (a) Be a minimum of 10 feet wide and 20 feet long; and,
- (b) Not impede on-site and off-site vehicular, bicycle, or pedestrian movements or movements into or out of required off-street parking spaces.

Attachment: 2- Currituck Station Draft UDO 12-3-19 (PB 19-25 Currituck County - Currituck Station)

## 5.1.4. Configuration of Vehicular Use Areas

### A. General Standards for Off-Street Parking, Stacking, and Loading Areas

#### (4) Surfacing

- (a) Except for development within PD districts, SFR district, single-family dwellings on lots of three acres in area or larger, and as provided for in Section 5.1.6.F, Alternative Materials, all off-street parking, loading, and circulation areas shall be surfaced with asphalt, concrete, brick, crushed stone, pavers, aligned concrete strips, or an equivalent material. These materials shall be maintained in a smooth, well-graded condition.
- (b) All required parking, loading, and circulation areas within PD districts shall be surfaced with asphalt, concrete, brick pavers, or an equivalent material. The use of crushed stone or similar material is not permitted.
- (c) Religious institutions overflow parking and parking for special events may take place on grass surfaces.

#### (11) Curbs and Motor Vehicle Stops

All off-street parking, loading, and circulation areas shall be designed to prevent vehicles from overhanging a sidewalk or walkway less than six feet wide, or adjacent property. Motor vehicle stops shall be prefabricated concrete or recycled plastic product manufactured specifically for this use. The use of railroad ties or other non-traditional stops shall not be permitted. Nothing shall prevent planting islands from serving as stormwater management devices (see Figure 5.1.4.11, Parking Lot Stormwater Devices).

## 5.1.6. Alternative Parking Plans

### F. On-Street Parking

An on-street alternative parking plan may be allowed for uses listed in the Planned Development-Currituck Station district (PD-CS) in accordance with the following standards:

#### (1) Center Station Sub-District

- (a) No more than 20% of the total number of spaces shall be designated as on-street parking.
- (b) On-street parking shall be located within 1,000 feet walking distance of the primary pedestrian entrance to a building.
- (c) On-street parking shall not be permitted on major arterial or collector streets.

#### (2) Charter Sub-District, Junction Sub-District, Newtown Sub-District, and Moyock Run Sub-District

- (a) No more than 10% of the total number of spaces shall be designated as on-street.

- (b) On-street parking shall be located within 1,000 feet walking distance of the primary pedestrian entrance to a building.
- (c) On-street parking shall not be permitted on major arterial or collector streets.

**(3) Cypress Sub-District**

- (a) On-street parking shall not be permitted on major arterial or collector streets.

### **5.1.7. Bicycle Parking**

~~Lots located within a Full Service area (as depicted in the Land Use Plan), used for residential development with 30 or more dwelling units and nonresidential development with 5,000 or more square feet of gross floor area shall provide individual or shared bicycle parking facilities in accordance with the following standards. Nonresidential uses of up to 30,000 square feet in size may share bicycle parking facilities in accordance with this section.~~

Planned developments, mixed-use developments, and non-residential uses of 15,000 square feet or more shall provide individual or shared bicycle parking facilities or racks in accordance with the following standards: General Standards

- ~~(1) — Bicycle parking facilities shall be conveniently located, but in no case shall such facilities be located more than 150 feet from the primary building entrance.~~
- ~~(2) — Bicycle parking spaces shall be provided at the rate of one bicycle parking space for every 30 residential dwelling units and/or every 5,000 square feet of nonresidential floor area.~~
- ~~(3) — Bicycle facilities shall include a rack or other device to enable bicycles to be secured.~~

Bicycle parking facilities or racks shall:

- (1) Be located in a visible, well-lit ground-level area;
- (2) Be within 75 feet of the primary pedestrian building entrance;
- (3) Be securely anchored to the ground;
- (4) Enable bicycles to be secured;
- (5) Not interfere with pedestrian or vehicular traffic; and,
- (6) Be consistent in material and style of the development.

#### **B. Shared Bicycle Parking**

~~Nonresidential uses of 30,000 square feet in size or less may share bicycle parking spaces provided:~~

- ~~(1) — Each use provides or is served by improved pedestrian access from the bicycle parking facility to the primary building entrance; and~~
- ~~(2) — The shared bicycle parking facility and improved pedestrian access is depicted on a site plan (see Section 2.4.7).~~

Uses on the same or opposing block faces may establish shared or consolidated bicycle parking spaces in central or mid-block locations.

**5.1.8. Loading Spaces**

**A. Number of Required Off-street Loading Berths**

The following uses shall provide on-site loading areas or berths in accordance with the standards in Table 5.1.8, Required Off-Street Loading Berths.

<b>TABLE 5.1.8: REQUIRED OFF-STREET LOADING BERTHS [1]</b>			
<b>USE OR ACTIVITY</b>	<b>MINIMUM NUMBER OF LOADING BERTHS</b>		
	<u>Gross Floor Area (GFA)</u>	<u>Short Loading</u>	<u>Long Loading</u>
Offices and personal service establishments in multi-story buildings [2]	7,500 sf or more	1	0
All other commercial or industrial use types [1]	7,500 sf – 30,000 sf	1	0
	<del>30,001 sf or more-</del> 50,000 sf	1	1
	Over 50,000	0	2
	Each additional 100,000 sf	0	1

NOTES:

[1] Residential floor area within a mixed-use development is not included within the GFA.

[2] Single-story buildings may utilize temporary loading spaces.

**B. Standards**

**(1) Minimum Dimensions**

Each loading space required by this subsection shall be at least 12 feet wide by 30 feet long (or deep) for small-sized trucks; with at least 15 feet of overhead clearance. Each off-street loading space shall have adequate, unobstructed means for the ingress and egress of vehicles.

**(a) Short Loading Areas or Berths**

Unless otherwise specified, a required short loading area or berth shall be at least 12 feet in width and 30 feet in length, exclusive of aisle and maneuvering space and shall have a vertical clearance of at least 15 feet.

**(b) Long Loading Areas or Berths**

Unless otherwise specified, a required long loading area or berth shall be at least 12 feet in width by at least 50 feet in length, exclusive of aisle and maneuvering space, and shall have a vertical clearance of at least 15 feet.

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## 5.2. LANDSCAPING STANDARDS

### 5.2.1. Purpose and Intent

The purpose of this section is to promote and protect the public health, safety, and general welfare by providing for the planting, maintenance, and preservation of trees, shrubs, and other plants within the county. The intent of this section is to promote this purpose by:

- A.** Ensuring and encouraging the planting, maintenance, restoration and survival of trees, shrubs, and other plants;
- B.** Contributing to the protection of community residents and visitors from personal injury and property damage, and the protection of the county from property damage, caused or threatened by the improper planting, maintenance or removal of trees, shrubs or other plants;
- C.** Mitigating against erosion and sedimentation;
- D.** Reducing stormwater runoff and the costs associated therewith;
- E.** Encouraging low impact development techniques like bio-retention and other best management practices for dealing with stormwater, in appropriate locations;
- F.** Preserving and protecting the water table and surface waters;
- G.** Restoring soils and land denuded as a result of construction and/or grading;
- H.** Increasing the tree canopy to provide shade and moderate the effect of urban heat islands;
- I.** Providing incentives for greater use of sustainable development practices like green roofs, use of native plant materials, and techniques to reduce the need for irrigation;
- J.** Protecting and enhancing property values and aesthetic qualities;
- K.** Providing additional improvements to air quality through the carbon dioxide uptake process provided by trees and landscaping; ~~and~~
- L.** Soften the appearance of expansive paved areas and building mass; and,
- M.** Providing visual screening, where appropriate.

### 5.2.3. General Requirements

#### **A. Landscape Plan**

- (1)** To ensure compliance with the standards of this section, a landscape plan demonstrating how landscaping will be planted on a development site shall be included as a part of any application for site plan, subdivision plan, zoning compliance permit, or temporary use permit, as appropriate.
- (2)** In the event of phased development, a landscape plan shall be required for each distinct phase of the development, and shall depict landscaping associated with the particular phase of development.

- (3) A landscape plan shall contain, at minimum, the following:
- (a) Location and identification of required and proposed streetscape, site landscaping, vehicular use area landscaping, buffers, and screening;
  - (b) Calculations of required and proposed landscaping;
  - (c) Locations, species, and caliper of existing vegetation to be retained that is to be counted towards the minimum landscaping requirement;
  - (d) Reforestation areas; and,
  - (e) Heritage tree inventory, trees identified to be removed and required mitigation plantings, and proposed tree protection zones.

#### 5.2.4. Site Landscaping

---

##### A. General

Site landscaping, for the purpose of this section, is landscaping that is not:

- (1) Required vehicular use area landscaping;
- (2) Located within a required perimeter buffer; or
- (3) Required screening.

##### B. Purpose and Intent

Site landscaping material is intended to soften the visual impact of the building base and provide for the even dispersal of trees and other plantings across a development site.

##### C. Site Landscaping Standards

Except for single-family detached dwellings, site landscaping shall be required for all development, and shall be supplied in the amounts identified in Table 5.2.4, Required Site Landscaping Plantings, and Figure 5.2.4, Site Landscaping Placement. Site landscaping shall meet the minimum size standards for new planting specified in the Administrative Manual.

##### D. Location

Required shrubs shall be planted along building facades facing streets. Required shrubs may be planted up to 15 feet from the building provided there is a sidewalk located between the planting area and the building wall. Foundation planting shall be continuous, except as needed for stairs, sidewalk connection, or building entrance.

#### 5.2.6. Perimeter Landscape Buffers

---

##### A. Purpose and Intent

Perimeter landscape buffers are intended to mitigate potential negative effects of contiguous uses in different zoning.

## B. Applicability

All development shall provide a perimeter landscape buffer to separate it from adjacent lands with a different zoning district designation, in accordance with Table 5.2.6.A, Buffer Types, and Table 5.2.6.B, Buffer Type Application.

## C. Types of Buffers

~~(1) Table 5.2.6.A, Buffer Types, describes three different buffering options in terms of their function, opacity, width, and planting requirements. Where a particular buffer type is required in Table 5.2.6.B, Buffer Type Applications, the requirement may be met using either Option 1 or Option 2. Option 3 is available for use within the CC and VC districts only. Where an option utilizing a fence is selected, the fence shall comply with the standards of Section 5.3, Fences and Walls, as appropriate.~~

When development subject to the requirements of this section is in a zoning district adjacent to a different zoning district, the buffer requirement of this section shall apply. Where a particular buffer type is required in Table 5.2.6.B., Buffer Type Applications, the requirement may be met using either Option 1 or Option 2, as appropriate. Option 3 is available for use in the CC and VC districts only. Where an option utilizing a fence is selected, the fence shall comply with the standards of Section 5.3., Fences and Walls, as appropriate.

**D. Buffer Type Application**

Table 5.2.6.B, Buffer Type Application, specifies the type of perimeter landscape buffer that new development shall provide between it and adjacent property, based on the zoning district of the development site and that of the adjacent property. The buffer type is indicated by a letter corresponding to one of the three buffer types depicted in Table 5.2.6.A, Buffer Types.

**TABLE 5.2.6.B: BUFFER TYPE APPLICATION [1]**  
 A= Type A Buffer    B = Type B Buffer    C = Type C Buffer    D = Type D Buffer  
 N/A = No Buffer Required

ZONING CLASSIFICATION OF PROPOSED SITE [2]	ZONING CLASSIFICATION OF ADJACENT DEVELOPMENT					
	RC & AG	SFM, SFO, SFR, SFI, & SINGLE-FAMILY DEVELOPMENT	MXR, GB, & LB	VC & CC	LI	HI
RC, AG	N/A	N/A	N/A	N/A	N/A	N/A
SFM, SFO, SFR, SFI	N/A	N/A	N/A	N/A	N/A	N/A
MXR, GB, LB	A	B	N/A	N/A	N/A	N/A
PD	[2]	[2]	[2]	[2]	[2]	[2]
VC, CC	B	B	N/A	N/A	N/A	N/A
LI	C	C	C	N/A	N/A	N/A
HI	D	D	D	D	C	N/A

NOTES:

[1] Letters correspond to the buffer types in Table 5.2.6.A.

[2] Development in PD districts is subject to perimeter buffer requirements in the PD master plan and additional district specific standards. In cases where development is proposed next to an existing PD district having no perimeter buffer, the proposed development shall provide a perimeter buffer that is consistent with the type of buffer required if the adjacent use was in a differing base district appropriate for the type of use.

**E. Responsibility for Buffer Installation**

**(1) Vacant Parcels**

Where a developing parcel is adjacent to a vacant parcel and a perimeter buffer is required in accordance with this section, the developing parcel shall provide a minimum of one-half of the perimeter buffer required adjacent to the vacant land.

**(2) Existing Land Uses**

- (a)** Where a perimeter buffer meeting the standards in this section has already been provided by the adjoining existing development, the proposed development shall be responsible for 50 percent of the minimum buffer width and screening required in Table 5.2.6.A, Buffer Types, and Table 5.2.6.B, Buffer Type Application, if there is a written recorded agreement documenting the buffer requirements for each property. ~~Where a developing parcel is adjacent to an existing use and a perimeter buffer is required in accordance with this section, the developing parcel shall provide the full perimeter buffer required adjacent to the existing use in accordance with Table 5.2.6.A, Buffer Types, and Table 5.2.6.B, Buffer Type Application, unless~~

~~a portion or all of a perimeter buffer that complies with the standards of this section already exists between the lots. Where part of a perimeter buffer exists, but the buffer does not fully comply with the standards of this section, the developing parcel shall be responsible for providing only the additional planting material on site necessary to meet the standards of this section.~~

- (b)** ~~The landscape plan shall include photographs and a description of existing vegetation on adjacent lands that are to be counted towards meeting the perimeter buffer requirements in this section.~~

### 5.2.10. Maintenance

#### A. General

The owner shall be responsible for the maintenance of all landscape areas not in the public right-of-way. Such areas shall be maintained in accordance with the approved Landscape Plan or Alternative Landscape Plan and shall present a healthy and orderly appearance free from refuse and debris. All ~~plant life~~ vegetation shown on an approved Landscape Plan or Alternative Landscape Plan shall be replaced if it dies, is seriously damaged, or removed. This section is not intended to prevent normal, routine maintenance.

## 5.3. FENCES AND WALLS

### 5.3.1. Purpose and Intent

The purpose and intent of this section is to regulate the location, height, and appearance of fences and walls to maintain visual harmony within neighborhoods and throughout the county, protect adjacent properties from the indiscriminate placement and unsightliness of fences and walls, and ensure the safety, security, and privacy of properties.

### 5.3.2. Applicability

- A.** The provisions of this section shall apply to all construction, substantial reconstruction, or replacement of fences or walls not required for support of a principal or accessory structure, and to any other linear barrier intended to delineate different portions of a lot.
- B.** Temporary fences for construction sites, sand fencing in beachfront areas, or tree protection fencing are exempted from these standards, but shall comply with the requirements of the State Building Code adopted by the county, and the standards of Section 4.4, Temporary Use Standards.
- C.** In the event of any inconsistency between the provisions of this section and any screening standard in Section 5.2.7, Screening, the standards in Section 5.2.7 shall control.
- D.** Fences and walls located in the Planned Development – Currituck Station district shall be subject the standards of this section and the additional standards for the individual sub-district.

## 5.4. EXTERIOR LIGHTING

### 5.4.2. Applicability

#### A. General

The provisions of this section shall apply to all development in the county unless exempted in accordance with Section 5.4.2.D, Exemptions.

#### B. Time of Review

Review for compliance with the standards of this section shall occur as part of the review of an application for a site plan (Section 2.4.7), planned development master plan (Section 2.4.5), or zoning compliance permit (Section 2.4.9), as appropriate.

#### C. Existing Development

Compliance with these standards, to the maximum extent practicable, shall also apply to redevelopment of an existing structure, building, or use when it is expanded, enlarged, or otherwise increased in intensity equivalent to or beyond 50 percent.

#### D. Exemptions

The following is exempted from the exterior lighting standards of this section:

- (1) FAA-mandated lighting associated with a utility tower or airport;
- (2) Lighting associated with navigational beacons, the United States flag, North Carolina flag, or Currituck County flag;
- (3) Holiday lighting during the months of November, December, and January, provided the lighting does not create unsafe glare on street rights-of-way;
- (4) Battery-powered emergency lighting; and
- (5) Architectural lighting of 450 lumens or less;
- (6) Temporary lighting for circuses, fairs, carnivals, theatrical, and other performances provided such lighting is discontinued upon completion of the performance; and,
- (7) Lighting for public monuments or statuary.

### 5.4.3. Lighting Plan

To ensure compliance with the standards of this section, a lighting plan demonstrating how exterior lighting will comply with the standards of this section shall be included as part of any application for site plan, planned development master plan, subdivision, zoning compliance permit, or temporary use permit, as appropriate. The lighting plan shall include:

- A. Exterior lighting fixture type, shielding, and mounting height;
- B. Exterior lighting pole height;
- C. Footcandle measurements and lumens levels;

- D. Hours of illumination; and,
- E. Certification by person preparing the lighting plan that the proposed development complies with the exterior lighting standards of this ordinance.

#### 5.4.4. Prohibited Lighting

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The following lighting is prohibited:

- A. Light fixtures that imitate an official highway or traffic control light or sign;
- B. Light fixtures in the direct line of vision with any traffic control light or sign;
- C. Light fixtures that have a flashing or intermittent pattern of illumination, except for time and temperature displays;
- D. Privately-owned light fixtures located in the public right-of-way; or
- E. Searchlights, except when used by Federal, State or local authorities.
- F. In the PD-CS district, light types of limited spectral emission, such as high/low pressure sodium or mercury vapor lights.

#### 5.4.5. Street Lighting

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- A. Private streets, public streets dedicated to the North Carolina Department of Transportation, sidewalks, and other common areas or facilities in developments may be illuminated to ensure the security of land and the safety of persons using such roads, sidewalks, and other common areas or facilities. When provided, illumination shall be in accordance with a plan designed by the utility company.
- B. All street lights shall be located inside full cut-off fixtures mounted on non-corrosive poles served by underground wiring.
- C. The light structure and light color of street lights shall be consistent throughout the subdivision.
- D. Illumination standards must be met prior to final plat approval (see Section 2.4.8) or prior to occupancy, when final plat approval is not required.

#### 5.4.6. General Standards for On-Site Exterior Lighting

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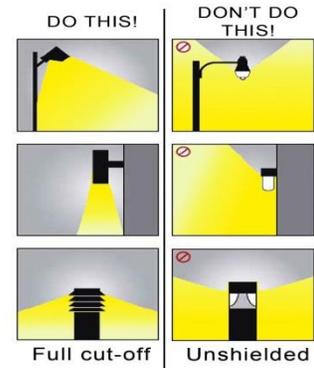
##### A. Hours of Illumination

Institutional uses, commercial uses, and industrial uses that are adjacent to existing residential development shall extinguish all exterior lighting—except lighting necessary for security or emergency purposes—by 10:00 P.M. or within one hour of closing, whichever occurs first. For the purposes of this subsection, lighting “necessary for security or emergency purposes” shall be construed to mean the minimum amount of exterior lighting necessary to illuminate possible points of entry or exit into a structure, to illuminate exterior walkways, or to illuminate outdoor storage areas. Lighting activated by motion sensor devices is strongly encouraged.

**B. Shielding with Full Cut-Off Fixtures**

Except for single-family detached and two-to-four family dwellings, all exterior luminaries, including security lighting, shall be full cut-off fixtures and directed downward, consistent with Figure 5.4.6.B, Full Cut-Off Fixtures. In no case shall lighting be directed above a horizontal plane through the lighting fixture.

**Figure 5.4.6.B, Full Cut-Off Fixtures**



**C. Maximum Height**

Except for athletic fields or performance areas, the height of outdoor lighting, whether mounted on poles, walls, or by other means, shall be no greater than 25 feet above grade comply with the standards in Table 5.4.6.C., Maximum Height for Exterior Lighting.

**TABLE 5.4.6.C: MAXIMUM HEIGHT FOR EXTERIOR LIGHTING**

TYPE OF USE [1]	MAXIMUM HEIGHT (MEASURED FROM GRADE)
Residential (street lights)	15 feet
Commercial, Institutional, Industrial, [2]	25 feet
Industrial use	25 feet

**NOTES:**

[1] Exterior lighting located within 100 feet of a Residential base zoning district shall not exceed 15 feet.

[2] Includes mixed-use development, but excludes residential uses in mixed-use, multi-story buildings.

**D. Maximum Illumination Value**

- (1) All outdoor lighting and indoor lighting visible from outside shall be designed and located so that the maximum illumination measured in footcandles at ground level at a lot line shall not exceed the standards in Table 5.4.6.D, Maximum Illumination Levels, and Figure 5.4.6.D, Maximum Illumination Value.
- (2) In no instance shall illumination levels within a site or development exceed 30 footcandles.

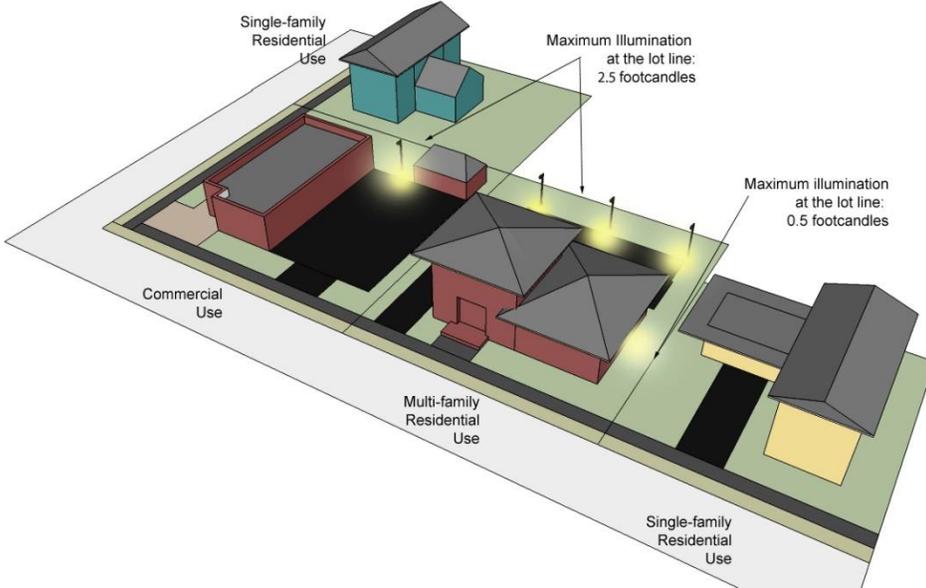
**TABLE 5.4.6.D: MAXIMUM ILLUMINATION LEVELS**

TYPE OF USE ABUTTING A LOT LINE [1]	MAXIMUM ILLUMINATION LEVEL AT LOT LINE (FOOTCANDLES)
Residential uses or vacant land zoned for residential development	0.5
Institutional use	1.0
Commercial use, or vacant land <u>zoned for commercial development</u> [2]	2.0

<b>TABLE 5.4.6.D: MAXIMUM ILLUMINATION LEVELS</b>	
TYPE OF USE ABUTTING A LOT LINE [1]	MAXIMUM ILLUMINATION LEVEL AT LOT LINE (FOOTCANDLES)
Industrial use	3.0
Parking lot	2.5

NOTES:  
 [1] See Table 4.1.1A. and Table 4.1.1.B. Summary Use Table  
 [2] Includes mixed-use development

**Figure 5.4.6.D, Maximum Illumination Value**



**E. Signage**

Lighting for signage shall be governed by the standards in Section 5.12, Signage.

**5.4.7. Design Standards for Specific Uses and Site Features**

**F. Pedestrian Lighting**

Pedestrian lighting shall comply with the following:

- (1)** Bollard lamps shall be mounted no higher than 4 feet above grade; and
- (2)** Light poles shall not exceed 15 feet above grade.

Attachment: 2- Currituck Station Draft UDO 12-3-19 (PB 19-25 Currituck County - Currituck Station)

## 5.6. COMMUNITY FORM STANDARDS

### 5.6.4. Internal Street Connectivity

#### A. Minimum Connectivity Index Score Required

All development shall achieve an internal street connectivity score in accordance with Table 5.6.4, Minimum Street Connectivity Index:

<b>TABLE 5.6.4: MINIMUM STREET CONNECTIVITY INDEX</b>	
<b>ZONING DISTRICT WHERE DEVELOPMENT IS PROPOSED</b>	<b>MINIMUM CONNECTIVITY INDEX SCORE</b>
AG, SFM, SFO, SFI, HI	1.20
MXR, GB, LB, LI, PD-M	1.40
CC, VC, PD-R, PD-O, <u>PD-CS</u>	1.60

### 5.6.7. Driveway and Access Standards

#### E. Restricted Access Streets

When sufficient frontage on a separate street is not available or access from a separate street is not practicable, the following standards shall apply to driveways on a major arterial street.

##### (1) Where Applied

The following restricted access street standards shall apply to lots fronting the following streets:

- (a) US 158;
- (b) NC 168;
- (c) NC 12;
- (d) NC 34;
- (e) NC 136;
- (f) NC 615;
- (g) SR 1222 (Tulls Creek Road); ~~and~~
- (h) SR 1131 (Poplar Branch Road); and.
- (i) SR 1227 (South Mills Road).

### 5.6.10. Sidewalks and Pedestrian Circulation

#### A. Location

- (1) Sidewalks shall be located within an existing public street right of way or an easement dedicated to the public and running parallel to the street. Pedestrian pathways and trails shall be located within open space set-asides.
- (2) Sidewalks shall be required on both sides of all streets, except:

- (a) In the SFR and SFI districts, and in subdivisions of five or fewer lots (where no sidewalks are required);
- (b) Along alleys (where no sidewalks are required);
- (c) In residential subdivisions where the average lot area is greater than one acre in size, or there are fewer than 20 lots (in these instances, pedestrian pathways or trails are required that provide an equivalent level of pedestrian circulation);
- (d) On cul-de-sacs less than 500 feet in length (where sidewalks are required only on one side of the street);
- (e) Where an existing or proposed sidewalk or pedestrian pathway paved with asphalt, concrete, or other hard-surface material located outside a street right-of-way trail can provide an equivalent level of pedestrian circulation to all lots in the subdivision; and
- (f) In cases where environmental or topographic conditions make such provision prohibitive and no practicable alternative design is available.

## B. Configuration

- (1) ~~Sidewalks shall be ADA-accessible, at least five feet wide, and may be required to match the width of a connecting sidewalk that exceeds five feet in width. Except as otherwise provided in this ordinance,~~
  - (a) Sidewalks shall be ADA-accessible, at least five feet in width, may be required to match the width of a connecting sidewalk that exceeds five feet in width; and,
  - (b) Pedestrian pathways, walkways, and trails shall be ADA-accessible and at least eight feet in width;
- (2) Sidewalks, pedestrian pathways, and trails shall be constructed of asphalt, concrete, or other hard-surface materials, consistent with an approved site plan, or with the established sidewalk patterns in the general area of the development;
- (3) Pedestrian street crossings and crosswalks shall be:
  - (a) Ten feet in width on major arterial streets and no less than six feet in width on all other streets; and,
  - (b) Well defined and raised above the adjacent street level, be a different material, or be striped as a traffic-calming measure.
- (4) Sidewalks, ~~and~~ pedestrian pathways, and trails shall connect with existing or planned sidewalks at property boundaries. In cases where the property boundary is within a drainage swale or ditch, the sidewalk connection shall terminate at the edge of the swale, ditch, or drainage easement. An easement shall be established that will grant current and future owners of the abutting properties the right to construct sidewalk connections. The new development shall connect to the sidewalk stub to form pedestrian circulation; and

- (5) New nonresidential, mixed-use, and multi-family development shall provide at least one on-site improved connection between the development and the adjacent public sidewalk system (planned or existing).

**5.7. MULTI-FAMILY DESIGN STANDARDS**

**5.7.1. Purpose and Intent**

These purpose for these multi-family design standards is to:

- A.** Promote greater compatibility between multi-family development and other allowable uses in the county; and
- B.** Establish a minimum level of quality for multi-family development.

**5.7.2. Applicability**

**A. General**

These standards apply to all new multi-family and townhouse development in the county.

**B. Time of Review**

Review for compliance with the standards of this section shall occur during review of a site plan (Section 2.4.7), planned development master plan (Section 2.4.5), or zoning compliance permit (Section 2.4.9), as appropriate.

**C. Existing Development or Redevelopment**

Redevelopment of an existing multi-family or townhouse development that exceeds 33 percent of the building’s assessed value shall require the newly redeveloped portions to comply with the standards of this section, to the maximum extent practicable.

**D. Community Compatibility Standards**

Multi-family development located adjacent to single-family detached development shall also be subject to the community compatibility standards in Section 5.10.

**5.7.3. Multi-Family Design Standards**

New multi-family and townhouse development shall comply with the following standards:

**A. Access and Circulation**

**(1) Street Network**

- (a)** On sites including new streets, an interconnected network of streets shall be provided, to the maximum extent practicable.
- (b)** The internal network of streets shall meet NCDOT standards and shall connect to adjacent existing streets to better integrate the development with its context.

**(2) Pedestrian Circulation**

Attachment: 2- Currituck Station Draft UDO 12-3-19 (PB 19-25 Currituck County - Currituck Station)

- (a) A clearly defined, visible, and identifiable pedestrian network (combination of pathways, low shrub or ground cover plantings, and trees) shall be provided between parking lots, public street sidewalks, open spaces, recreational facilities, and individual buildings.
- (b) ~~Pedestrian pathways shall be at least five feet in width.~~
- (c) The owner or an owners association shall maintain pedestrian walkways ~~outside public rights-of-way.~~
- (d) Where possible, pedestrian walkways shall be connected within parks, open spaces or common areas internal and external to the site.
- (e) Both vehicular and pedestrian access must be visible from the street or alley serving the development.
- (f) Pedestrian entrances to the site shall be accentuated through the use of landscaping, special paving, gateways, or smaller features.

### (3) Parking Area Design and Location

- (a) Except as otherwise provided in this ordinance, No more than 50 percent of the required off-street parking shall not be provided between the front of the principal building(s) and the street it fronts unless the principal building and the parking is screened from view by other development and landscaping (excluding alleys).
- (b) Vehicular access to the development shall be provided from a secondary street or an alley, when present.
- (c) All parking and vehicular access ways shall be surfaced with concrete, asphalt, or pervious pavement, subject to an approved alternative parking plan (see Section 5.1.6, Alternative Parking Plans).
- (d) Off-street parking lots of 200 or more spaces shall be organized into a series of parking bays surrounded by buildings, landscaping or accessways.

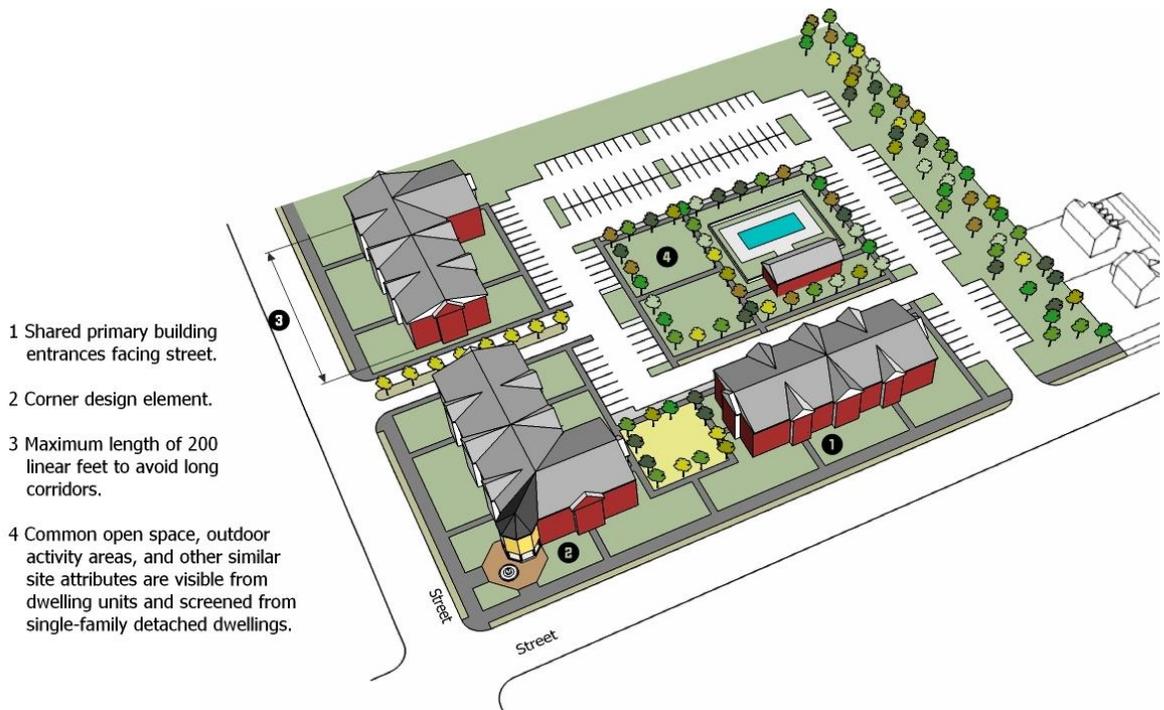
## B. Building Placement

### (1) Building Orientation and Entryways

- (a) The primary entrance of a single building development shall face the street.
- (b) Multi-building development shall create a street edge and be configured so that the primary building entrances are oriented towards external streets, when possible.
- (c) All buildings with shared entrances shall be oriented so that primary entrances face the street. In case of corner lots, the primary entrance(s) shall face the street from which the building derives its street address (see Figure 5.7.3.B.1, Multi-family Development Orientation).

- (d) Individual multi-family buildings shall be configured so that no single building exceeds 200 linear feet in any direction.
- (e) Long corridors and hidden entrance(s) to buildings shall be avoided.
- (f) Buildings that do not have a direct and visible pedestrian entrance from a public street shall, at a minimum, have windows or patios facing the street. A Type A perimeter buffer shall be provided between the side and/or rear building façade and the street.
- (g) Common open spaces and children's play areas shall be clearly visible from the dwelling units on the site.

**Figure 5.7.3.B.1, Multi-Family Building Orientation**

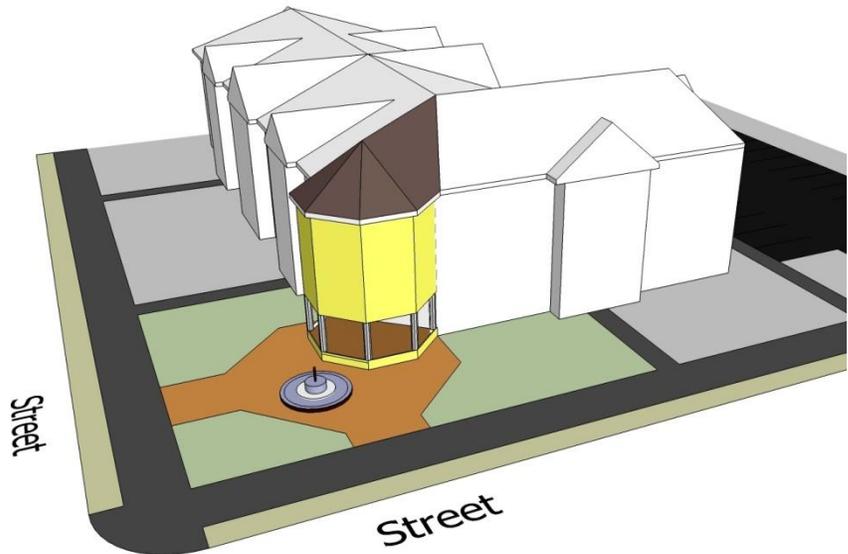


## (2) Building Features

- (a) Developments abutting public street corners shall provide one or two or more of the following elements on the building facades closest to the intersection (see Figure 5.7.3.B.2, Corner Buildings):
  - (i) Placement of primary pedestrian entry;
  - (ii) Distinctive roof form (e.g. recess, projection, tower, turret, pediment); or
  - (iii) Other architectural features (e.g. porches, canopies).
- (b) Building details, including roof forms, siding materials, windows, doors, and trim shall reflect a similar level of quality and architectural detailing on all sides facing:
  - (i) A street;

- (ii) Abutting existing single-family development; and
- (iii) Vacant land designated as a single-family district on the official zoning map.

**Figure 5.7.3.B.2, Corner Buildings**



### C. Building Design

#### (1) Maximum Height

Buildings subject to the standards of this section shall have a maximum height of two stories within 100 feet of a lot with an existing single-family detached residential dwelling, without an intervening public street. This standard shall apply regardless of any incentives, administrative adjustments, or master plans.

#### (2) Facade Articulation

- (a) Street-facing building facades shall be articulated with wall offsets at least two feet deep for every 30 feet of facade frontage.
- (b) In addition to wall offsets, front facades facing streets shall provide a minimum of three of the following articulation elements (see Figure 5.7.3.C, Front Facades):
  - (i) A covered porch;
  - (ii) One or more dormer windows or cupolas;
  - (iii) Pillars, posts or pilasters;
  - (iv) One or more bay windows with a minimum twelve-inch projection from the facade plane;
  - (v) Multiple windows with a minimum of four-inch-wide trim;
  - (vi) Raised corniced parapets over the door;
  - (vii) Eaves with a minimum of four-inch-wide trim; or

- (viii) Integral planters that incorporate landscaped areas and/or places for sitting.
- (c) Side and rear facades shall maintain the architectural design, articulation, level of detail, and materials consistent with the front façade. Side and rear facades shall maintain at least ten percent of the facade area as windows.
- (d) A covered entryway with a four foot minimum dimension shall be provided at the primary entrance.

**Figure 5.7.3.C, Front Facades and Roof Forms**



**(3) Roof Form**

- (a) Development shall incorporate sloped roofs greater than or equal to one foot of vertical rise for four feet of horizontal run (3:12), and less than or equal to one foot of vertical rise for every one foot of horizontal run (12:12), or shall incorporate a three-foot parapet with a dimensional cornice around a flat roof. Alternative roof forms or pitches may be allowed for small roof sections over porches, entryways, or similar features.
- (b) All roof vents, pipes, antennas, satellite dishes, and other roof penetrations and equipment (except chimneys) shall be located on the rear elevations or otherwise be configured to the degree practicable, to have a minimal visual impact as seen from the street.

**(4) Garage Location**

Attached garages shall be located to the side or rear of buildings (see Figure 5.7.3.D, Garage Placement) and accessed from alleys or secondary streets, to the maximum extent practicable.

**(5) Elevated Ground Floor Height**

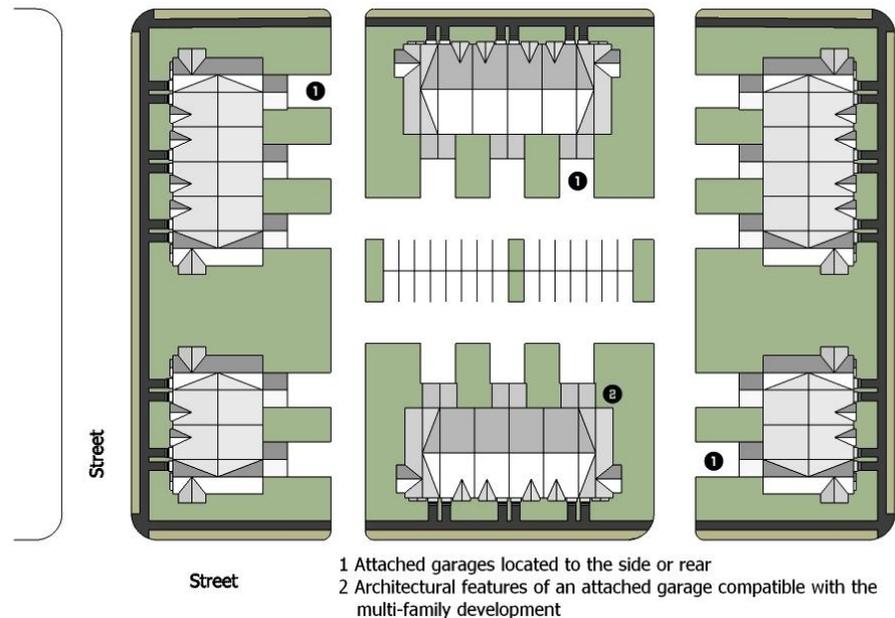
The minimum ground finished floor elevation shall be elevated two feet above established or finished grade.

**D. Site Features**

**(1) Accessory Structures**

- (a) Street-facing detached garages on corner lots shall be located to the side or rear of buildings (see Figure 5.7.3.D, Garage Placement).
- (b) Access to accessory structures (such as garages, carports, storages, trash receptacles) shall be provided from alleys or secondary streets, to the maximum extent practicable.

**Figure 5.7.3.D, Garage Placement**



- (c) Accessory buildings shall include exterior materials, colors, and roof form designed to be consistent with the principal structure.
- (d) Accessory structures shall not physically obstruct pedestrian entrances.
- (e) Centralized trash receptacles, if provided, shall be located in an enclosed area located to the rear of principal buildings.
- (f) HVAC and mechanical equipment shall be integrated into the overall building design and not visible from adjoining streets or other open space set-asides. Through-wall units and vents shall not be located along the street frontages, unless recessed within a balcony or similar feature.

## 5.8. NONRESIDENTIAL DESIGN STANDARDS

### 5.8.1. Purpose and Intent

These design standards are intended to identify the county's goals and expectations for commercial, office, and mixed-use development quality as a means of establishing higher quality development that is more compatible with residential development in the county. More specifically, the purposes of this section are to:

- A.** Encourage establishment of a strong sense of place with vibrant commercial, office, and mixed-use development in key areas of the county;
- B.** Encourage a more pedestrian-friendly environment through attention to human-scale design and site features;
- C.** Foster greater compatibility between adjacent residential and nonresidential development;
- D.** Limit the impacts of automobile-oriented development in commercial, office, and mixed-use areas; and
- E.** Enhance the appearance of development along major arterial streets.

### 5.8.2. Applicability

#### **A. General**

- (1)** Unless exempted in accordance with Section 5.8.2.D, Exemptions, the standards in this section shall apply to all new development located within the GB, LB, CC, VC, PD, and MXR zoning districts.
- (2)** Large retail development shall comply with the Nonresidential Design Standards in Section 5.8.3 as well as the Large Retail Design Standards in Section 5.8.4. In the event of conflict, the standards in Section 5.8.4, Large Retail Design Standards, shall control.

#### **B. Timing of Review**

Review of proposed development to ensure compliance with the standards of this section shall occur at time of site plan (Section 2.4.7), planned development master plan (Section 2.4.5), or zoning compliance permit (Section 2.4.9), as appropriate.

#### **C. Existing Development and Redevelopment**

Development existing prior to January 1, 2013 as well as redevelopment of buildings established before January 1, 2013 are encouraged to, but not required to, comply with these standards.

#### **D. Exemptions**

Residential, institutional, and agricultural development, development subject to the standards in Section 5.7, Multi-Family Design Standards, and development subject to the standards in Section 5.9, Shopping Center Design Standards, shall be exempted from the standards of this section.

#### **E. Community Compatibility Standards**

Commercial, industrial, mixed-use, and multi-family development located adjacent to single-family detached development shall also be subject to the community compatibility standards in Section 5.10.

### 5.8.3. Nonresidential Design Standards

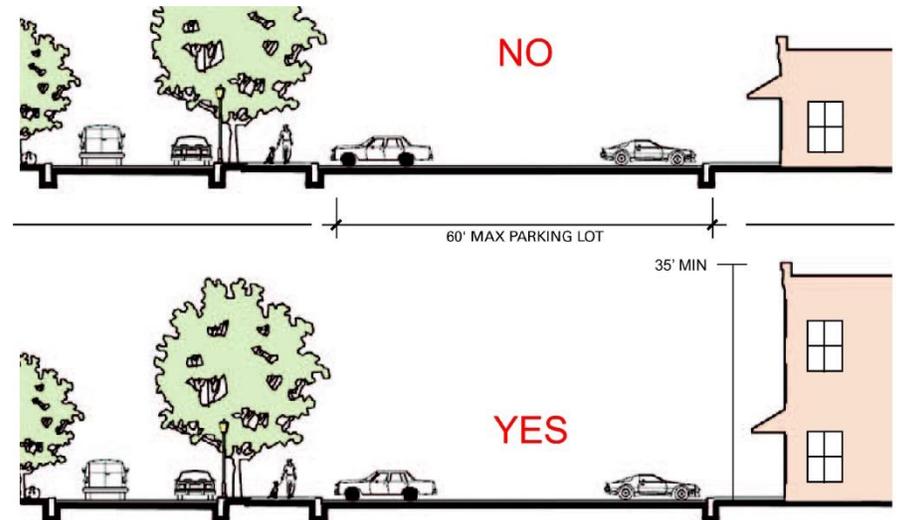
All development subject to this section shall comply with the following standards:

#### **A. Access and Circulation**

##### **(1) Off-Street Parking Location**

**(a) CC and VC Districts**

Single-story commercial, office, and mixed-use development in the CC and VC districts shall be configured to locate all required surface off-street parking to the side or rear of the building. Buildings of two or more stories may locate up to two rows of off-street parking between the primary entrance and the street it faces. In no instance shall more than two rows of off-street parking be located between the building and the street it faces (see Figure 5.8.3.A, Parking Location).

**Figure 5.8.3.A, Parking Location****(b) PD-CS District**

- (i)** In the Center Station sub-district, all off-street parking shall be configured to the side or rear of the building.
- (ii)** In the Charter, Junction, and Newtown sub-districts, no more than 25 percent of the required off-street parking shall be located between the principal building(s) and abutting streets, and shall be limited to no more than one double-loaded bay of parking.
- (iii)** Off-street parking lots with 200 or more spaces shall be organized into a series of parking bays surrounded by buildings, landscaping, or accessways designed to appear as streets.

**(c) All Other Zoning Districts**

No more than 50 percent of the required off-street parking shall be located between the building's primary façade and the street it fronts.

**B. Building Placement****~~(1) Fronting Streets~~ Building Orientation and Entryways**

- (a)** Service facilities and operations shall be oriented away from public view or screened to the extent practicable. The front façade of all buildings, as defined by the primary entrance,

shall face the primary street. Nothing shall prohibit a secondary entrance from facing a surface parking area, secondary street open space, or courtyard.

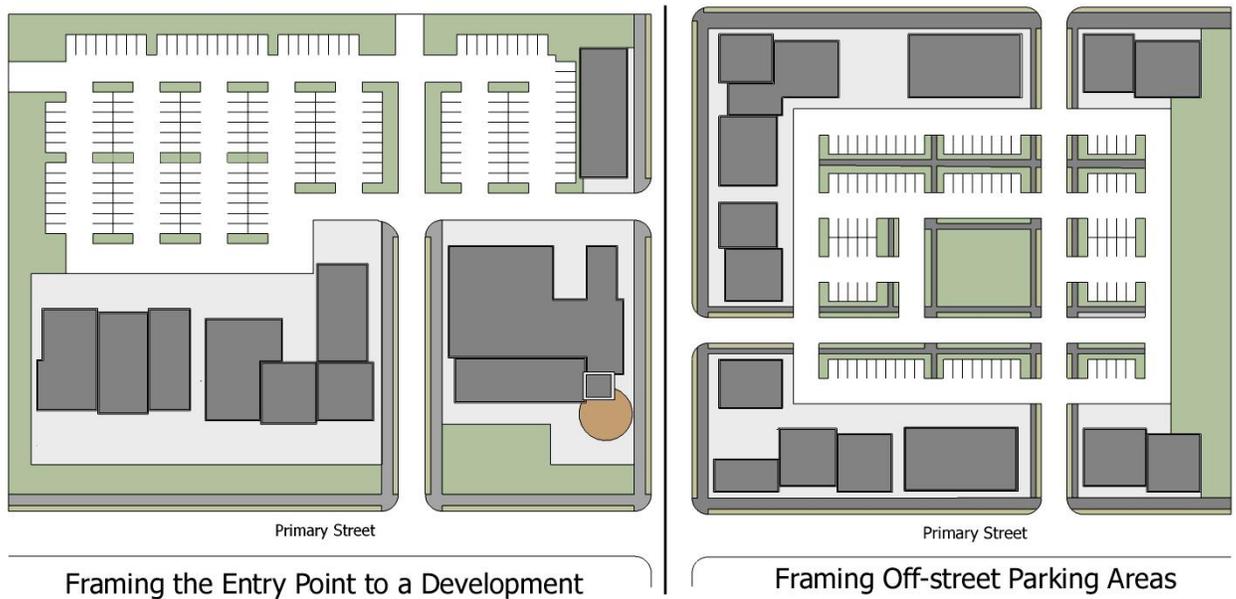
**(2) Single Building Development**

All single building development shall be oriented parallel or perpendicular to the street it fronts rather than being sited at unconventional angles.

**(3) Multi-Building Development**

**(a)** Development comprised of multiple buildings shall be configured with two or more of the following design elements (see Figure 5.8.3.B.2, Multi-Building Development):

**Figure 5.8.3.B.2, Multi-Building Development**



- (i)** Site configuration as a series of smaller “blocks” defined by buildings fronting on-site streets and internal vehicle access ways, utilizing pedestrian oriented design such as walkways, or other circulation routes and multi-modal transportation access/waiting areas when appropriate;
- (ii)** Corner buildings designed to front both sides of an adjacent street intersection or entry point to the development in an “L” configuration;
- (iii)** Buildings facing each other across a relatively narrow vehicular access area with pedestrian amenities in a "main street" character;
- (iv)** Buildings framing and enclosing at least three sides of parking areas, public spaces, or other site amenities; or,
- (v)** Buildings framing and enclosing outdoor dining or gathering spaces for pedestrians between buildings.

**(b)** The primary entrances of buildings shall be oriented towards a street along the perimeter of a development, towards streets interior to the development, or towards open space areas.

## C. Building Design

### (1) Design Features

~~Front building facades and facades facing streets shall provide a minimum of three of the following six design features. Buildings subject to these standards shall be configured so that no single façade visible from a street shall extend more than 50 feet without inclusion of three of the following features:~~ (see Figure 5.8.3.C.1, Required Building Design Features):

**Figure 5.8.3.C.1, Required Building Design Features**



- (a) The use of projections or recesses in the building façade wall with a depth of 18 inches from the primary façade plane and a minimum width of 10 feet;
- (b) ~~Facades of 60 feet in width or wider shall incorporate wall offsets of at least one foot in depth a minimum of every 40 feet. Each offset shall have a minimum width of ten feet;~~
- (c) ~~Façade color changes following the same dimensional standards as the offset standards in (a) above;~~
- (d) ~~A series of four or more pilasters having a minimum depth of eight inches, a minimum width of eight inches, and a minimum height of 80 percent of the façade's height;~~
- (e) ~~Roofline changes, coupled with correspondingly aligned wall offset facade material changes, including changes in the roof planes or changes in the height of a parapet wall (such as extending the top of pilasters above the top of the parapet wall);~~
- (f) ~~A covered front porch occupying at least 25 percent of the front façade (counted as two features); or~~
- (g) ~~Glazing of at least 30 percent of the width of street level frontage with visibly permeable windows or doorways.~~

**Figure 5.8.3.C.2, Roof Form**

**(2) Roofs**

- (a)** Structures with a flat roof shall include parapet walls with a decorative three-dimensional cornice (see Figure 5.8.3.C.2, Roof Form).
- (b)** All rooftop equipment shall be screened from view from all streets.
- (c)** Buildings in the Outer Banks shall use a pitched roof.
- (d)** A pitched roof shall have eaves that extend a minimum of 12 inches from the building face.

**(3) Fenestration**

- (a)** Buildings subject to these standards shall be configured so that building facades visible from streets include a window or functional general access doorway at least every 20 feet along the façade. False or display casements are an allowable alternative, as approved by the Planning Director.
- (b)** At least 30 percent of the first 10 feet in height of a façade facing a street shall be transparent.
- (c)** First floor windows facing a street shall remain visually permeable and shall not be obstructed by window signs.
- (d)** Ventilation grates or emergency exit doors located at the first floor level oriented toward a street shall be decorative.

**(4) Outbuildings**

Outbuildings located in front of other buildings within the same development shall include a consistent level of architectural detail on all four sides of the building as well as exterior materials and colors that are compatible with the primary building in the development.

**(5) Prohibited Materials**

Metal siding shall not be used on front building facades and facades facing visible from streets. Overly bright, neon, or “day-glow” colors shall not be used as the primary exterior building color.

**D. Site Features****(1) Loading, Storage, and Service Areas**

- (a)** Loading, service, and equipment areas shall be located in a manner that minimizes their visibility from off-site areas, to the maximum extent practicable.
- (b)** Loading, service, and equipment areas that are associated with an outparcel building shall be screened through the use of

structural elements and similar materials attached to and integrated with the building.

## 5.14 CURRITUCK STATION DEVELOPMENT AND COMMUNITY FORM STANDARDS

### 5.14.1. Purpose and Intent

The purpose of this section is to create a form and design for development within the Planned Development-Currituck Station (PD-CS) district that will result in greater predictability in an area of the county that is intended for more intense form of development. The intent of these standards is to:

- (1)** Create a well-planned, desirable development to shop, dine, recreate, and live;
- (2)** Provide effective transitions between different uses;
- (3)** Establish a strong sense of place;
- (4)** Establish and maintain vibrant pedestrian-oriented areas where different uses can operate in close proximity to each other; and,
- (5)** Provide an appropriate building and architectural scale through high quality design.

### 5.14.2. Applicability

#### A. General

Unless exempted in accordance with Section 5.14.2.E., Exemptions, the standards of this section shall apply to all development located in the PD-CS district.

#### B. Time of Review

Review of proposed development to ensure compliance with the standards of this section shall occur at the time of planned development master plan (Section 2.4.5.), site plan (Section 2.4.7.), subdivision (Section 2.4.8), or zoning compliance permit (Section 2.4.9.), as appropriate.

#### C. Existing Development or Redevelopment

Compliance with these standards shall also apply to redevelopment of an existing building, structure, or use when it is expanded, enlarged or otherwise increased in intensity in amount equivalent to or beyond:

- (1)** 15% or 1,000 square feet of building area, whichever is the lesser amount; or,
- (2)** 15% or 10 parking spaces, whichever is the lesser amount.

#### D. Conflict

In the event of conflict between these development and community form standards and other standards in this Ordinance, these standards shall control.

### 5.14.3. General Design Standards

#### A. Currituck Station Pattern Book

The *Currituck Station Pattern Book* supplements the standards of this ordinance and provides additional guidance specific to developing properties

within the Planned Development Currituck Station district (PD-CS). The pattern book establishes and reinforces the desired character and historic architectural patterns of Currituck County.

**B. Currituck Station Master Plan**

All development shall be consistent with the Currituck Station master plan.

**C. Compliance with Other Regulations**

In addition to the standards in this section, all development in the PD-CS district shall be subject to Section 5.8 Nonresidential Design Standards, Section 5.9 Shopping Center Design Standards, Section 5.10 Community Compatibility Standards, all applicable zoning district requirements, use-specific standards, and other applicable development standards of this Ordinance.

**5.14.4. Street and Connectivity Standards**

**A. Blocks**

**(1) General**

- (a)** Block form of development utilizing an interconnected street grid shall apply to all development in the Center Station, Charter, and Cypress sub-districts.
- (b)** Blocks shall be laid out to provide a functional street pattern, connectivity, and circulation in accordance with the standards in this section.
- (c)** Block length shall be measured using the mid-point of the intersecting streets.
- (d)** The maximum block length may be extended by 50% where the block includes a pedestrian passage or mid-block alley that connects two streets on each block face to accommodate off-street parking and service functions.
- (e)** Deviations to the block standards of this section may be considered as part of the planned development master plan (Section 2.4.5, Planned Developments) when roadways, pre-existing development, tree preservation areas, environmental or topographical limitations, utilities, open space set-aside, stormwater facilities or other impractical situation would result from application of a complete block.

**(2) Center Station and Charter Sub-District**

- (a)** Blocks in the Center Station and Charter sub-district shall be at least 200 feet in length and no more than 800 feet in length except as necessary to secure efficient use of land or desired features of the street pattern, or to reflect the size and configuration of the site.

**(3) Cypress Sub-District**

- (a)** Blocks in the Cypress sub-district shall not exceed 1,000 feet in length.

- (b) Blocks shall have sufficient width to provide for two tiers of residential lots, except when single tier lots are required to accommodate single loaded streets across from a public park, waterway, or open space to allow for unusual topographical conditions or when adjacent to outer perimeter of the development.

**B. Dead-End Streets and Cul-de-Sacs**

- (1) New dead-end streets and cul-de-sacs are prohibited, except for temporary cul-de-sacs, development in the Cypress sub-district, and when topography or natural features make connections infeasible, as determined by the Planning Director and County Engineer.
- (2) Development in the Cypress sub-district shall be limited to no more than two cul-de-sacs for the development.
- (3) New dead-end streets and cul-de-sacs allowed by the Planning Director and County Engineer shall be no longer than 300 feet and shall be provided with a cul-de-sac.

**5.14.5. Non-Residential and Mixed Uses**

**A. Access and Circulation**

**(1) Sidewalks, Pedestrian Access and Connectivity**

- (a) Unless otherwise specified, all sidewalks and pedestrian walkways shall be ADA-accessible, at least six feet in width, and match the width of a sidewalk extension that exceeds six feet in width.
- (b) Sidewalks shall be provided on both sides of the streets and comply with the following:
- (i) **Center Station and Charter Sub-Districts**
- (A) Primary sidewalks shall be 15 feet in width and located adjacent to both sides of the roadway.
- (B) Secondary sidewalks shall be 8 feet in width and located adjacent to both sides of the roadway.
- (c) Sidewalks shall be constructed of asphalt, concrete, or other hard-surface materials, consistent with the approved master plan or with the established sidewalk patterns in the general area of the development.
- (d) Pedestrian street crossings and crosswalks shall be:
- (i) Ten feet in width on major arterial streets and no less than six feet in width on all other streets; and,
- (ii) Raised above the adjacent street level, constructed of a different material, or striped as a traffic-calming measure.
- (e) Where rear parking is provided, buildings greater than 150 feet in width shall provide a lighted pedestrian passageway between the building frontage and rear parking facilities as follows:

- (i) One passageway shall be provided for every 150 feet in building width; and,
- (ii) Each passageway shall be a minimum 10 feet in width and shall be in the form of a well-landscaped courtyard, pedestrian arcade or integrated pathway into a building atrium.
- (f) All internal pedestrian walkways shall be distinguished from driving surfaces through the use of durable, low-maintenance surface materials such as pavers, bricks, or scored/stamped concrete or asphalt to enhance pedestrian safety as well as the attractiveness of the walkways.
- (g) Designated pedestrian access shall be provided between parking areas and major entrances of a building or use which is required to provide 50 or more parking spaces, in accordance with Section 5.1, Off-Street Parking and Loading, of this Ordinance.
- (h) Designated pedestrian access shall be provided between the principal building entrance and the sidewalk on the closest public right of way.

## **B. Building Orientation and Configuration**

### **(1) Single-Building Development**

All single-building developments shall:

- (a) Be oriented parallel or perpendicular to the street it fronts rather than being sited at unconventional angles.
- (b) Not exceed 150 feet in length along the public or private roadway frontage; including drive aisle. Deviations to the building length may be considered by the Board of Commissioners when:
  - (i) The building exceeds two stories; and,
  - (ii) The building is configured to appear as individual volumes containing 15,000 or fewer square feet through the use of offsets and lateral connections like breezeways.

### **(2) Multi-Building Development**

Development composed of multiple buildings shall be configured to:

- (a) Break up the site into a series of smaller blocks defined by buildings fronting on-site streets and internal vehicle accessways utilizing pedestrian oriented design;
- (b) Frame the corner of an adjacent street intersection or entry point to the development in an "L" configuration;
- (c) Frame and enclose a "Main Street" pedestrian or vehicular access corridor with the development site; and

- (d) Frame and enclose at least three sides of the off-street parking area.

### C. **Building Design and Materials**

#### (1) **Prohibited Building Materials**

- (a) Aluminum siding, vinyl siding, metal siding, or other metal cladding is prohibited on any façade visible from a street right of way. Nothing shall limit the use of high quality, decorative metal (e.g. brass, copper, steel) as a building accent material.
- (b) Overly bright, neon or day-glow colors shall not be used as the primary exterior building colors.

#### (2) **Building Entrance**

- (a) The primary building entrance shall face the street from which the building is addressed. No upper story entrance shall be visible from an adjacent public street right-of-way.
- (b) All buildings with 100 feet or more frontage on a street shall have at least one pedestrian entrance onto a street for every 100 feet of frontage on the street.
- (c) Where two major arterial streets intersect pedestrian entrances shall be provided on both streets. A corner entrance shall be permitted as a design alternative. Additional entrances are encouraged facing streets, parking lots, plazas, and adjacent buildings.
- (d) The primary building entrance shall be designed with a protruding front gable, pilaster, columns, stoop, or other projection or recession that clearly identifies the entrance and shall incorporate at least two of the following elements:
- (i) Canopies or porticos above the entrance;
  - (ii) Roof overhangs above the entrance;
  - (iii) Arcades that are physically integrated with the entrance;
  - (iv) Raised corniced parapets above the entrance;
  - (v) Gable roof forms or arches above the entrance;
  - (vi) Outdoor patios or plazas adjacent to the entrance; or,
  - (vii) Integrated planters or wing walls that incorporate landscaped areas or seating areas.

#### (3) **Building Façade and Massing**

- (a) The front façade of all buildings, as defined by the primary entrance, shall be oriented on and front a street (when present), courtyard, or plaza.

- (b)** The façade of each building shall be designed with consistent architectural style, detail, and trim features. Facades should be designed to break up large elements of mass and scale.
- (c)** Buildings on corner lots shall incorporate additional height, massing, distinctive architectural treatments, or other distinguishing features to emphasize their prominent location.
- (d)** Side building walls that do not face a street and exceed 50 feet in length shall have façade-articulating elements such as columns and/or changes in plane, texture, or masonry pattern of not less than one foot wide and extending horizontal along the building façade.
- (e)** Blank, windowless walls facing sidewalks, streets, and other public places are prohibited.
- (f)** Outbuildings shall include a consistent exterior material and color, façade articulation and architectural detail on the building sides as the primary building in the development.
- (g)** Street facing facades greater than 50 feet length shall reduce the perceived massing and scale of the building by incorporating at least three of the following elements, spaced no more than 50 feet apart:
  - (i)** Differences in roof form and parapet heights of at least one foot in height;
  - (ii)** Pronounced recesses and/or projections in the wall plane of at least one foot in depth and eight feet in length;
  - (iii)** Distinct changes in texture and/or color of wall surfaces;
  - (iv)** Pilasters that are at least eight inches deep and at least eight inches wide, that have a height of at least 80 percent of the facades height;
  - (v)** Architectural covered features over public entrances of either fabric or hard treatments taking the form of a projected awning or hard canopy with a width of at least that of the entrance and projecting a minimum of three feet from the building entrance;
  - (vi)** Second floor galleries or balconies;
  - (vii)** Raised cornices; or,
  - (viii)** Projected or recessed entries;
- (h)** **Currituck Vernacular**  
Buildings in the PD-CS district are encouraged to utilize architectural patterns and features in the building design that are consistent with the Currituck vernacular as provided in the *Currituck Station Pattern Book*. Simple design elements typically known as the Currituck vernacular, include but are not limited to:

- (i) Deep porches;
- (ii) Pitched roofs with overhangs;
- (iii) Shutters (fixed, operable, Bermuda);
- (iv) Roof dormers;
- (v) Chimneys;
- (vi) Elevated buildings; and,
- (vii) Gabled masses that break up long facades

**(4) Base, Middle, and Top**

- (a) Buildings of three or more stories shall include a clearly recognizable base, middle, and top of the front façade configured in accordance with the following standards:
  - (i) The building base shall incorporate one or more of the following that distinguishes the building base from the remainder of the façade:
    - (A) Thicker walls, ledges, and/or sills; or,
    - (B) Change in materials
  - (ii) The building top shall include one or more of the following features that distinguish the building top from the remainder of the façade:
    - (A) Three-dimensional cornice treatments;
    - (B) Sloping roofs with overhangs and brackets;
    - (C) Stepped parapets; or,
    - (D) Aligned openings and articulations.

**(5) Roof Form**

- (a) Roof pitches less than 3:12 shall require a parapet wall on all sides visible from the street. Parapet walls shall fully screen all roof-top mechanical equipment from the street. Parapet walls shall have decorative cornices or caps.
- (b) Any principal building greater than 15,000 square feet in area with a sloped roof, shall include two or more different sloping roof planes, each with a minimum 3:12 pitch.
- (c) Parapet walls shall extend at least three feet above the roof line and have three dimensional cornice treatments that project at least eight inches outward from the parapet façade plane.
- (d) A pitched roof shall have eaves that extend a minimum of 12 inches from the building face.
- (e) Alternate roof forms or pitches are allowed for small roof sections over porches, entryways, or similar features.

**D. Outparcel Development**

- (1) Outparcels and their buildings shall be configured and located to define street edges, development entry points, and spaces for gathering or seating between buildings to the maximum extent practicable.
- (2) Spaces between buildings on outparcels shall be configured with small scale pedestrian amenities such as seating areas, gathering spaces, and pedestrian connections.

**E. Site Features****(1) Loading, and Refuse Collection**

- (a) Loading, service, and equipment areas shall be located in a manner that minimizes their visibility from off-site views.
- (b) Outdoor storage areas shall be fully screened from adjacent streets and detached single-family dwellings.
- (c) Centralized trash receptacles shall be located in an enclosed area located to the rear of the principal structure(s).

**(2) Equipment Areas****(a) Roof Mounted Equipment**

- (i) Roof-mounted equipment shall be screened from line of sight of adjacent streets and sidewalks by a parapet wall, freestanding screen wall, or similar architectural feature. The height of the screening shall equal or exceed that of the equipment being screened.
- (ii) The screening shall incorporate materials, colors, and design that are visually consistent with the building' architectural design.

**(b) Ground and Wall Mounted Equipment**

- (i) Excluding alleys, ground and wall mounted equipment shall not be located on any surface that directly faces a public right of way.
- (ii) Ground mounted or wall mounted equipment located on any surface that is visible from public rights of way or sidewalks must be fully screened by landscaping or an opaque screen compatible with the principal building in terms of texture, quality, material, and color.
- (iii) The screening shall incorporate at least one of the primary materials of the nearest wall of the primary structure on the lot.

**(3) Drive-Through Service Facilities**

- (a) In addition to the general standards in Section 4.3.3.H., Drive-Through, drive-through service facilities located in the PD-CS district shall meet the requirements of this section.
- (b) Pick-up windows and order boxes shall not be located on the front façade of the building they serve.
- (c) A by-pass lane with a minimum width of 12 feet shall be provided to ensure the free flow of traffic throughout the site.
- (d) Any portion of the drive-through lane adjacent to and between an order box and a pick-up window, shall provide a landscaped planting area at least three feet in width or a masonry wall at least 30 inches in height and utilize exterior finish materials compatible with the principal use shall be provided along the outside perimeter of the lane.
- (e) Any portion of the drive-through lane that is located between the principal building and the street shall provide a landscaped planting area at least three feet in width or a masonry wall at least 30 inches in height and utilize exterior finish materials compatible with the principal use shall be provided along the outside perimeter of the lane.
- (f) Any drive-through facility located along a major arterial shall install a masonry screen wall at least 30 inches in height and utilize exterior finish materials compatible with the principal use shall be provided at the setback and along the major arterial road.

**(4) Screen Walls**

- (a) In the Center Station and Charter sub-districts, a screen wall shall be required when the principal building does not meet the building setback requirements (build-to-zone) of Section 3.7.6.
- (b) Screen walls shall be:
  - (i) Installed at the required setback;
  - (ii) Designed to imitate the architecture of the building;
  - (iii) A minimum three feet and a maximum of five feet in height. Walls greater than three feet in height above grade shall not exceed 50% opacity;
  - (iv) Screen walls shall be constructed of wrought iron, brick, masonry, stone, or other decorative materials consistent with the architecture of the building.



**Item 6:** That Chapter 6 is amended by deleting the following strikethrough language and adding the underlined language

# CHAPTER 6.

## SUBDIVISION AND INFRASTRUCTURE STANDARDS

### 6.2. REQUIRED INFRASTRUCTURE

Unless exempted, all development in the county shall comply with the standards in this section.

#### 6.2.1. Street Standards

##### (9) Deceleration Lanes

Developments with new streets shall:

- (a) Install a deceleration lane in accordance with NCDOT standards if the subdivision is for nonresidential development or includes 40 or more residential lots and includes access onto major arterial streets (US 158, NC 168, NC 34, NC 136, NC 615, and NC 12).
- (b) Install left turn and deceleration lanes in accordance with NCDOT standards if the subdivision includes 40 or more residential lots and includes access onto Tulls Creek Road (SR 1222), ~~or~~ Poplar Branch Road (SR 1131) and South Mills Road (SR 1227).
- (c) Provide at least one deceleration lane per street front in accordance with NCDOT standards if located along a major arterial and the use is capable of generating more than 60 trips per peak hour, as estimated in the *ITE Trip Generation Manual*.

##### (10) Dedication and Maintenance

Be designated for dedication to NCDOT or for maintenance by an established homeowners or property owners association.

#### D. Minimum Street Width

All streets in a subdivision subject to these standards shall comply with the minimum street width standards in Table 6.2.1.D, Minimum Street Width Standards.

**TABLE 6.2.1.D: MINIMUM STREET WIDTH STANDARDS**

Subdivision Type	Minimum Right of Way Width (feet)	Local Street		Collector Street		NCDOT Design Standards Applicable?	NCDOT Construction Standards Applicable?
		Minimum Pavement Width (feet)	Minimum Shoulder Width (feet)	Minimum Pavement Width (feet)	Minimum Shoulder Width (feet)		
Family Subdivision	20	16	2	N/A	N/A	No	No
Residential Subdivision	See NCDOT <i>Subdivision Roads Minimum Construction Standards</i> Manual					Yes	Yes
Nonresidential Subdivision						Yes	Yes
Conservation Subdivision	30	20 [1]	N/A	N/A	N/A	No	Yes
Planned Unit and Planned Development [2]	30	20 [1]	N/A	N/A	N/A	No	Yes

NOTES:

[1] See Section 6.2.1.G for one-way street pavement width requirements

[2] Streets in Planned Developments shall be installed in accordance with the approved master plan and the requirements of this section.

**Item 7:** That Chapter 10 is amended by deleting the following strikethrough language and adding the underlined language:

# CHAPTER 10.

## DEFINITIONS AND MEASUREMENT

### 10.4. USE CLASSIFICATIONS, USE CATEGORIES, AND USE TYPES

#### 10.4.1. General

##### A. Purpose

This section is intended to provide a systematic framework for identifying, describing, categorizing, and consolidating or distinguishing land uses in a way that makes it easier to determine how a particular land use activity, or combination of activities, is to be considered in applying the use table and other provisions in this Ordinance. This section is also intended to provide support in identifying instances where a new or unanticipated land use not identified in the use table is of such a nature, function, or duration that the impact of allowing it in a particular zoning district is so similar to that of a use type already identified in the use table as allowed in the zoning district that allowing the new or unanticipated land use should be interpreted as being consistent with the intent of the zoning district and the use regulations.

##### B. Structure of this Section

###### (1) General

This section identifies each of the five use classifications in Table 4.1.1.A. and Table 4.1.1.B. Summary Use Table, and includes a section under each use classification identifying each use category. There are “characteristics” and “examples” subsections under each use category (individual use types are defined in Chapter 10: Definitions).

### 10.5. DEFINITIONS

#### ACTIVE RECREATION USES

Uses or structures intended for specific active recreational uses such as play grounds, ball fields, tennis courts and other similar uses typically located in open space set-aside areas or parks.

## PLANNED DEVELOPMENT

When used to describe a type of development, a tract of land that is planned and developed as an integral unit in accordance with a master plan and flexible development standards that illustrate and address land uses, circulation, utilities, parking, setbacks, housing densities, land coverage, landscaping and buffers, open space, and similar features of the project.

When used as a process, terms describe type of zoning district change procedure reviewed and approved or denied by the Board of Commissioners in accordance with Section 2.4.5, Planned Development.

## PORCH

A covered projection (can be glazed or screened) from the main wall of a building, usually attached to or part of and with direct access to a building entrance, with a separate roof, that is not used for livable space.

## SIGN, INSTITUTIONAL

A freestanding sign located on property that contains a use classified as an Institutional Use in Table 4.1.1.A and Table 4.1.1.B.

## STEPBACK

A building setback of a specified distance that occurs at a prescribed number of stories from the property line.

## VERNACULAR ARCHITECTURE

A style of architecture characterized by common building techniques based on the forms and materials of a particular period, region, or group of people.

### Item 8: Staff suggested Statement of Consistency and Reasonableness:

The requested zoning text amendment is consistent with the 2006 Land Use Plan because:

- POLICY AG3: County actions concerning infrastructure (e.g. schools, parks, and utilities) and regulations shall serve to direct new development first to targeted growth areas near existing settlements identified as Full Service Areas on the Future Land Use Map, rather than leapfrogging to locations in the midst of farmland and greenspace identified as Rural and Conservation areas on the Future Land Use Map.
- Policy HN3: Currituck County shall especially encourage two forms of residential development, each with the objective of avoiding traditional urban sprawl. One of the forms of development is compact, mixed use developments or developments near a mixture of uses that promote a return to balanced, self-supporting community centers generally served by centralized water and sewer. The types of development are contemplated for the Full Service Areas identified on the Future Land Use Map.
- Policy CD8: Mixed-use developments, properly planned from the outset, which allow for a compatible mixture of residential and non-residential uses with a pedestrian scale and design, are encouraged. Similarly, businesses may be located adjoining and therefore convenient to an existing residential area, when such businesses can be shown to satisfy design consideration similar to a newly planned, pedestrian-scaled, mixed use development.
- Policy CW1: Currituck County may elect to amend or incorporate adopted small area plans into the Land Use Plan as needed. This includes consideration of citizen initiated amendments or county lead planning efforts that recognize changing demographic, economic, or environmental conditions.

The requested zoning text amendment is consistent with the Moyock Small Area Plan (MSAP) because:

- The MSAP supplements the LUP to more specifically address the needs and issues of the study area and establishes a new focus for growth and development.
- The Moyock Small Area Plan, adopted in 2014, identified this area as an employment activity area.
- Policy CC1: Encourages and fosters development that is compatible with rural atmosphere, transitional areas, and a small town, main street feel consistent with the vision, policies and future land use map of the plan.
- Policy CC2: Encourages non-residential and mixed use development that incorporates building and site design to enhance community appearance, promote human scale, and create a unique sense of place. This may include common themed building materials, forms, and site amenities.
- Policy ST 1: Promote the establishment of an area dedicated to community serving businesses that foster a small town, main street feel.
- Policy BI 2: Encourage well planned mixed use developments to include a range of intensities and diverse housing types which capitalize on seasonal traffic volumes and provide increased opportunities for local residents and businesses. Large scale mixed use projects should be carefully located in areas supported by the future land use map and adequately served by infrastructure and county services.
- Action FLU 2A: Explore establishment of a Community Center district, and associated sub-districts, that will implement the vision and policies of the plan by creating development standards specific to Moyock study area.
- Action CC 2A: Develop regulations and incentives for non-residential and mixed use development that establish design standards specific to each activity center in this plan.
- Action CC 3B: Amend the UDO to create regulations that enhance public investment into entryways. This includes appropriate land uses, overall site design, landscaping, signage, and screening/location of outdoor storage.
- Action BI 3B: Develop regulations or incentives that require large scale residential development that utilizes centralized sewer to include a supporting non-residential component and interconnection to existing businesses.

The requested zoning text amendment is consistent with the Moyock Mega Site Master Plan and Figure ES-1.

The request is reasonable and in the public interest because:

- The Moyock Small Area Plan adopted in 2014 identified this area as an employment activity area.
- The 2016 Feasibility Study served as the guide for potential land use demands.
- Addresses a demonstrated community need because the master plan development process was designed to produce a market driven plan responsive to projected demand for a mix of land uses specific to the local market and formed by regional influences.
- Addresses a demonstrated community need by implementing the MSAP and Moyock Mega Site Master Plan, a long-term vision for a mixed use development for approximately 3,500 acres of land that is strategically positioned to serve as a connective center between Virginia and North Carolina, resulting in logical and orderly development patterns.

**Item 9:** The provisions of this Ordinance are severable and if any of its provisions or any sentence, clause, or paragraph or the application thereof to any person or circumstance shall be held unconstitutional or violative of the Laws of the State of North Carolina by any court of competent

jurisdiction, the decision of such court shall not affect or impair any of the remaining provisions which can be given effect without the invalid provision or application.

**Item 10:** This ordinance amendment shall be in effect from and after the \_\_\_\_\_ day of \_\_\_\_\_, 2019.

\_\_\_\_\_  
Board of Commissioners' Chairman  
Attest:

\_\_\_\_\_  
Leeann Walton  
Clerk to the Board

DATE ADOPTED: \_\_\_\_\_  
MOTION TO ADOPT BY COMMISSIONER: \_\_\_\_\_  
SECONDED BY COMMISSIONER: \_\_\_\_\_  
VOTE: \_\_\_\_\_AYES\_\_\_\_\_NAYS  
.....

PLANNING BOARD DATE: 1/14/2020  
PLANNING BOARD RECOMMENDATION: Approved  
VOTE: 6 AYES 0 NAYS  
ADVERTISEMENT DATE OF PUBLIC HEARING: 1/22/2020 & 1/29/2020  
BOARD OF COMMISSIONERS PUBLIC HEARING: 2/3/2020  
BOARD OF COMMISSIONERS ACTION: \_\_\_\_\_  
POSTED IN UNIFIED DEVELOPMENT ORDINANCE: \_\_\_\_\_  
AMENDMENT NUMBER: \_\_\_\_\_

Attachment: 2- Currituck Station Draft UDO 12-3-19 (PB 19-25 Currituck County - Currituck Station)

# CURRITUCK STATION PATTERN BOOK

2020



CURRITUCK STATION



11-04-19 DRAFT

# CURRITUCK STATION PATTERN BOOK

2019

The Currituck County Board of Commissioners:

*Bob White, Chairman*  
*Mike H. Payment, Vice Chairman*  
*Kevin E. McCord*  
*Mary "Kitty" Etheridge*  
*Selina Jarvis*  
*Paul M. Beaumont*  
*J. Owen Etheridge*

The Currituck County Department of Planning and Community Development

The Currituck County Department of Economic Development

Work Program Architects

Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)



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# Section 1

## Introduction to the Pattern Book

### Purpose of the Pattern Book

The Currituck Station District is a 3,000 acre mixed-use district established to provide high quality economic development supported by a pedestrian-friendly environment.

The Currituck Station District Pattern Book is designed to supplement the Currituck County Unified Development Ordinance. The Unified Development Ordinance provides general criteria for the placing of buildings on sites, and their setback, height, and parking requirements. The Pattern Book references these standards and then provides additional guidance specific to developing properties which will establish and reinforce the desired character of the Currituck Station District. This guidance primarily relates to the characteristics of the architecture, open space, and public spaces within the District.

The character of the Currituck Station District has been developed to reflect the architectural patterns that formed the foundation of Currituck County.



Use of the Pattern Book should result in a place that is comfortable for all residents & visitors for generations to come.



Aerial view of the Currituck Station site today  
Satellite Imagery © 2018 Google

Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

# How to use the Pattern Book

Follow the steps below when developing your project in Currituck Station. Keep in mind that this book is a description of typical patterns of design, it should not be used as a “how-to” manual for developing architecture. It is intended to provide guidance and ideas to help each new building complement the rest of the development.

## Step 1: Find your Sub-District

Find the section of the Pattern Book that relates to the sub-district in which you are building. See the map on page 6 to determine the appropriate sub-district.

## Step 2: Find your building use

Each sub-district section is broken down into sub-sections describing the different building uses allowed in that sub-district. Find the one that relates to the type of building that you are designing.

## Step 3: Choose a building size and orientation

The pattern book addresses buildings based on their size and orientation to the street. The size and orientation of the building establish a basic form, called the building mass.

## Step 4: Understand the details

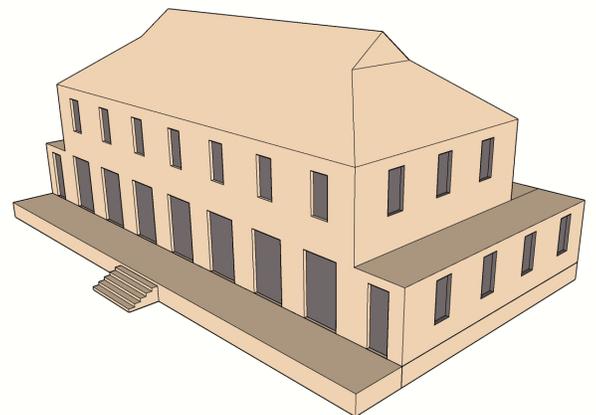
Details like roof shape, window placement, shutters and awnings, and railings and columns are what give the building an identity. These are covered in the sub-section on details.

## Step 5: Develop open space and public amenities

A mixed-use development is most successful when there is active public life on the street. Comfortable, functional, and beautiful open spaces encourage people to gather or observe activity. Public amenities encourage people to explore and provide for various needs such as bike storage and hydration.

## Step 6: Develop the Public Right-of-Way

Pedestrian paths and appropriately sized and configured streets are critical to the walkable nature of the Currituck Station District. Use the appropriate street sections to lay out the development.



## Section 2

# The architectural patterns of Currituck

### Residential and Small Commercial Buildings

Currituck has a strong tradition of highly functional, yet beautiful architecture that makes the most of simple forms and small embellishments.

Pitched roofs and deep porches are found on buildings of all scales and uses. Porch overhangs are typically deep enough to require separate support posts which typically feature a small brace or embellishment where they connect to the beam. The small size of these posts coupled with the repetitive, small braces contribute to the sturdy, but visually delicate appearance of typical Currituck architecture.



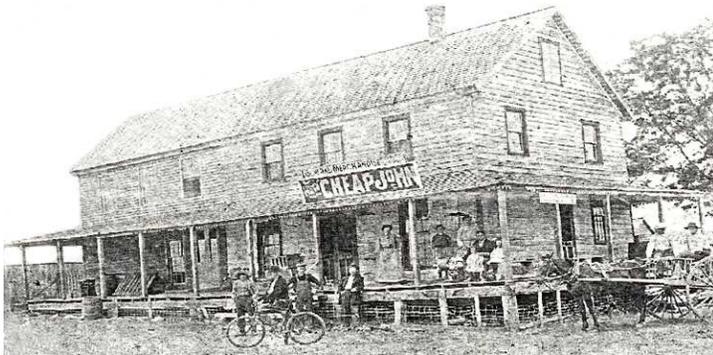
*Vernacular porch with decorative brackets and trim*



*Pea Island Station, Dare County, NC*



*Caffey's Inlet Station, Dare County, NC*



*Cheap John's Store, Currituck County, NC*



*The Hitching Post, Currituck County, NC*

Currituck's Main Street-type streets have typically been filled in over a period of time with residences being used for, or becoming, businesses and dedicated commercial buildings filling in between. This pattern of development leads to a varied street frontage with buildings of several scales adjacent to one another.



*Courthouse Road, Currituck, NC*



*Creekmore Brother's Store, Currituck County, NC*

### Large Commercial and Institutional Buildings

Large buildings in Currituck traditionally have 2 to 3 stories with a large gabled or hipped roof, multiple roof dormers, sheltered entries, and gabled masses breaking up long facades.



*Moyock High School, Moyock, NC*



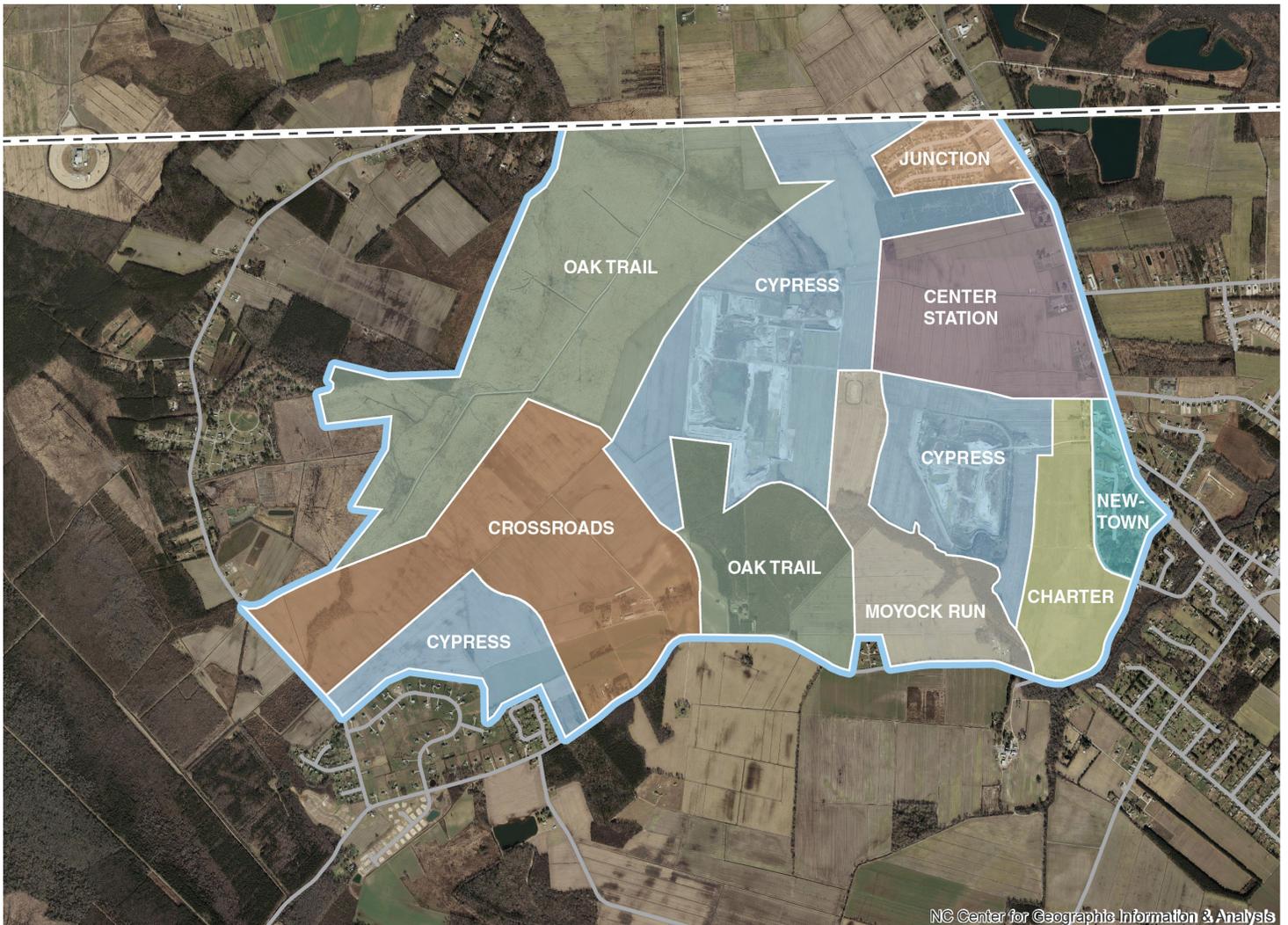
*The Whalehead Club, Currituck County, NC*

Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

### Guiding principles

#### The Currituck Station District and Sub-Districts

The purpose of the Currituck Station District is to accommodate a high-quality mixed-use development supported by a pedestrian-friendly environment. The standards have been developed to provide a mix of uses and densities needed to sustain the mixed-use development.



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## The Currituck Station Transect



### Center Station

The Center Station sub-district is intended to be the center of the Currituck Station District and is designed to support the primary economic and social components of the District by providing an integrated mix of uses in a downtown-oriented setting including retail, convenience, entertainment, civic, and public uses with supporting residential attached or on upper stories.

### Charter

The Charter sub-district is intended to provide a more conventional development pattern that reinforces the walkable nature of Center Station with interconnected sidewalks and multi use paths. The vertical mixing of residential developments with office and retail is encouraged. Horizontal mixing of stand-alone nonresidential development providing well-integrated uses, access and circulation, and compatible design that supports the adjacent neighborhood-scale residential development.

### Cypress

The Cypress sub-district is primarily residential in nature. However, limited non-residential uses including neighborhood-serving office and commercial uses are permitted.

### Junction and Newtown

The Junction and Newtown sub-districts are intended to support the District through redevelopment opportunities. Care should be given to ensure compatibility of new development with the existing.

### Crossroads

The Crossroads sub-district is intended to provide for industrial and job-producing non-residential developments. Uses in the sub-district should be designed to provide buffers and, where necessary, additional mitigation techniques to protect the adjacent residential and environmentally sensitive areas.

### Moyock Run

The Moyock Run sub-district is intended to provide centralized governmental and public oriented uses including, but not limited to schools, parks, utilities, transportation, regional facilities, environmental, and mitigation areas.

### Oak Trail

The Oak Trail sub-district is intended to reflect those areas generally identified as environmentally sensitive including, but not limited to, native habitats, wetlands, riparian buffers, and floodplain. Low density residential uses and low impact recreational uses may be permitted upon determination of the environmental area limits.

The following pages provide examples of building massing and architectural styles that are compatible with the Currituck Station design intent. Designers are not expected to copy these, only to use them as a guide towards their own compatible designs.

## Center Station Development Principles

The Center Station sub-district forms the mixed-use downtown core of the Currituck Station District. The downtown pattern of design includes a grid of streets supporting pedestrian activities, on-street parallel or angled parking, wide sidewalks with consistent tree canopy, outdoor café seating, and public spaces such as plazas and parks.

To maintain the walkable, pedestrian-oriented nature of Center Station, off-street parking lots are located behind buildings or vegetated screens, towards the interior of the lot. Development is designed to support vibrant, walkable streets by placing buildings close to the street with active and public uses on the ground floor and pedestrian entrances accessed from the street. An interconnected network of sidewalks, multi-use paths, public spaces, and parks provide access to the mix of uses found throughout the sub-district.

Center Station is the gateway to Currituck Station. In order to present a welcoming and high-quality face to the public, development along Caratoke Highway requires additional attention to detail through architectural and aesthetic treatments, integrated signage, landscape and buffer enhancements, and interconnected developments.

The Currituck Unified Development Ordinance allows for, and provides specific guidance on the development of, more generic suburban-style buildings than the examples in this Pattern Book.

### Dimensions

The allowable lot dimensions for the Center Station sub-district are designated in the Currituck Unified Development Ordinance.

### Allowable uses

This is an abbreviated table of uses for reference only. For a current and complete table of uses, see the Unified Development Ordinance

Residential Uses	Commercial Uses	Industrial Uses	Civic Uses
Mixed with Commercial	Retail	None	Office
Multifamily	Convenience		Recreation
Single Family (Attached)	Entertainment		Community
	Office		Government Facilities
	Health Care		



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# Center Station Commercial & Mixed Use Buildings

The commercial and mixed-use development in Center Station is the heart of the Currituck Station community. These streets will be lined with lively businesses, shops, offices, and restaurants. Residential and office uses above the street-level businesses provide an in-place market for the commercial concerns as well as contributing to a round-the-clock level of activity.

All of the buildings have a pedestrian-scale facade on the primary street that welcomes people in, allows them to window shop or observe the activity within, and encourages people to sit and stay a while.



Courthouse Road, Currituck County, NC



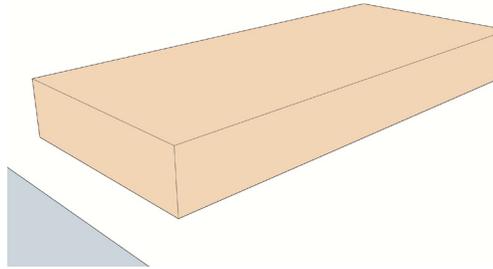
Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

## Center Station Commercial & Mixed Use

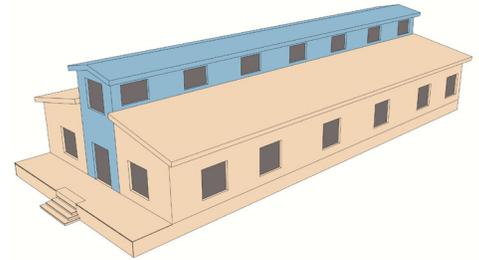
### Small 2-story building

#### Clerestory building

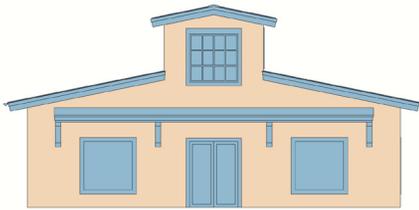
1. Use features such as clerestory to give a small building a 2-story appearance. The second floor may or may not be occupied.
2. Simple details give the building a pedestrian-friendly facade while keeping costs low.



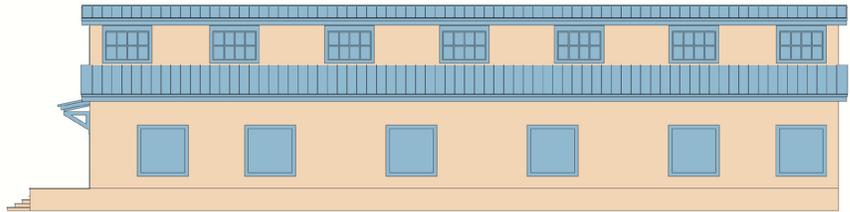
*A 1-story building program in a rectangular footprint. A 1-story building can only be accommodated if the facade is at least 20' tall giving a 2-story appearance.*



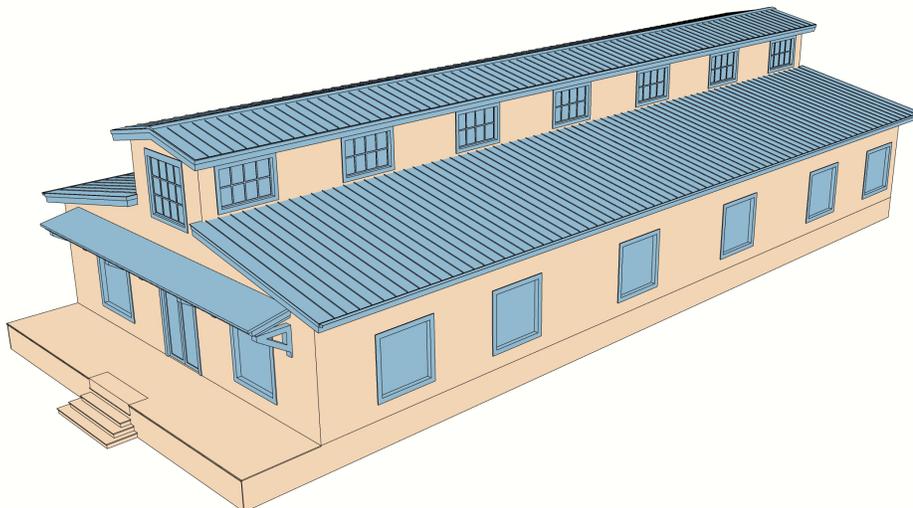
*Adding a clerestory gives the building a usable 2nd story for offices or studio apartments. Alternatively, the clerestory can be used to provide additional natural light and an airy feeling to the 1st floor space while meeting the 20' building height requirement.*



*A front facade with doors and generous windows centered on bays is functional and pedestrian friendly. The width of the clerestory section can be adjusted to suit the building program needs.*



*Ground floor side windows near the street are necessary to maintain visual permeability of the street frontage. Side windows towards the rear of the building are optional, but encouraged.*



*A small, simple fixed awning with support brackets and simple trim profiles add detail to the building and give it a coastal appearance.*

## Center Station Commercial & Mixed Use

### 1. Roof

Standing seam metal due to low pitch

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Board & batten, shake, or clapboard

### 4. Clerestory windows

Divided lite, clear glass windows

### 5. Awning

Sloped with simple brackets

### 6. Railings

Storefront doors with clear (untinted) glass

### 8. Ground floor windows

Storefront or folding glass windows with clear (untinted) glass

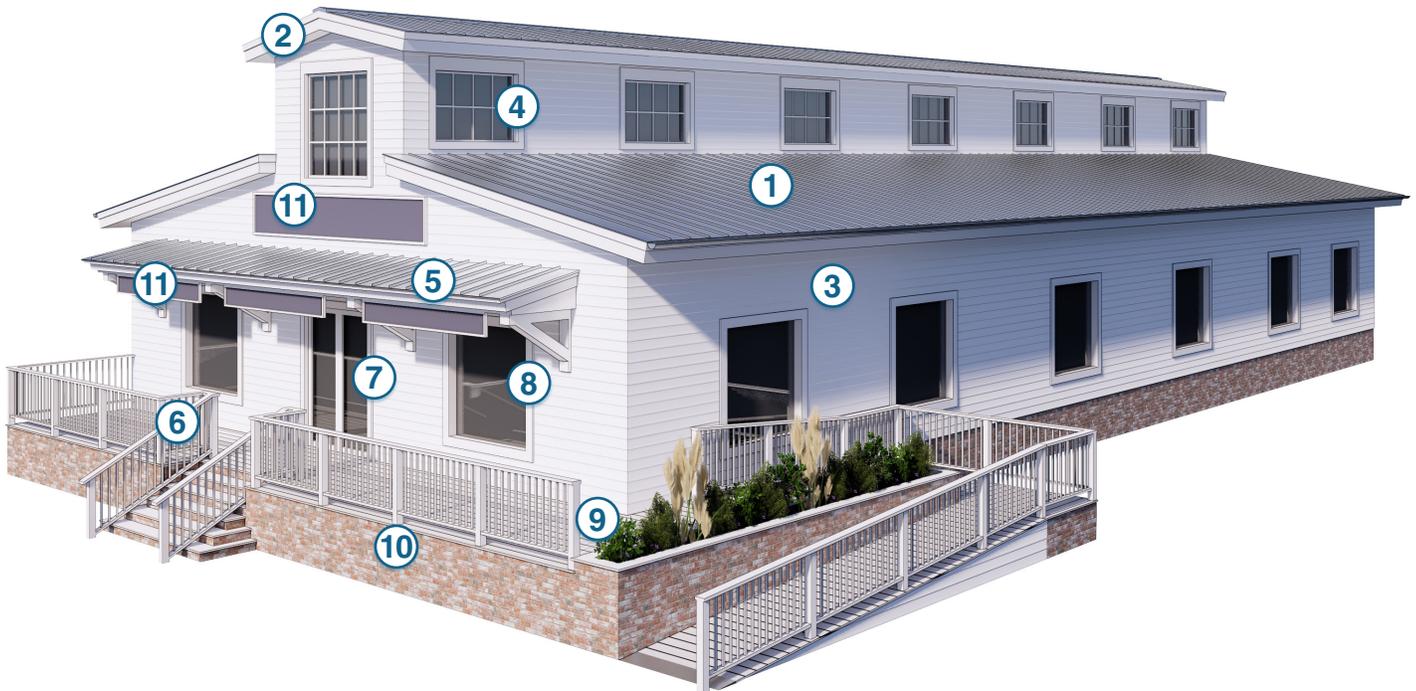
### 9. Decking

### 10. Foundation

Solid in appearance

### 11. Sign boards

Recessed or raised perimeter to differentiate from cladding



Local Pie in Bluffton, SC



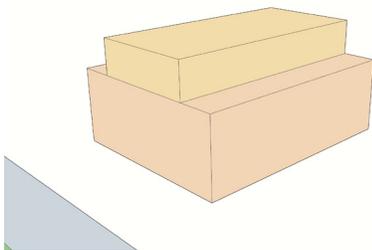
The Hitching Post (The Spry Store) in Currituck, NC

# Center Station Commercial & Mixed Use

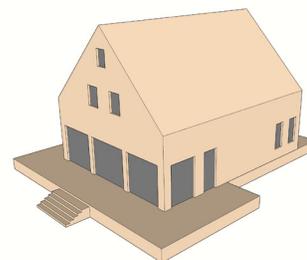
## Small 2-story building

### A-Frame with Porch

1. An iconic building form along the eastern seaboard, the A-frame is easy to construct, weathers storm events, and provides ample floor space while presenting a compact facade to the public.
2. A deep wrap-around porch shades the windows from hot summer sun and makes an inviting place for customers to enjoy.



A 2-story building program in a rectangular footprint.



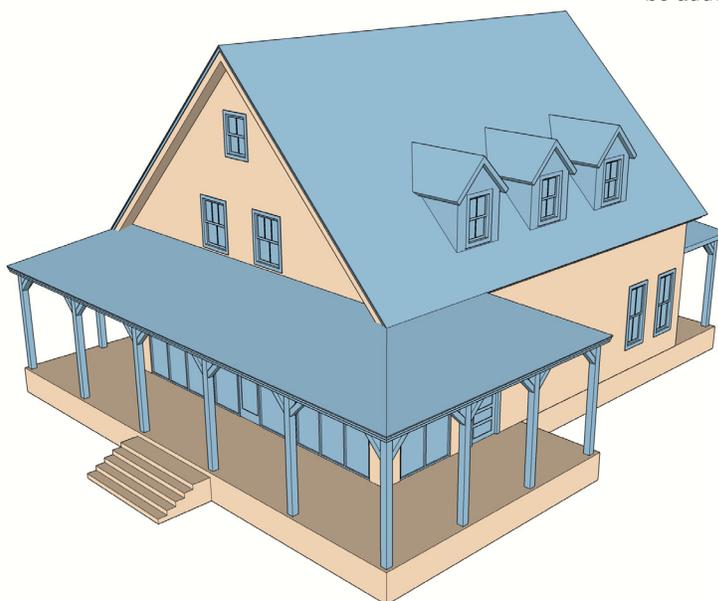
A steeply pitched roof encompasses the entire upper story.



A front facade with storefront glazing and transom windows allows natural light into the lower story space and invites pedestrians in.



Dormers let light into the upper story space and provide additional floor space. If the upper story is to be occupied by a different tenant, or is for a residential use, then a separate side entry can be added.



Straight or braced columns support the deep porch. Dormers can be separate or combined into one large dormer.

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## Center Station Commercial & Mixed Use

### 1. Roof

Shingle or standing seam metal

### 2. Trim

Simple profile, multi-layered

### 3. Upper story windows

Divided lite, clear glass windows

### 4. Porch roof

Shingle or standing seam metal

### 5. Columns and brackets

Wood or composite

### 6. Ground floor windows

Storefront with clear (untinted) glass

### 7. Doors

Storefront with clear (untinted) glass

### 8. Railings and stairs

### 9. Decking

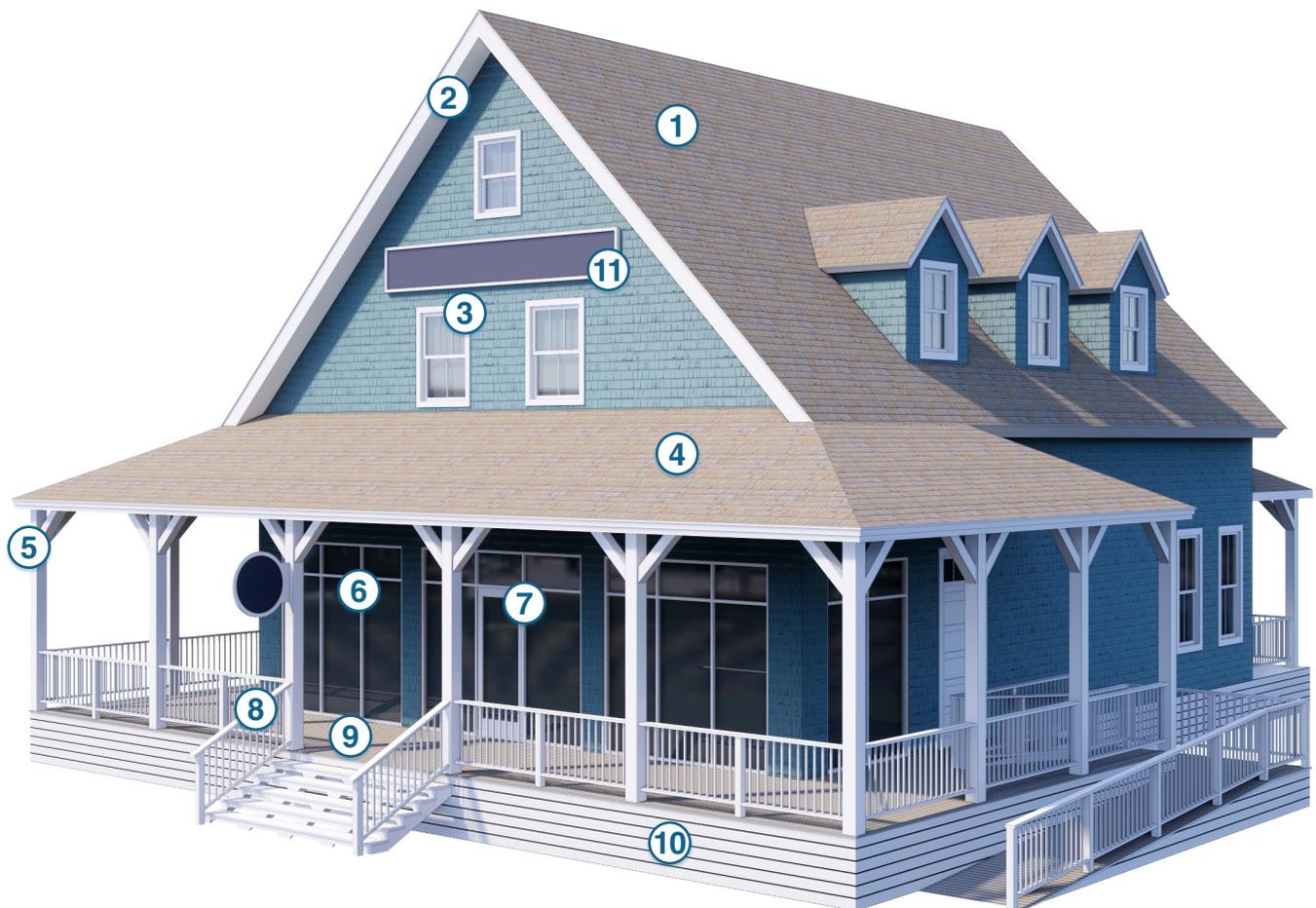
Wood (rot resistant) or composite

### 10. Foundation skirt

Solid or slatted in appearance

### 11. Sign board

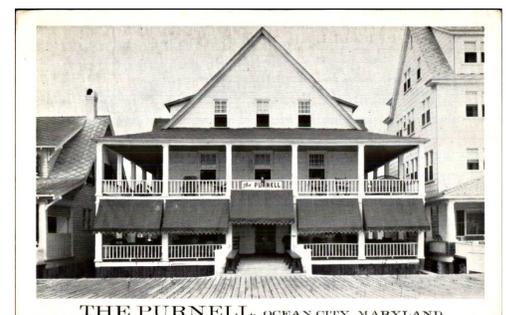
Recessed or raised perimeter to differentiate from cladding



Caffey's Inlet Lifesaving Station  
in Dare County, NC



Seatack Station, Virginia Beach, VA



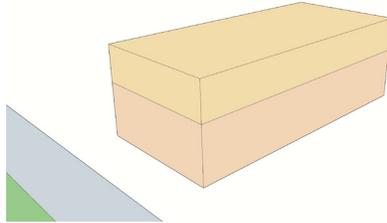
The Purnell Hotel, Ocean City, MD

## Center Station Commercial & Mixed Use

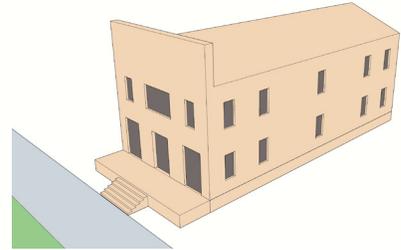
### Medium 2-3 story building

#### Parapet-front building

1. A parapet is used to give a more commercial appearance to a pitched-roof building.
2. The building can be used for a single use, or a vertical mix of uses with offices or residential units on the upper floors.



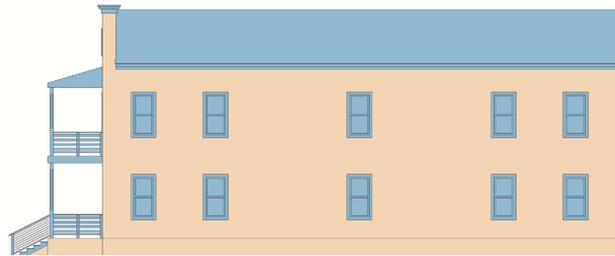
*A 2-story building program in a rectangular footprint.*



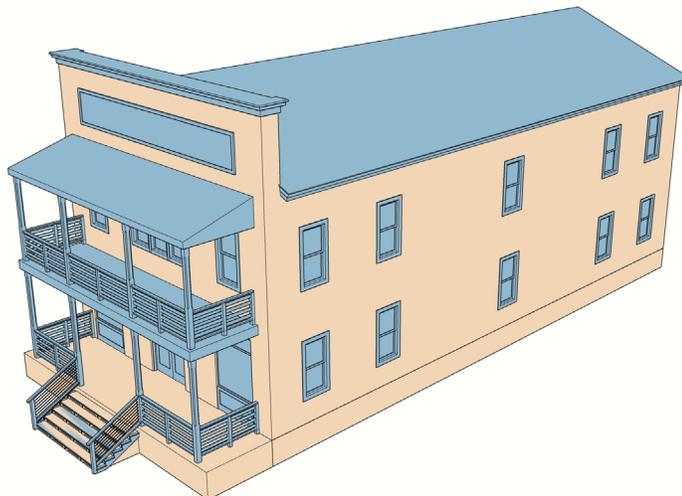
*A false front facade makes use of a parapet to make the pitched roof building appear to be a traditional flat roofed commercial building.*



*A front facade with doors and generous windows centered on bays is functional and pedestrian friendly. Adding a second story porch makes upper floor residential uses more inviting.*



*Ground floor side windows are necessary to maintain the visual permeability of the street frontage. Side windows towards the rear of the building are optional, but encouraged.*



*Roof and window types, awnings, trim, and brackets add detail to the building and give it a coastal appearance.*

## Center Station Commercial & Mixed Use

### 1. Roof

Shingle or standing seam metal

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Masonry, shake, or clapboard

### 4. Upper story windows

Double hung, divided lite, clear (untinted) glass windows

### 5. Porch roof

Match main roof or standing seam metal

### 6. Columns

### 7. Doors

Storefront with transoms with clear (untinted) glass

### 8. Ground floor windows

Storefront with transoms with clear (untinted) glass

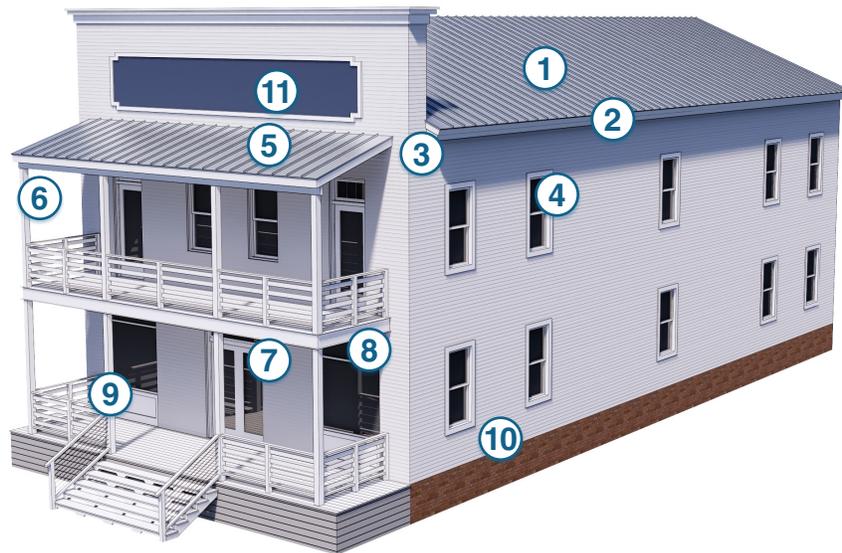
### 9. Railing and stairs

### 10. Foundation skirt

Solid in appearance with optional slatted material at porch

### 11. Sign board

Recessed or raised perimeter to differentiate from cladding

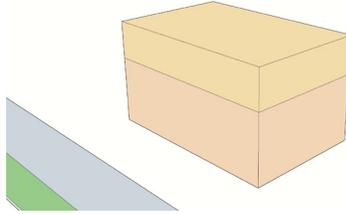


## Center Station Commercial & Mixed Use

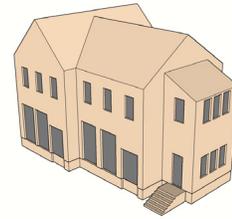
### Medium 2-story building

#### L-shaped

1. Use gable ends to break up longer facades and create the appearance of a series of smaller masses.
2. Simple details give the building a pedestrian friendly facade while keeping construction costs low.



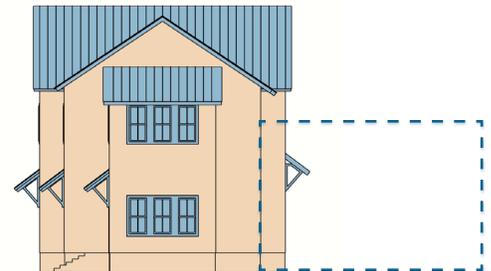
A 2-story building program in a rectangular broadfront footprint.



Adding a gable end breaks up the building facade and provides multiple distinct storefronts.



Storefront windows and doors allow pedestrians to see into the ground floor spaces. A stair and elevator tower on the end of the building gives private access to the upper story if desired.



An optional addition on the rear of the building adds ground floor space.



Awnings with brackets and shutters protect the windows from sun and rain.

## Center Station Commercial & Mixed Use

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Masonry, shake, or clapboard

### 4. Upper story windows

Divided lite, clear (untinted) glass windows

### 5. Shutters

1/2 width and full height of window, operable louvered or solid shutters

### 6. Awning

### 7. Doors

Storefront with transoms and clear (untinted) glass

### 8. Ground floor windows

Storefront with transoms with clear (untinted) glass and optional opaque paneling

### 9. Foundation

Solid in appearance

### 10. Residential door

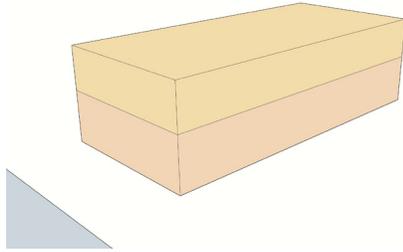


# Center Station Commercial & Mixed Use

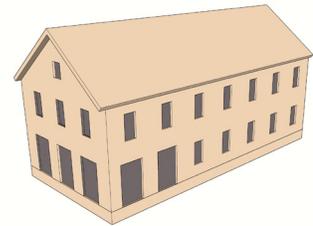
## Medium 2-3 story building

### Gable-front building

1. A classic and versatile building form lends itself well to a vertical mix of uses.
2. Simple details give the building a pedestrian friendly facade while keeping costs low.



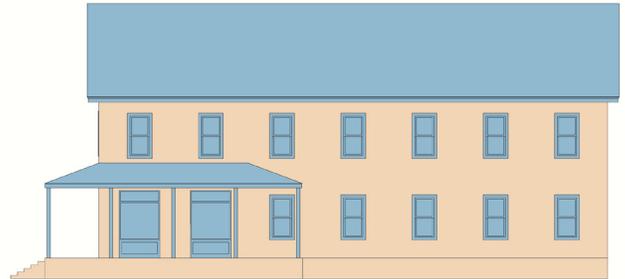
A 2 to 3-story building program in a rectangular footprint.



A gable above the upper story makes an easy-to-construct, adaptable, and durable building.



Minimal overhangs are typical, but the eaves can be extended up to 3' or 4' with exposed rafter tails for a more Craftsman appearance.



Evenly spaced side windows on all levels produce usable light throughout the building.



The wraparound porch can be supported by simple square columns or brackets.

## Center Station Commercial & Mixed Use

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Upper story windows

Single or divided lite, clear (untinted) glass windows

### 4. Porch roof

Match main roof or standing seam metal

### 5. Columns and brackets

4"-8" square or round

### 6. Ground floor windows

Storefront or folding glass windows with clear (untinted) glass

### 7. Doors

Storefront with clear (untinted) glass

### 8. Railings and stairs

### 9. Decking

Wood (rot resistant) or composite

### 10. Foundation skirt

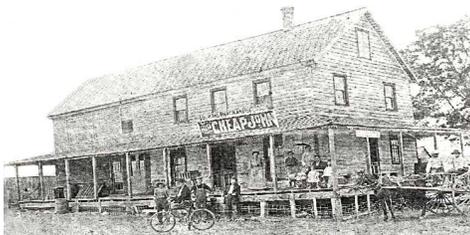
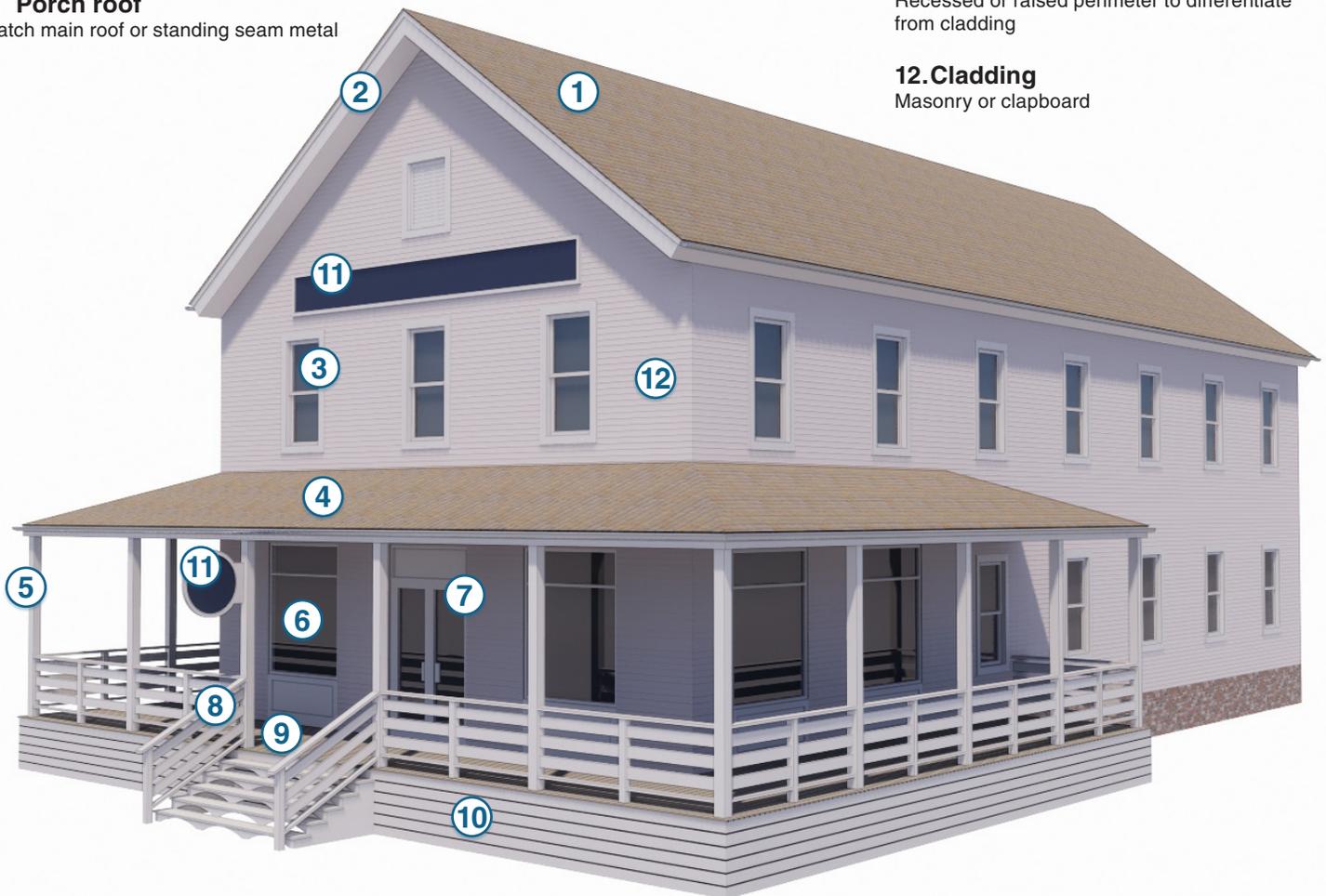
Solid in appearance with optional slatted material at porch

### 11. Sign board

Recessed or raised perimeter to differentiate from cladding

### 12. Cladding

Masonry or clapboard



Cheap John's Store, Currituck County, NC



Creekmore Brothers Store, Currituck County, NC



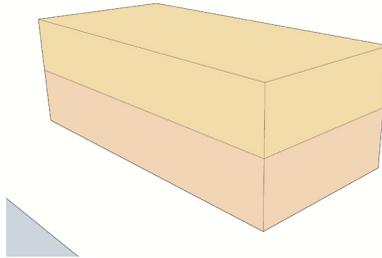
JM Roberts House, Currituck County, NC

## Center Station Commercial & Mixed Use

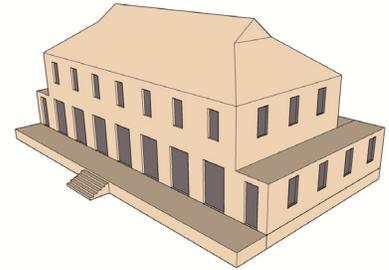
### Medium 2-story building

#### Broad-front building

1. Occupy the frontage of a wide site and provide continuous street-level access to multiple tenants.
2. Upper stories can be used for offices or residential and are pulled back from the street.



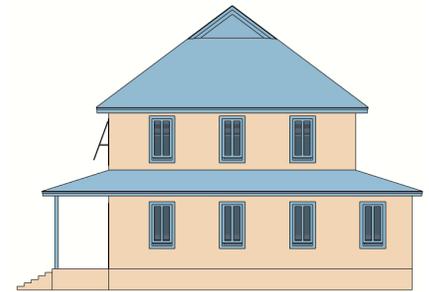
A 2 to 3-story building program in a rectangular broadfront footprint.



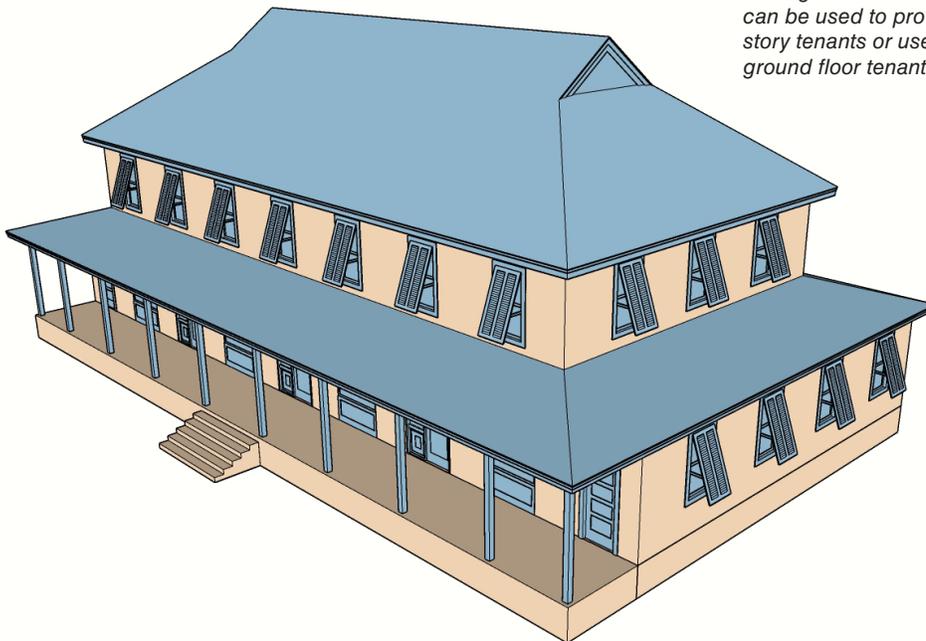
Closely spaced large openings at the ground floor are important to making the long facade feel permeable and welcoming to pedestrians.



A broad front facade allows for more space behind the building to be used for parking and allows for more individual tenants to have street frontage.



Ground floor side windows are necessary to maintain the visual permeability of the street frontage. The enclosed areas under the porch roof can be used to provide building access for upper story tenants or used as enclosed porches for ground floor tenants.



A gabled roof is used here, but gable or hipped roofs would also be appropriate. Bermuda shutters have been used to give the building a coastal appearance.

## Center Station Commercial & Mixed Use

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Shake or clapboard

### 4. Upper story windows

Double hung, divided lite, clear (untinted) glass windows with shutters

### 5. Shutters

Bermuda shutters. full width and length of window

### 6. Porch roof

Match main roof

### 7. Columns

Square or round simple columns

### 8. Doors

Storefront with transoms with clear (untinted) glass

### 9. Ground floor windows

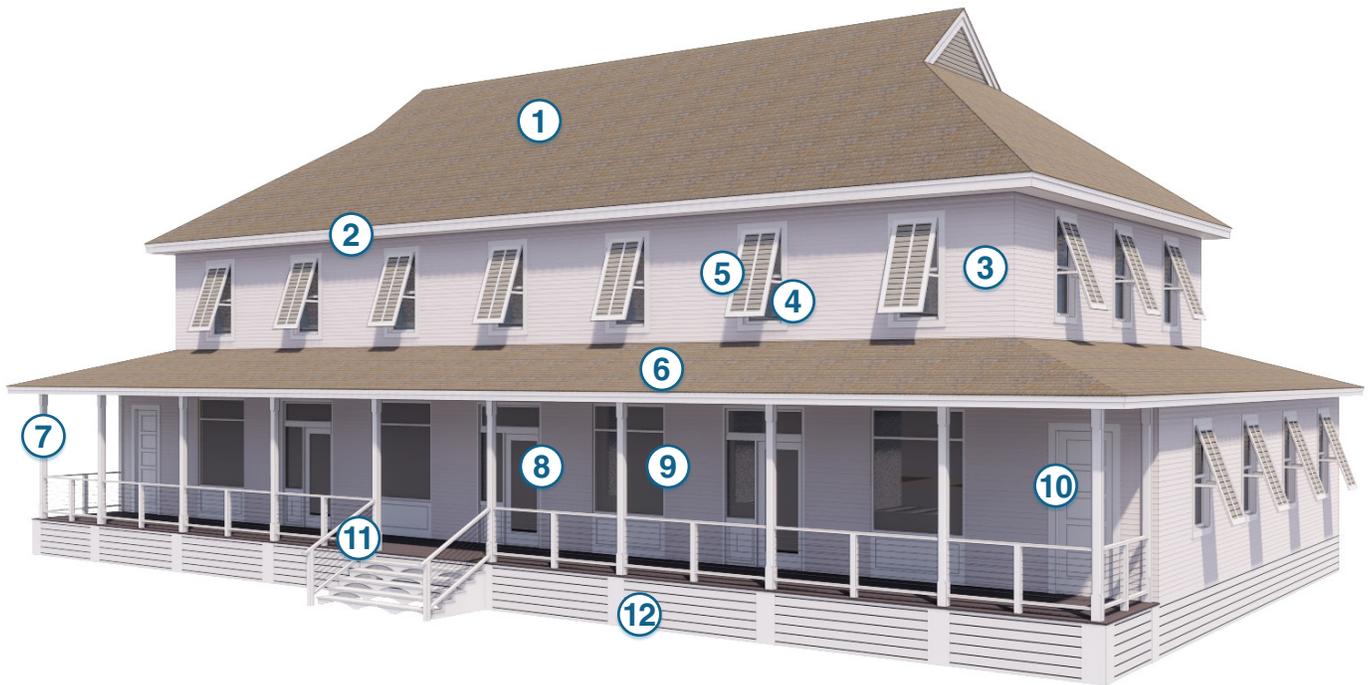
Storefront with transoms with clear (untinted) glass

### 10. Residential/Upper story door

### 11. Railing and stairs

### 12. Foundation skirt

Solid or slatted in appearance

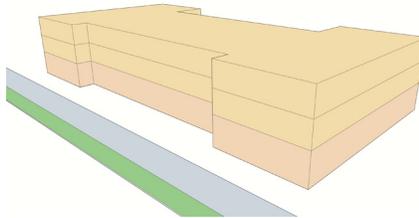


## Center Station Commercial & Mixed Use

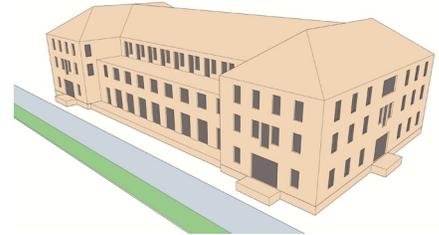
### Large 3-story building

#### I-shaped building

1. Multi-tiered porches break up the facade.
2. Repetitive use of residential scale windows and features on upper stories makes a large building feel less imposing.



A 2 to 4-story building program in a rectangular broadfront footprint.



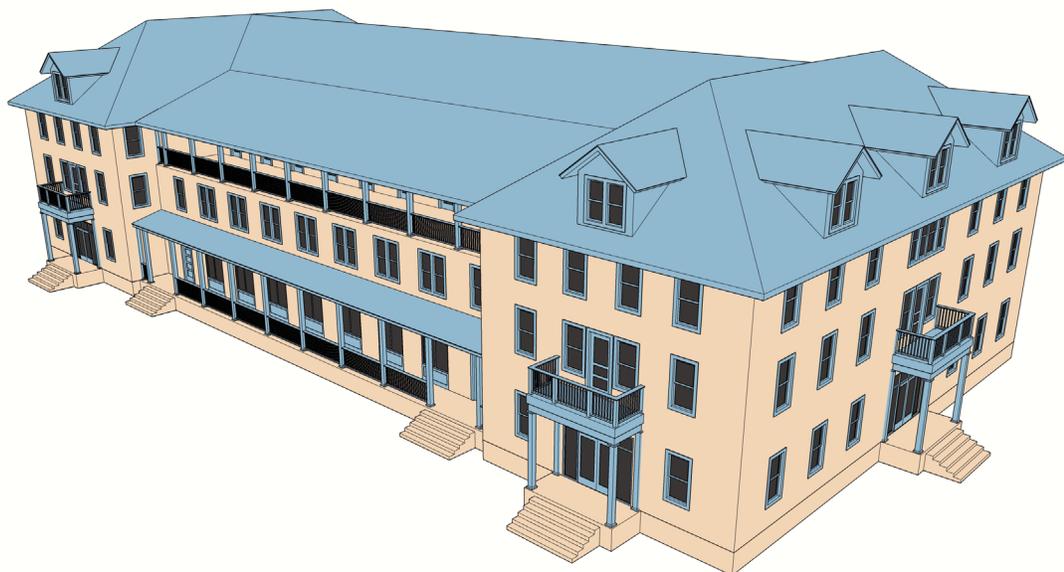
Multi-tiered porches provide building stepbacks and break up the mass of the building. Gables bookend the middle of the building.



A broad facade is broken up by regularly spaced large windows. The scale of the windows and facade treatment decreases at each level, with the smallest and most residential scale features closest to the top.



Ground floor side windows are necessary to maintain the visual permeability of the street frontage. Large buildings should allow entry along the sides as well as the front and back.



Minimal ornament is needed if the windows and other features are scaled appropriately. Dormers break up the large roof expanses and can be used to bring daylight into loft spaces or to ventilate mechanical spaces.

## Center Station Commercial & Mixed Use

### 1. Roof

Shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Masonry, shake, or clapboard

### 4. Upper story windows

Divided lite, clear (untinted) glass windows with optional shutters

### 5. Commercial windows

Storefront with transoms or operable, divided lite, clear (untinted) glass windows

### 6. Porches

Wood or composite

### 7. Balconies

Deck with railings and standing seam metal roof. Operable out-swing doors with large glazed area

### 8. Columns

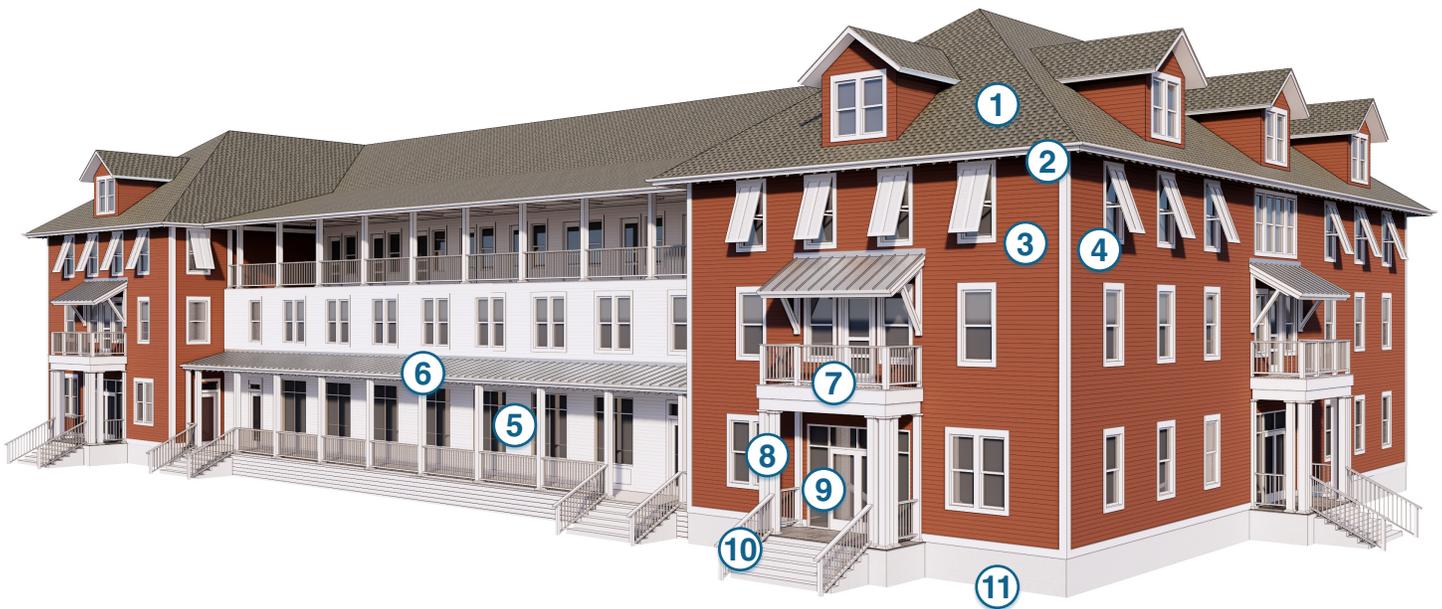
### 9. Commercial doors

Storefront with transoms with clear (untinted) glass

### 10. Railing and stairs

### 11. Foundation skirt

Solid in appearance with optional slatted appearance at porch

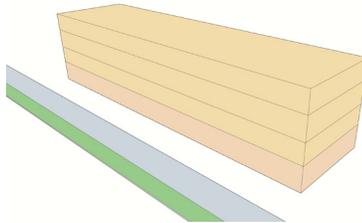


## Center Station Commercial & Mixed Use

### Large 4-story building

#### Large broad-front building

1. A very simple footprint allows for efficient construction and use of interior space.
2. Gable ends that break the facade can be used as central entries, or to separate multiple tenants who each have a side or section of the building.



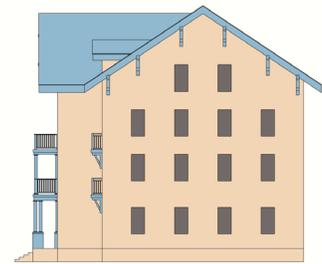
A 2 to 3-story building program in a rectangular broadfront footprint.



Adding a gable end breaks the facade into multiple masses and makes for a clear point of entry.



Ground floor porches flanking the entry provide pedestrian scale spaces.



Side windows are necessary to keep the building from appearing to be monolithic.



Minimal ornament is needed if the windows and other features are scaled appropriately. Dormers break up the large roof expanses and can be used to bring daylight into loft spaces or to ventilate mechanical spaces.

## Center Station Commercial & Mixed Use

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered and wood or composite brackets

### 3. Cladding

Shake or shingle with optional masonry base story

### 4. Upper story windows

Operable, divided lite, clear (untinted) glass windows with shutters

### 5. Commercial windows

Storefront with transoms or operable, divided lite, clear (untinted) glass windows

### 6. Porches

Deck with railings and standing seam metal roof due to low pitch

### 7. Balconies

Deck with railings. Operable out-swing doors with large glazed area

### 8. Columns

Square or round with masonry piers where ganged

### 9. Commercial doors

Storefront with transoms with clear (untinted) glass

### 10. Residential doors

### 11. Railing and stairs

### 12. Foundation skirt

Solid in appearance with optional slatted appearance at porches



## Center Station Multifamily Buildings

### Multifamily development

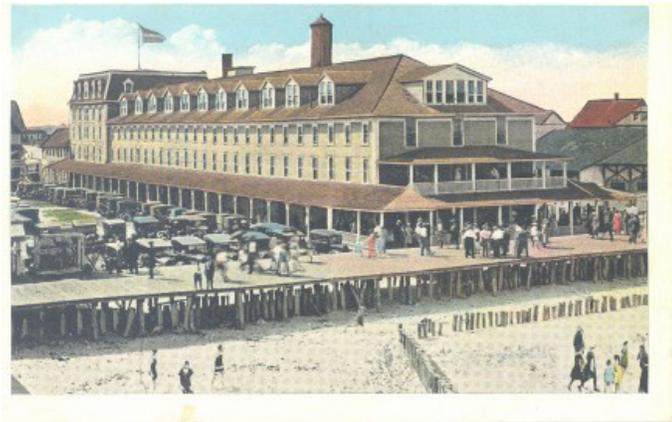
Multifamily development in Currituck Station is broken into two scales for the purposes of this pattern book:

For smaller multifamily developments up to 12 units, stacked urban apartment and mansion apartment buildings are appropriate. These can come in a variety of forms ranging from triplexes to courtyard apartment buildings.

Large scale, high density development should follow the pattern of traditional grand beachfront hotels. These buildings feature continuous porches, which can be interrupted internally to afford some privacy to individual units, delicate columns and railings, and simple roof forms. They frequently employ dormers for additional daylighting of loft spaces or ventilating mechanical spaces, which also serve to break up the mass of the long roof form.



# Center Station Multifamily Buildings



The old Atlantic Hotel in Ocean City, MD.



The First Colony Inn of Nags Head is a great example of a grand hotel-style building of modest scale. The delicate stacked porches and regularly spaced dormers effectively break down the scale of the building while being simple to construct.



While not a grand hotel, the Moyock High School is a good precedent for large scale multi family buildings in Currituck Station.



The Pleasant View Hotel in Nags Head is another example of the grand hotel-style that can be adapted to multifamily housing.

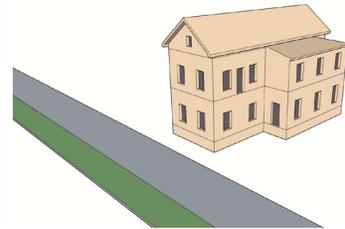
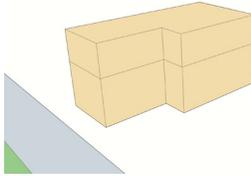
Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

# Center Station Multifamily Buildings

## Small 2-story building

### Duplex Style Apartment

1. A 2-story long, narrow building with a recessed side extension presents a narrow facade to the street.
2. The recessed entry provides privacy to the residents while giving the street frontage more depth.



A 2 to 3-story building program in a rectangular footprint.

A side entry provides privacy on busy streets.



The narrow front facade is easily made pedestrian-friendly with just a few windows.



With the entries on the side, they can be less ornate and still be appropriate.



The side porches add detail to the front elevation while providing a private outdoor space for residents.

## Center Station Multifamily Buildings

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Masonry, shake, or clapboard

### 4. Upper story windows

Operable, divided lite, clear (untinted) glass windows

### 5. Porch roof

Match main roof

### 6. Columns

Square 4"-8"

### 7. Doors

Storefront with transoms with clear (untinted) glass

### 8. Ground floor windows

Storefront with transoms with clear (untinted) glass

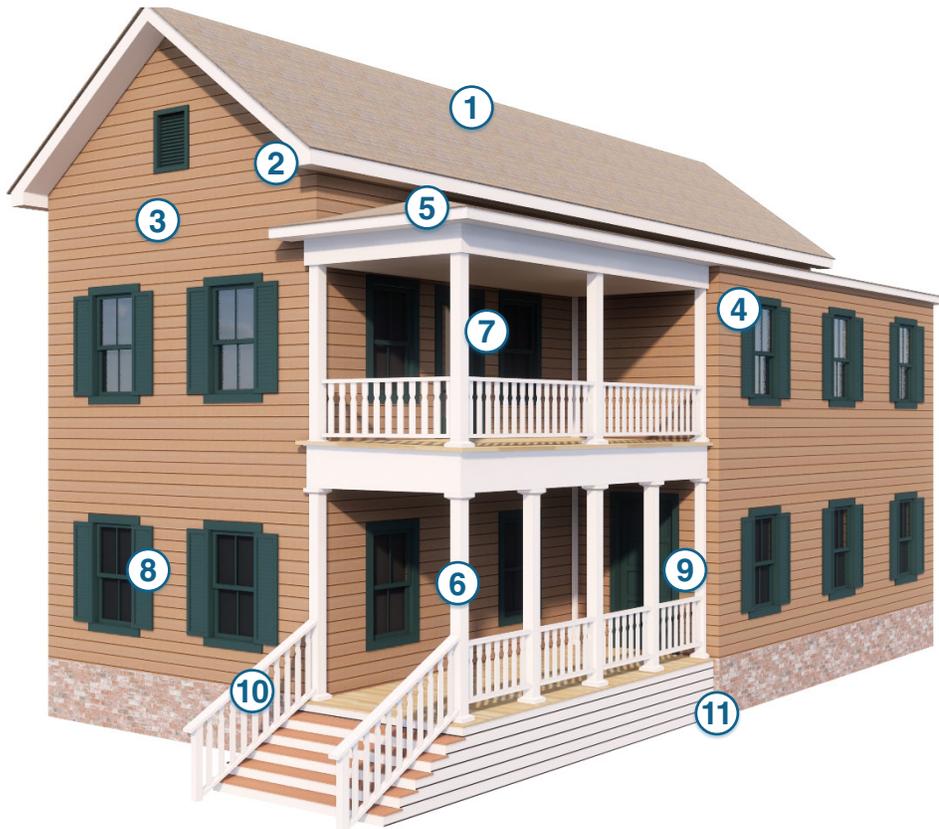
### 9. Building Main Entry

Residential entry system

### 10. Railings

### 11. Foundation skirt

Solid appearance with optional slatted appearance at porch

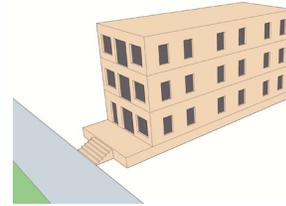
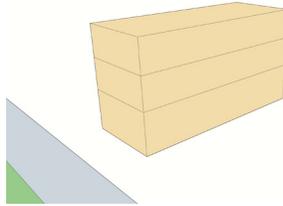


# Center Station Multifamily Buildings

## Small 3-story building

### Triplex Style Apartment

1. A narrow building with 3 stacked apartments.
2. A compact footprint and simple detailing keep construction costs low while providing more density.

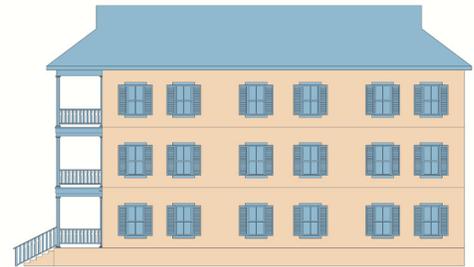


A 2 to 3-story building program in a rectangular footprint.

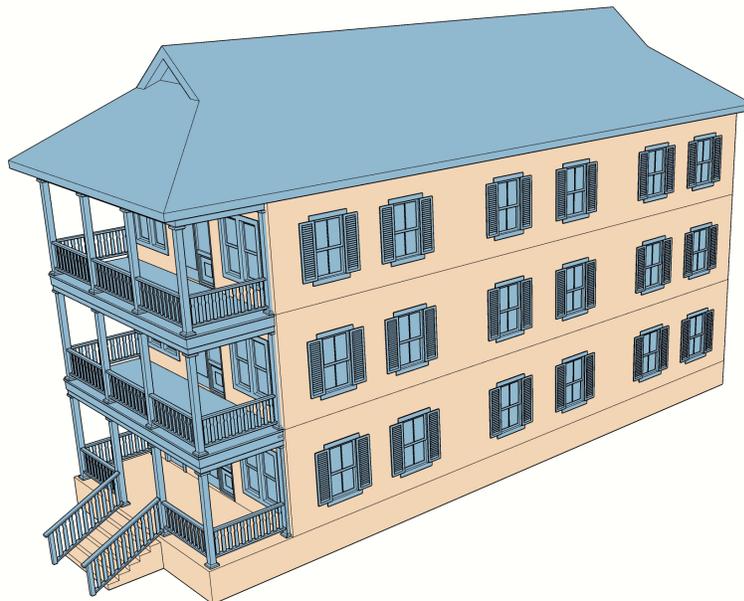
A side entry provides privacy on busy streets.



The front facade is a simple composition with regularly spaced windows and doors.



Windows should be placed to daylight and ventilate all interior spaces.



A hipped or gabled roof helps to step the mass of the building back.

## Center Station Multifamily Buildings

### 1. Roof

standing seam metal or shingle

### 2. Trim

Simple profile

### 3. Cladding

Masonry, shake, or clapboard

### 4. Windows

Operable, divided lite, with clear (untinted) glass

### 5. Columns

Square 4"-8"

### 6. Building Main Entry

Residential or commercial entry system

### 7. Railings

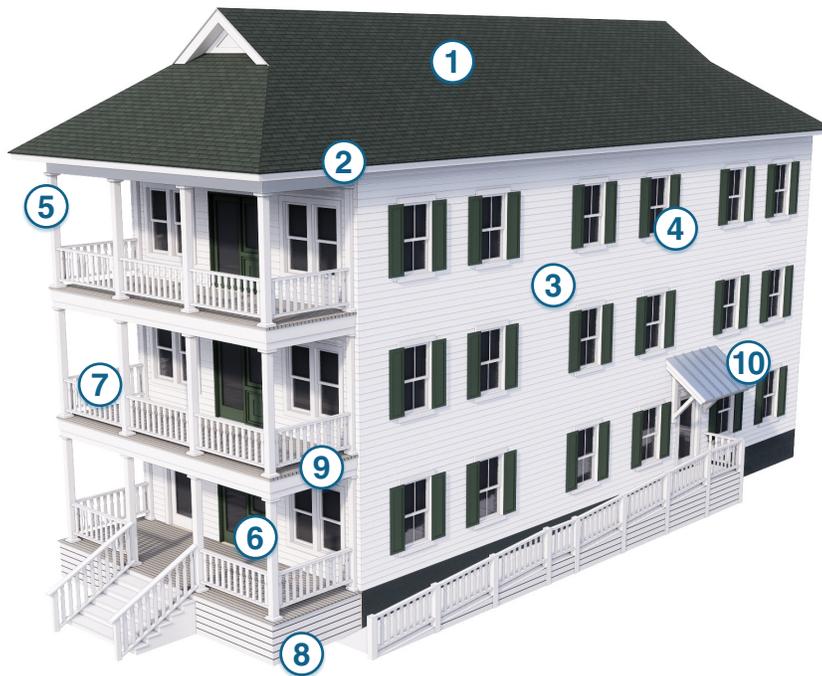
### 8. Foundation skirt

Solid appearance with optional slatted appearance at porch

### 9. Porch Decking

### 10. Awnings

Match main roof or standing seam metal with wood or metal structure

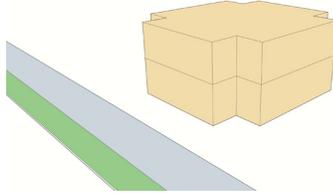


# Center Station Multifamily Buildings

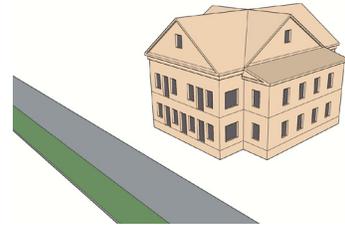
## Medium 2-story building

### Mansion Style Apartment

1. Intended to appear like a large house that has been broken up into smaller dwelling units, the design should be that of a large residence.
2. Simple details give the building a pedestrian friendly facade while keeping costs low.



*A 2 to 3-story building program in a rectangular or square footprint.*



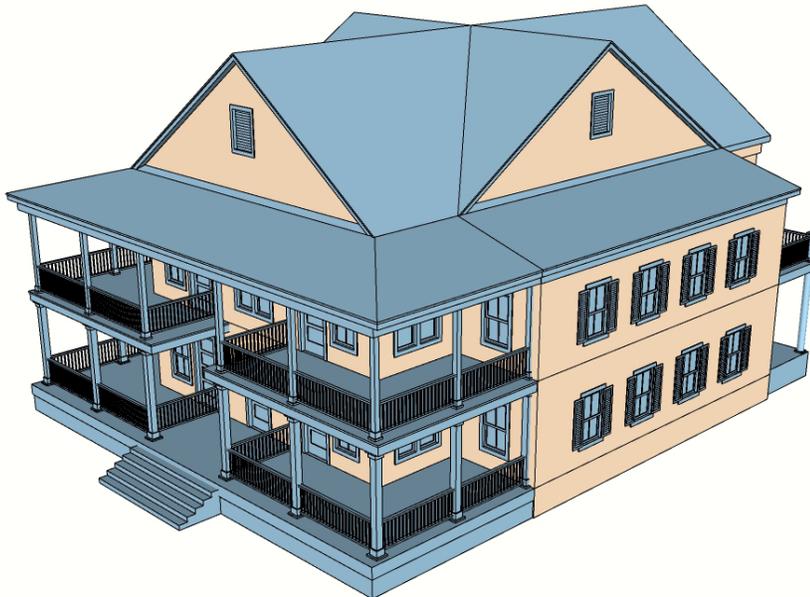
*Typical residential windows and details are appropriate.*



*A front facade with doors and generous windows centered on bays is functional and pedestrian friendly.*



*The windows should continue around the perimeter of the building to provide daylighting for all of the interior spaces.*



*Wrap-around porches provide outdoor space for residents and accommodate a coastal lifestyle.*

## Center Station Multifamily Buildings

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Brick, shake, or clapboard

### 4. Windows

Operable, divided lite, clear (untinted) glass windows

### 5. Porch roof

Match main roof

### 6. Columns

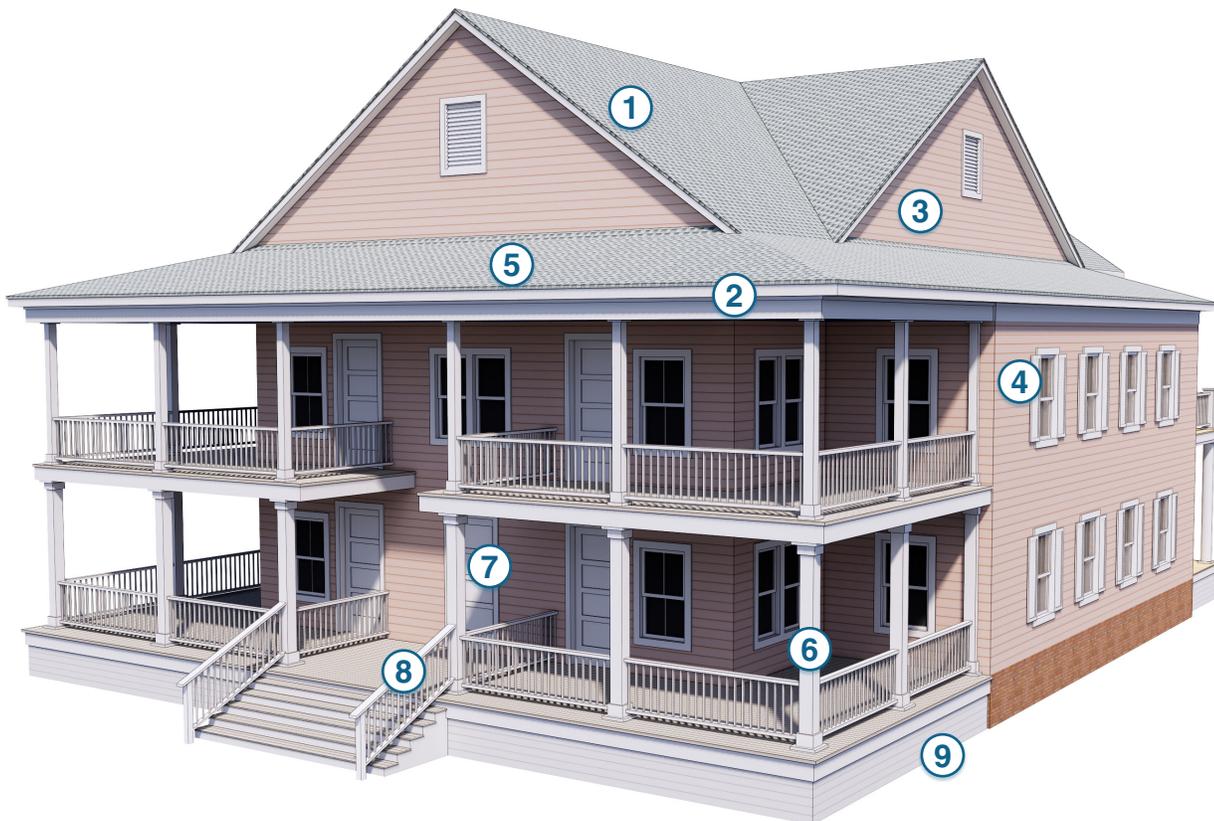
Square 6"-12"

### 7. Entry Doors

### 8. Railings

### 9. Foundation skirt

Solid appearance with optional slatted appearance at porch

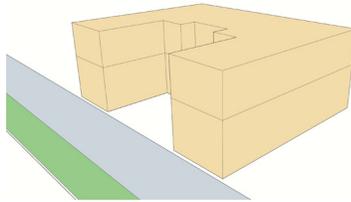


# Center Station Multifamily Buildings

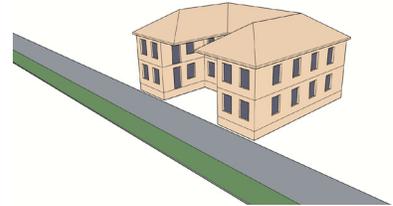
## Medium 2-story building

### Courtyard Style Apartment

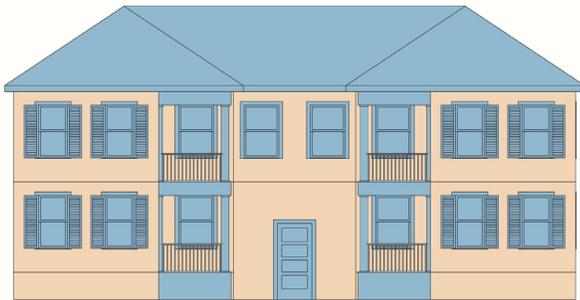
1. A multifamily building with a small scale.
2. A courtyard allows all units to be naturally lit from multiple directions.



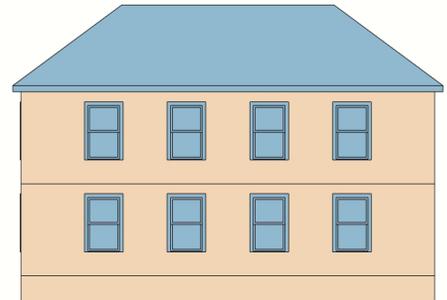
A 2 to 3-story building program in a square or rectangular footprint with a courtyard.



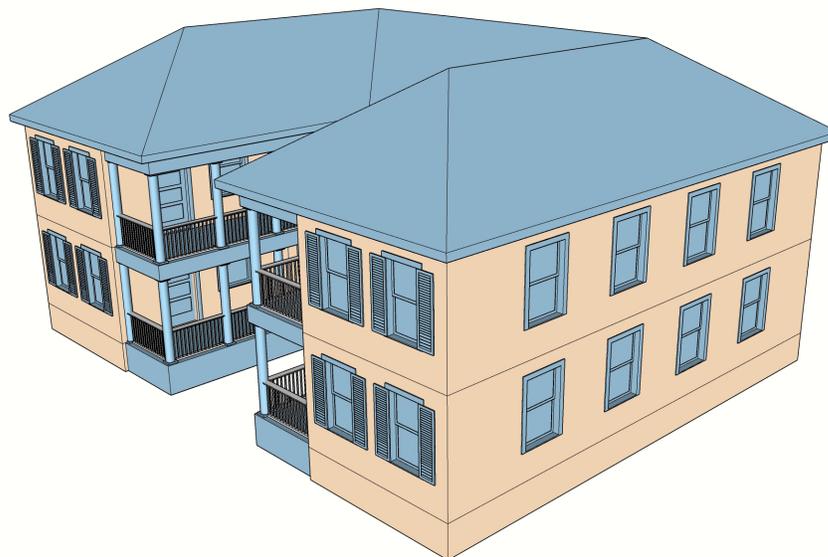
Windows and porches are placed to take advantage of the natural light available around the exterior perimeter, and the privacy afforded by the courtyard.



A central courtyard provides a semi-private entrance and porch area while maintaining the building facade line of the block.



Ground floor side windows are necessary to maintain the visual permeability of the street frontage. Side windows towards the rear of the building are optional, but encouraged.



Roof and window types, awnings, trim, and brackets add detail to the building and give it a coastal appearance.

## Center Station Multifamily Buildings

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Masonry, shake, or clapboard

### 4. Upper story windows

Operable, divided lite windows with clear (untinted) glass

### 5. Columns

Square or round

### 6. Entry Doors

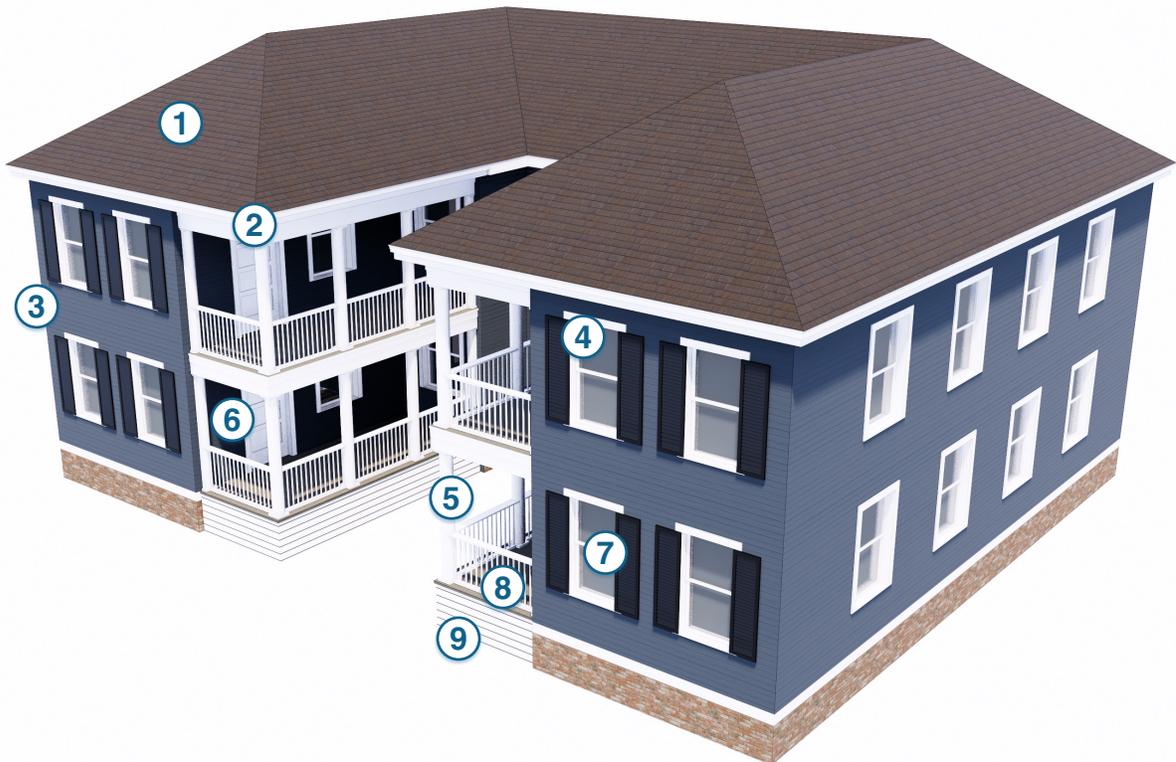
### 7. Ground floor windows

Storefront with transoms and clear (untinted) glass

### 8. Railings

### 9. Foundation skirt

Solid appearance with optional slatted appearance at porches

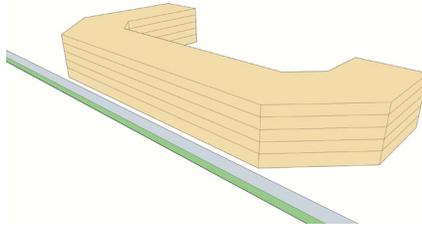


# Center Station Multifamily Buildings

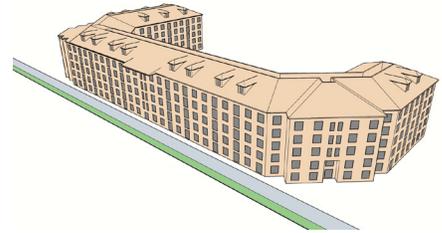
## Large 4-story building

### Grand Hotel Style Apartment

1. Multi-tiered porches are a defining feature of the grand hotel style. Their repetitive nature is efficient for construction and reduces the visual scale of the building.
2. Simple details give the building a pedestrian friendly facade while keeping costs low.



A 2 to 5-story building program in a rectangular broadfront footprint.



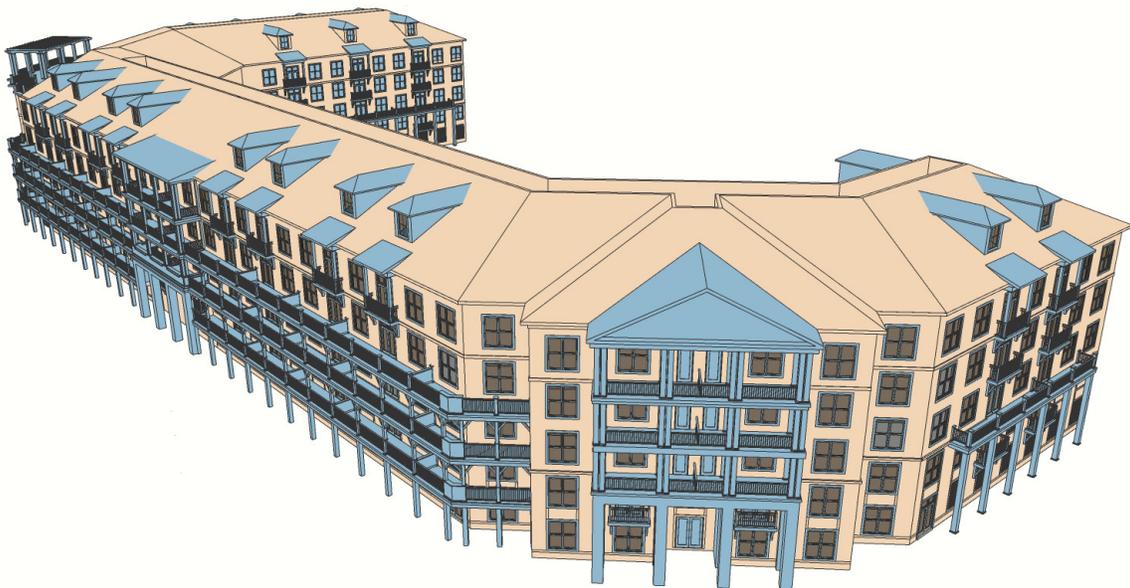
Turning the ends of the building back in to the lot provides a courtyard for parking or building amenities.



A front facade with doors and generous windows centered on bays is functional and pedestrian friendly.



Carrying the same elements of the front facade around the sides creates a coherent building.



Roof and window types, awnings, trim, and brackets add detail to the building and give it a coastal appearance.

## Center Station Multifamily Buildings

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Brick, shake, or clapboard

### 4. Upper story windows

Operable, divided lite, clear (untinted) glass windows

### 5. Dormers

### 6. Porch roof

Match main roof

### 7. Columns

Square or round

### 8. Doors

Storefront with transoms with clear (untinted) glass

### 9. Ground floor windows

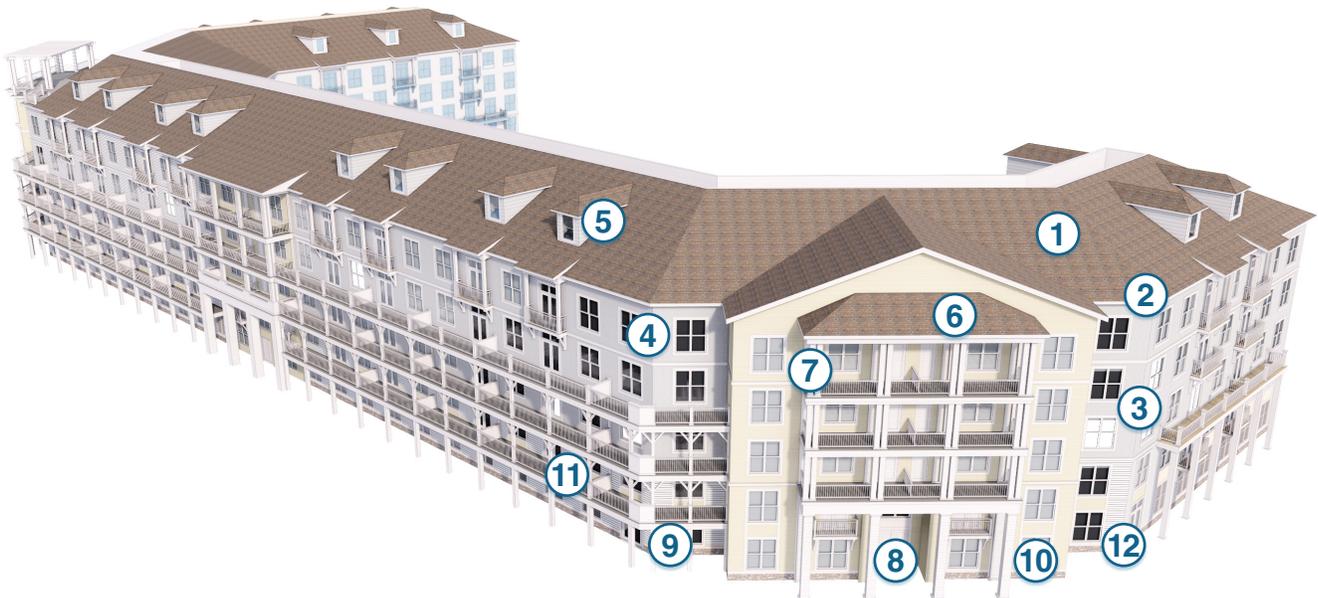
Storefront with transoms with clear (untinted) glass

### 10. Screening Of Podium Parking

### 11. Railings

### 12. Foundation skirt

Solid appearance

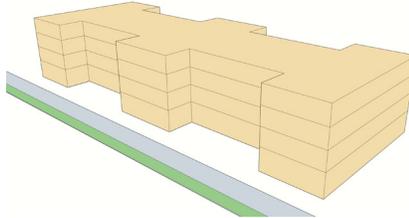


# Center Station Multifamily Buildings

## Large 4-story building

### Grand Hotel Style Apartment

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A 2 to 5-story building program in a rectangular broadfront footprint.



Turning the ends of the building back in to the lot provides a courtyard for parking or building amenities.



A front facade with doors and generous windows centered on bays is functional and pedestrian friendly.



Carrying the same elements of the front facade around the sides creates a coherent building.



Roof and window types, awnings, trim, and brackets add detail to the building and give it a coastal appearance.

## Center Station Multifamily Buildings

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Brick, shake, or clapboard

### 4. Upper story windows

Operable, divided lite, clear (untinted) glass windows

### 5. Dormers

### 6. Entry Balcony

Match style of balconies

### 7. Columns

Square

### 8. Doors

Storefront with transoms with clear (untinted) glass

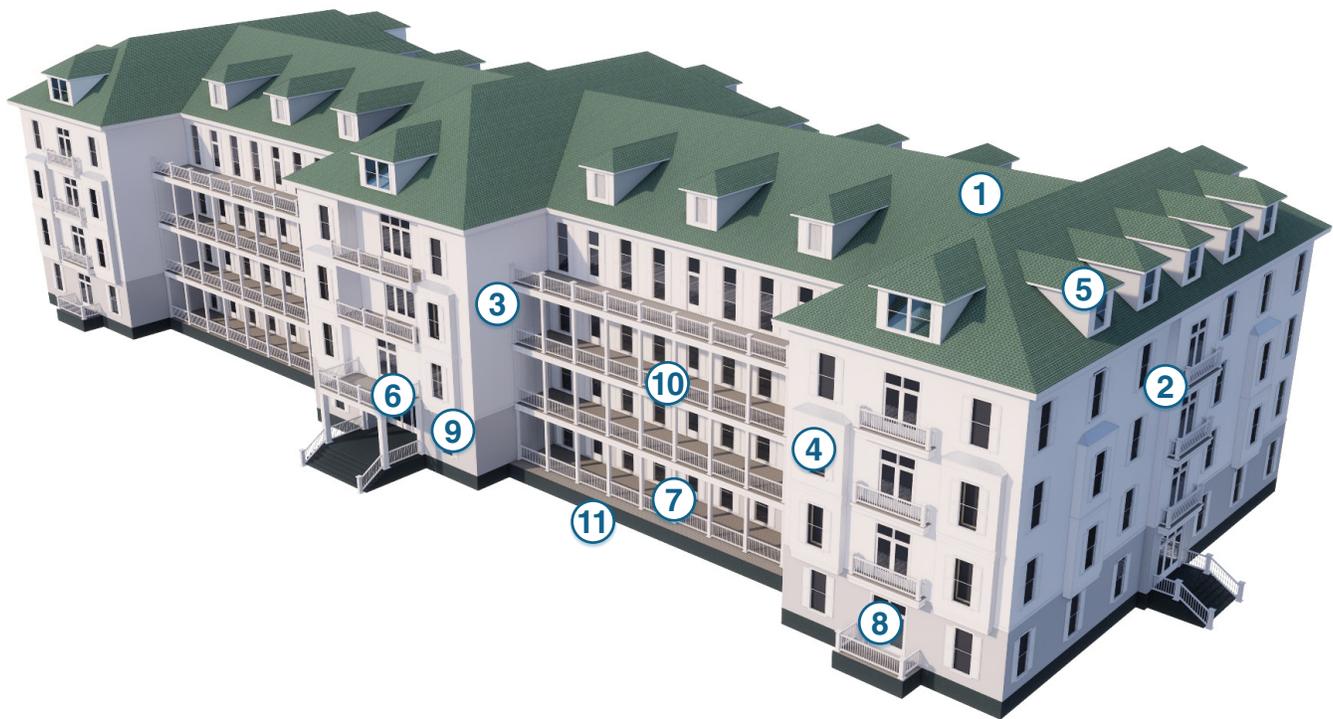
### 9. Ground floor windows

Storefront with transoms with clear (untinted) glass

### 10. Railings

### 11. Foundation skirt

Solid appearance



# Center Station Commercial, Mixed Use, & Multifamily

## Building details

### Roof materials



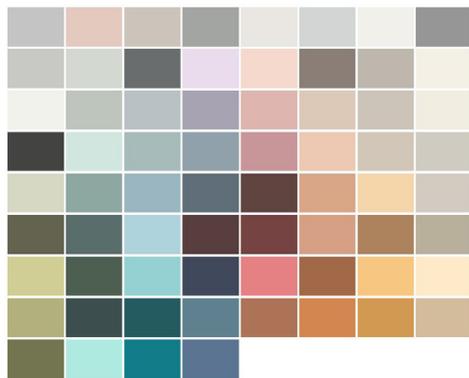
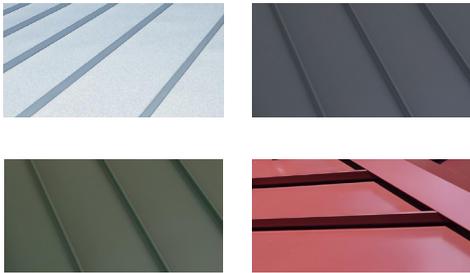
Architectural shingles. Dark green, medium brown, medium gray, or dark red in color.



Standing seam metal. 1 1/2" vertical seam with 12"-18" spacing. Silver metallic, medium gray, dark green, or dark red in color



Cedar or cypress shingles.



### Exterior cladding details



Full brick buildings as well as buildings that have brick foundations and clapboard or shingle cladding above are appropriate.



Wood or fiber cement clapboard siding in a variety of colors in whites, grays, earth tones, and pastels are appropriate. Exposures should be 8" or less.



Cedar shake or fiber cement simulated shake siding, stained, painted, or natural, is appropriate. Colors should be whites, grays, earth tones, or pastels.

### Windows and doors details



Doors and windows set in brick - recessed with brick mold with brick lintels or arches above and brick or wood/composite sills below.



Doors and windows set in clapboard siding with trim 3" to 6" in width. The door/window trim width should always be less than the width of the corner trim of the building.



Doors and windows set in cedar or cypress shake siding with 1 1/2" to 3" flat trim.

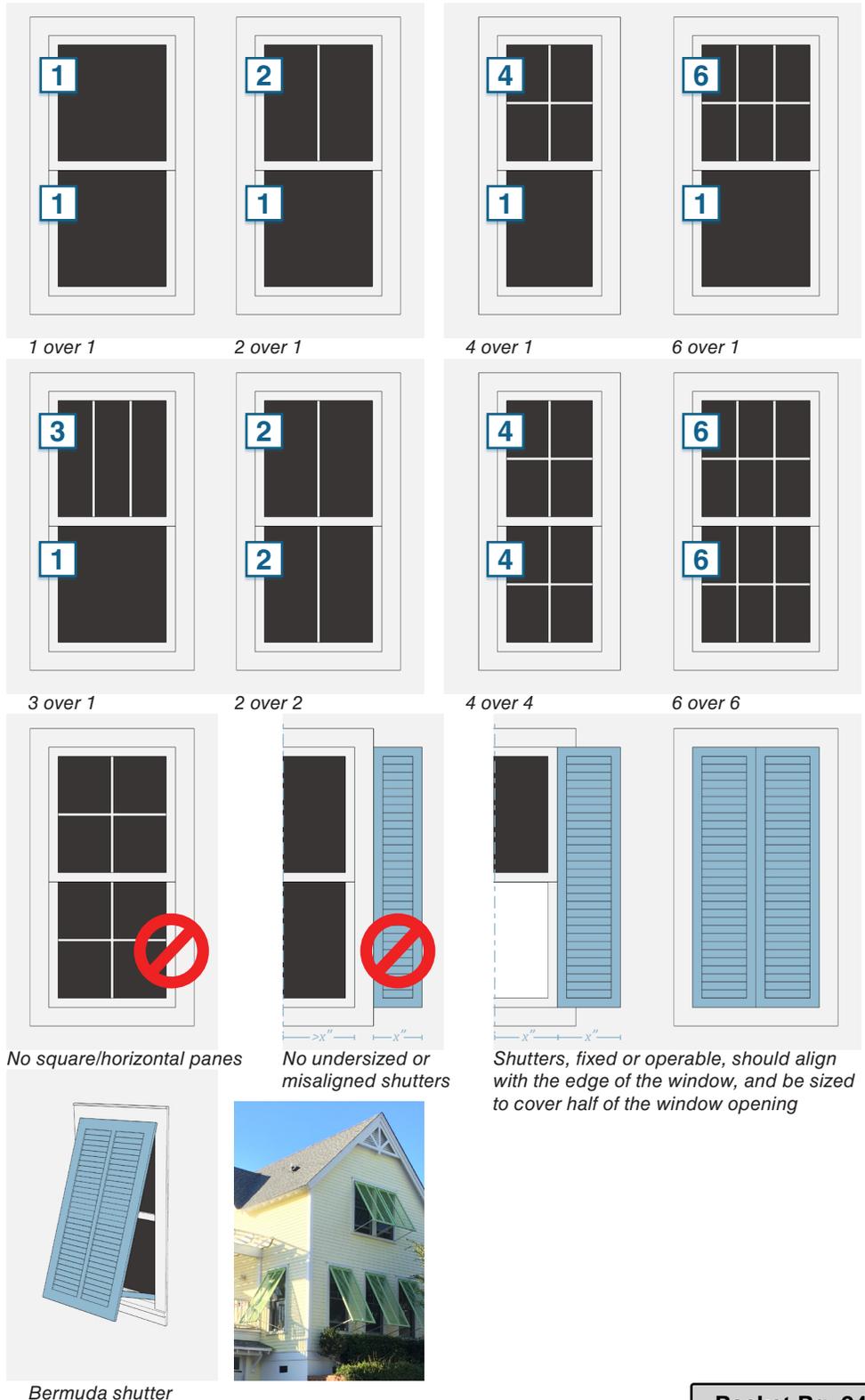
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# Center Station

## Commercial, Mixed Use, & Multifamily

### Double-hung windows

- Upper story windows, and windows in residential spaces on any level should be operable.
- Casement and awning windows are appropriate in circumstances where the window sill is higher than typical, such as in bathrooms or over a counter top.
- Double-hung windows are the most common and are appropriate in most circumstances.
  - Muntin divisions should result in individual panes (also called *lites/lights*) with a vertical proportion, rather than being square or horizontal.
  - Vary the number of divisions in differently-sized windows in order to maintain a similar pane proportion from one window to the next rather than maintaining the same number of divisions for every window.
- Operable shutters are preferred for protection from storm-borne debris as well as their functionality in maintaining privacy and reducing solar gain while windows are open in nice weather.
  - Shutters should always be sized (whether operable or not) so that they would fully cover the window when closed.
  - Mulled windows should not have shutters alongside them unless each shutter is sized to cover half of the mullered unit.
- Operable Bermuda shutters are a great solution in coastal environments to protect from hot sun and storm-borne debris.
  - Bermuda shutters should be sized to overlap the window by at least 1" on all sides.



# Center Station Commercial, Mixed Use, & Multifamily

## Building details

### Railing Styles



**Cable Railing**  
Contemporary style of railing with high visibility and horizontal emphasis



**Decorative Railing**  
Transitional style of railing with low visibility and horizontal emphasis



**Plank Railing**  
Rustic style of railing with low visibility and horizontal emphasis



**Picket Railing**  
Traditional style of railing with vertical emphasis, picket styles vary

### Column Styles



**Doric Column**  
Classical style



**Square Column**  
Simple style, tapered sides shown above

### Bracket Styles



**Ornate Bracket**



**Double Columns on plinths**  
Contemporary style. Plinths can be wood or masonry



**Square Column**  
Simple style, shown with brackets above



**Simple Bracket**

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# Center Station Public Space

Center Station, the gateway to Currituck Station, is a mixed-use sub-district. Commercial, office, and residential uses in this sub-district will produce more pedestrian activity than other sub-districts.

A public space is an area that a resident or visitor has access to from the public right-of-way (R.O.W.), typically a sidewalk in Center Station.

There are various types of public spaces appropriate for Center Station. The following requirements guide developing all public space in Currituck Station.



The Center Station public space patterns are intended to result in a walkable, pedestrian-scaled environment similar to the rendering above.

## Center Station Public Space

### Linear Lakes



#### *Linear lakes*

1. Tiered park that can store storm water temporarily.
2. The lowest tier contains continuously flowing water.
3. Paths and overlooks may be around the perimeter of the lake, along with site amenities.
4. Some appropriate site amenities are benches, trash receptacles, informational signage, path lighting, and tables with chairs.
5. The top-most tiers should be planted with native vegetation that can survive damp environments.

### Amenities



*Seating along edges of walkable paths*

1. Benches should remain lit at night.
2. Benches facing each other are encouraged.
3. Plaza amenities may vary. At a minimum, provide shaded seating and paved walking paths.



*Shaded Bike Racks and Drinking Fountain*

1. Located adjacent to paved walkways and public spaces.
2. Bike racks are typical near commercial and office functions.
3. Shade structure should not be opaque - allowing full visibility of any locked bikes.

# Center Station Public Space

## Plazas



*Mid-Block Plaza*

1. Located along paved walkway
2. Neighboring properties may share a plaza.
3. Plaza minimum size 40' x 40'
4. Plaza paving should be designed to reflect heat. Color to contrast adjacent paving or ground cover.
5. Plaza amenities may vary. At a minimum, provide shaded seating.



*Corner Plaza*

1. Located adjacent to paved walkways at one corner of a block.
2. Plaza minimum size 80' x 60'
3. Plaza paving to reflect heat, not glare. Color to contrast adjacent paving or ground cover.
4. Plaza amenities may vary. At a minimum, provide shaded seating and paved walking paths.

## Squares



*Mid-Block Square*

1. Located along paved walkway, between 2 businesses.
2. Adjacent businesses may use squares.
3. Square minimum size 60' x 80'
4. Square paving to reflect heat, not glare. Color to contrast adjacent paving or ground cover.
5. Square amenities may vary, at a minimum, provide shaded seating.



*Intersection Square*

1. Located at the nexus of adjacent sub-districts.
2. Vehicular traffic surrounds the square.
3. Square minimum size 100' x 100'
4. Maintain a buffer between vehicular traffic and pedestrians inside square.
5. Suitable buffers are a continuous row of trees, planters, and/or on-street parking.

# Center Station Public Space

## Parks



*Pocket Parks*

1. Located within the open space of one lot.
2. Typically between 100 - 200 sq. ft.
3. These small parks are most useful on lots with multiple residences.
4. Paving at perimeter of space should be of contrasting color.
5. Seating and landscaping are common amenities here.



*Neighborhood Playgrounds*

1. Located within or near large public space.
2. Neighborhood playgrounds must be visible from the right-of-way.
3. Provide non-opaque fencing that is continuous and secured around the perimeter.
4. Seating, water fountains, and trash receptacles are common amenities to include.



*Dog Parks*

1. Fencing around dog park should be continuous.
2. Wood and metal picket fencing are acceptable. Provide top caps so that collars won't get caught on pickets.
3. Planting at base of fence is encouraged, to eliminate burrowing.
4. When possible, provide water fountain for pets.
5. The ground cover should be artificial turf or seeded grass.
6. Ensure good drainage to prevent muddy areas from regular wear & tear
7. A trash container should be provided for collection of animal waste.



*Fitness Stations*

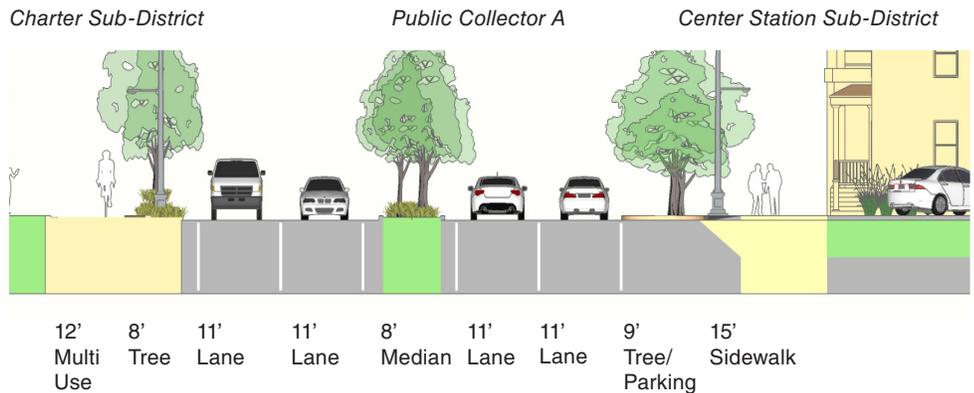
1. Paving should be soft and durable, rubber tiles and pourable rubber compounds are typical.
2. Paving color should have a low heat reflectivity.
3. Fitness station paving to reflect heat, not glare. Color to contrast adjacent paving or ground cover.
4. Fitness station amenities may vary, at a minimum, provide shaded seating and paved walking paths.

# Center Station Public Right-of-Way

## Collector streets

### Center Station - Charter Boundary

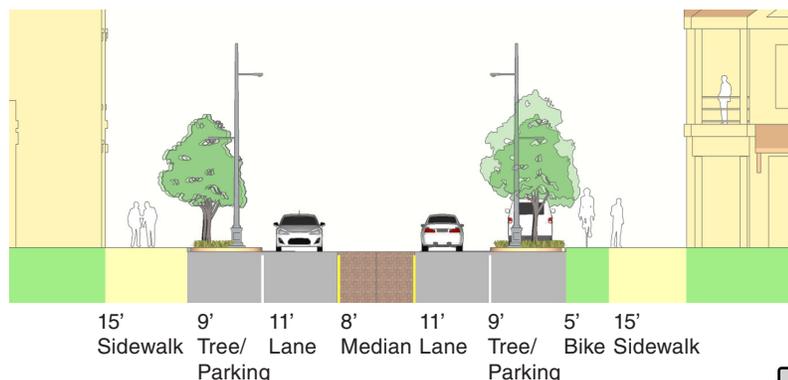
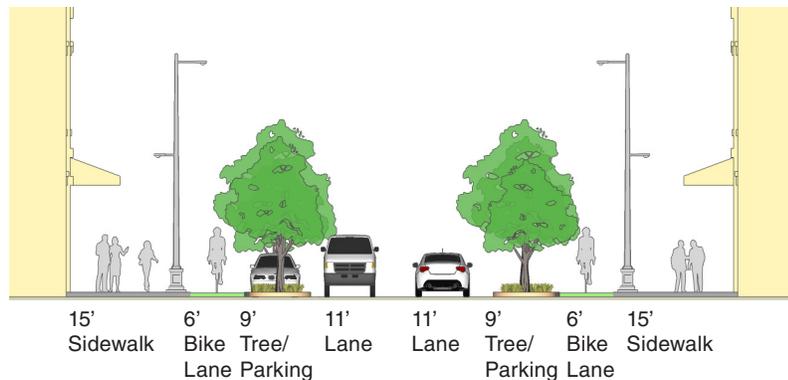
1. The lower density development of the Charter sub-district allows for a wide multi-use path that accommodates both pedestrians and cyclists along a pleasant, meandering path.
2. The 2 travel lanes exiting the development allow for morning rush traffic to more effectively stack at Caratoke Highway.
3. Parallel parking, and generous sidewalks help establish the pedestrian-friendly, retail-oriented nature of the Center Station sub-district.



## Local streets

### Interior Street Options

1. Sidewalks are sized to accommodate pedestrian movement as well as pedestrian oriented amenities such as seating areas and outdoor dining.
2. The bicycle lane is protected from traffic by a row of parallel parking and trees.
3. The parallel parking lane is used to accommodate tree wells at regular intervals of 2 to 3 parking spaces.
4. Trees shall be placed at regular intervals and of a species sized to provide continuous shade to pedestrians.
5. Lighting shall be provided for both vehicular and pedestrian paths; lighting shall be 2700K maximum in color temperature and provide a CRI of 80 or better.



# Center Station Public Right-of-Way

## Pedestrian paths

1. Pedestrian paths in Center Station are wide enough to accommodate multiple uses.
2. Along East/ West Streets, are bike lanes on either side of the roadway.
3. Along North/South streets, the bike lane is located on the least commercially dense side to interfere with as few pedestrians as possible.
4. The pedestrian paths at the connection of Center Station and the neighboring Charter sub-district are wide multi use paths that can be populated by either cyclists, or ambulatory pedestrians.



10' 10' 5' 10'  
Center Station - Interior Streets - East/West travel direction



Center Station - Interior Street - North/South travel direction



Center Station - Charter Boundary 2' 12' 6'

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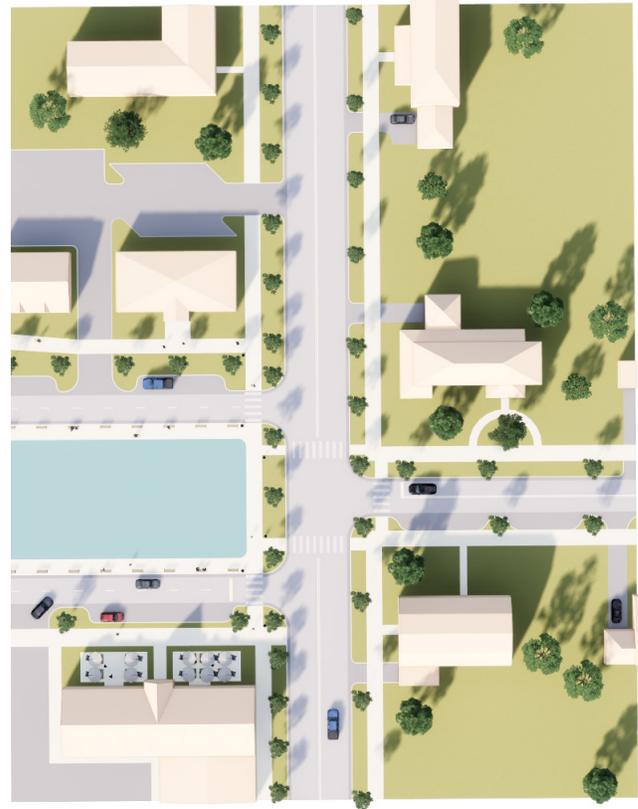
The following pages provide examples of building massing and architectural styles that are compatible with the Currituck Station design intent. Designers are not expected to copy these, only to use them as a guide towards their own compatible designs.

## Charter Development Principles

The Charter sub-district provides a more conventional development pattern that reinforces the walkable nature of the Center Station Sub-District. The pattern of development includes a grid of streets supporting pedestrian activities, on-street parallel or angled parking, wide sidewalks with consistent tree canopy, outdoor café seating, and public spaces such as plazas and parks.

To reinforce the walkable, pedestrian-oriented nature of Charter’s adjacent sub-district, off-street parking lots are located behind buildings or vegetated screens, towards the interior of the lot. Development is designed to support vibrant, walkable streets by placing buildings close to the street with active and public uses on the ground floor and pedestrian entrances accessed from the street. An interconnected network of sidewalks, multi-use paths, public spaces, and parks provide access to the mix of uses found throughout the sub-district.

The Charter sub-district is ideal for vertical mixing of residential, retail, and/or office functions within one building. Horizontal mixing of functions, within separate buildings is permitted with attention to detail through architectural and aesthetic treatments, integrated signage, landscape and buffer enhancements, and interconnected developments.



### Dimensions

The allowable dimensions for the Charter sub-district are designated in the Currituck Unified Development Ordinance.

### Allowable uses

This is an abbreviated table of uses for reference only. For a current and complete table of uses, see the Unified Development Ordinance

#### Residential Uses

- Multifamily
- Single Family (Attached)
- Mixed-Use

#### Commercial Uses

- Retail
- Office
- Health Care
- Restaurant
- Recreation
- Hotel

#### Industrial Uses

- Warehouse

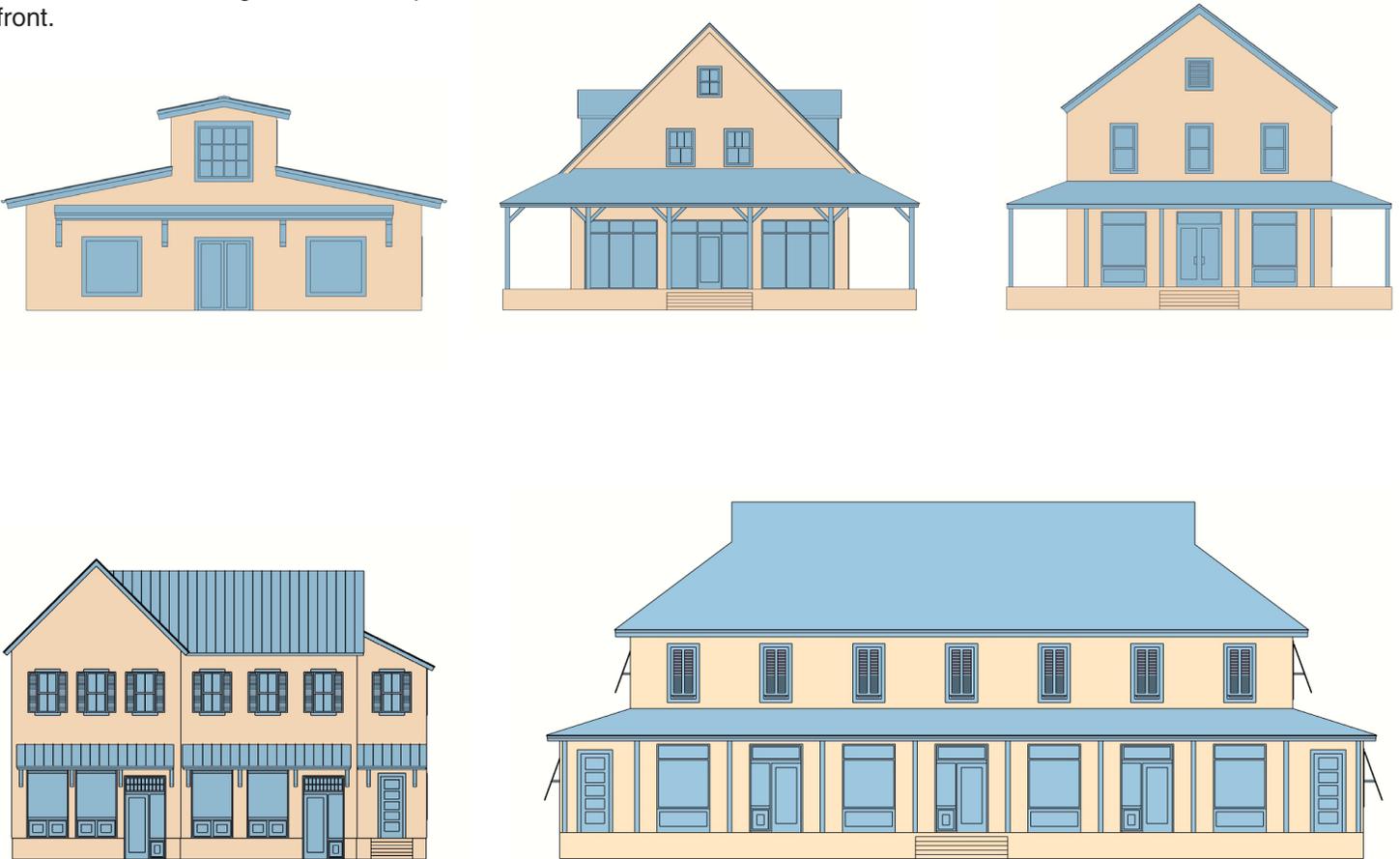
#### Civic Uses

- Public Space
- Community
- Education

# Charter Commercial & Mixed Use Buildings

Charter sub-district commercial and mixed use developments follow the patterns of the small and medium Center Station buildings.

Additions such as drive-through canopies and garage doors support the more auto-oriented nature of the Charter sub-district. These additions should be placed at the rear of the building to maintain a pedestrian oriented front.

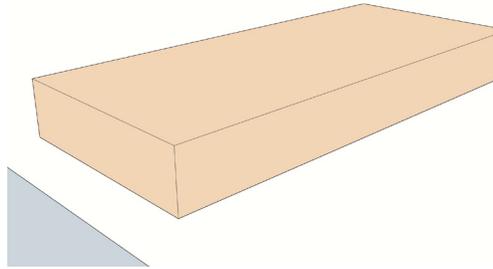


## Charter Commercial & Mixed Use Buildings

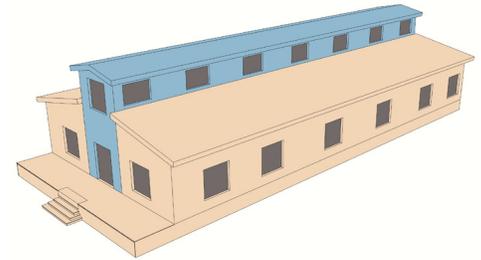
### Small 2-story building

#### Clerestory building

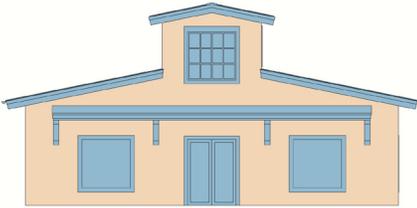
1. Use features such as clerestory to give a small building a 2-story appearance. The second floor may or may not be occupied.
2. Simple details give the building a pedestrian-friendly facade while keeping costs low.



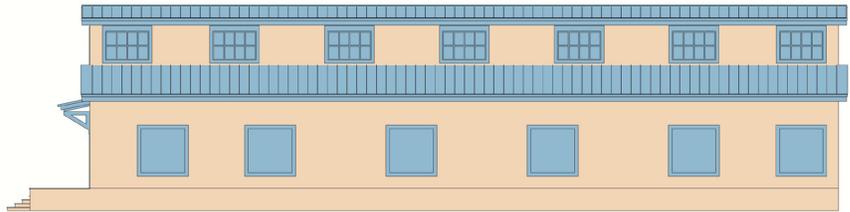
*A 1-story building program in a rectangular footprint. A 1-story building can only be accommodated if the facade is at least 20' tall giving a 2-story appearance.*



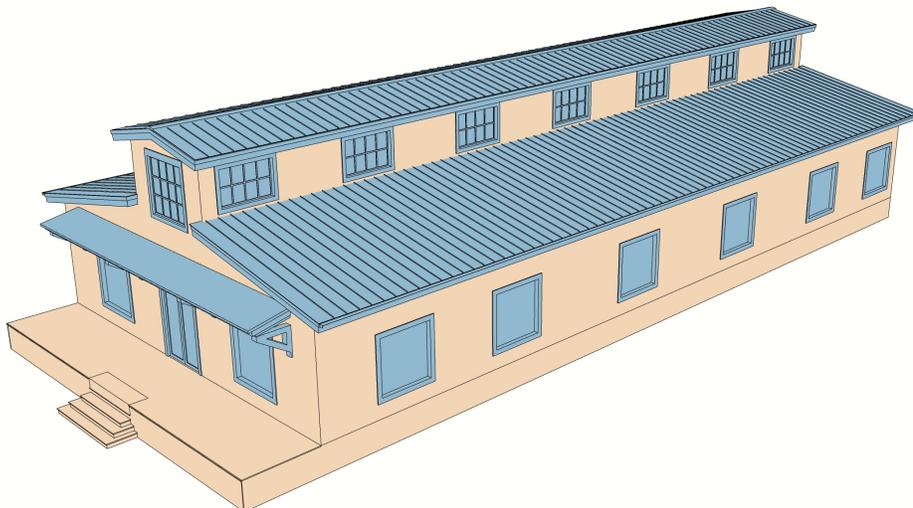
*Adding a clerestory gives the building a usable 2nd story for offices or studio apartments. Alternatively, the clerestory can be used to provide additional natural light and an airy feeling to the 1st floor space while meeting the 20' building height requirement.*



*A front facade with doors and generous windows centered on bays is functional and pedestrian friendly. The width of the clerestory section can be adjusted to suit the building program needs.*



*Ground floor side windows near the street are necessary to maintain visual permeability of the street frontage. Side windows towards the rear of the building are optional, but encouraged.*



*A small, simple fixed awning with support brackets and simple trim profiles add detail to the building and give it a coastal appearance.*

## Charter Commercial & Mixed Use Buildings

### 1. Roof

Standing seam metal due to low pitch

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Board & batten, shake, or clapboard

### 4. Clerestory windows

Divided lite, clear glass windows

### 5. Awning

Sloped with simple brackets

### 6. Railings

Storefront doors with clear (untinted) glass

### 8. Ground floor windows

Storefront or folding glass windows with clear (untinted) glass

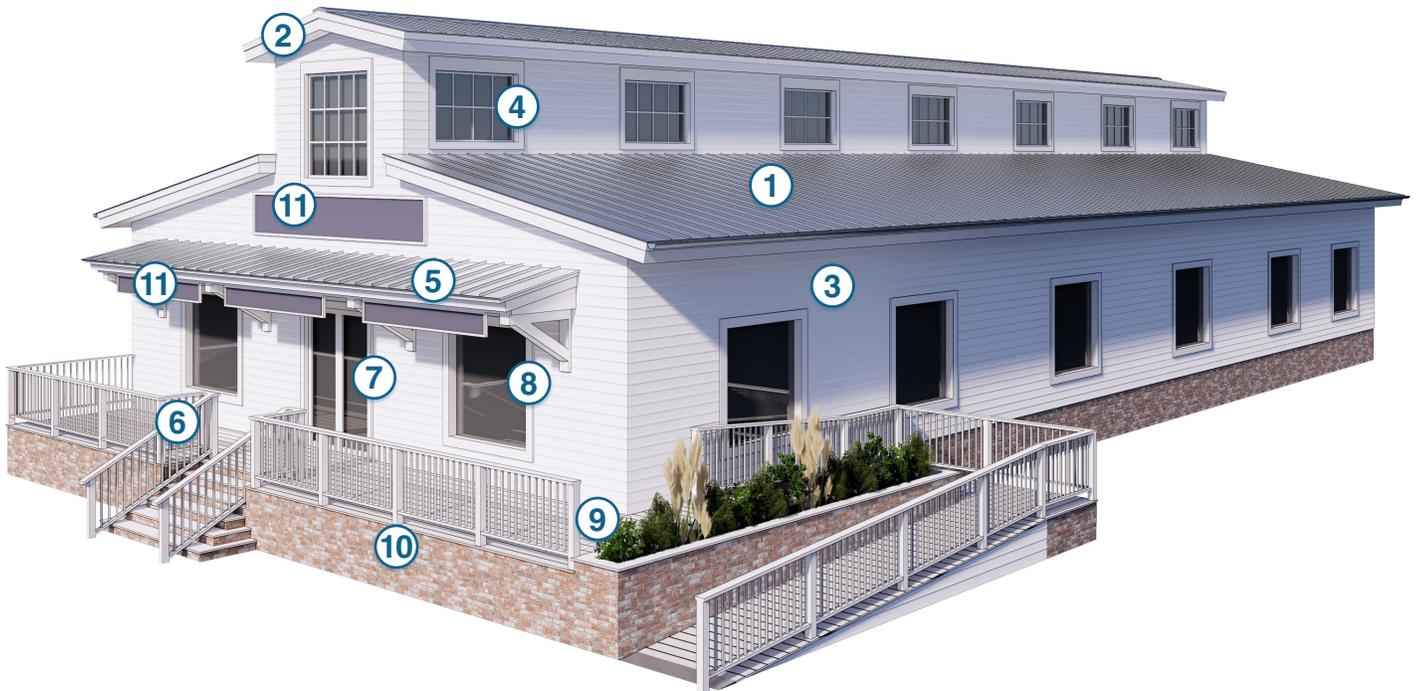
### 9. Decking

### 10. Foundation

Solid in appearance

### 11. Sign boards

Recessed or raised perimeter to differentiate from cladding



Local Pie in Bluffton, SC



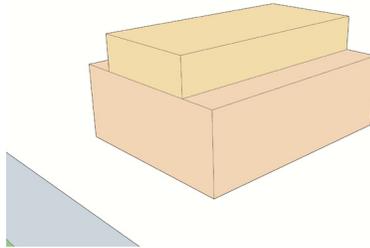
The Hitching Post (The Spry Store) in Currituck, NC

# Charter Commercial & Mixed Use Buildings

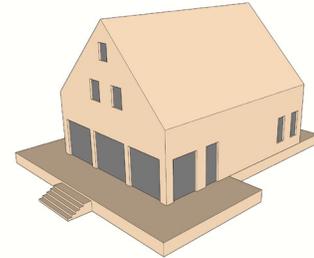
## Small 2-story building

### A-Frame with Porch

1. An iconic building form along the eastern seaboard, the A-frame is easy to construct, weathers storm events, and provides ample floor space while presenting a compact facade to the public.
2. A deep wrap-around porch shades the windows from hot summer sun and makes an inviting place for customers to enjoy.



A 2-story building program in a rectangular footprint.



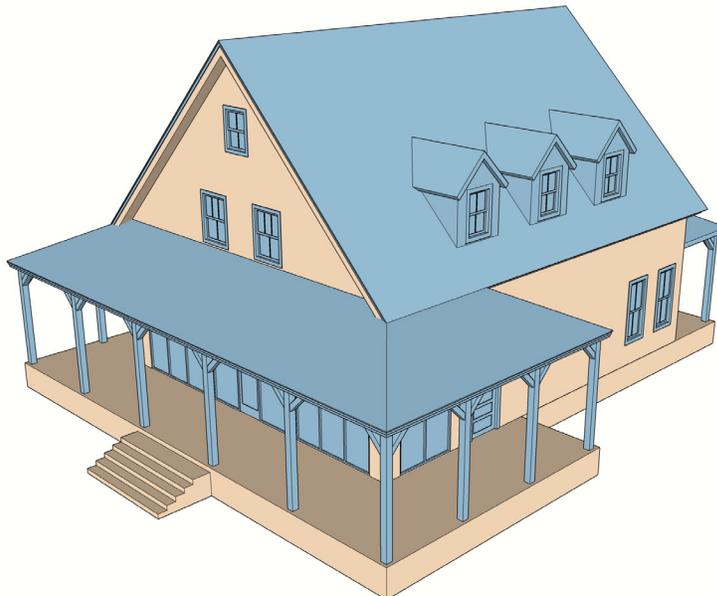
A steeply pitched roof encompasses the entire upper story.



A front facade with storefront glazing and transom windows allows natural light into the lower story space and invites pedestrians in.



Dormers let light into the upper story space and provide additional floor space. If the upper story is to be occupied by a different tenant, or is for a residential use, then a separate side entry can be added.



Straight or braced columns support the deep porch. Dormers can be separate or combined into one large dormer.

## Charter Commercial & Mixed Use Buildings

### 1. Roof

Shingle or standing seam metal

### 2. Trim

Simple profile, multi-layered

### 3. Upper story windows

Divided lite, clear glass windows

### 4. Porch roof

Shingle or standing seam metal

### 5. Columns and brackets

Wood or composite

### 6. Ground floor windows

Storefront with clear (untinted) glass

### 7. Doors

Storefront with clear (untinted) glass

### 8. Railings and stairs

### 9. Decking

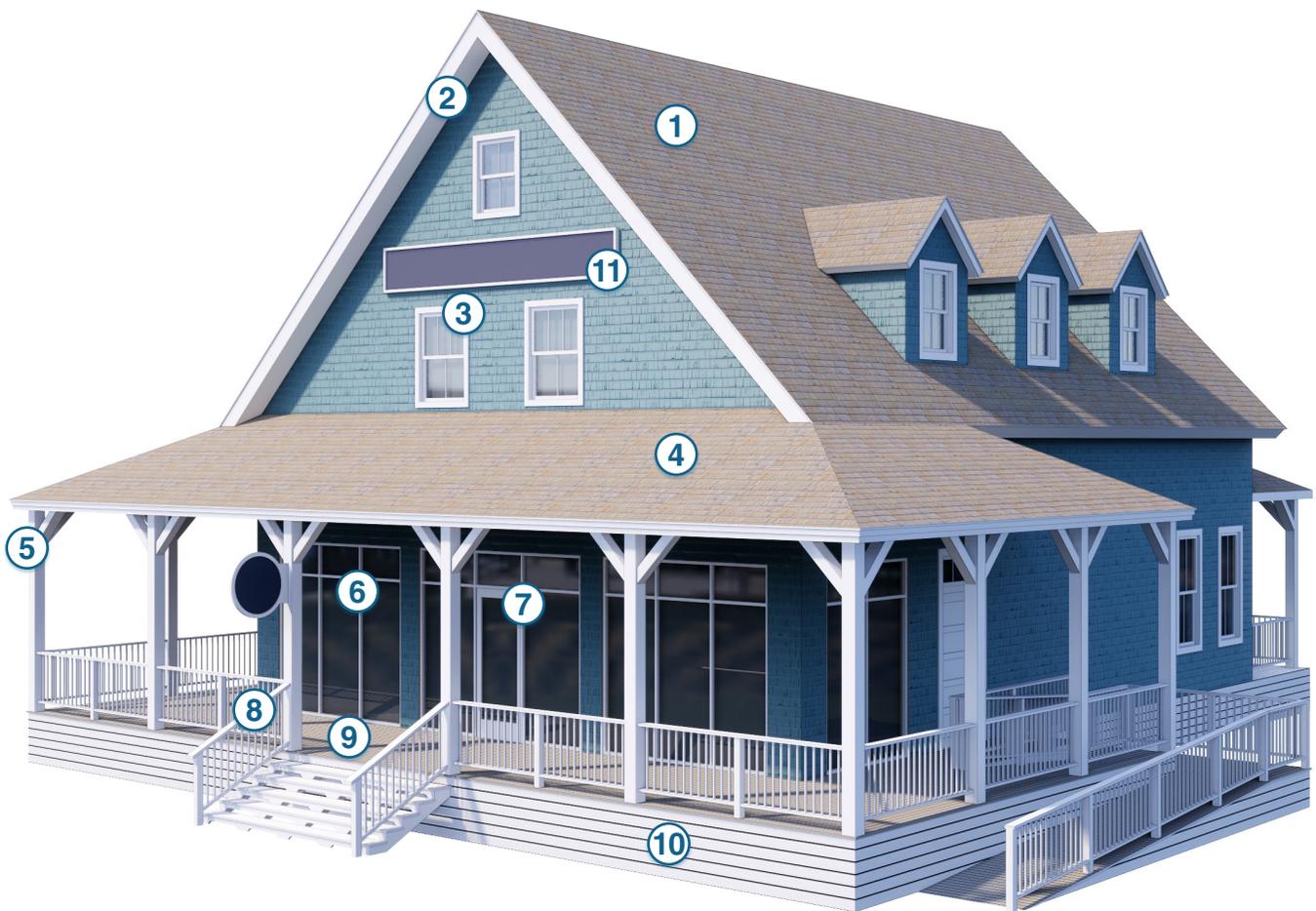
Wood (rot resistant) or composite

### 10. Foundation skirt

Solid or slatted in appearance

### 11. Sign board

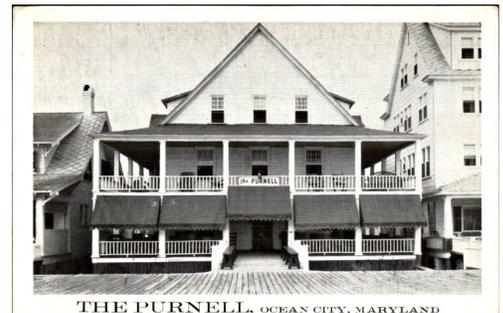
Recessed or raised perimeter to differentiate from cladding



Caffey's Inlet Lifesaving Station  
in Dare County, NC



Seatack Station, Virginia Beach, VA



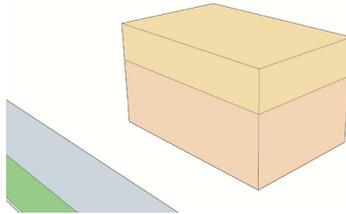
The Purnell Hotel, Ocean City, MD

## Charter Commercial & Mixed Use Buildings

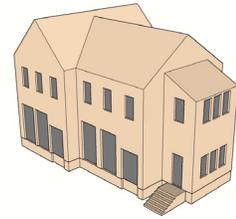
### Medium 2-story building

#### L-shaped

1. Use gable ends to break up longer facades and create the appearance of a series of smaller masses.
2. Simple details give the building a pedestrian friendly facade while keeping construction costs low.



A 2-story building program in a rectangular broadfront footprint.



Adding a gable end breaks up the building facade and provides multiple distinct storefronts.



Storefront windows and doors allow pedestrians to see into the ground floor spaces. A stair and elevator tower on the end of the building gives private access to the upper story if desired.



A drive-through canopy added to the rear of the building allows auto-oriented uses to occupy the building while maintaining the vernacular character



Awnings with brackets and shutters protect the windows from sun and rain.

## Charter Commercial & Mixed Use Buildings

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Masonry, shake, or clapboard

### 4. Upper story windows

Divided lite, clear (untinted) glass windows

### 5. Shutters

1/2 width and full height of window, operable louvered or solid shutters

### 6. Awning

### 7. Doors

Storefront with transoms and clear (untinted) glass

### 8. Ground floor windows

Storefront with transoms with clear (untinted) glass and optional opaque paneling

### 9. Foundation

Solid in appearance

### 10. Residential door

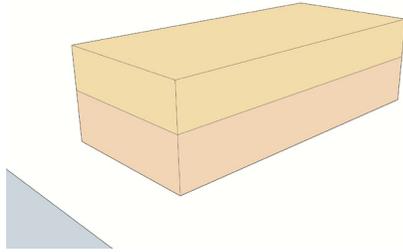


# Charter Commercial & Mixed Use Buildings

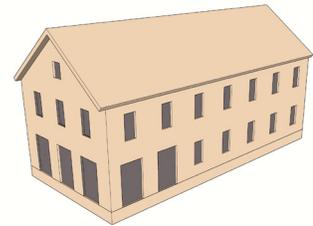
## Medium 2-story building

### Gable-front building

1. A classic and versatile building form lends itself well to a vertical mix of uses.
2. Simple details give the building a pedestrian friendly facade while keeping costs low.



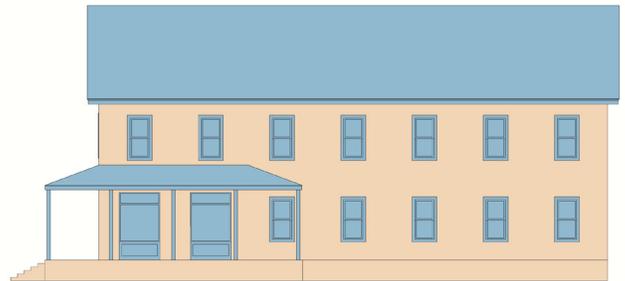
A 2 to 3-story building program in a rectangular footprint.



A gable above the upper story makes an easy-to-construct, adaptable, and durable building.



Minimal overhangs are typical, but the eaves can be extended up to 3' or 4' with exposed rafter tails for a more Craftsman appearance.



Evenly spaced side windows on all levels produce usable light throughout the building.



The wraparound porch can be supported by simple square columns or brackets.

# Charter Commercial & Mixed Use Buildings

**1. Roof**

Standing seam metal or shingle

**2. Trim**

Simple profile, multi-layered

**3. Upper story windows**

Single or divided lite, clear (untinted) glass windows

**4. Porch roof**

Match main roof or standing seam metal

**5. Columns and brackets**

4"-8" square or round

**6. Ground floor windows**

Storefront or folding glass windows with clear (untinted) glass

**7. Doors**

Storefront with clear (untinted) glass

**8. Railings and stairs**

**9. Decking**

Wood (rot resistant) or composite

**10. Foundation skirt**

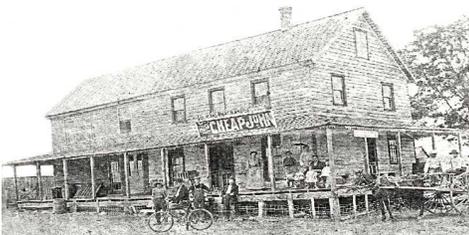
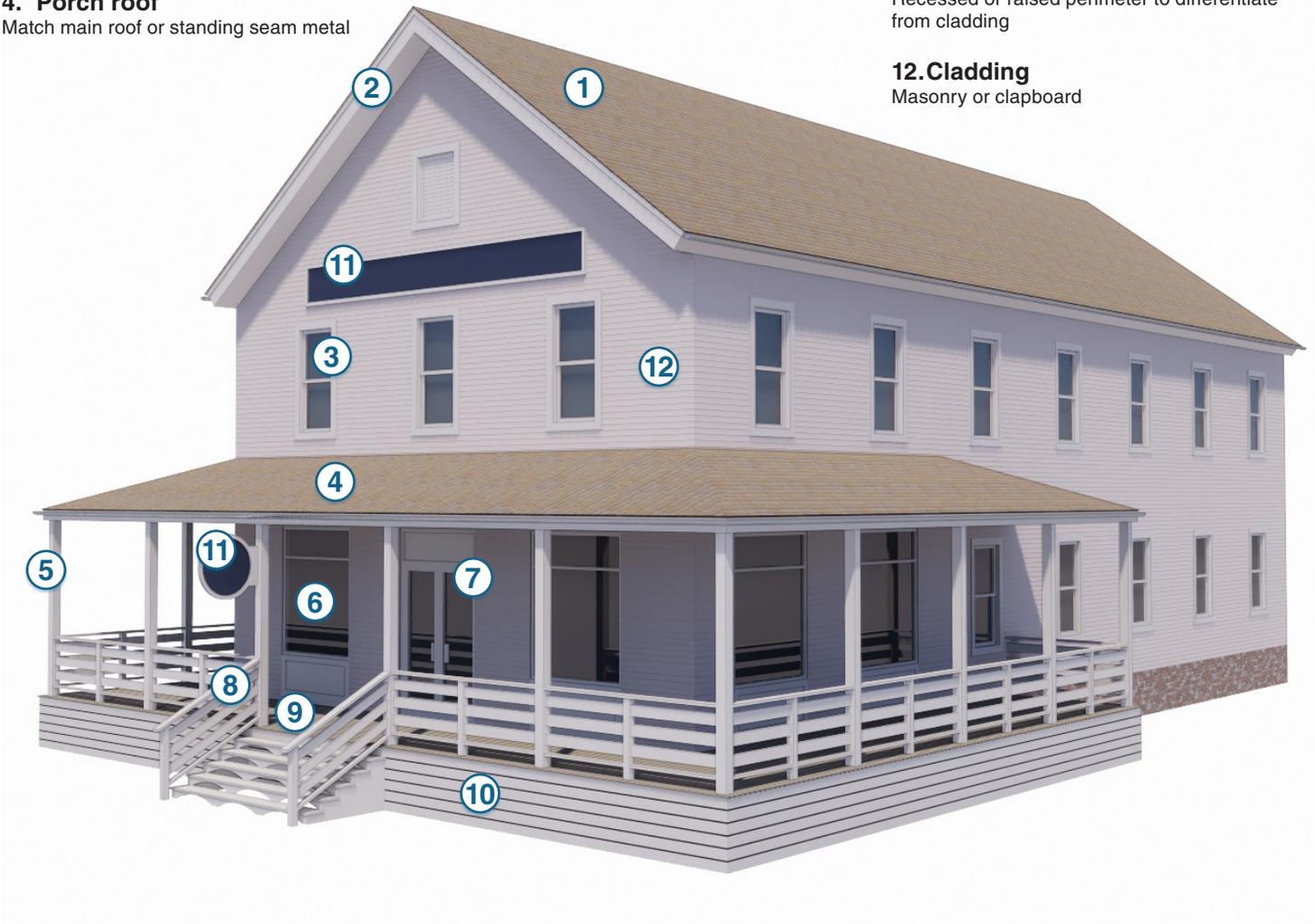
Solid in appearance with optional slatted material at porch

**11. Sign board**

Recessed or raised perimeter to differentiate from cladding

**12. Cladding**

Masonry or clapboard



Cheap John's Store, Currituck County, NC



Creekmore Brothers Store, Currituck County, NC



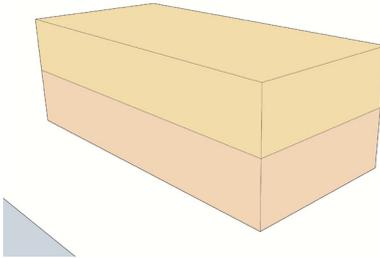
JM Roberts House, Currituck County, NC

# Charter Commercial & Mixed Use Buildings

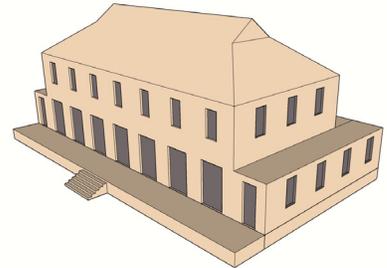
## Medium 2-story building

### Broad-front building

1. Occupy the frontage of a wide site and provide continuous street-level access to multiple tenants.
2. Upper stories can be used for offices or residential and are pulled back from the street.



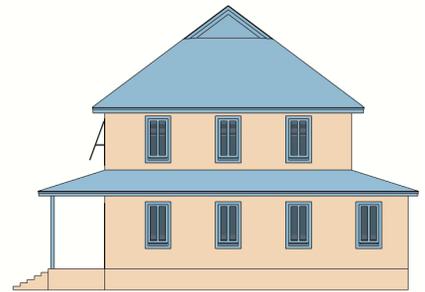
A 2 to 3-story building program in a rectangular broadfront footprint.



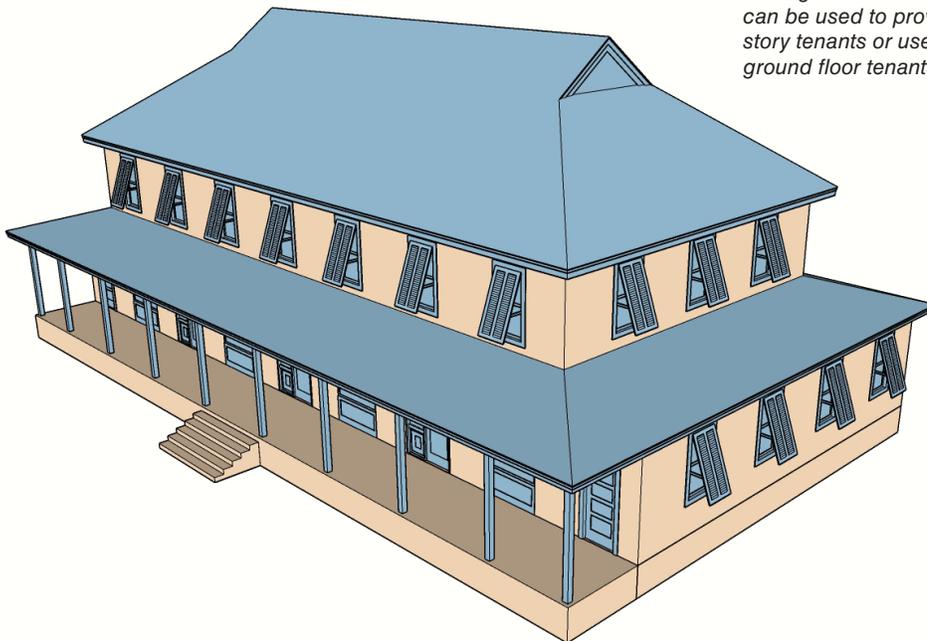
Closely spaced large openings at the ground floor are important to making the long facade feel permeable and welcoming to pedestrians.



A broad front facade allows for more space behind the building to be used for parking and allows for more individual tenants to have street frontage.



Ground floor side windows are necessary to maintain the visual permeability of the street frontage. The enclosed areas under the porch roof can be used to provide building access for upper story tenants or used as enclosed porches for ground floor tenants.



A gabled roof is used here, but gable or hipped roofs would also be appropriate. Bermuda shutters have been used to give the building a coastal appearance.

## Charter Commercial & Mixed Use Buildings

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Shake or clapboard

### 4. Upper story windows

Double hung, divided lite, clear (untinted) glass windows with shutters

### 5. Shutters

Bermuda shutters. full width and length of window

### 6. Porch roof

Match main roof

### 7. Columns

Square or round simple columns

### 8. Doors

Storefront with transoms with clear (untinted) glass

### 9. Ground floor windows

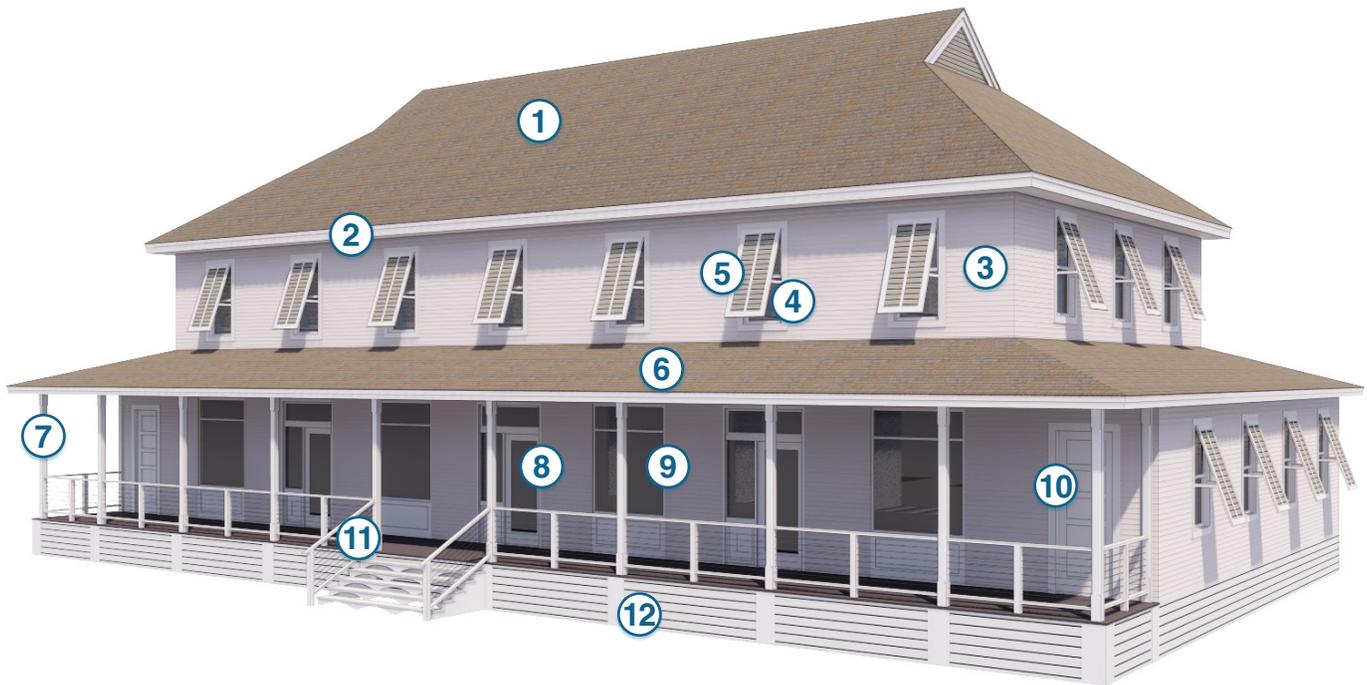
Storefront with transoms with clear (untinted) glass

### 10. Residential/Upper story door

### 11. Railing and stairs

### 12. Foundation skirt

Solid or slatted in appearance





# Charter Multifamily Buildings

Multifamily development in the Charter sub-district should be composed of neighborhood-scale buildings that can be clustered around a common amenity or parking area if a greater number of units is desired.

Multifamily buildings of this type can support up to 12 units. Shown in this section are examples of a duplex, a triplex, a courtyard apartment, and a mansion apartment. These basic building types can be scaled up or down to accommodate the site and number of units in the development.

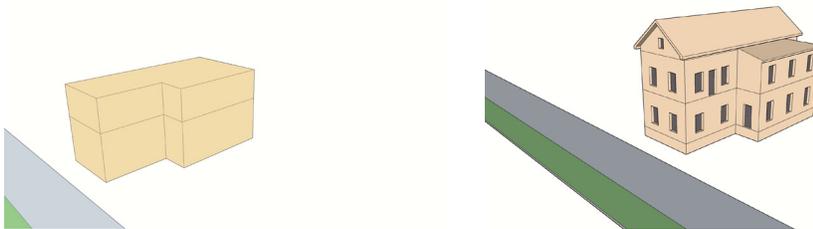


# Charter Multifamily Buildings

## Small 2-story building

### Duplex Style Apartment

1. A 2-story long, narrow building with a recessed side extension presents a narrow facade to the street.
2. The recessed entry provides privacy to the residents while giving the street frontage more depth.



A 2 to 3-story building program in a rectangular footprint. A side entry provides privacy on busy streets.



The narrow front facade is easily made pedestrian-friendly with just a few windows.



With the entries on the side, they can be less ornate and still be appropriate.



The side porches add detail to the front elevation while providing a private outdoor space for residents.

## Charter Multifamily Buildings

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Masonry, shake, or clapboard

### 4. Upper story windows

Operable, divided lite, clear (untinted) glass windows

### 5. Porch roof

Match main roof

### 6. Columns

Square 4"-8"

### 7. Doors

Storefront with transoms with clear (untinted) glass

### 8. Ground floor windows

Storefront with transoms with clear (untinted) glass

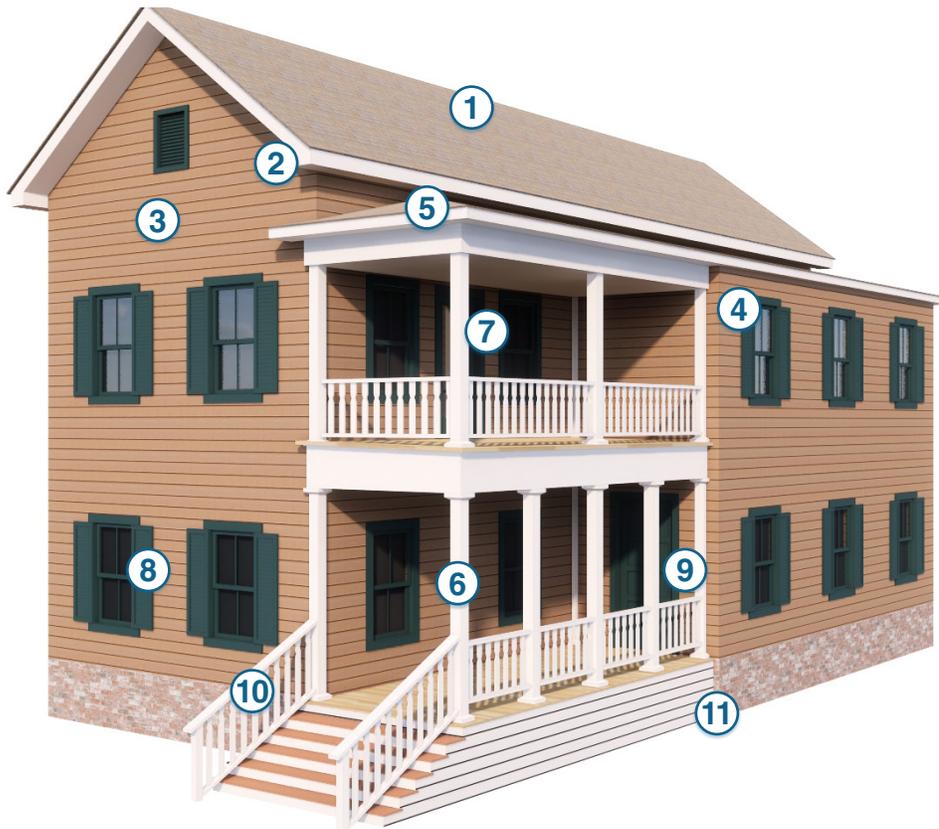
### 9. Building Main Entry

Residential entry system

### 10. Railings

### 11. Foundation skirt

Solid appearance with optional slatted appearance at porch

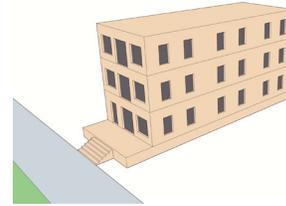
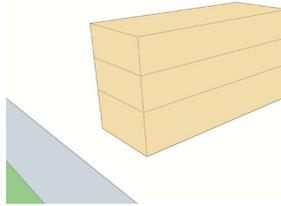


# Charter Multifamily Buildings

## Small 3-story building

### Triplex Style Apartment

1. A narrow building with 3 stacked apartments.
2. A compact footprint and simple detailing keep construction costs low while providing high density.

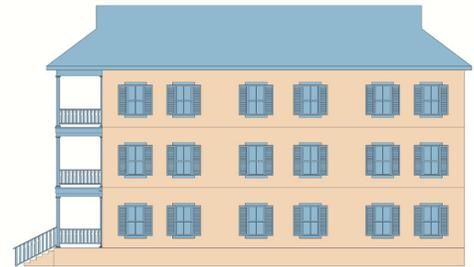


A 2 to 3-story building program in a rectangular footprint.

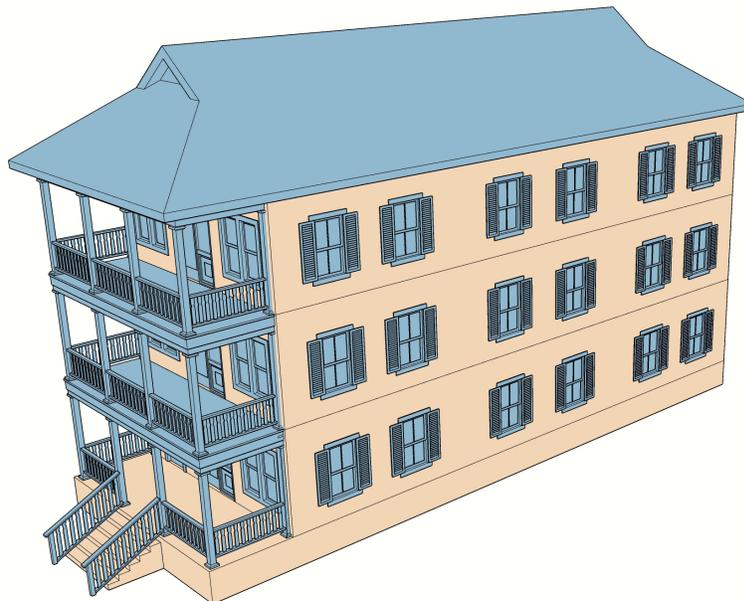
A side entry provides privacy on busy streets.



The front facade is a simple composition with regularly spaced windows and doors.



Windows should be placed to daylight and ventilate all interior spaces.



A hipped or gabled roof helps to step the mass of the building back.

## Charter Multifamily Buildings

### 1. Roof

standing seam metal or shingle

### 2. Trim

Simple profile

### 3. Cladding

Masonry, shake, or clapboard

### 4. Windows

Operable, divided lite, with clear (untinted) glass

### 5. Columns

Square 4"-8"

### 6. Building Main Entry

Residential or commercial entry system

### 7. Railings

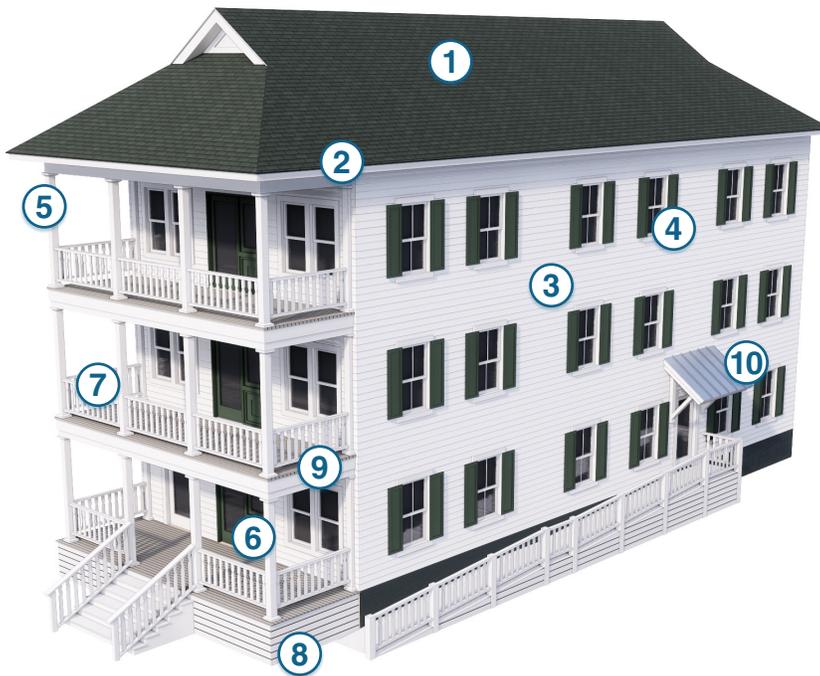
### 8. Foundation skirt

Solid appearance with optional slatted appearance at porch

### 9. Porch Decking

### 10. Awnings

Match main roof or standing seam metal with wood or metal structure

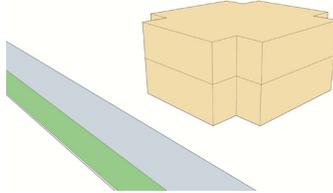


# Charter Multifamily Buildings

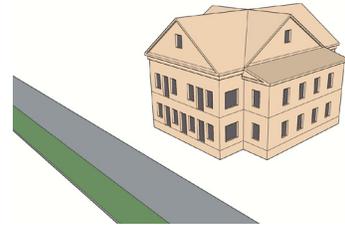
## Medium 2-story building

### Mansion Style Apartment

1. Intended to appear like a large house that has been broken up into smaller dwelling units, the design should be that of a large residence.
2. Simple details give the building a pedestrian friendly facade while keeping costs low.



A 2 to 3-story building program in a rectangular or square footprint.



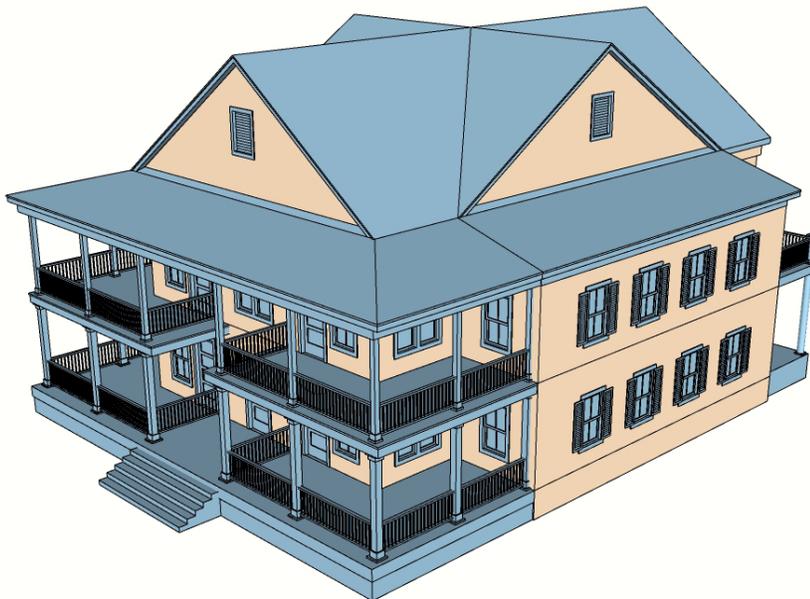
Typical residential windows and details are appropriate.



A front facade with doors and generous windows centered on bays is functional and pedestrian friendly.



The windows should continue around the perimeter of the building to provide daylighting for all of the interior spaces.



Wrap-around porches provide outdoor space for residents and accommodate a coastal lifestyle.

## Charter Multifamily Buildings

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Brick, shake, or clapboard

### 4. Windows

Operable, divided lite, clear (untinted) glass windows

### 5. Porch roof

Match main roof

### 6. Columns

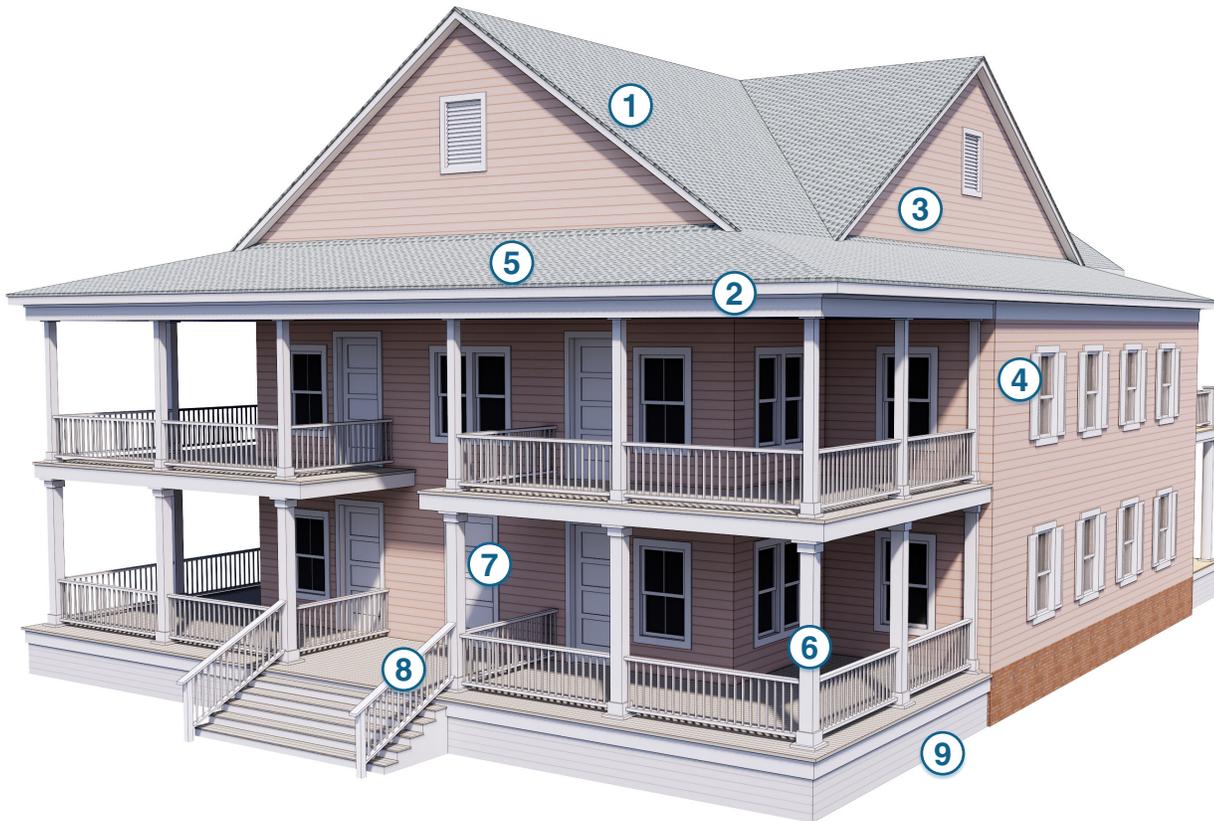
Square 6"-12"

### 7. Entry Doors

### 8. Railings

### 9. Foundation skirt

Solid appearance with optional slatted appearance at porch

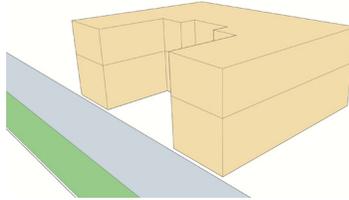


# Charter Multifamily Buildings

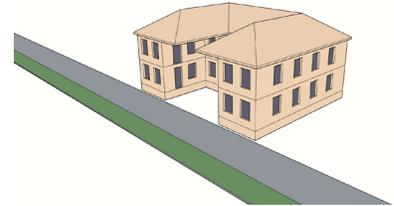
## Medium 2-story building

### Courtyard Style Apartment

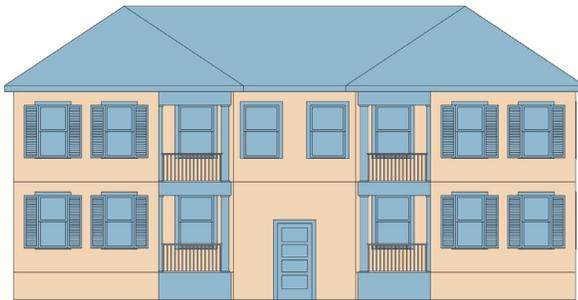
1. A multifamily building with a small scale.
2. A courtyard allows all units to be naturally lit from multiple directions.



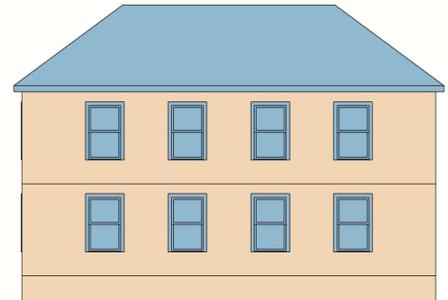
A 2 to 3-story building program in a square or rectangular footprint with a courtyard.



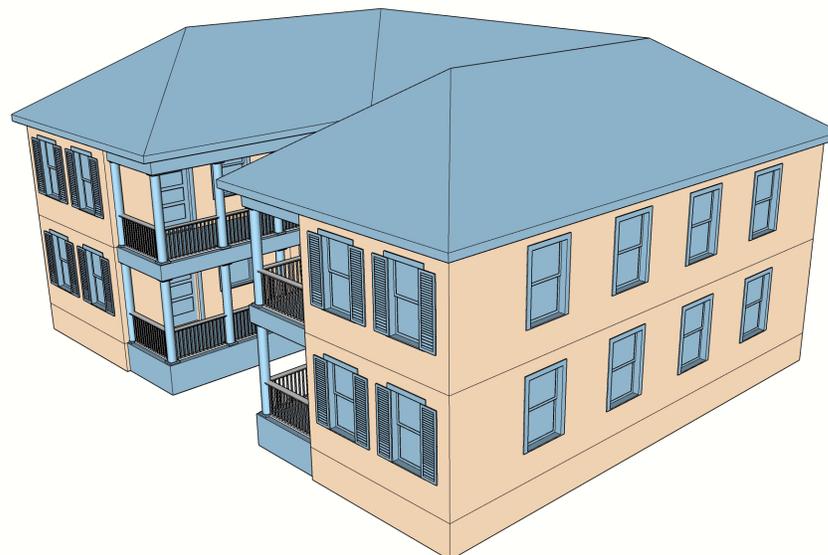
Windows and porches are placed to take advantage of the natural light available around the exterior perimeter, and the privacy afforded by the courtyard.



A central courtyard provides a semi-private entrance and porch area while maintaining the building facade line of the block.



Ground floor side windows are necessary to maintain the visual permeability of the street frontage. Side windows towards the rear of the building are optional, but encouraged.



Roof and window types, awnings, trim, and brackets add detail to the building and give it a coastal appearance.

## Charter Multifamily Buildings

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Masonry, shake, or clapboard

### 4. Upper story windows

Operable, divided lite windows with clear (untinted) glass

### 5. Columns

Square or round

### 6. Entry Doors

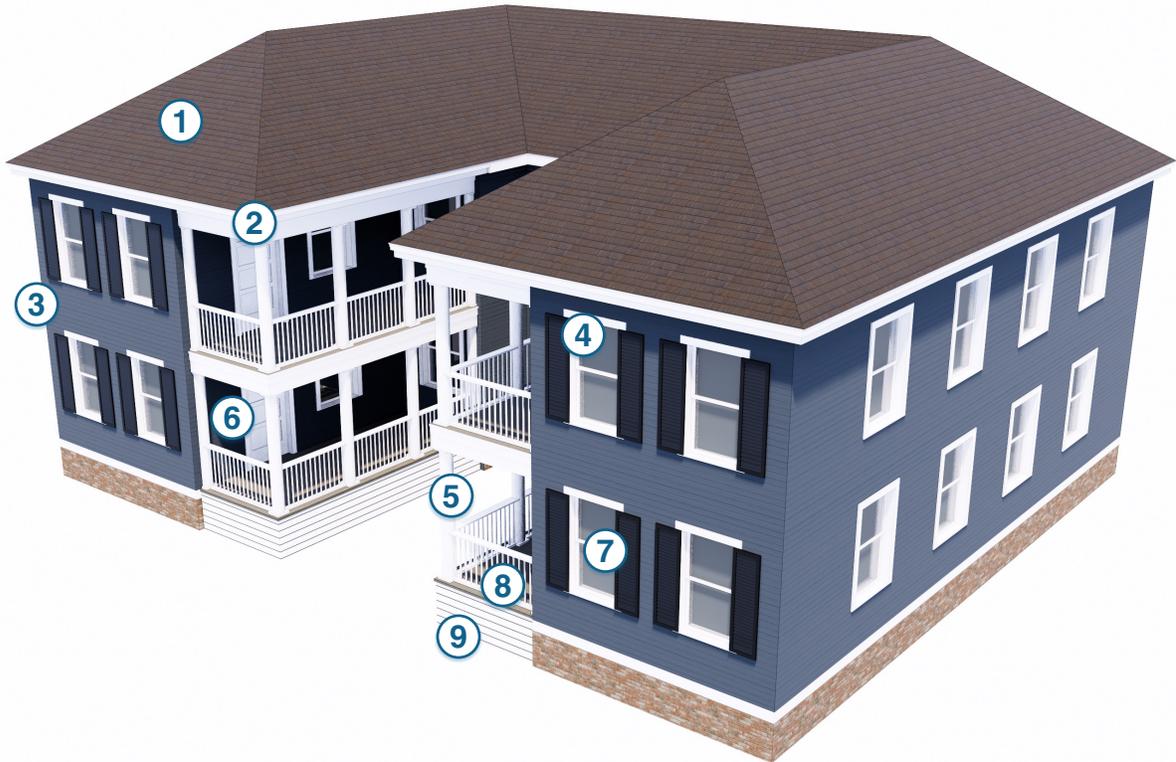
### 7. Ground floor windows

Storefront with transoms and clear (untinted) glass

### 8. Railings

### 9. Foundation skirt

Solid appearance with optional slatted appearance at porches



# Charter Commercial, Mixed Use & Multifamily

## Building details

### Roof materials



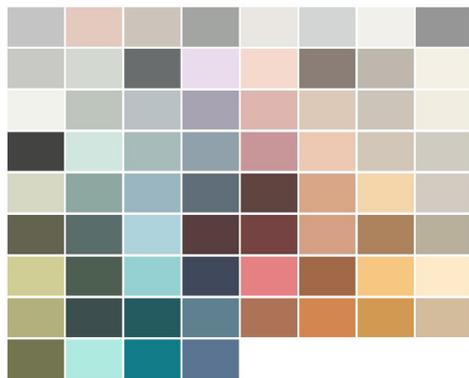
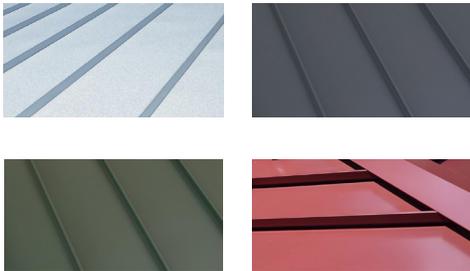
Architectural shingles. Dark green, medium brown, medium gray, or dark red in color.



Standing seam metal. 1 1/2" vertical seam with 12"-18" spacing. Silver metallic, medium gray, dark green, or dark red in color



Cedar or cypress shingles.



### Exterior cladding details



Full brick buildings as well as buildings that have brick foundations and clapboard or shingle cladding above are appropriate.



Wood or fiber cement clapboard siding in a variety of colors in whites, grays, earth tones, and pastels are appropriate. Exposures should be 8" or less.



Cedar shake or fiber cement simulated shake siding, stained, painted, or natural, is appropriate. Colors should be whites, grays, earth tones, or pastels.

### Windows and doors details



Doors and windows set in brick - recessed with brick mold with brick lintels or arches above and brick or wood/composite sills below.



Doors and windows set in clapboard siding with trim 3" to 6" in width. The door/window trim width should always be less than the width of the corner trim of the building.



Doors and windows set in cedar or cypress shake siding with 1 1/2" to 3" flat trim.

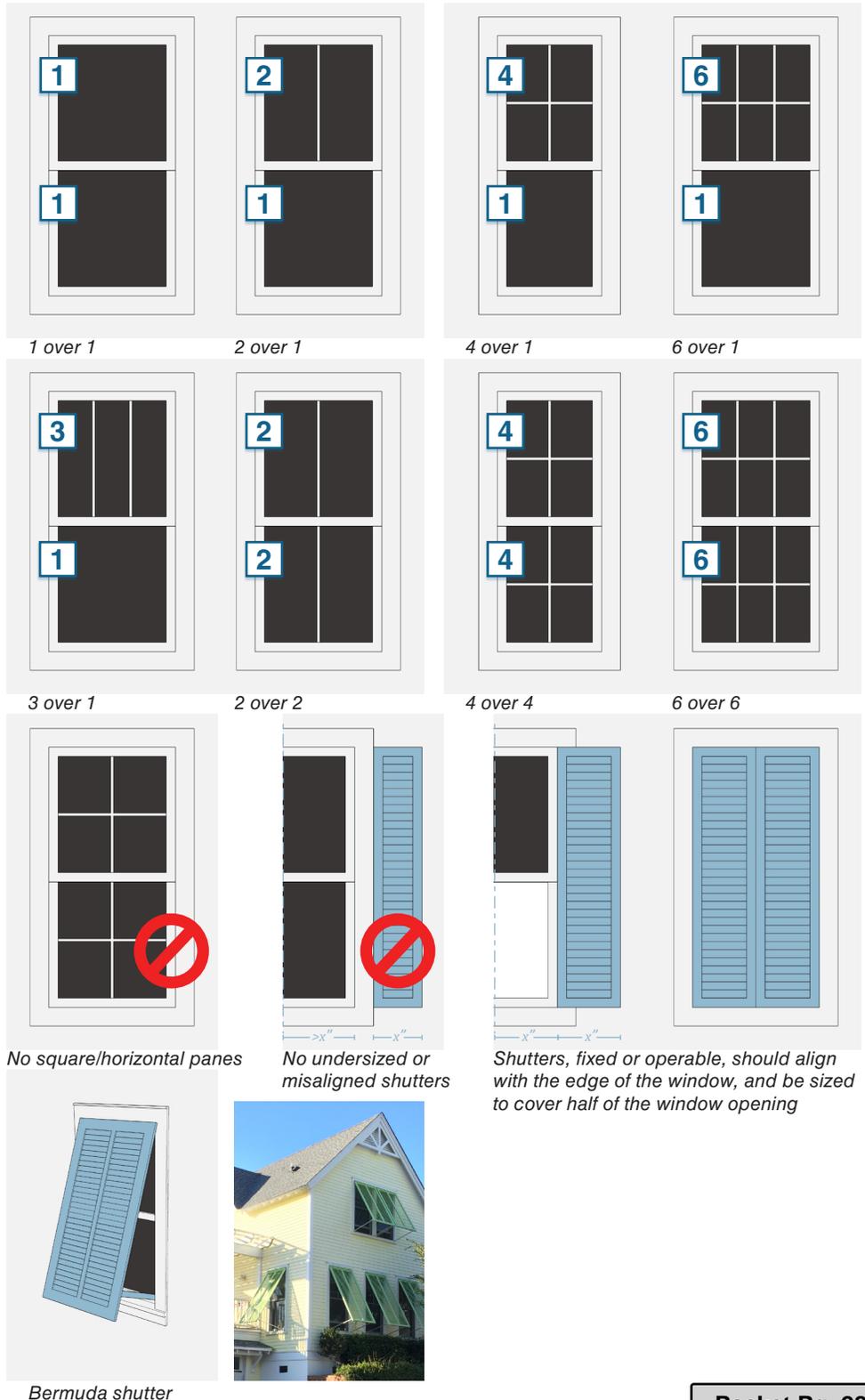
Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

# Charter

## Commercial, Mixed Use & Multifamily

### Double-hung windows

- Upper story windows, and windows in residential spaces on any level should be operable.
- Casement and awning windows are appropriate in circumstances where the window sill is higher than typical, such as in bathrooms or over a counter top.
- Double-hung windows are the most common and are appropriate in most circumstances.
  - Muntin divisions should result in individual panes (also called *lites/lights*) with a vertical proportion, rather than being square or horizontal.
  - Vary the number of divisions in differently-sized windows in order to maintain a similar pane proportion from one window to the next rather than maintaining the same number of divisions for every window.
- Operable shutters are preferred for protection from storm-borne debris as well as their functionality in maintaining privacy and reducing solar gain while windows are open in nice weather.
  - Shutters should always be sized (whether operable or not) so that they would fully cover the window when closed.
  - Mulled windows should not have shutters alongside them unless each shutter is sized to cover half of the mullered unit.
- Operable Bermuda shutters are a great solution in coastal environments to protect from hot sun and storm-borne debris.
  - Bermuda shutters should be sized to overlap the window by at least 1" on all sides.



# Charter Commercial, Mixed Use & Multifamily

## Building details

### Railing Styles details



**Cable Railing**  
Contemporary style of railing with high visibility and horizontal emphasis



**Decorative Railing**  
Transitional style of railing with low visibility and horizontal emphasis



**Plank Railing**  
Rustic style of railing with low visibility and horizontal emphasis



**Picket Railing**  
Traditional style of railing with vertical emphasis, picket styles vary

### Column Styles



**Doric Column**  
Classical style



**Square Column**  
Simple style, tapered sides shown above



**Double Columns on plinths**  
Contemporary style. Plinths can be wood or masonry



**Square Column**  
Simple style, shown with brackets above

### Bracket Styles



**Ornate Bracket**



**Simple Bracket**

Attachment: 3- 20-1-28\_Currutuck Station\_Pattern Book (PB 19-25 Currutuck County - Currutuck Station)



# Charter Public Space

The Charter sub-district is intended to be a little more conventional in its pattern of development, though it should still be a pleasantly walkable place. Connections between developments and connections to the adjacent sub-districts are the primary focus.

Intersections of primary roadways and the focal points of developments within the sub-district are the main opportunities for amenity spaces and community assets.

The following public space types are appropriate to use in the Charter sub-district.



Attachment: 3- 20-1-28\_Currutuck Station\_Pattern Book (PB 19-25 Currutuck County - Currutuck Station)

# Charter Public Space

## Squares



*Mid-block Square*

1. Located along paved walkway, between 2 businesses.
2. Business adjacent may use squares.
3. Square minimum size 60' x 80'.
4. Square paving to reflect heat, not light.
5. Color to contrast adjacent paving or ground cover.
6. Square amenities may vary, at a minimum, provide shaded seating.



*Intersection Square*

1. Located at the nexus of adjacent sub-districts.
2. Vehicular traffic surrounds the square.
3. Maintain a buffer between vehicular traffic outside of square and pedestrians inside square.
4. Suitable buffers are a continuous row of trees, planters, and/or on-street parking.

## Greenbelts



*Greenbelt Path*

1. Gently meandering path.
2. Greenbelts widen at building entrances.
3. Benches, trash receptacles, and water fountains shall be regularly spaced along the greenbelt.



*Greenbelt Activity Node*

1. Activity nodes are to be set back from the path, while remaining visible from the R.O.W.
2. Activity nodes should offer various options for visitors:
3. Exercise equipment.
4. Rest areas.
5. Scenic looks.
6. Picnic venues.
7. Bike repair stations.

# Charter Public Space

## Parks



*Pocket Parks*

1. Located within the open space of one lot
2. Typically between 100 - 200 sq. ft.
3. These small parks are most useful on lots with multiple residences.
4. Paving at perimeter of space should be of contrasting color.
5. Seating and plants are common amenities.



*Neighborhood Playgrounds*

1. Located within or near large public space.
2. Neighborhood playgrounds must be visible from the right-of-way.
3. Provide non-opaque fencing that is continuous and secured around the perimeter.
4. Seating, water fountains, and trash receptacles are common amenities to include:



*Dog Parks*

1. Continuous Fencing around dog parks.
2. Bull fencing and picket fencing are acceptable styles.
3. Planting at base of fence is encouraged, to eliminate burrowing.
4. When possible, provide water fountain for pets
5. The ground cover should be artificial turf or seeded grass.
6. A trash container should be provided for collection of animal waste.



*Fitness stations*

1. Paving should be soft and durable, rubber mats are typical.
2. Paving color should have a low heat reflectivity.
3. Plaza paving to reflect heat, not glare. Color to contrast adjacent paving or ground cover
4. Plaza amenities may vary, at a minimum, provide shaded seating and paved walking paths.

## Amenities



*Area of concentrated amenities including shaded bike racks, drinking fountains, and trash receptacles.*



*Benches placed across from each other for social interaction*

# Charter Public Right-of-Way

## Local streets

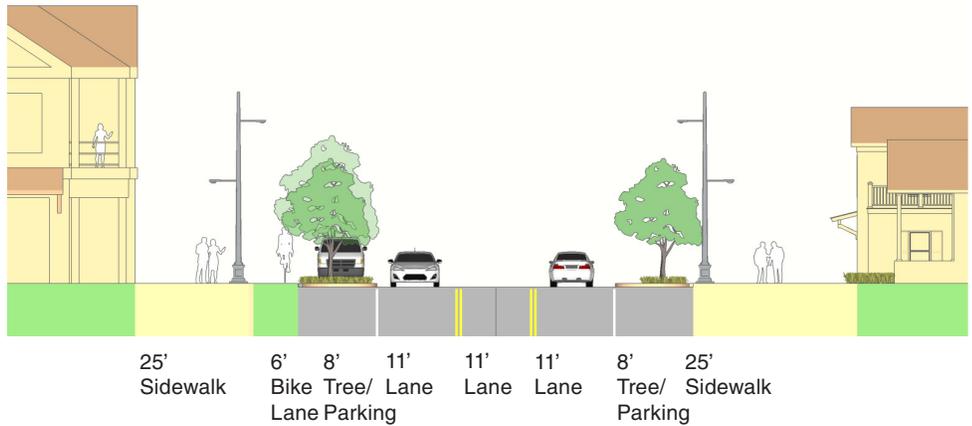
### Interior Streets

1. Sidewalks are sized to accommodate pedestrian movement as well as pedestrian oriented amenities such as seating area and outdoor dining
2. The bicycle lane is protected from traffic by a row of parallel parking and trees
3. The center lane is for turning vehicles and may be upgraded to a planted median as required
4. The parallel parking lane is used to accommodate tree wells at regular intervals of 2 to 3 parking spaces
5. Trees shall be placed at regular intervals and of a species sized to provide continuous shade to pedestrians
6. Lighting shall be provided for both vehicular and pedestrian paths; lighting shall be no cooler than 3000K in color temperature and

Charter District

Interior Streets

Charter District



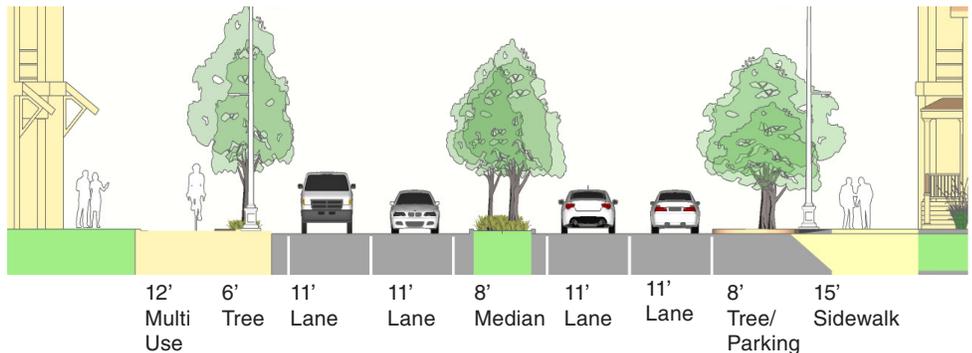
### Center Station - Charter Boundary

1. The lower density development of the Charter sub-district allows for a wide multi-use path that accommodates both pedestrians and cyclists along a pleasant, meandering path
2. The 2 travel lanes exiting the development allow for morning rush traffic to more effectively stack at Caratoke Highway
3. Parallel parking, and generous sidewalks help establish the pedestrian-friendly, retail-oriented nature of the Charter sub-district

Charter Sub-District

Public Collector A

Center Station Sub-District



Attachment: 3- 20-1-28\_Currutuck Station\_Pattern Book (PB 19-25 Currutuck County - Currutuck Station)

# Charter Public Right-of-Way

## Pedestrian paths

1. Pedestrian paths within Charter may serve several uses.
2. Where there is little commercial activity, it is appropriate to designate a bike lane along the pedestrian path.
3. Bike lanes are to be painted in a contrasting color with the adjacent pavement
4. Pedestrian paths crossing a street are to be raised when in the middle of a block.
5. Pedestrian path lighting shall be continuous and even.
6. Pedestrian multi-use paths shall gently meander in locations where the building setback exceeds 20'.



Charter - Interior Streets

20'



Center Station - Charter Boundary

2'

12'

6'

The following pages provide examples of building massing and architectural styles that are compatible with the Currituck Station design intent. Designers are not expected to copy these, only to use them as a guide towards their own compatible designs.

## Cypress Development Principles

The Cypress sub-district forms the low density, single-family residential development of the Currituck Station District. To support the residences in this sub-district, small, neighborhood-serving commercial and office uses are permitted. These non-residential uses may be provided when adjacent to or fronting a collector street or arterial street.

To maintain the residential, family-oriented nature of Cypress, parking garages should be set back, towards the interior of the lot. Development is designed to support the residential nature of the sub-district. Small office and retail shall use vegetated buffers to screen off-street parking, mechanical equipment, trash enclosures, and other non-residential features. An interconnected network of sidewalks, multi-use paths, public spaces, and parks provide access to the mix of uses found throughout the sub-district.

Cypress is the low-density family sub-district of Currituck Station, in order to present a welcoming and safe environment to the families here, landscape and buffer enhancements are encouraged.



### Dimensions

The allowable dimensions for the Center Station sub-district are designated in the Currituck Unified Development Ordinance.

### Allowable uses

This is an abbreviated table of uses for reference only. For a current and complete table of uses, see the Unified Development Ordinance

Residential Uses	Commercial Uses	Industrial Uses	Civic Uses
Single Family (Attached) Duplex	Neighborhood-serving retail Office	None	Community-serving amenities Education Government



## Cypress Commercial Development

Commercial development in the Cypress sub-district is limited to neighborhood serving retail and services. These buildings should not dramatically stand out from the neighborhood houses in either style or scale. Small to medium sized, residential-type buildings are adaptable enough to suit the needs of the variety of businesses which are needed to support daily life, while not being so out of character for the neighborhood that they are not identified as being part of it.

If drive-through retail is necessary for the business occupying the building, then the drive-through lanes and canopies should be placed at the rear of the building, out of sight of the primary street frontage. The canopies should be of a similar roof type to the primary building, matching either the main roof or the porch roof construction.

Additions such as drive-through canopies and garage doors support the more auto-oriented nature of the Charter sub-district. These additions should be placed at the rear of the building to maintain a pedestrian oriented front.

### Building massing

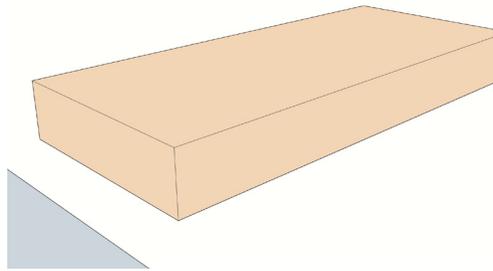


# Cypress Commercial Development

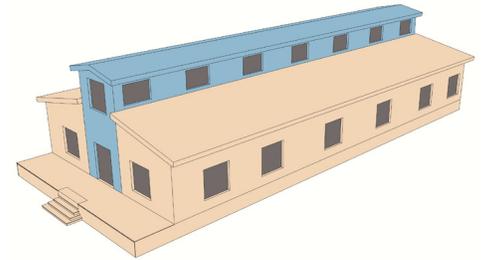
## Small 2-story building

### Clerestory building

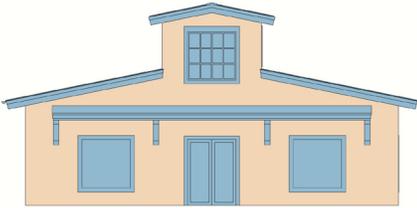
1. Use features such as clerestory to give a small building a 2-story appearance. The second floor may or may not be occupied.
2. Simple details give the building a pedestrian-friendly facade while keeping costs low.



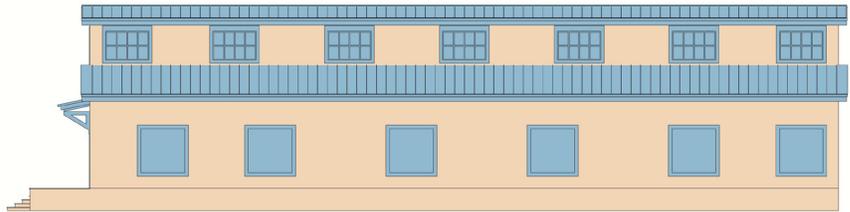
*A 1-story building program in a rectangular footprint. A 1-story building can only be accommodated if the facade is at least 20' tall giving a 2-story appearance.*



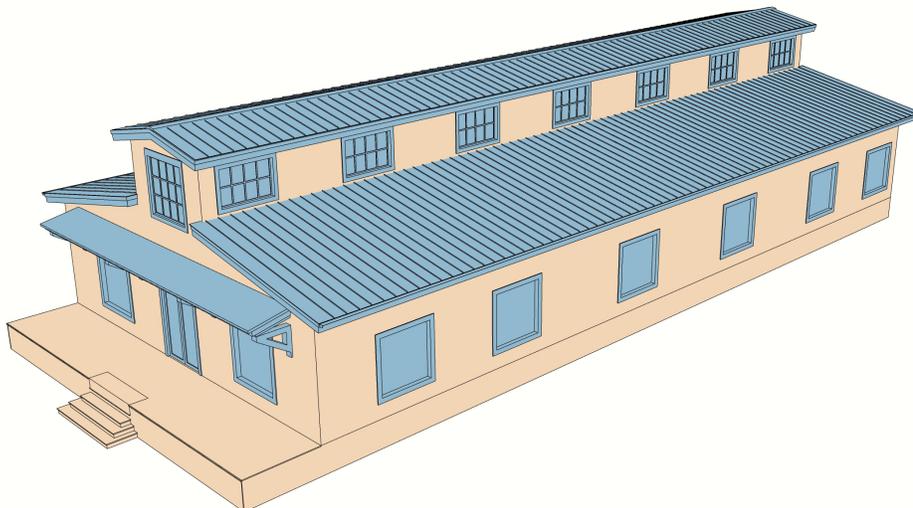
*Adding a clerestory gives the building a usable 2nd story for offices or studio apartments. Alternatively, the clerestory can be used to provide additional natural light and an airy feeling to the 1st floor space while meeting the 20' building height requirement.*



*A front facade with doors and generous windows centered on bays is functional and pedestrian friendly. The width of the clerestory section can be adjusted to suit the building program needs.*



*Ground floor side windows near the street are necessary to maintain visual permeability of the street frontage. Side windows towards the rear of the building are optional, but encouraged.*



*A small, simple fixed awning with support brackets and simple trim profiles add detail to the building and give it a coastal appearance.*

## Cypress Commercial Development

### 1. Roof

Standing seam metal due to low pitch

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Board & batten, shake, or clapboard

### 4. Clerestory windows

Divided lite, clear glass windows

### 5. Awning

Sloped with simple brackets

### 6. Railings

Storefront doors with clear (untinted) glass

### 8. Ground floor windows

Storefront or folding glass windows with clear (untinted) glass

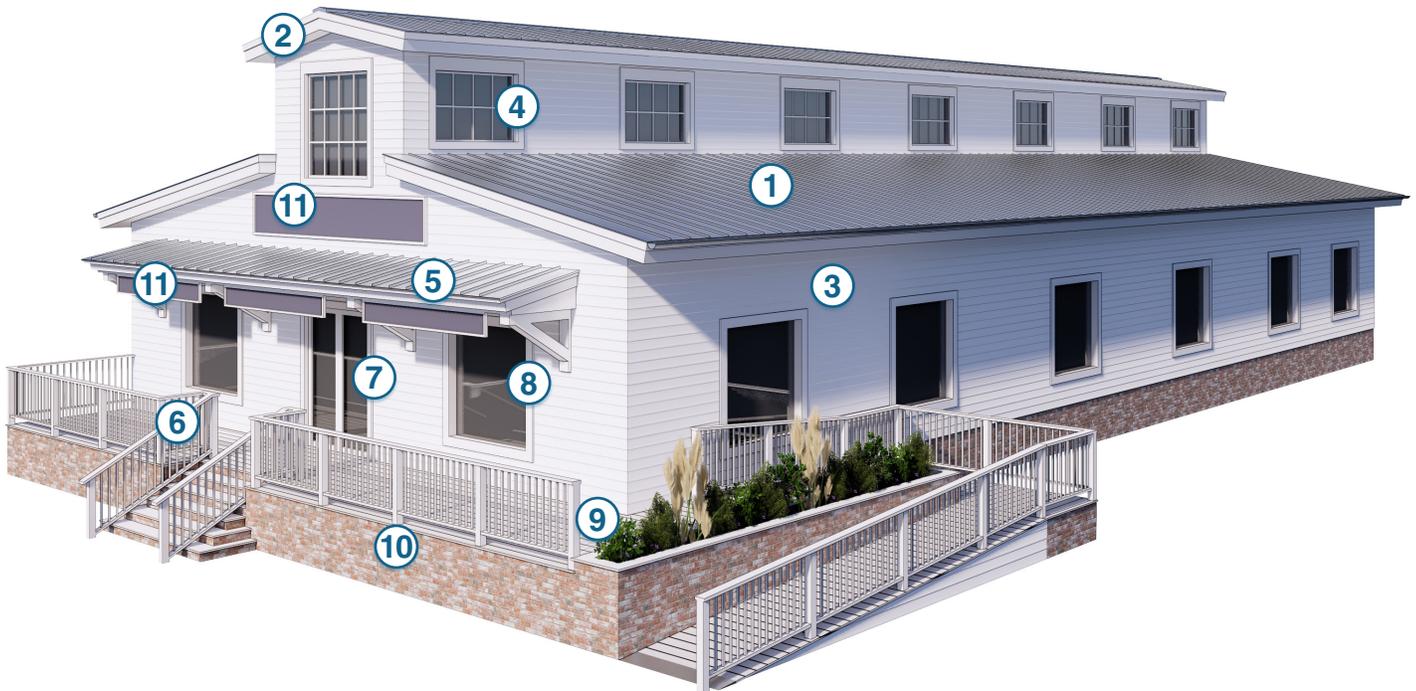
### 9. Decking

### 10. Foundation

Solid in appearance

### 11. Sign boards

Recessed or raised perimeter to differentiate from cladding



Local Pie in Bluffton, SC



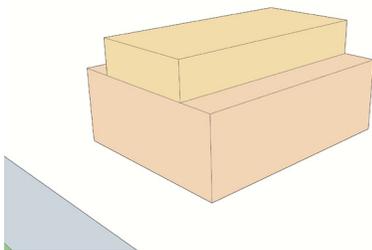
The Hitching Post (The Spry Store) in Currituck, NC

# Cypress Commercial Development

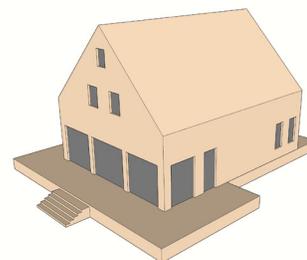
## Small 2-story building

### A-Frame with Porch

1. An iconic building form along the eastern seaboard, the A-frame is easy to construct, weathers storm events, and provides ample floor space while presenting a compact facade to the public.
2. A deep wrap-around porch shades the windows from hot summer sun and makes an inviting place for customers to enjoy.



A 2-story building program in a rectangular footprint.



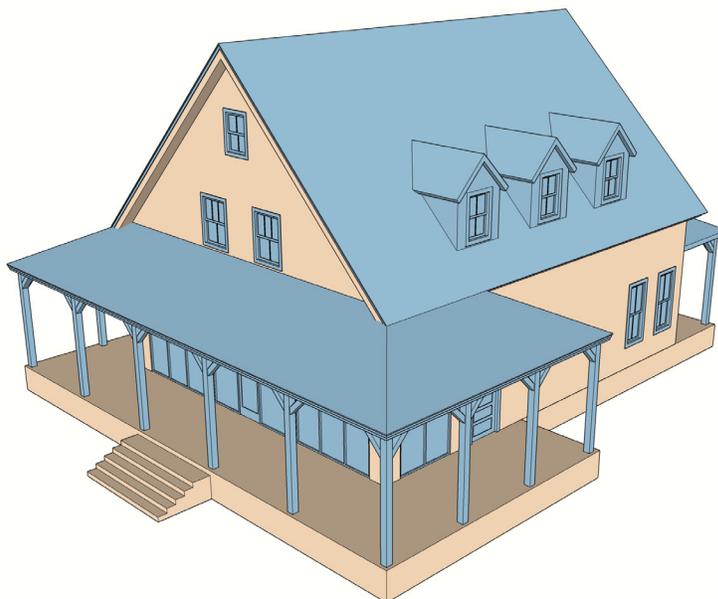
A steeply pitched roof encompasses the entire upper story.



A front facade with storefront glazing and transom windows allows natural light into the lower story space and invites pedestrians in.



Dormers let light into the upper story space and provide additional floor space. If the upper story is to be occupied by a different tenant, or is for a residential use, then a separate side entry can be added.



Straight or braced columns support the deep porch. Dormers can be separate or combined into one large dormer.

Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

## Cypress Commercial Development

### 1. Roof

Shingle or standing seam metal

### 2. Trim

Simple profile, multi-layered

### 3. Upper story windows

Divided lite, clear glass windows

### 4. Porch roof

Shingle or standing seam metal

### 5. Columns and brackets

Wood or composite

### 6. Ground floor windows

Storefront with clear (untinted) glass

### 7. Doors

Storefront with clear (untinted) glass

### 8. Railings and stairs

### 9. Decking

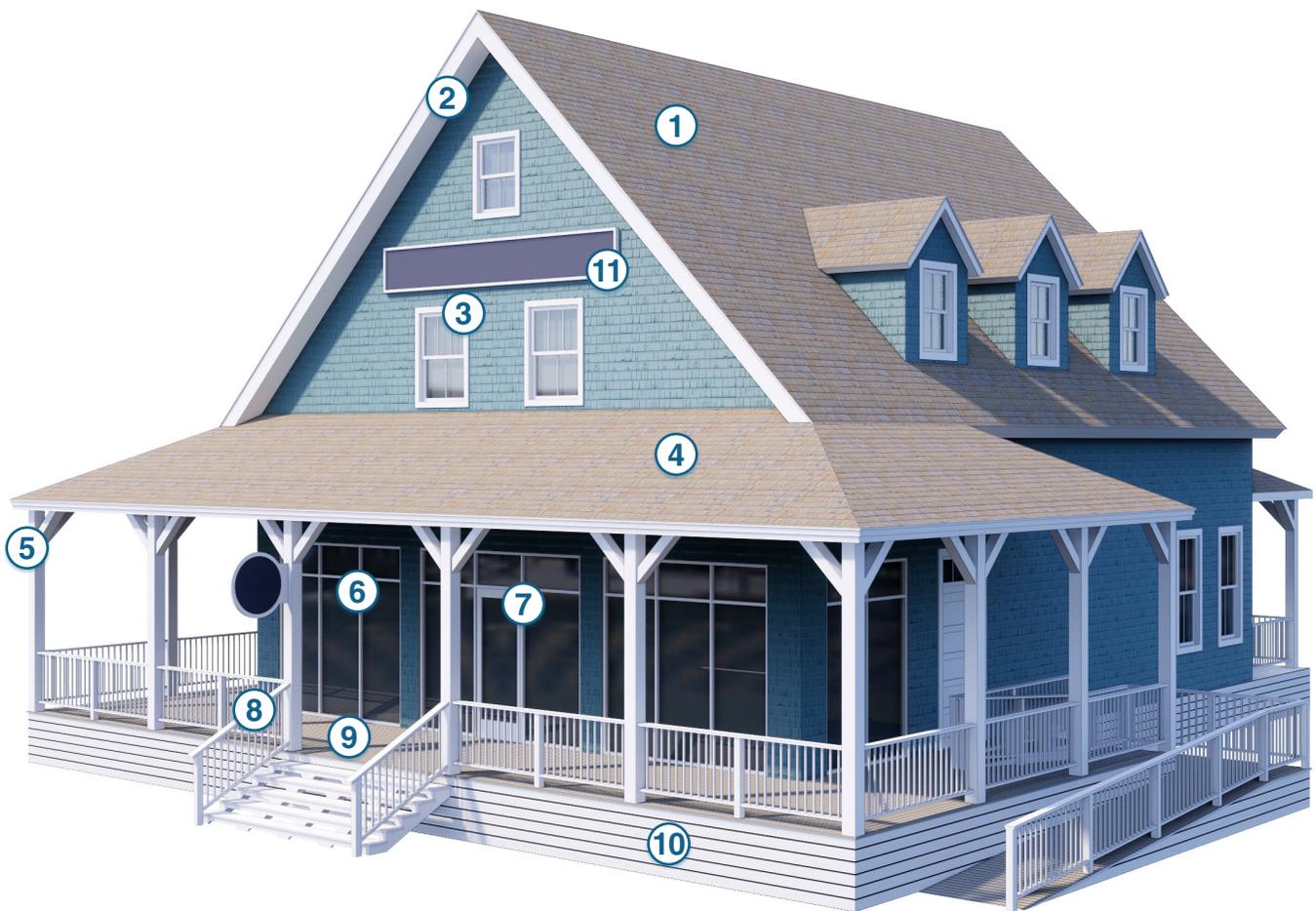
Wood (rot resistant) or composite

### 10. Foundation skirt

Solid or slatted in appearance

### 11. Sign board

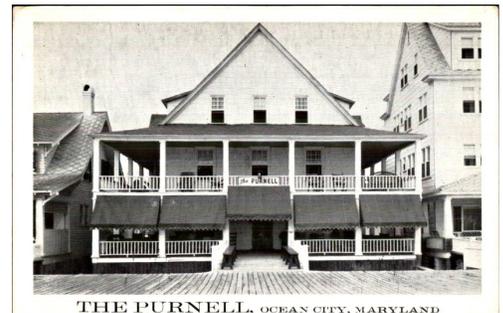
Recessed or raised perimeter to differentiate from cladding



Caffey's Inlet Lifesaving Station  
in Dare County, NC



Seatack Station, Virginia Beach, VA



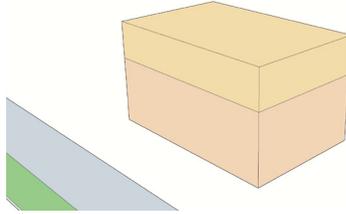
The Purnell Hotel, Ocean City, MD

# Cypress Commercial Development

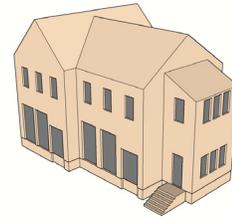
## Medium 2-story building

### L-shaped

1. Use gable ends to break up longer facades and create the appearance of a series of smaller masses.
2. Simple details give the building a pedestrian friendly facade while keeping construction costs low.



A 2-story building program in a rectangular broadfront footprint.



Adding a gable end breaks up the building facade and provides multiple distinct storefronts.



Storefront windows and doors allow pedestrians to see into the ground floor spaces. A stair and elevator tower on the end of the building gives private access to the upper story if desired.



A drive-through canopy added to the rear of the building allows auto-oriented uses to occupy the building while maintaining the vernacular character



Awnings with brackets and shutters protect the windows from sun and rain.

# Cypress Commercial Development

## 1. Roof

Standing seam metal or shingle

## 2. Trim

Simple profile, multi-layered

## 3. Cladding

Masonry, shake, or clapboard

## 4. Upper story windows

Divided lite, clear (untinted) glass windows

## 5. Shutters

1/2 width and full height of window, operable louvered or solid shutters

## 6. Awning

## 7. Doors

Storefront with transoms and clear (untinted) glass

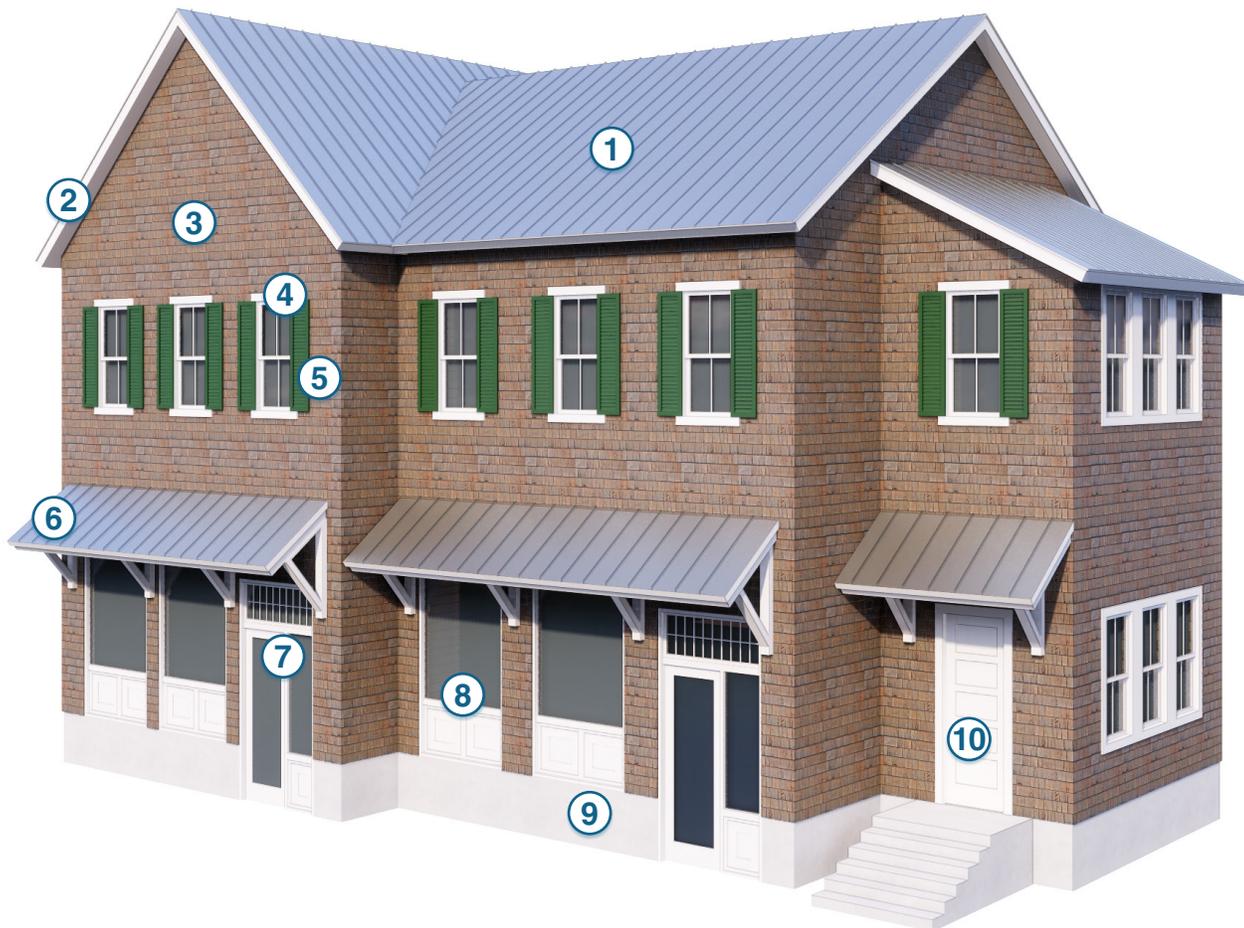
## 8. Ground floor windows

Storefront with transoms with clear (untinted) glass and optional opaque paneling

## 9. Foundation

Solid in appearance

## 10. Residential door

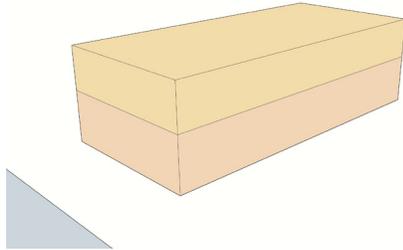


# Cypress Commercial Development

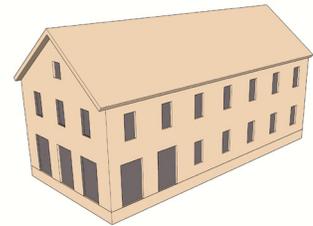
## Medium 2-story building

### Gable-front building

1. A classic and versatile building form lends itself well to a vertical mix of uses.
2. Simple details give the building a pedestrian friendly facade while keeping costs low.



A 2 to 3-story building program in a rectangular footprint.



A gable above the upper story makes an easy-to-construct, adaptable, and durable building.



Minimal overhangs are typical, but the eaves can be extended up to 3' or 4' with exposed rafter tails for a more Craftsman appearance.



Evenly spaced side windows on all levels produce usable light throughout the building.



The wraparound porch can be supported by simple square columns or brackets.

## Cypress Commercial Development

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Upper story windows

Single or divided lite, clear (untinted) glass windows

### 4. Porch roof

Match main roof or standing seam metal

### 5. Columns and brackets

4"-8" square or round

### 6. Ground floor windows

Storefront or folding glass windows with clear (untinted) glass

### 7. Doors

Storefront with clear (untinted) glass

### 8. Railings and stairs

### 9. Decking

Wood (rot resistant) or composite

### 10. Foundation skirt

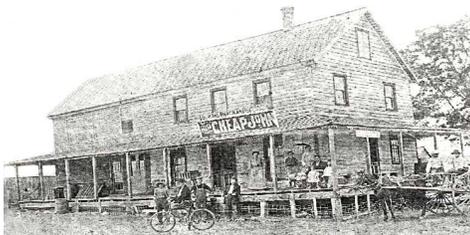
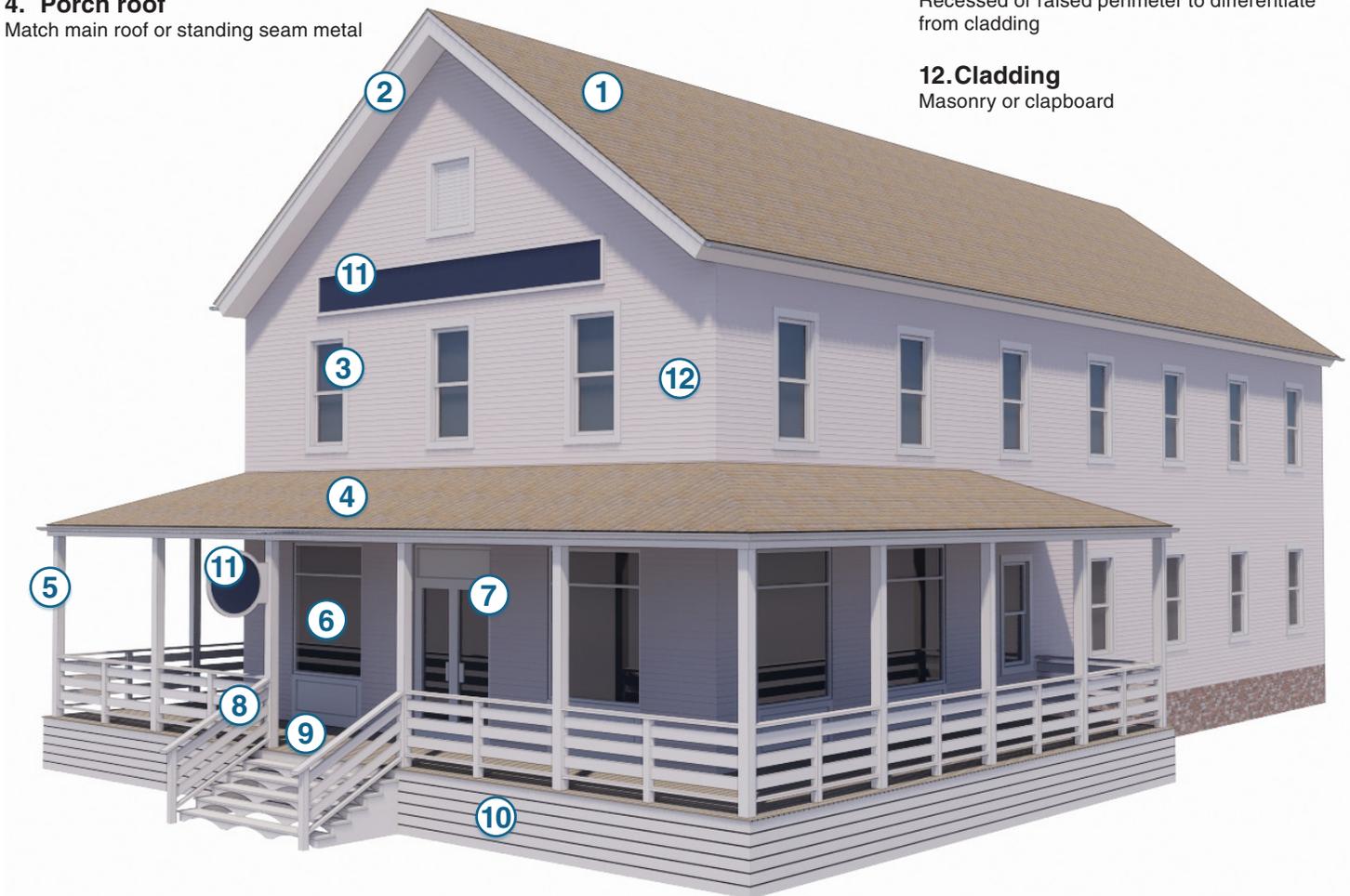
Solid in appearance with optional slatted material at porch

### 11. Sign board

Recessed or raised perimeter to differentiate from cladding

### 12. Cladding

Masonry or clapboard



Cheap John's Store, Currituck County, NC



Creekmore Brothers Store, Currituck County, NC



JM Roberts House, Currituck County, NC

# Cypress Commercial Development

## Building details

### Roof materials



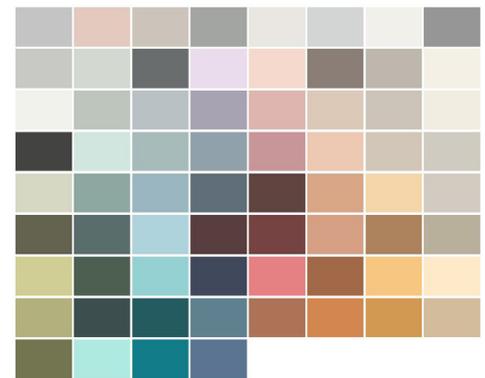
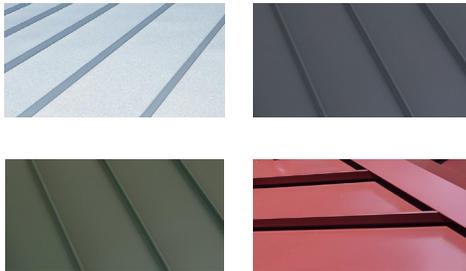
Architectural shingles. Dark green, medium brown, medium gray, or dark red in color.



Standing seam metal. 1 1/2" vertical seam with 12"-18" spacing. Silver metallic, medium gray, dark green, or dark red in color



Cedar or cypress shingles.



### Exterior cladding details



Full brick buildings as well as buildings that have brick foundations and clapboard or shingle cladding above are appropriate.



Wood or fiber cement clapboard siding in a variety of colors in whites, grays, earth tones, and pastels are appropriate. Exposures should be 8" or less.



Cedar shake or fiber cement simulated shake siding, stained, painted, or natural, is appropriate. Colors should be whites, grays, earth tones, or pastels.

### Windows and doors details



Doors and windows set in brick - recessed with brick mold with brick lintels or arches above and brick or wood/composite sills below.



Doors and windows set in clapboard siding with trim 3" to 6" in width. The door/window trim width should always be less than the width of the corner trim of the building.



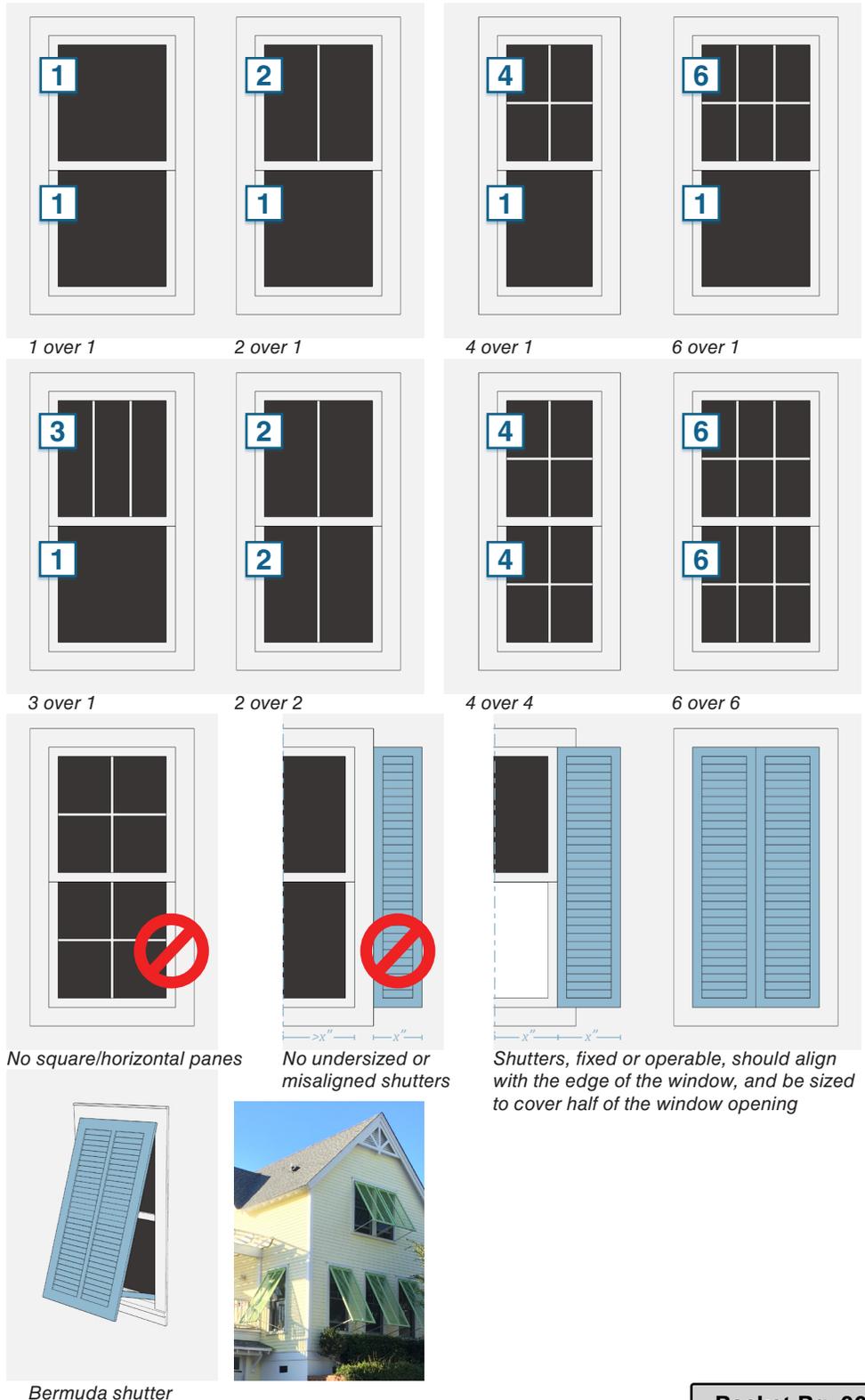
Doors and windows set in cedar or cypress shake siding with 1 1/2" to 3" flat trim.

# Cypress

## Commercial Development

### Double-hung windows

- Upper story windows, and windows in residential spaces on any level should be operable.
- Casement and awning windows are appropriate in circumstances where the window sill is higher than typical, such as in bathrooms or over a counter top.
- Double-hung windows are the most common and are appropriate in most circumstances.
  - Muntin divisions should result in individual panes (also called *lites/lights*) with a vertical proportion, rather than being square or horizontal.
  - Vary the number of divisions in differently-sized windows in order to maintain a similar pane proportion from one window to the next rather than maintaining the same number of divisions for every window.
- Operable shutters are preferred for protection from storm-borne debris as well as their functionality in maintaining privacy and reducing solar gain while windows are open in nice weather.
  - Shutters should always be sized (whether operable or not) so that they would fully cover the window when closed.
  - Mulled windows should not have shutters alongside them unless each shutter is sized to cover half of the mullered unit.
- Operable Bermuda shutters are a great solution in coastal environments to protect from hot sun and storm-borne debris.
  - Bermuda shutters should be sized to overlap the window by at least 1" on all sides.



# Cypress Commercial Development

## Building details

### Railing Styles details



**Cable Railing**  
Contemporary style of railing with high visibility and horizontal emphasis



**Decorative Railing**  
Transitional style of railing with low visibility and horizontal emphasis



**Plank Railing**  
Rustic style of railing with low visibility and horizontal emphasis



**Picket Railing**  
Traditional style of railing with vertical emphasis, picket styles vary

### Column Styles



**Doric Column**  
Classical style



**Square Column**  
Simple style, tapered sides shown above



**Double Columns on plinths**  
Contemporary style. Plinths can be wood or masonry



**Square Column**  
Simple style, shown with brackets above

### Bracket Styles



**Ornate Bracket**



**Simple Bracket**

## Cypress Public space

The Cypress sub-district is intended to be suburban in its pattern of development, though it should still be a pleasantly walkable place. Connections between developments, public amenities, and recreation areas, and connections to the adjacent sub-districts are the primary focus of the public space.

Intersections of primary roadways and the focal points of developments within the sub-district are the main opportunities for amenity spaces and community assets.

The following public space types are appropriate to use in the Cypress sub-district.



# Cypress Public Space

## Amenities



Area of concentrated amenities.



Benches placed across from each other for social interaction.

## Mail collection areas



Mail collection areas should be covered, complement the coastal vernacular architecture, and have a pull-off area large enough to accommodate at least 3 automobiles.

Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

# Cypress Public Space

## Squares



*Mid-block square*

1. Located along paved walkway, between 2 businesses.
2. Business adjacent may use squares.
3. Square minimum size 60' x 80'.
4. Square paving to reflect heat, not light.
5. Color to contrast adjacent paving or ground cover.
6. Square amenities may vary, at a minimum, provide shaded seating.



*Intersection square*

1. Located at the nexus of adjacent sub-districts.
2. Vehicular traffic surrounds the square.
3. Maintain a buffer between vehicular traffic outside of square and pedestrians inside square.
4. Suitable buffers are a continuous row of trees, planters, and/or on-street parking.

## Greenbelts



*Greenbelt path*

1. Gently meandering path.
2. Greenbelts widen at building entrances.
3. Benches, trash receptacles, and water fountains shall be regularly spaced along the greenbelt.



*Greenbelt activity node*

1. Activity nodes are to be set back from the path, while remaining visible from the R.O.W.
2. Activity nodes should offer various options for visitors:
  - a. Exercise equipment.
  - b. Rest areas.
  - c. Scenic looks.
  - d. Picnic venues.
  - e. Bike repair stations.

# Cypress Public Space

## Parks



*Pocket Parks*

1. Located within the open space of one lot
2. Typically between 100 - 200 sq. ft.
3. These small parks are most useful on lots with multiple residences.
4. Paving at perimeter of space should be of contrasting color.
5. Seating and plants are common amenities here



*Neighborhood Playgrounds*

1. Located within or near large public space.
2. Neighborhood playgrounds must be visible from the right-of-way.
3. Provide non-opaque fencing that is continuous and secured around the perimeter.
4. Seating, water fountains, and trash receptacles are common amenities to include



*Dog Parks*

1. Fencing around dog park should be continuous.
2. Bull fencing and picket fencing are acceptable styles.
3. Planting at base of fence is encouraged, to eliminate burrowing.
4. When possible, provide water fountain for pets
5. The ground cover should be artificial turf or seeded grass.
6. A trash container should be provided for collection of animal waste.



*Fitness stations*

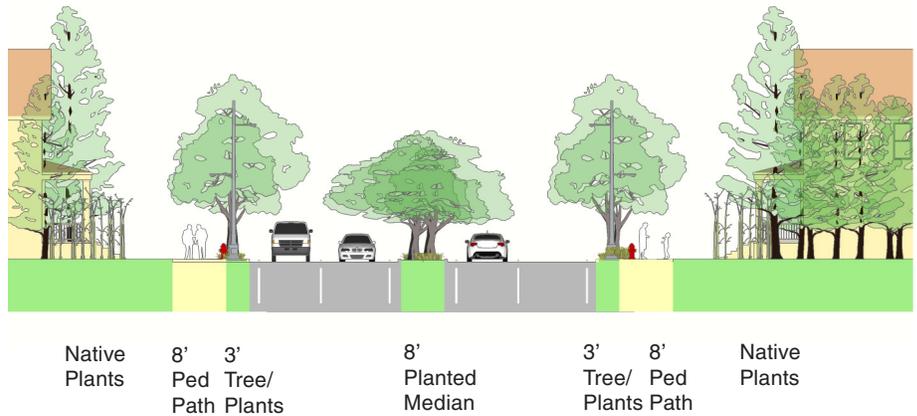
1. Paving should be soft and durable, rubber mats are typical.
2. Paving color should have a low heat reflectivity
3. Plaza paving to reflect heat, not glare. Color to contrast adjacent paving or ground cover
4. Plaza amenities may vary, at a minimum, provide shaded seating and paved walking paths

# Cypress Public Right-of-Way

## Local Streets

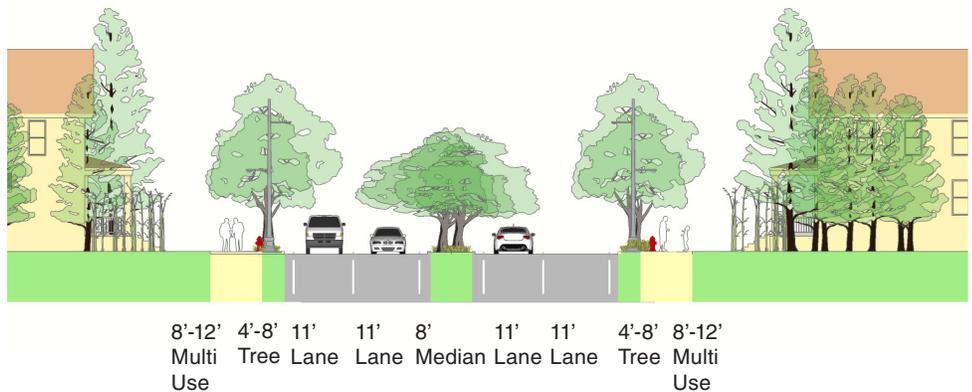
### Interior Streets

1. Sidewalks are protected from traffic by a row of trees.
2. A central median, planted with trees, provide a shaded roadway as well as traffic-calming.
3. 2 travel lanes may be utilized to as needed to facilitate commuter traffic.
4. Outer travel lane may be marked for parking in front of commercial uses as needed.
5. Trees shall be placed at regular intervals and of a species sized to provide continuous shade to pedestrians
6. Lighting shall be provided for both vehicular and pedestrian paths; lighting shall be no cooler than 3000K in color temperature and provide a CRI of 80 or better



## Arterials First

1. The lower density development of the Cypress sub-district allows for a wide multi-use path that accommodates both pedestrians and cyclists along a pleasant, meandering path
2. Parking for individual lots can be handled on-site
3. The 4 total lanes are divided by a central median to create a continuous tree canopy.



# Cypress Public Right-of-Way

## Pedestrian paths

1. Pedestrian paths are multi-use.
2. Pedestrian path should have porous paving to facilitate drainage.
3. Pedestrian path street crossings shall be raised, colored to contrast adjacent road.
4. Pedestrian paths shall be continuously lit.
5. Pedestrian multi-use paths may gently meander.
6. Where driveways interrupt pedestrian paths, pedestrian path must be indicated with contrasting pavement.
7. Driveway pavement should be permeable.
8. The maximum driveway apron width is 24'

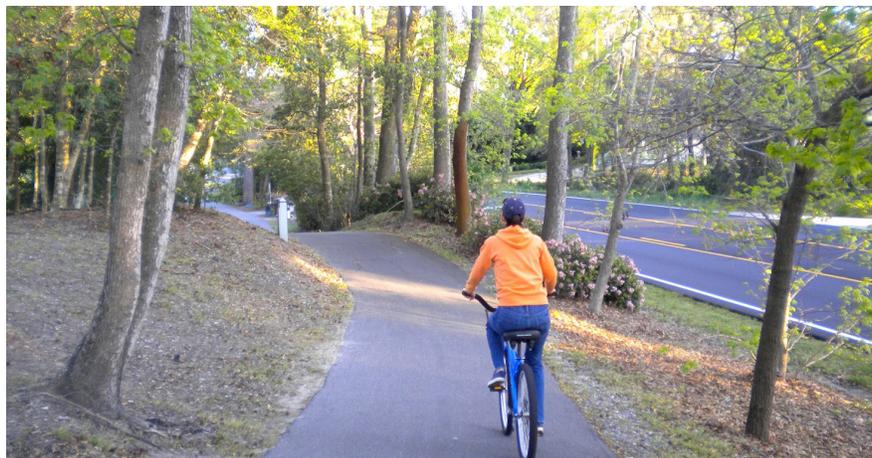


Cypress - Interior Street Option A

8'



Cypress - Driveways at pedestrian paths



Corolla Greenway is an example of a good pedestrian path.

The following pages provide examples of building massing and architectural styles that are compatible with the Currituck Station design intent. Designers are not expected to copy these, only to use them as a guide towards their own compatible designs.

# Junction & Newtown Development Principles

## Development principles

The Junction and Newtown sub-districts support the district through residential, single-family, detached development. The Junction sub-district is an existing residential development. Newtown will be developed in a similar manner. Limited single family attached and mixed-use development is permitted.

To maintain the family-oriented nature of Junction and Newtown, mixed use and single-family attached development is encouraged along collectors and arterial streets. Single-family detached residential will be developed within the interior of the sub-district. Non-residential development is designed to support residents by placing buildings close to the street with active and public uses on the ground floor and pedestrian entrances accessed from the street. An interconnected network of sidewalks, multi-use paths, public spaces, and parks provide access to the mix of uses found throughout the sub-district.

Junction, as it is currently developed, is a low density residential sub-division. Typically these developments require paving new roads, resulting in the need to store stormwater in ponds on-site. In order to provide a healthy, and active place for the public, large developments are encouraged to provide pedestrian paths and site amenities around landscaped retention ponds.



## Dimensions

The allowable dimensions for the Junction and Newtown sub-district are designated in the Currituck Unified Development Ordinance.

## Allowable uses

This is an abbreviated table of uses for reference only. For a current and complete table of uses, see the Unified Development Ordinance

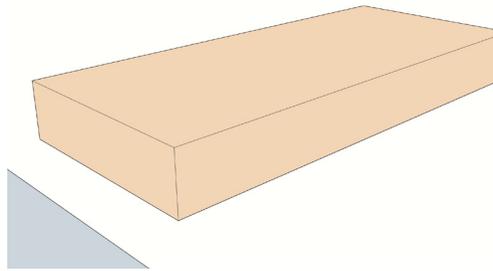
Residential Uses	Commercial Uses	Industrial Uses	Civic Uses
Single Family (Attached) Multifamily	Neighborhood-serving retail Restaurant Office	None	Community-serving amenities Education Government

# Junction & Newtown Commercial Development

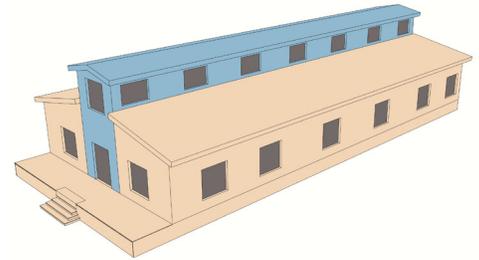
## Small 2-story building

### Clerestory building

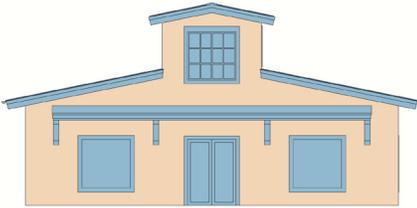
1. Use features such as clerestory to give a small building a 2-story appearance. The second floor may or may not be occupied.
2. Simple details give the building a pedestrian-friendly facade while keeping costs low.



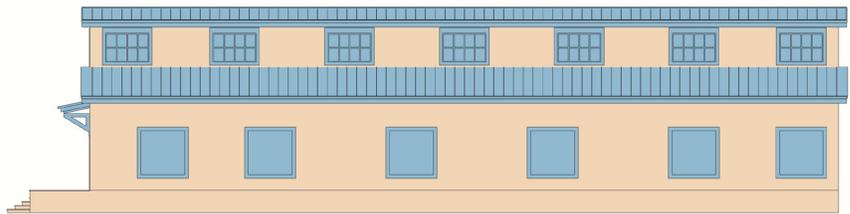
*A 1-story building program in a rectangular footprint. A 1-story building can only be accommodated if the facade is at least 20' tall giving a 2-story appearance.*



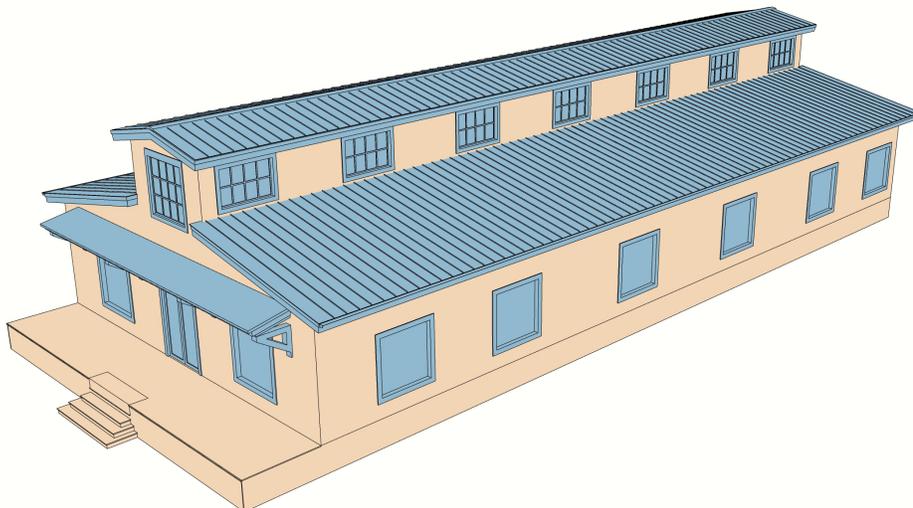
*Adding a clerestory gives the building a usable 2nd story for offices or studio apartments. Alternatively, the clerestory can be used to provide additional natural light and an airy feeling to the 1st floor space while meeting the 20' building height requirement.*



*A front facade with doors and generous windows centered on bays is functional and pedestrian friendly. The width of the clerestory section can be adjusted to suit the building program needs.*



*Ground floor side windows near the street are necessary to maintain visual permeability of the street frontage. Side windows towards the rear of the building are optional, but encouraged.*



*A small, simple fixed awning with support brackets and simple trim profiles add detail to the building and give it a coastal appearance.*

# Junction & Newtown Commercial Development

**1. Roof**  
Standing seam metal due to low pitch

**2. Trim**  
Simple profile, multi-layered

**3. Cladding**  
Board & batten, shake, or clapboard

**4. Clerestory windows**  
Divided lite, clear glass windows

**5. Awning**  
Sloped with simple brackets

**6. Railings**

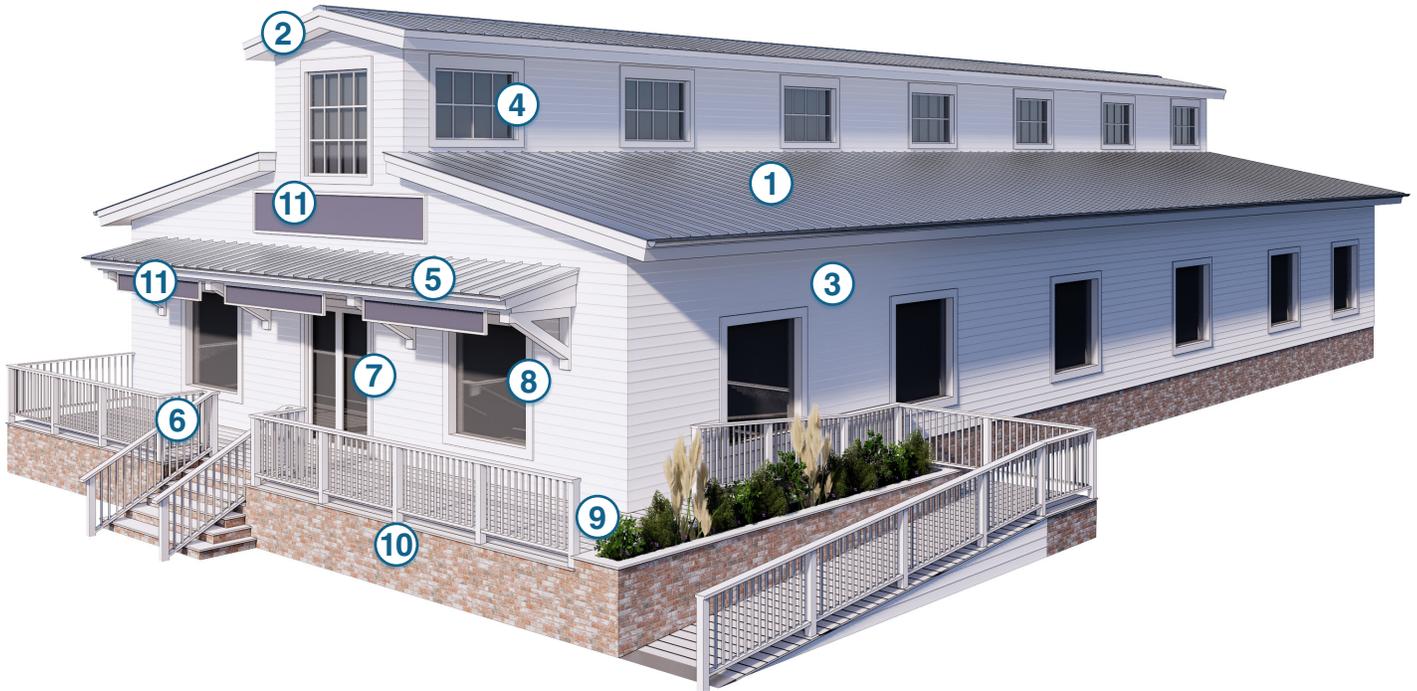
**7. Doors**  
Storefront doors with clear (untinted) glass

**8. Ground floor windows**  
Storefront or folding glass windows with clear (untinted) glass

**9. Decking**

**10. Foundation**  
Solid in appearance

**11. Sign boards**  
Recessed or raised perimeter to differentiate from cladding



Local Pie in Bluffton, SC



The Hitching Post (The Spry Store) in Currituck, NC

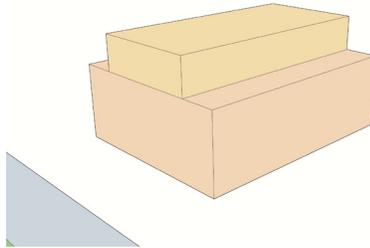
Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

# Junction & Newtown Commercial Development

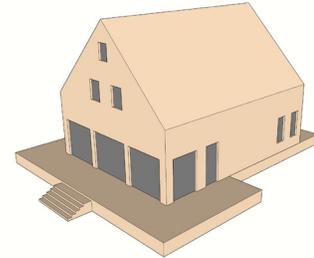
## Small 2-story building

### A-Frame with Porch

1. An iconic building form along the eastern seaboard, the A-frame is easy to construct, weathers storm events, and provides ample floor space while presenting a compact facade to the public.
2. A deep wrap-around porch shades the windows from hot summer sun and makes an inviting place for customers to enjoy.



A 2-story building program in a rectangular footprint.



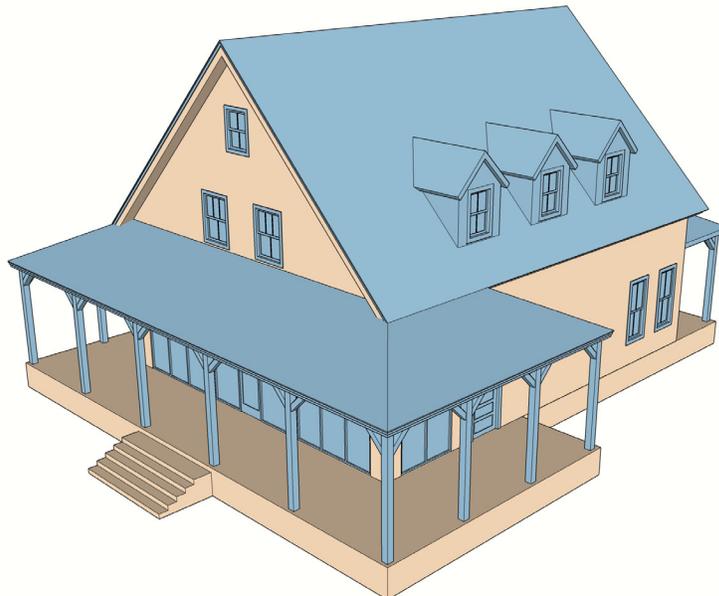
A steeply pitched roof encompasses the entire upper story.



A front facade with storefront glazing and transom windows allows natural light into the lower story space and invites pedestrians in.



Dormers let light into the upper story space and provide additional floor space. If the upper story is to be occupied by a different tenant, or is for a residential use, then a separate side entry can be added.



Straight or braced columns support the deep porch. Dormers can be separate or combined into one large dormer.

## Junction & Newtown Commercial Development

### 1. Roof

Shingle or standing seam metal

### 2. Trim

Simple profile, multi-layered

### 3. Upper story windows

Divided lite, clear glass windows

### 4. Porch roof

Shingle or standing seam metal

### 5. Columns and brackets

Wood or composite

### 6. Ground floor windows

Storefront with clear (untinted) glass

### 7. Doors

Storefront with clear (untinted) glass

### 8. Railings and stairs

### 9. Decking

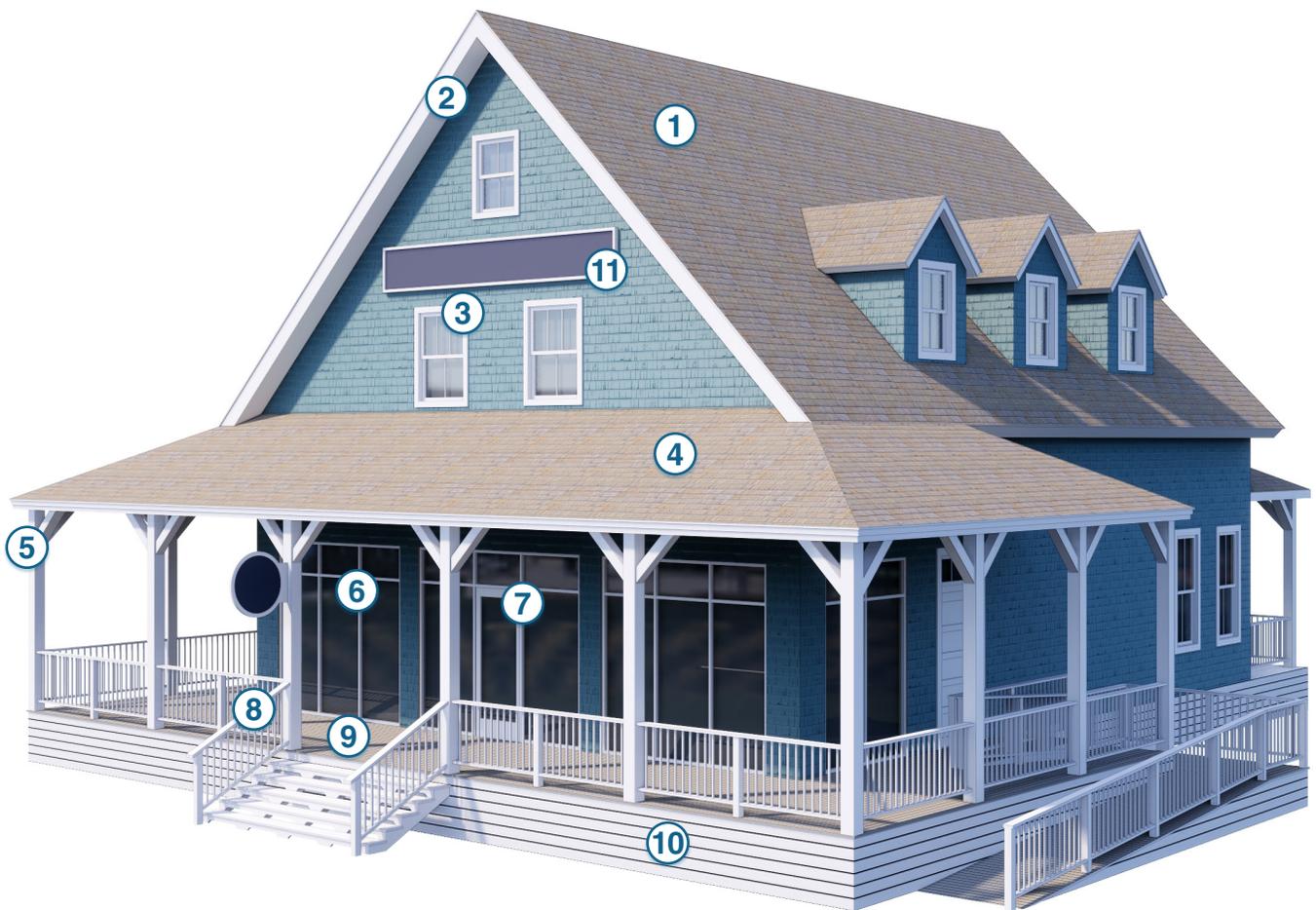
Wood (rot resistant) or composite

### 10. Foundation skirt

Solid or slatted in appearance

### 11. Sign board

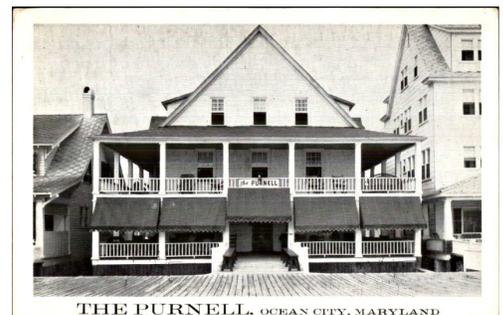
Recessed or raised perimeter to differentiate from cladding



Caffey's Inlet Lifesaving Station  
in Dare County, NC



Seatack Station, Virginia Beach, VA



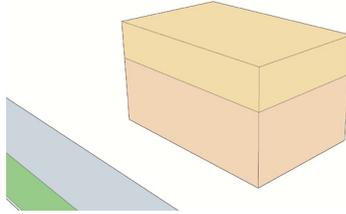
The Purnell Hotel, Ocean City, MD

## Junction & Newtown Commercial Development

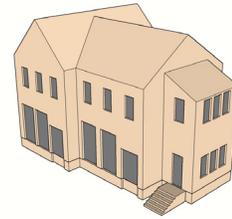
### Medium 2-story building

#### L-shaped

1. Use gable ends to break up longer facades and create the appearance of a series of smaller masses.
2. Simple details give the building a pedestrian friendly facade while keeping construction costs low.



A 2-story building program in a rectangular broadfront footprint.



Adding a gable end breaks up the building facade and provides multiple distinct storefronts.



Storefront windows and doors allow pedestrians to see into the ground floor spaces. A stair and elevator tower on the end of the building gives private access to the upper story if desired.



A drive-through canopy added to the rear of the building allows auto-oriented uses to occupy the building while maintaining the vernacular character



Awnings with brackets and shutters protect the windows from sun and rain.

## Junction & Newtown Commercial Development

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Cladding

Masonry, shake, or clapboard

### 4. Upper story windows

Divided lite, clear (untinted) glass windows

### 5. Shutters

1/2 width and full height of window, operable louvered or solid shutters

### 6. Awning

### 7. Doors

Storefront with transoms and clear (untinted) glass

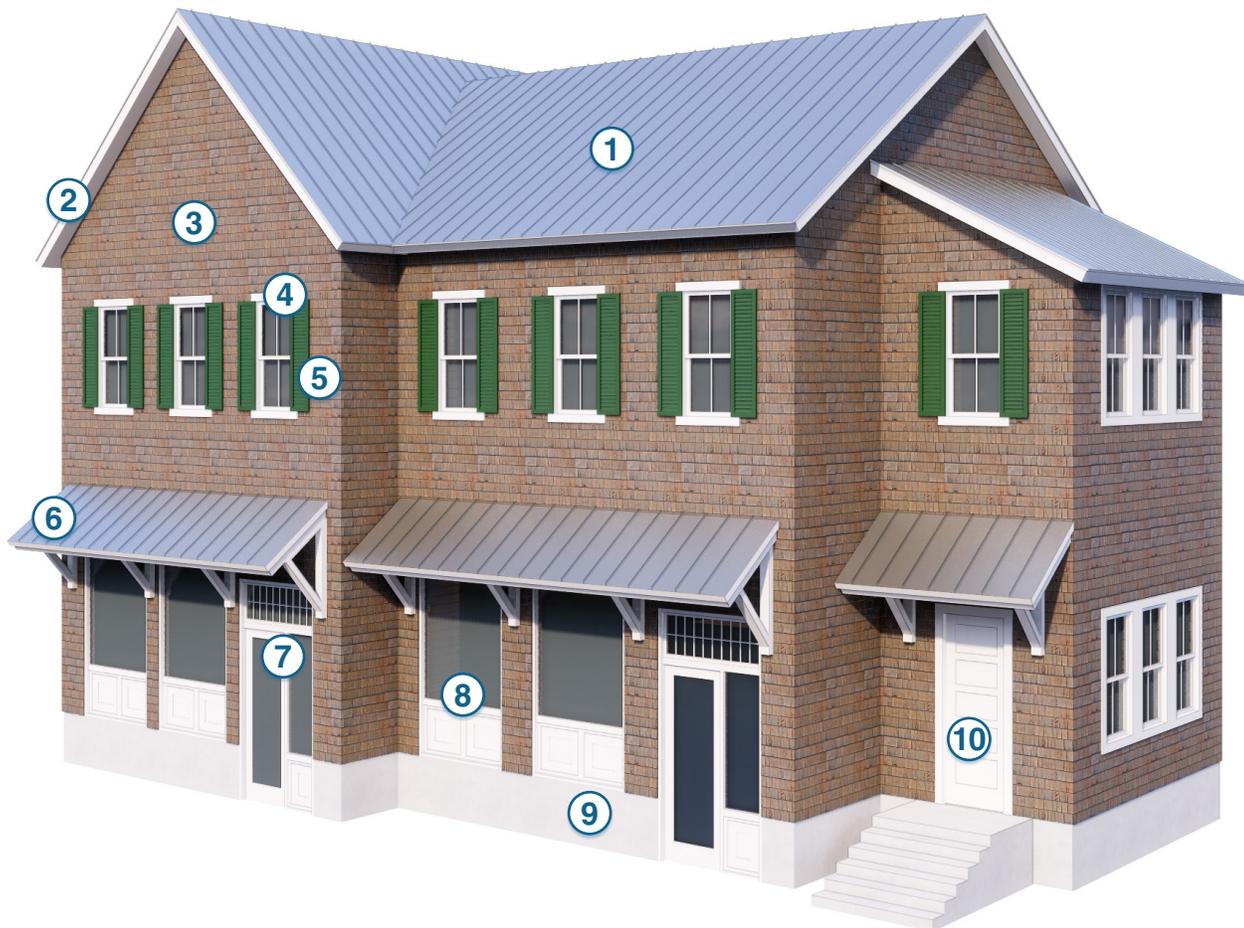
### 8. Ground floor windows

Storefront with transoms with clear (untinted) glass and optional opaque paneling

### 9. Foundation

Solid in appearance

### 10. Residential door

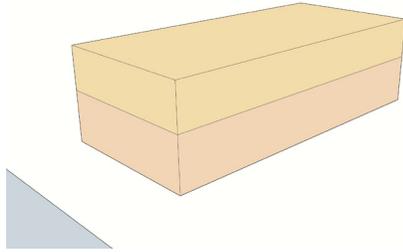


# Junction & Newtown Commercial Development

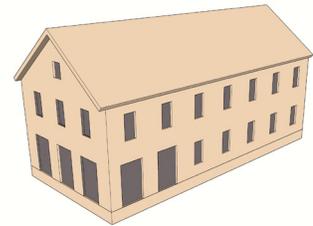
## Medium 2-story building

### Gable-front building

1. A classic and versatile building form lends itself well to a vertical mix of uses.
2. Simple details give the building a pedestrian friendly facade while keeping costs low.



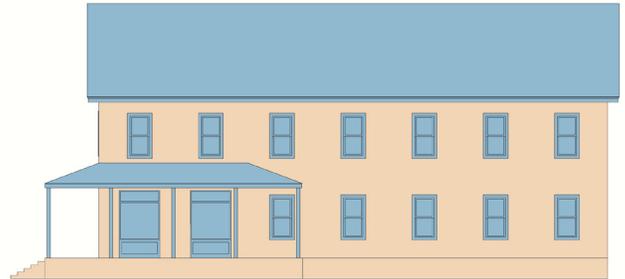
A 2 to 3-story building program in a rectangular footprint.



A gable above the upper story makes an easy-to-construct, adaptable, and durable building.



Minimal overhangs are typical, but the eaves can be extended up to 3' or 4' with exposed rafter tails for a more Craftsman appearance.



Evenly spaced side windows on all levels produce usable light throughout the building.



The wraparound porch can be supported by simple square columns or brackets.

## Junction & Newtown Commercial Development

### 1. Roof

Standing seam metal or shingle

### 2. Trim

Simple profile, multi-layered

### 3. Upper story windows

Single or divided lite, clear (untinted) glass windows

### 4. Porch roof

Match main roof or standing seam metal

### 5. Columns and brackets

4"-8" square or round

### 6. Ground floor windows

Storefront or folding glass windows with clear (untinted) glass

### 7. Doors

Storefront with clear (untinted) glass

### 8. Railings and stairs

### 9. Decking

Wood (rot resistant) or composite

### 10. Foundation skirt

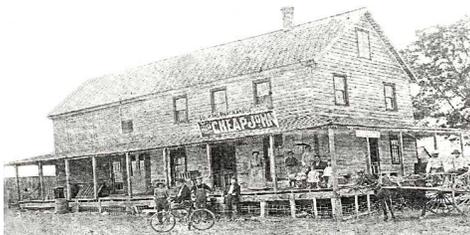
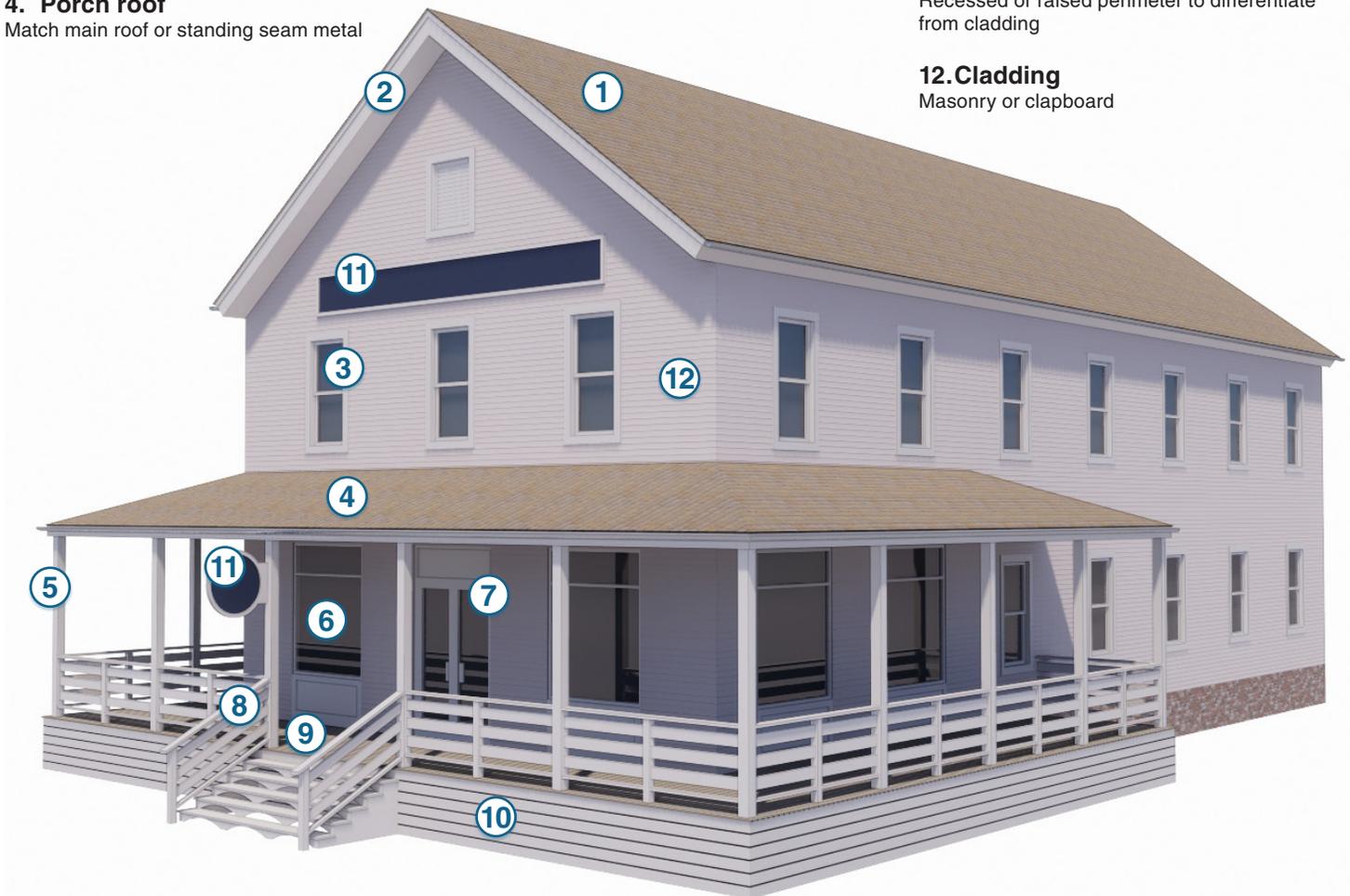
Solid in appearance with optional slatted material at porch

### 11. Sign board

Recessed or raised perimeter to differentiate from cladding

### 12. Cladding

Masonry or clapboard



Cheap John's Store, Currituck County, NC



Creekmore Brothers Store, Currituck County, NC



JM Roberts House, Currituck County, NC

## Junction & Newtown Public Spaces

Junction and Newtown sub-districts, the established single-family region of Currituck Station, is primarily a place to live and play. Residential development will feature dispersed public spaces for families to use.

A public space, is an area that a resident or visitor has access to from the public right-of-way (R.O.W.), typically a road in Junction and Newtown.

There are various types of public spaces appropriate for Junction and Newtown. The following requirements guide developing all public space in the Junction and Newtown sub-districts.



# Junction & Newtown Public Spaces

## Greenbelts



*Greenbelt path*

1. Gently meandering path.
2. Greenbelts widen at building entrances.
3. Benches, trash receptacles, and water fountains shall be regularly spaced along the greenbelt.



*Greenbelt activity node*

1. Activity nodes are to be set back from the path, while remaining visible from the R.O.W.
2. Activity nodes should offer various options for visitors: Exercise equipment, Rest areas, Scenic looks, Picnic venues, Bike repair stations.

## Parks



*Dog Parks*

1. Fencing around dog park should be continuous.
2. Planting at base of fence is encouraged, to eliminate burrowing.
3. When possible, provide trash receptacles and water fountain for pets.
4. The ground cover should be artificial turf or seeded grass.



*Fitness stations*

1. Paving should be soft and durable, rubber mats are typical.
2. Paving color should have a low heat reflectivity.
3. Plaza amenities may vary, at a minimum, provide shaded seating and paved walking paths.

## Amenities

1. Drinking fountains
2. Benches/Seating
3. Bike racks



*Area of concentrated amenities*



*Benches placed across from each other for social interaction*

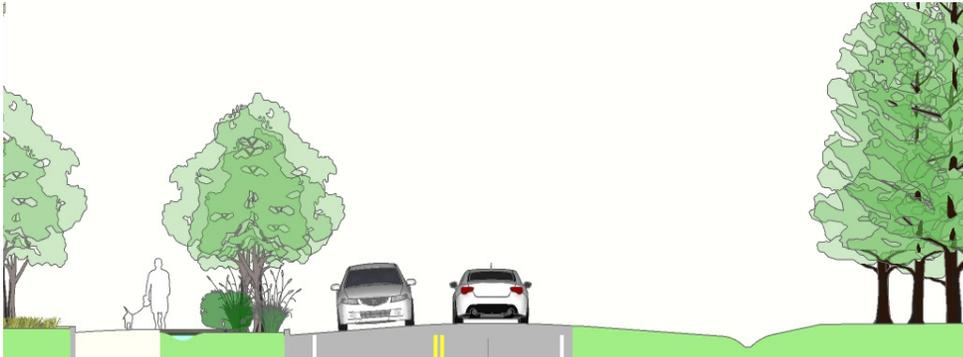
*Currituck Station*

# Junction & Newtown Public Right-of-Way

## Local streets

### Interior Street Options

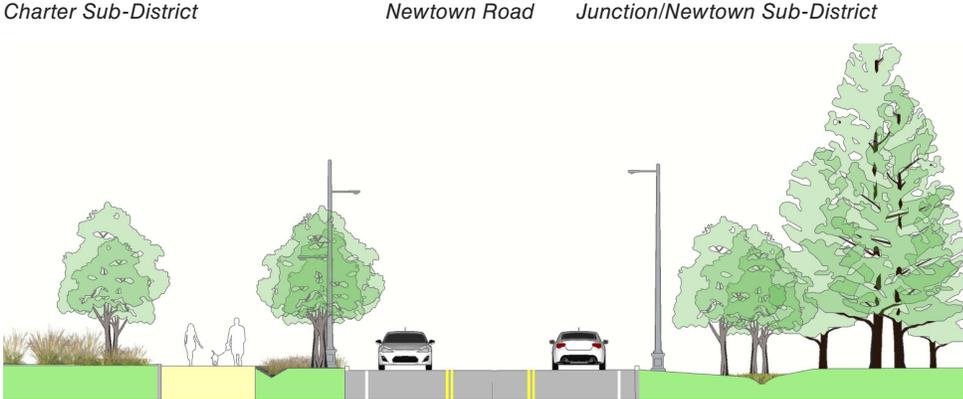
1. New streets will be designed to match the character of existing streets within the Junction and Newtown sub-districts



Representative of existing streets

## Arterials First

1. New development in the Junction and Newtown sub-districts should allow for a wide multi-use path that accommodates both pedestrians and cyclists along a pleasant, meandering path
2. The pedestrian path meanders with a constant width of 12'
3. The pedestrian path is screened with a row trees to serve as a barrier from any errant vehicles.
4. Trees shall be placed at regular intervals and of a species sized to provide continuous shade to pedestrians



Native Plants	12' Ped Path	9' Tree/Plants	11' Lane	11' Lane	11' Lane
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## Pedestrian paths

1. Pedestrian path types will be a multi-use path in these districts
2. Pedestrian path paving
3. Pedestrian path street crossings must be striped with pedestrian crossing signs for approaching vehicles.
4. Pedestrian path lighting shall be continuous, and even
5. Pedestrian multi use paths shall gently meander



Junction & Newtown - Interior Street

Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

The following pages provide examples of building massing and architectural styles that are compatible with the Currituck Station design intent. Designers are not expected to copy these, only to use them as a guide towards their own compatible designs.

# Crossroads Development Principles

The Crossroads sub-district forms the industrial and job producing, non-residential core of the Currituck Station District. In addition to industrial development, Crossroads is appropriate for small offices, professional services, childcare facilities, medical facilities, and limited restaurant or cafe uses.

Uses within the Crossroads sub-district will provide buffers and, where necessary, additional stormwater mitigation techniques to the adjacent residential and environmentally identified areas of the district. Where retention ponds are required for treating stormwater, pedestrian paths are encouraged to allow workers and visitors an accessible amenity.

Crossroads is the place where many people in Currituck Station will come to work. In order to facilitate the increased traffic in this area, additional attention to detail of circulation for pedestrians and vehicles is required. The industrial commotion of Crossroads is insulated from neighboring residential development with significant perimeter buffers.



## Dimensions

The allowable dimensions for the Crossroads sub-district are designated in the Currituck Unified Development Ordinance.

## Allowable uses

This is an abbreviated table of uses for reference only. For a current and complete table of uses, see the Unified Development Ordinance

Residential Uses	Commercial Uses	Industrial Uses	Civic Uses
None	Office Restaurant	Yes	Education Government

## Crossroads Industrial Development

### Building massing

While not as architecturally sensitive as other sub-districts in Currituck Station, developments in Crossroads should still welcome visitors and those passing through with a human-scaled entry facing the street.



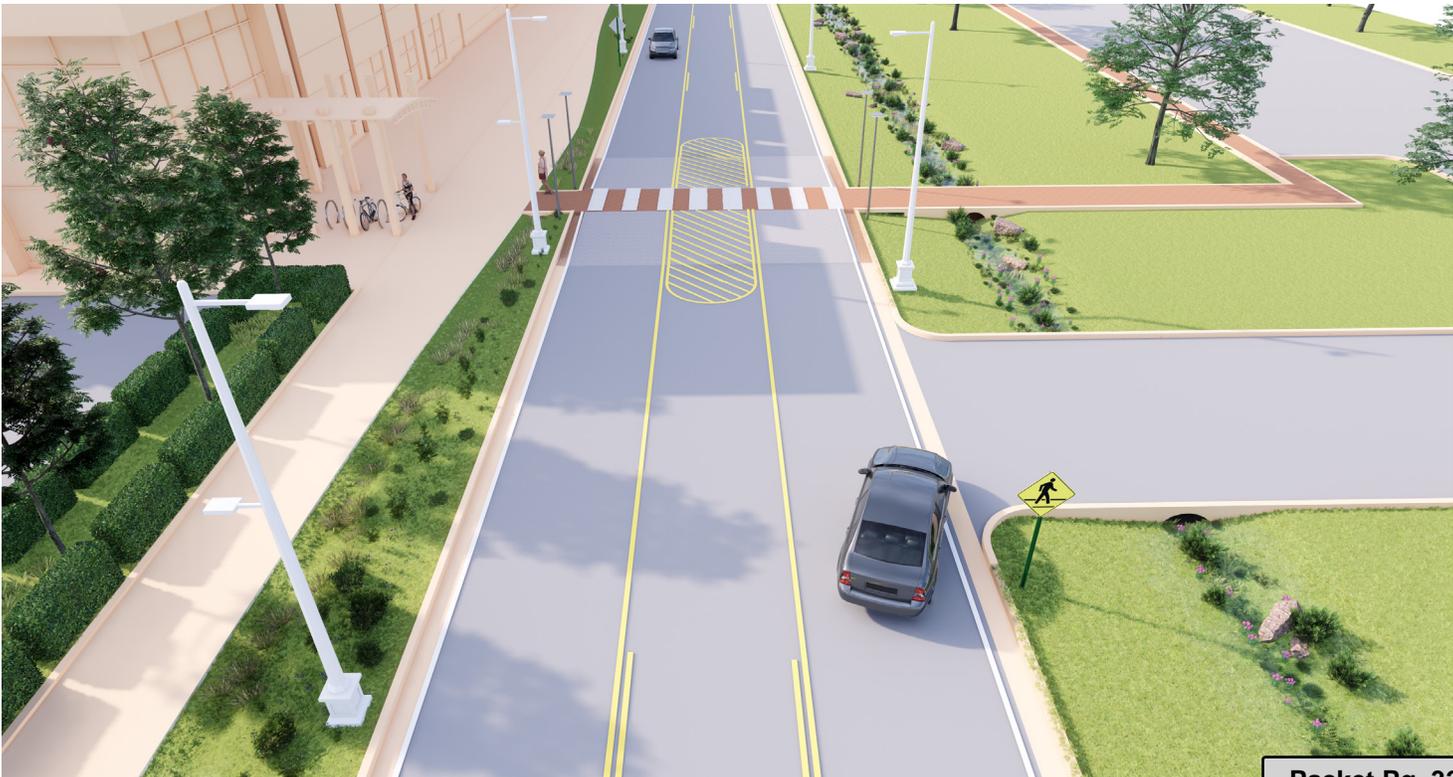
*Industrial buildings should have a welcoming entry prominently located closest to the frontage street.*

# Crossroads Public Right-of-Way

Crossroads, the industrial region of Currituck Station, is primarily a place to work and conduct business. Commercial and office uses in this sub-district will produce more vehicular activity than other sub-districts.

A public space, is an area that a resident or visitor has access to from the public right-of-way (R.O.W.), typically a multi-use path in Crossroads.

There are various types of public spaces appropriate for Crossroads. The following requirements guide developing all public space in the Crossroads sub-district.

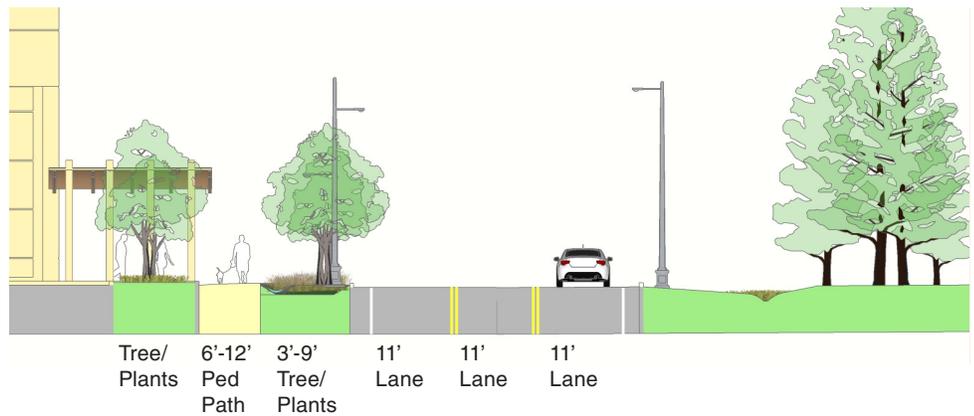


# Crossroads Public Right-of-Way

## Local Streets

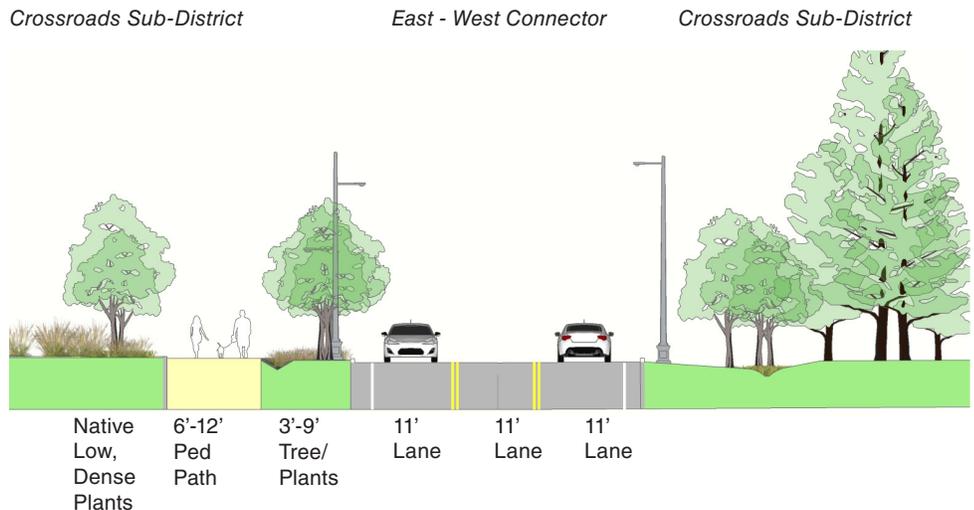
### Interior Street Options

1. Sidewalks are protected from traffic by a row of trees
1. Pedestrian paths widen towards the verge at building entrances to facilitate increased pedestrian volumes
2. The center lane is for turning vehicles and may be upgraded to a planted median, as required
3. Trees shall be placed at regular intervals and of a species sized to provide continuous shade to pedestrians
4. Lighting shall be provided for both vehicular and pedestrian paths; lighting shall be no cooler than 3000K in color temperature and provide a CRI of 80 or better



## Arterials First

1. A wide multi-use path that accommodates both pedestrians and cyclists along a pleasant, meandering path
2. Pedestrian paths widen towards the verge at building entrances to facilitate increased pedestrian volumes
3. The center lane is for turning vehicles and may be upgraded to a planted median as required
4. A row of native grasses and trees protects pedestrians on the multi-use path from errant vehicles
5. A screen planting may be utilized to hide industrial facilities from pedestrian view



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## Crossroads Public Right-of-Way

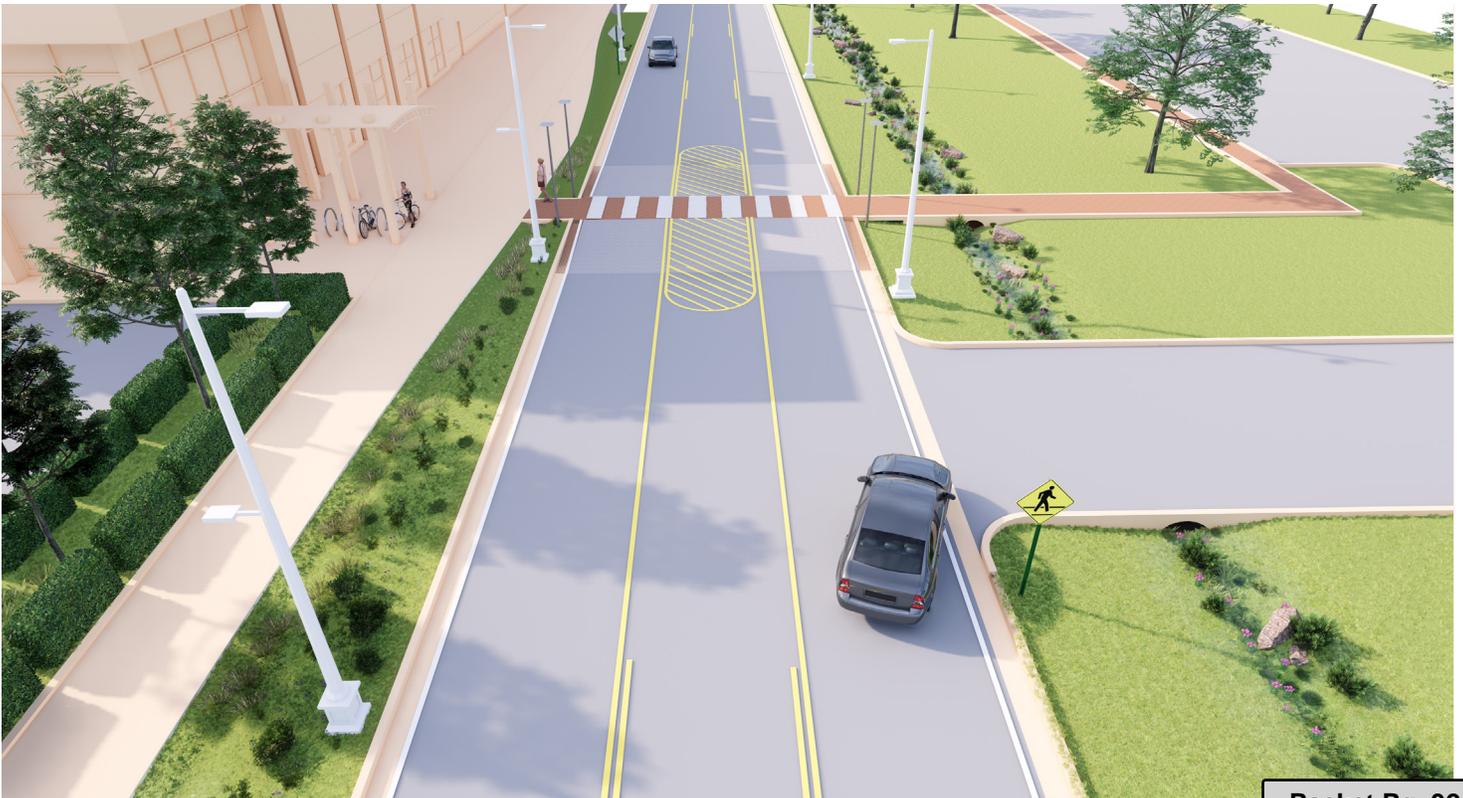
### Pedestrian paths

1. Pedestrian paths are multi-use.
2. Pedestrian path should have porous paving to facilitate drainage.
3. Pedestrian path street crossings shall be raised, colored to contrast adjacent road.
4. Pedestrian paths shall be continuously lit.
5. Pedestrian multi-use paths may gently meander.



Crossroads - Interior Street

6' - 12'



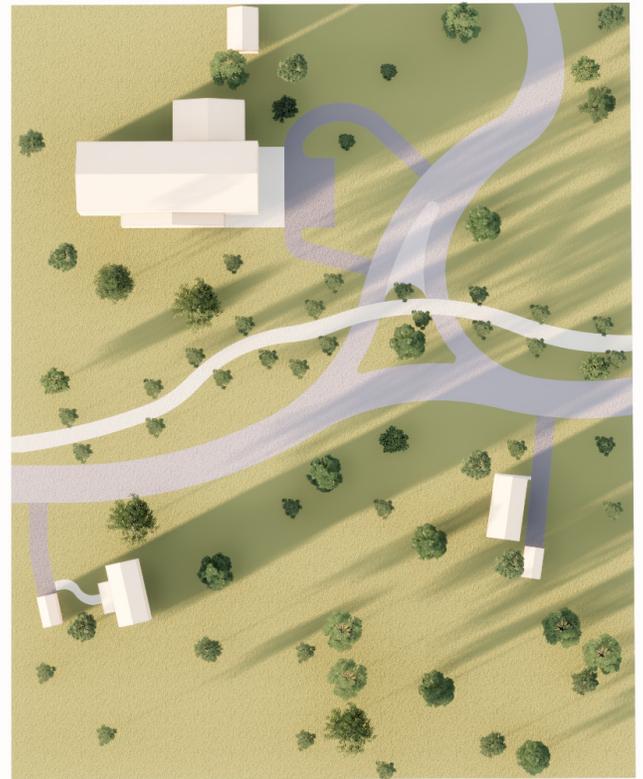
The following pages provide examples of building massing and architectural styles that are compatible with the Currituck Station design intent. Designers are not expected to copy these, only to use them as a guide towards their own compatible designs.

# Moyock Run Development Principles

The Moyock Run sub-district provides centralized governmental and public oriented uses. These uses include, but are not limited to, schools, parks, regional facilities as well as environmental and mitigation areas. Moyock Run is intended to be a natural setting in which these public facilities are placed.

To maintain the civic nature of Moyock Run buildings are encouraged along collectors and arterial streets. Each development should implement centralized (internal) roads and paths that connect to the public right-of-way. An interconnected network of sidewalks, multi-use paths, public spaces, and parks provide access to the civic and public uses found throughout the sub-district.

Moyock Run is intended to be a mostly natural and green area within the Currituck Station district. To maximize the available natural land available, connected and shared parking areas are encouraged.



Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

## Dimensions

The allowable dimensions for the Moyock Run sub-district are designated in the Currituck Unified Development Ordinance.

## Allowable uses

This is an abbreviated table of uses for reference only. For a current and complete table of uses, see the Unified Development Ordinance

Residential Uses	Commercial Uses	Industrial Uses	Civic Uses
None	None	None	Community-serving facilities Education Government

# Moyock Run Public Spaces

Moyock Run, the park-like civic region of Currituck Station, is primarily a place to conduct business and government. Civic uses in this sub-district will produce more vehicular activity than other sub-districts.

A public space, is an area that a resident or visitor has access to from the public right-of-way (R.O.W.), typically a multi-use path in Moyock Run.

There are various types of public spaces appropriate for Crossroads. The following requirements guide developing all public space in Moyock Run.



Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

# Moyock Run Public Spaces

## Squares



*Intersection square*

1. Located at the nexus of adjacent sub-districts.
2. Vehicular traffic surrounds the square.
3. Maintain a buffer between vehicular traffic outside of square and pedestrians inside square.
4. Suitable buffers are a continuous row of trees, planters, and/or on-street parking.

## Greenbelts



*Greenbelt path*

1. Gently meandering path.
2. Greenbelts widen at building entrances.
3. Benches, trash receptacles, and water fountains shall be regularly spaced along the greenbelt.



*Greenbelt activity node*

1. Activity nodes are to be set back from the path, while remaining visible from the R.O.W.
2. Activity nodes should offer various options for visitors: Exercise equipment, Rest areas, Scenic looks, Picnic venues, Bike repair stations.

Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

# Moyock Run Public Spaces

## Parks



*Fitness stations*

1. Paving should be soft and durable, rubber mats are typical.
2. Paving color should have a low heat reflectivity.
3. Fitness station amenities may vary, at a minimum, provide shaded seating and paved walking paths.



*Neighborhood Playgrounds*

1. Located within or near large public space.
2. Neighborhood playgrounds must be visible from the right-of-way.
3. Provide non-opaque fencing that is continuous and secured around the perimeter.
4. Seating, water fountains, and trash receptacles are common amenities to include:

## Amenities



*Buildings and parks should have amenities at their entrance.*



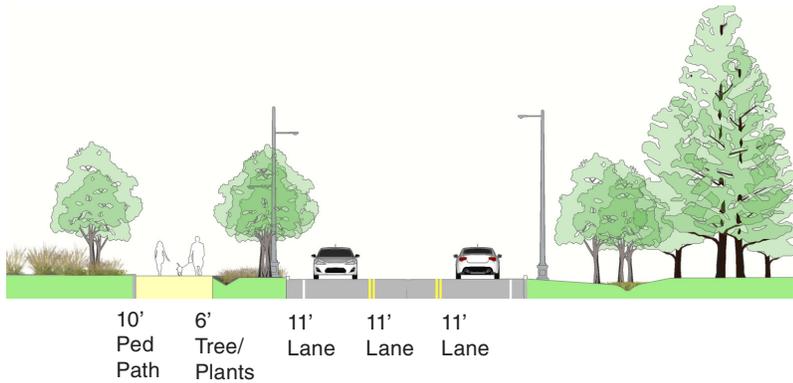
*Water fountains and trash cans are to be provided within parks in Moyock Run.*

# Moyock Run Public Right-of-Way

## Local Streets

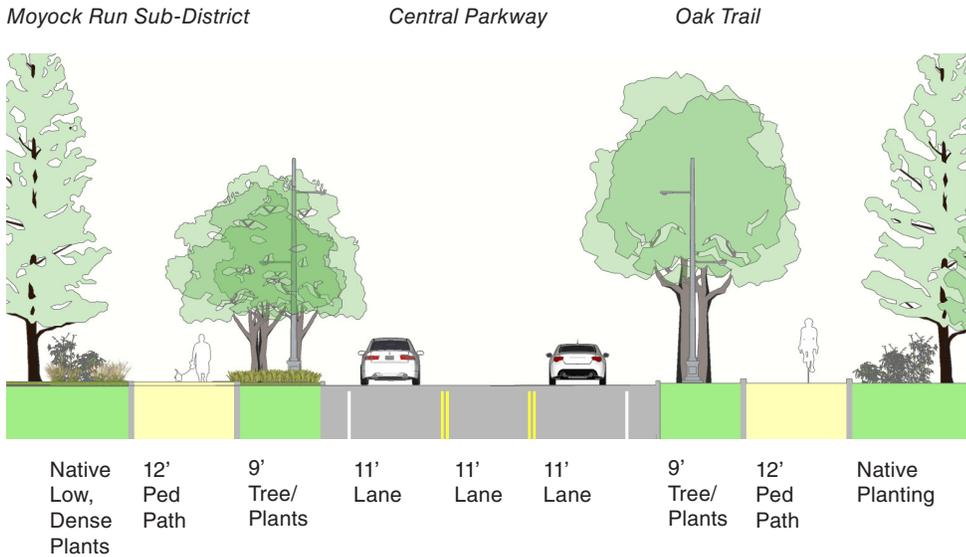
### Interior Street Options

1. The large setback in this district allows for a wide winding path that facilitate cyclists in opposing travel directions.
2. The center lane is for turning vehicles or passing traffic
3. Trees shall be placed at regular intervals and of a species sized to provide continuous shade to pedestrians
4. Lighting shall be provided for both vehicular and pedestrian paths; lighting shall be no cooler than 3000K in color temperature and provide a CRI of 80 or better



## Arterials

1. The pedestrian path is along the Moyock Run sub-district.
2. The 12' wide path gently meanders.
3. Rows of native trees and grasses serve as a physical barrier for any errant vehicles



Attachment: 3- 20-1-28\_Currituck Station\_Pattern Book (PB 19-25 Currituck County - Currituck Station)

## Moyock Run Public Right-of-Way

### Pedestrian paths

1. Pedestrian path types will be a multi-use path.
2. Pedestrian path paving should be soft and porous.
3. Pedestrian path street crossings must be striped, with signage alerting oncoming vehicles of crossing pedestrians.
4. Pedestrian path lighting shall be continuous, and even.
5. Pedestrian multi use paths shall gently meander, typically widening at entrances.



Moyock Run - Interior Street

6' - 12'

The following pages provide examples of building massing and architectural styles that are compatible with the Currituck Station design intent. Designers are not expected to copy these, only to use them as a guide towards their own compatible designs.

# Oak Trail Development Principles

The Oak Trail sub-district is intended reflect areas generally identified as environmentally sensitive. This includes, but is not limited to, native habitats, wetlands, riparian buffers, and floodplains. Low density residential uses may be permitted upon determination of environmental area limits.

The Oak Trail sub-district is generally at the periphery of the Currituck Station district. Connecting the new development with the existing rural lands surrounding by an interconnected network of greenbelt paths, parks, activity nodes, and scenic lookouts.

To maintain as much of the existing natural land as possible, development should be oriented towards collector and arterial roads. Shared parking and access roads should be utilized to minimize the amount of required paved surfaces. When paving is necessary, permeable materials should be utilized where there is no negative environmental effect.



## Dimensions

The allowable dimensions for the Junction and Newtown sub-district are designated in the Currituck Unified Development Ordinance.

## Allowable uses

This is an abbreviated table of uses for reference only. For a current and complete table of uses, see the Unified Development Ordinance

Residential Uses	Commercial Uses	Industrial Uses	Civic Uses
Single Family (Detached)	None	None	Environmental Facilities Government

## Oak Trail Public Spaces

Oak Trail, the natural open and scenic region of Currituck Station, is primarily a place to conduct business and government. Civic uses in this sub-district will produce more vehicular activity than other sub-districts.

A public space, is an area that a resident or visitor has access to from the public right-of-way (R.O.W.), typically a path or trail in Oak Trail.

There are various types of public spaces appropriate for Oak Trail. The following requirements guide developing all public space in the Oak Trail sub-district.



# Oak Trail Public Spaces

## Greenbelts



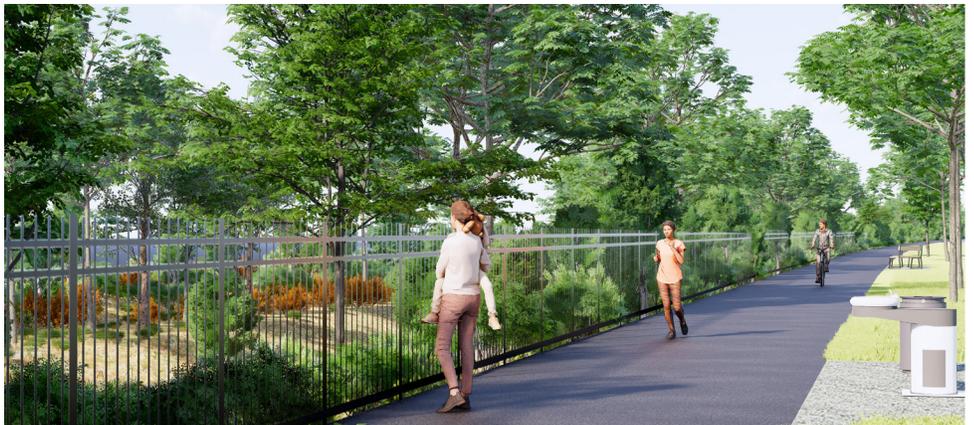
*Greenbelt path*

1. Gently meandering path.
2. Greenbelts widen at building entrances.
3. Benches, trash receptacles, and water fountains shall be regularly spaced along the greenbelt.



*Greenbelt activity node*

1. Activity nodes are to be set back from the path, while remaining visible from the R.O.W.
2. Activity nodes should offer various options for visitors: Exercise equipment, Rest areas, Scenic looks, Picnic venues, Bike repair stations.



*Greenbelt path*

1. Located within or near environmental features.
2. Fencing should allow visibility to natural site features where possible
3. Provide non-opaque fencing that is continuous and secured around the perimeter.
4. Seating, water fountains, and trash receptacles are common amenities to include

# Oak Trail Public Spaces

## Parks



*Fitness stations*

1. Paving should be soft and durable, rubber mats are typical.
2. Paving color should have a low heat reflectivity.
3. Fitness station amenities may vary, at a minimum, provide shaded seating and paved walking paths.



*Neighborhood Playgrounds*

1. Located within or near large public space.
2. Neighborhood playgrounds must be visible from the right-of-way.
3. Provide non-opaque fencing that is continuous and secured around the perimeter.
4. Seating, water fountains, and trash receptacles are common amenities to include:

## Amenities



*Buildings and parks should have amenities at their entrance.*



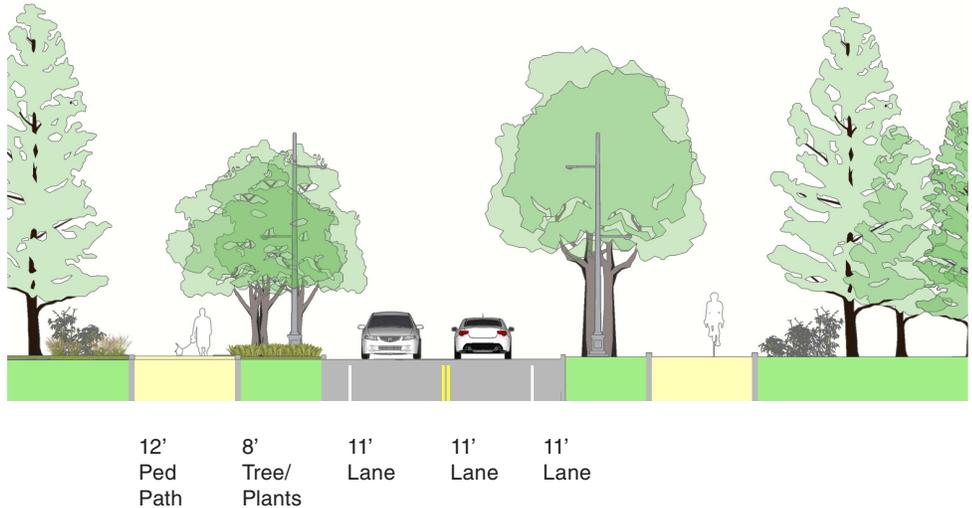
*Water fountains and trash cans are to be provided within parks in Moyock Run.*

# Oak Trail Public Right-of-Way

## Local Streets

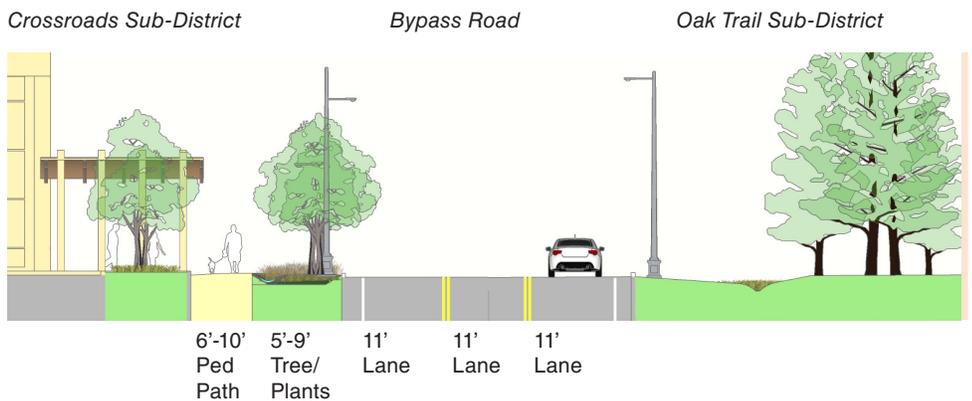
### Interior Street Options

1. The large setback in this district allows for a wide winding path that facilitate cyclists in opposing travel directions.
2. The center lane is for turning vehicles or passing traffic
3. Trees shall be placed at regular intervals and of a species sized to provide continuous shade to pedestrians
4. Lighting shall be provided for both vehicular and pedestrian paths; lighting shall be no cooler than 3000K in color temperature and provide a CRI of 80 or better



## Arterials First

1. The pedestrian path is closest to the crossroads sub-district, facilitating connection to building entrances
2. The pedestrian path is a minimum of 6' where there is no building frontage adjacent.
3. The path widens at building frontages to facilitate increased pedestrian traffic
4. A row of trees serve as a barrier to protect the pedestrian path from errant vehicles



## Oak Trail Public Right-of-Way

### Pedestrian paths

1. Pedestrian path types will be multi-use paths.
2. Pedestrian path paving should be soft and porous.
3. Pedestrian path street crossings must be striped, with signage alerting oncoming vehicles of crossing pedestrians.
4. Pedestrian path lighting shall be continuous, and even.
5. Pedestrian multi use paths shall gently meander, typically widening at entrances.



6' - 12'

*Oak Trail - Street at Building Entrance*





**CURRITUCK COUNTY  
NORTH CAROLINA**

June 1, 2020

Minutes – Regular Meeting of the Board of Commissioners

**WORK SESSION**

**1. 4:00 PM Flora Farm Review (with Planning Board, at the Cooperative Extension Auditorium)**

The Board of Commissioners met for a Work Session at 4:00 PM in the Sanderlin Auditorium at the Currituck Cooperative Extension Center, Barco, to hear a presentation from applicant representatives of the Flora Farm development project. Currituck County Planning Board members also attended the work session.

Planning and Community Development Director, Laurie LoCicero, began with an overview of the request to rezone 224 acres from Agricultural to Planned Development-Residential for the project located in Moyock, North Carolina. During review, a comparison chart was displayed that showed the number of housing units, maximum densities and open space requirements allowable under current and alternative zoning.

Mark Bissell, project Engineer, used a powerpoint to present the Flora Farm master plan. The subdivision plat and renderings of residential and commercial components of the project were displayed. Construction phasing to moderate student generation was described as a means to allow time for the construction and opening of the new elementary planned for August, 2023. Mr. Bissell responded to questions posed by Board members and confirmed that a separate wastewater plant is proposed for Flora Farm but the hope is ultimately combine the utility with the neighboring Fost Tract project. Addressing parking concerns, he noted homes will be set back to allow for more driveway space. Traffic improvements, connectivity and compatibility with nearby properties were presented.

Commissioner Beaumont reported recent budget discussions with the school board suggested that funding issues could delay the school opening by at least two years. Mr. Bissell responded to questions on the impacts to the Flora Farm project should there be a delay in school construction, and said further consideration would be needed based on the new information.

Jamie Schwedler, Attorney for the applicant, presented. She noted the proposed development plan, density, and expected growth in the area are all consistent with the 2006 Land Use Plan. Conditions and consistency statements were reviewed, and she said the North Carolina Department of Transportation has signed off on the project transportation plan. She reported the school site designated on the Flora Farm tract was selected by the Board of Education. Ms. Schwedler said adequate public facilities should not be considered

Communication: Minutes for June 1, 2020 (Approval Of Minutes for June 1, 2020)

during the rezoning but when the use permit is considered, and the phasing plan was a good faith effort to help with student capacity issues in anticipation of the new school. The 2006 Land Use Plan language addressing school facilities was reviewed.

Commissioners and Ben Stikeleather, County Manager, discussed funding challenges, such as the Covid-19 pandemic impacting both local and state revenues, possibly resulting in the need for a bond referendum for school construction funding. Discussion continued regarding school capacities and staff expressed concerns with how the rezoning might impact adequate public facilities, including affects to middle and high school, when the Use Permit comes before the Board.

Discussion concluded and Chairman White thanked everyone for attending. The work session ended at 5:12 PM.

### 6:00 PM CALL TO ORDER

The Currituck County Board of Commissioners met at 6:00 PM for a regular meeting in the Board Room of the Historic Currituck Courthouse, 153 Courthouse Road, Currituck, North Carolina.

Attendee Name	Title	Status	Arrived
Bob White	Chairman	Present	
Mike H. Payment	Vice Chairman	Present	
Paul M. Beaumont	Commissioner	Present	
J. Owen Etheridge	Commissioner	Present	
Mary "Kitty" Etheridge	Commissioner	Present	
Selina S. Jarvis	Commissioner	Present	
Kevin E. McCord	Commissioner	Present	

Chairman White called the meeting to order.

#### A) Invocation & Pledge of Allegiance

A moment of silence was held, followed by the Pledge of Allegiance.

#### B) Approval of Agenda

Commissioner Payment moved to amend the agenda and added a Resolution to approve a fireworks display at Eagle Creek Golf Course. Commissioner McCord seconded and the motion carried.

Approved agenda:

#### Work Session

4:00 PM Flora Farm Review (with Planning Board, at the Cooperative

Extension Auditorium)

**6:00 PM Call to Order**

- A) Invocation & Pledge of Allegiance
- B) Approval of Agenda

**Public Comment**

*Please limit comments to matters other than those appearing on this agenda as a Public Hearing. Public comments are limited to 3 minutes.*

**Commissioner's Report**

**County Manager's Report**

**Administrative Reports**

- A) **Budget Presentation for FY 2020-2021**

**Public Hearings**

- A) **PB 19-17 Baxter Station:** Planned Development - Residential zoning to support 127 unit subdivision (103 single-family units; 24 duplex units) with a density of 2.37 units per acre and minimum lot size of approximately 10,000 sf with the construction on an onsite wastewater treatment plant.
- B) **PB 20-04 Currituck County Flood Ordinance Text Amendment:** Request to amend the Unified Development Ordinance, Chapter 1, Section 1.7.2. to reference the most recent effective date of the Flood Insurance Study (FIS) and Digital Flood Insurance Rate Map (DFIRM) and include auto adoption language for all revisions thereto after January 1, 2021.

**New Business**

- A) **Consideration and Action on Ordinance Amending Section 8-88 of the Currituck County Code of Ordinances Deferring Payment of Outdoor Tour Operator Annual License Fee**
- B) **Consideration of Bids for Moyock Wastewater Treatment Plant**
- C) **Board Appointments**
  - 1. Social Services Board-Megan Morgan
- D) **Consent Agenda**

1. Approval Of Minutes-May 18, 2020
2. Budget Amendments
3. Surplus Resolution-Chairs, Department of Social Services
4. Purchase of an Air Compressor System-Corolla Volunteer Fire Department
5. Purchase of Used Air Compressor System from Corolla VFD-Carova Beach VFD
6. Public Utilities Payment Plan per Exec. Order 124-Covid-19

**Amended-Consideration of Fireworks Resolutions (Pulled from Consent Agenda)**

Resolution Approving the Exhibition, Use and Discharge of Pyrotechnics in Historic Corolla Park, Corolla, North Carolina.

Amended Item: Resolution Approving the Exhibition, Use and Discharge of Pyrotechnics at Eagle Creek Golf Course, Moyock, North Carolina.

**Recess Regular Meeting**

**Special Meeting-Tourism Development Authority**

Tourism Annual Budget Presentation for FY 2020-2021

Budget Amendments-TDA

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Mike H. Payment, Vice Chairman
<b>SECONDER:</b>	Kevin E. McCord, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

**PUBLIC COMMENT**

***Please limit comments to matters other than those appearing on this agenda as a Public Hearing. Public comments are limited to 3 minutes.***

Chairman White opened the Public Comment period. No one was signed up to speak. Chairman White relayed two emails received from Corolla residents who did not think the County should hold a July 4th fireworks display.

There were no other comments and the Public Comment period was closed.

**COMMISSIONER'S REPORT**

Chairman White talked about the boat basin at Whalehead in the Historic Park and said the County Manager will be looking into whether the existing pier can be extended so boats could

more easily dock to access the park. He said the county is looking at an alternative for boater access also, but that plan is expensive and would be a long-term project. The Chairman acknowledged complaints received by Commissioners over the weekend about the new KOA campground in Waterlily who transported modular units down Waterlily Road for use at the facility.

Commissioner Payment also spoke of the KOA campground in Waterlily and reported staff is on top of the situation and will take appropriate action. He reminded everyone to continue to stay safe, as the Covid-19 virus is still active. He asked citizens to keep first responders in their thoughts and prayers with the current unrest taking place.

Commissioner Mary Etheridge thanked the citizens who participated in Operation Love Thy Neighbor and announced times and locations for distribution of the Food Lion gift cards that were purchased through donations.

Commissioner Beaumont was pleased with both business and visitor activity on the Outer Banks, and noted Forbes Magazine recommended Corolla as the number one place to visit. He observed the wider beaches following recent storms, and invited all to visit.

Commissioner McCord discussed complaints received about road construction and traffic, particularly in the Maple area. He said the Sheriff's Office is patrolling on Waterlily Road and discussed the visitor activity in Corolla. Commissioner McCord relayed positive comments received about Currituck County's local law enforcement and highway patrol and offered prayers to those dealing with the unrest taking place in other areas.

Commissioner J. Owen Etheridge noted the increased traffic as people are coming back to Currituck County. He discussed the trying times we are currently experiencing in our country and encouraged prayer and working together to get us back to where we need to be.

Commissioner Jarvis also discussed the current tensions and turbulent times. She spoke of recent conditions creating an "us versus them" mentality, and encouraged all to treat each other as we would want to be treated and to be part of the solution and not the problem. Commissioner Jarvis also thanked citizens for their contributions and participation in Operation Love Thy Neighbor.

### **COUNTY MANAGER'S REPORT**

County Manager, Ben Stikeleather, announced dates, times and locations for gift card distribution in Knotts Island and Corolla through Operation Love Thy Neighbor. He said staff was investigating the complaints received from Waterlily Road residents regarding the KOA campground transporting modular cabin structures to the facility, and reported the North Carolina State Highway Patrol cannot ticket because they did not witness the violation. Mr. Stikeleather addressed the county budget, and although he is encouraged with the number of visitors coming to Corolla, sales tax collections may be down. He updated the Board of Commissioners on the progress of county projects, reviewed the county's mosquito control program, and reported NARCAN kits were received from Trillium Health for use by Sheriff and Emergency Medical Services personnel to counter drug overdoses.

### **ADMINISTRATIVE REPORTS**

## A. Budget Presentation for FY 2020-2021

Ben Stikeleather, County Manager, presented the manager's recommended budget for Fiscal Year 2020-2021. Prior budget work sessions were announced and a powerpoint was used to display budgetary information for Commissioners and the public. Mr. Stikeleather presented the statutory requirements followed for developing the budget. He said property tax rates would remain at .48 cents/\$100, and he reported the effects Covid-19 had on budget preparation. No new positions or salary increases were recommended and most capital items were removed. The budget would be reviewed in December or January, 2021, at which time items could be reconsidered. School funding, tax rates and historical data for collections and revaluations, revenue sources and appropriations, debt funding, and operating budget were presented.

When asked, Mr. Stikeleather explained how Occupancy Taxes, which are paid by visitors, can be spent per statute, with a percentage designated for promotion and another percentage for tourism related expenses. Cuts to capital expenses and permit fee increases were reviewed, as well as proposed increases in solid waste and airport fees. Remaining capital projects were presented, for a total budget of \$84,769,482.

The presentation concluded and a date of June 15, 2020, was set for Public hearing and possible budget adoption, to take effect on July 1, 2020.

### **Motion for budgetary impacts presentation at Budget Adoption-Fire/EMT Salary increases**

Following presentation, Commissioner Beaumont talked of challenges retaining full-time Fire/Emergency Medical Technician (EMT) personnel in Corolla and recommended using money from the county's fund balance to raise pay for those employees. Mr. Stikeleather discussed the cost involved and expressed concerns with paying for those recurring costs out of fund balance.

Commissioners discussed the cause of the personnel issues in Corolla, one being that other areas were providing higher pay and requiring less hours. Mr. Stikeleather suggested the Corolla Fire service district could fund the higher salaries in lieu of using fund balance. Commissioners did not want to increase the district tax.

Following discussion, Commissioner Beaumont made a motion to add funds to the budget and move forward with pay raises for Fire/EMT Basic and Intermediate staff. The motion was seconded by Commissioner Jarvis.

The motion and second were withdrawn after Commissioner J. Owen Etheridge suggested the financial impacts of the increases be brought back for consideration. Commissioner Beaumont moved that an analysis with full impacts to the County budget, including FICA and overtime, for presentation at budget adoption. Commissioner J. Owen Etheridge seconded the motion. The motion carried.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Paul M. Beaumont, Commissioner
<b>SECONDER:</b>	J. Owen Etheridge, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

## RECESS

Chairman White called a recess at 7:17 PM. The meeting reconvened at 7:25 PM. Commissioners had no additional budget comments or questions, and the Chairman moved to the next item.

## PUBLIC HEARINGS

### A. PB 19-17 Baxter Station:

Planning and Community Development Director, Laurie LoCicero, reported a notice was received from the applicant's consultant and they were withdrawing the application for Baxter Station. This would delay for one year the applicant's ability to resubmit a similar project at the location. No action was necessary.

### B. PB 20-04 Currituck County Flood Ordinance Text Amendment:

To: Planning Board

From: Planning Staff

Date: May 26, 2020

Subject: PB 20-04 Currituck County Text Amendment  
Flood Ordinance

The county adopted a revised Flood Insurance Study (FIS) and revised Flood Insurance Rate Map (FIRM) panels effective December 21, 2018. Since the county shares FIRM panels with Dare County and Dare is currently in the process of adopting their revised FIS and FIRMs, Currituck County is required to revise our FIS and FIRM.

To remain in good standing and to continue participation in the National Flood Insurance Program (NFIP), the county is required to adopt the effective Flood Insurance Study (FIS) report and Digital Flood Insurance Rate Maps (DFIRM) prior to June 19, 2020. No significant changes have been made to the flood hazard data for Currituck County.

This text amendment will revise Chapter 1, Section 1.7.2. of the Unified Development Ordinance (UDO) to reference the most recent effective date of the FIS and FIRM and includes auto adoption language for any revisions thereto after January 1, 2021.

### Text Amendment Review Standards

The advisability of amending the text of the UDO is a matter committed to the legislative discretion of the Board of Commissioners and is not controlled by any one factor. In determining

whether to adopt or deny the proposed text amendment, the Board of Commissioners may weigh the relevance of and consider whether and the extent to which the proposed text amendment:

1. Is consistent with the goals, objectives, and policies of the Land Use Plan and other applicable county-adopted plans;
2. Is not in conflict with any provision of this Ordinance or the County Code of Ordinances;
3. Is required by changed conditions;
4. Addresses a demonstrated community need;
5. Is consistent with the purpose and intent of the zoning districts in this Ordinance, or would improve compatibility among uses and ensure efficient development within the county;
6. Would result in a logical and orderly development pattern; and
7. Would not result in significantly adverse impacts on the natural environment, including but not limited to water, air, noise, stormwater management, wildlife, vegetation, wetlands, and the natural functioning of the environment.

### Staff Recommendation

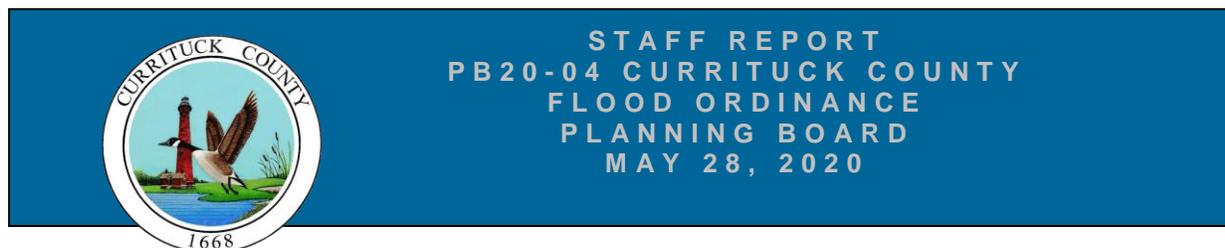
Staff recommends approval of the request as submitted and suggests the following Statement of Consistency and Reasonableness:

The requested zoning text amendment is consistent with the goals, objectives, and policies of the 2006 Land Use Plan including:

1. *Land Use and Development Goal #1*  
To protect and conserve the area's natural beauty and coastal resources as the County's greatest asset for economic development and a high quality of life.
2. *Land Use and Development Goal #7*  
To exercise caution, foresight, and common sense in dealing with the risks of coastal development.
3. *Natural Hazards Area CAMA Management Goal*  
Conserve and maintain barrier dunes, beaches, flood plains, and other coastal features for their natural storm protection functions and their natural resources, giving recognition to public health, safety, and welfare issues.
4. **POLICY NH1:** The County recognizes the risks to life and property that exist within SPECIAL FLOOD HAZARD AREAS (i.e. areas having a 1% chance of flooding in any year) that may be inundated during major storm events. The County will continue taking measures to mitigate these risks and will avoid taking any action in these areas that materially increases risks to life and property.

The request is reasonable and in the public interest because:

1. It continues the county's participation in the National Flood Insurance Program that enables property owners in participating communities to purchase insurance protection against losses from flooding.
2. It minimizes damage to public and private property due to flooding.



Amendment to the Unified Development Ordinance Chapter 1: General Provisions.

BE IT ORDAINED by the Board of Commissioners of the County of Currituck, North Carolina that the Unified Development Ordinance of the County of Currituck be amended as follows:

**Item 1:** That Chapter 1: General Provisions be amended by adding the following underlined language and deleting the struck-through language:

### **OFFICIAL ZONING MAP**

#### **1.7.2. Incorporated by Reference**

**A.** The Official Zoning Map and all the notations thereon is incorporated herein by reference and made part of this Ordinance.

The special flood hazard areas are those identified under the Cooperating Technical State (CTS) agreement between the State of North Carolina and FEMA in its Flood Insurance Study (FIS) dated June 19, 2020 ~~December 21, 2018~~ (as amended). The FIS for Currituck County and associated DFIRM panels, including any digital data developed as part of the FIS, are adopted by reference and declared a part of this Ordinance, and all revisions thereto after January 1, 2021. Future revisions to the FIS and DFIRM panels that do not change flood hazard data within the jurisdictional authority of Currituck County are also adopted by reference and declared a part of this Ordinance. Subsequent Letter of Map Revisions (LOMRs) and/or Physical Map Revisions (PMRs) shall be adopted within three months.

**Item 2:** Statement of Consistency and Reasonableness:

The requested text amendment, adoption of the FIRM, and FIS are consistent with the goals, policies, and objectives of the 2006 Land Use Plan because:

- It will conserve the floodplains and other coastal features for their natural storm protection functions and their natural resources giving recognition to public health, safety, and welfare issues. (Natural Hazards Area CAMA Management Goal)
- It includes requirements for development and redevelopment within special flood hazard areas to meet the standards of the NFIP and the flood damage prevention ordinance. (LUP POLICY NH2)
- It recognizes the potential risks to life and properties within the special flood hazard areas and the implements measures to mitigate the risks. (LUP POLICY NH1)

The request is reasonable and in the public interest because:

- It continues the county's participation in the National Flood Insurance Program that enables property owners in participating communities to purchase insurance protection against losses from flooding.
- Minimizes damage to public and private property due to flooding.

**Item 3:** The provisions of this Ordinance are severable and if any of its provisions or any sentence, clause, or paragraph or the application thereof to any person or circumstance shall be held unconstitutional or violative of the Laws of the State of North Carolina by any court of competent jurisdiction, the decision of such court shall not affect or impair any of the remaining provisions which can be given effect without the invalid provision or application.

**Item 4:** This ordinance amendment shall be in effect from and after the \_\_\_\_\_ day of \_\_\_\_\_, 2020.

\_\_\_\_\_  
Board of Commissioners' Chairman  
Attest:

\_\_\_\_\_  
Leeann Walton  
Clerk to the Board

Laurie LoCicero, Planning and Community Development Director, reviewed the text amendment to modify the effective date of the flood insurance rate map due to necessary revisions. The text amendment is required for the county to continue to participate in the National Flood Insurance Program. Language was included to provide for auto adoption of future revisions into the Unified Development Ordinance after January 1, 2021. Staff and Planning Board recommend approval of the text amendment which must be adopted prior to June 19, 2020.

Chairman White opened the Public Hearing. There were no speakers and the Public Hearing was closed.

Commissioner Jarvis moved to approve PB 20-04 because the request is consistent with the goals, objectives and policies of the Land Use Plan, including Land Use Policy NH2 and Land Use Policy NH1. And, the request is reasonable and in the public interest because it continues the county's participation in the National Flood Insurance Program that enables property owners in participating communities to purchase insurance protection against losses from flooding; and it minimizes damage to public and private property due to flooding.

Commissioner Payment seconded the motion. The motion carried.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Selina S. Jarvis, Commissioner
<b>SECONDER:</b>	Mike H. Payment, Vice Chairman
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

## NEW BUSINESS

### A. Motion for Recusal

Chairman White, as an owner of a tour operator business in Corolla, requested recusal from voting on the ordinance. Commissioner Payment moved to allow Chairman White to be recused. The motion was seconded by Commissioner McCord and the motion carried. Chairman White exited the Board Room and Commissioner Payment, Vice-Chair, assumed the duties of Chairman.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Mike H. Payment, Vice Chairman
<b>SECONDER:</b>	Kevin E. McCord, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

**B. Consideration and Action on Ordinance Amending Section 8-88 of the Currituck County Code of Ordinances Deferring Payment of Outdoor Tour Operator Annual License Fee**

Ike McRee, County Attorney, explained several tour operators on the Currituck Outer Banks requested the licensing fees be waived due to the unknown financial impacts from operating restrictions related to the Covid-19 pandemic. The Ordinance would defer this year's annual fee until September 1, 2020, at which time the Board could reassess and make a determination as to whether to waive, prorate, or collect the full fee. Staff responded to questions related to the fees paid and ordinance language.

Commissioner J. Owen Etheridge moved for adoption of the Ordinance. The motion was seconded by Commissioner Mary Etheridge. The motion carried. Chairman White was reseated with the Board following the vote.

**AN ORDINANCE OF THE CURRITUCK COUNTY BOARD OF COMMISSIONERS AMENDING SECTION 8-88 OF THE CURRITUCK COUNTY CODE OF ORDINANCES TO DEFER PAYMENT OF THE 2020 OUTDOOR TOUR OPERATOR LICENSE FEE UNTIL SEPTEMBER 1, 2020**

WHEREAS, on March 20, 2020 the Chairman of the Currituck County Board of Commissioners declared a State of Emergency under Chapter 166A of the General Statutes of North Carolina and Chapter 6, Article III of the Currituck County Code of Ordinances in response to the COVID-19 pandemic; and

WHEREAS, the March 20, 2020 Declaration of a State of Emergency prohibited visitor and non-resident property owner access to the Currituck County Outer Banks; and

WHEREAS, outdoor tour operators that operate on the Currituck County Outer Banks were effectively unable to provide tour services from March 20, 2020 until at least April 23, 2020 when non-resident property owners were permitted to enter the Currituck County Outer Banks; and

WHEREAS, although visitors are now permitted to enter the Currituck Outer Banks certain Center for Disease Control recommendations limit the number of customers outdoor tour operators may accommodate on each tour thus reducing revenue to those businesses; and

WHEREAS, the economic effects of the COVID-19 pandemic have detrimentally affected outdoor tour operators and it is deemed necessary to appropriate to defer payment of outdoor tour operator license fees; and

WHEREAS, pursuant to N.C. Gen. Stat. §153A-121 a county may by ordinance define, regulate, prohibit, or abate acts, omissions, or conditions detrimental to the health, safety, or welfare of its citizens and the peace and dignity of the county; and

WHEREAS, pursuant to N.C. Gen. Stat. §153A-134 a county may by ordinance regulate and license occupations, businesses, trades, and professions.

NOW, THEREFORE, BE IT ORDAINED by the Board of Commissioners for the County of Currituck, North Carolina as follows:

PART I. Section 8-88 of the Code of Ordinances, Currituck County, North Carolina is amended by adding paragraph (e) so that the section reads as follows:

**Sec. 8-88. License required.**

(a) It shall be unlawful for any person to engage in business as an outdoor tour operator or to operate an outdoor tour vehicle in the designated area without a license issued pursuant to this article. The license shall be effective only for term of two years stated in the license and upon payment of any annual fee, unless suspended or revoked sooner as provided for by ordinance and may not be transferred.

(b) No more than ten licenses to engage in business as an outdoor tour operator or to operate outdoor tour vehicles in the designated area shall be issued at any one time. In the event that a license is forfeited, revoked, not renewed or otherwise vacated, the county manager or county manager's designee may solicit by public notification applications for the available license. The county manager or county manager's designee shall then use a lottery in selecting from among the qualified license applicants.

(c) Each license may allow an outdoor tour operator to operate at any given point in time and only on approved routes five outdoor tour vehicles or the number of outdoor tour vehicles allowed in an issued special use permit, whichever is less.

(d) Any outdoor tour operator lawfully operating more outdoor tour vehicles than allowed pursuant to section 8-88(c) of this article on the date that this ordinance becomes effective shall be deemed a nonconforming use. Any use determined to be a nonconforming use by application of the provisions of section 8-88(d) of this article shall be permitted to continue for a period not to exceed one year from the effective date of the ordinance from which this article is derived.

(e) Payment of the annual license fee required in paragraph (a) of this section is deferred for the year 2020 until September 1, 2020.

PART II. Conflicts. All ordinances or parts of ordinances in conflict with this ordinance are hereby repealed.

PART III. Severability. In the event one or more of the provisions contained in this ordinance shall for any reason be held by a court to be invalid, illegal or unenforceable in any respect, such invalidity, illegality or unenforceability shall not affect the remaining provisions of this ordinance, and this ordinance shall be construed as if such invalid, illegal or unenforceable provision or provisions had never been contained herein.

PART III. This ordinance is effective upon its adoption.

ADOPTED this 1<sup>st</sup> day of June, 2020.

\_\_\_\_\_  
Bob White, Chairman

ATTEST:

\_\_\_\_\_  
Leeann Walton, Clerk to the Board

<b>RESULT:</b>	<b>APPROVED [6 TO 0]</b>
<b>AYES:</b>	Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner
<b>RECUSED:</b>	Bob White, Chairman

**C. Consideration of Bids for Moyock Wastewater Treatment Plant**

Communication: Minutes for June 1, 2020 (Approval Of Minutes for June 1, 2020)

Ben Stikeleather, County Manager, presented the bids received for the construction of a new Moyock Wastewater Treatment Plant and recalled his prior report to Commissioners during the budget work sessions where he noted the bids came in considerably higher than expected at almost twice the budgeted amount. Mr. Stikeleather recommended the bids be rejected due to the financial strain on the County.

Commissioner J. Owen Etheridge moved to accept the County Manager's recommendation and reject all bids. The motion was seconded by Commissioner McCord. The motion carried.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	J. Owen Etheridge, Commissioner
<b>SECONDER:</b>	Kevin E. McCord, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

#### D) Board Appointments

##### 1. Social Services Board-Megan Morgan

Commissioner Mary Etheridge moved to appoint Megan Morgan to the Social Services Board to replace Margarette Etheridge, whom she thanked for her years of service on the Board. Commissioner McCord seconded the motion. The motion carried.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Mary "Kitty" Etheridge, Commissioner
<b>SECONDER:</b>	Kevin E. McCord, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

#### Consent Agenda Motion to Remove Fireworks Resolutions for Additional Discussion

Prior to considering Consent Agenda, Commissioner J. Owen Etheridge addressed the Fireworks Resolution for Eagle Creek, which had been added to Consent Agenda at the start of the meeting. He said Eagle Creek may want to hold their display either July 3 or July 4, 2020. With the County's fireworks display scheduled for July 3, Ben Stikeleather, County Manager, said the possibility that two events might take place on the same day was of concern to the Sheriff's Office.

Commissioner J. Owen Etheridge moved to pull both Fireworks Resolutions from Consent Agenda for additional discussion. Commissioner McCord seconded. The motion carried.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	J. Owen Etheridge, Commissioner
<b>SECONDER:</b>	Kevin E. McCord, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

### E) Consent Agenda

The Fireworks Resolutions being considered separately, Commissioner Beaumont moved for approval of the remaining Consent Agenda items. The motion was seconded by Commissioner Payment. The motion carried.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Paul M. Beaumont, Commissioner
<b>SECONDER:</b>	Mike H. Payment, Vice Chairman
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

#### 1) Approval Of Minutes-May 18, 2020

##### 1. Minutes for May 18, 2020

##### 2. Budget Amendments



		Debit		Credit	
		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
<u>Account Number</u>	<u>Account Description</u>				
10330-445000	EMERGENCY MGMT			\$	18,279
10531-590000	CAPITAL OUTLAY	\$	10,000		
10531-532000	SUPPLIES	\$	6,779		
10531-516200	VEHICLE MAINTENANCE	\$	1,500		
		\$	18,279	\$	18,279
<b>Explanation:</b>	Emergency Management (10531) - 2020 Emergency Management Performance Grant Supplemental Funds to be used to purchase an event management trailer (remaining balance of project), replace EM truck tires, purchase various office supplies and re-entry permits.				
<b>Net Budget Effect:</b>	Operating Fund (10) - Increased by \$18,279.				
		Debit		Credit	
		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
<u>Account Number</u>	<u>Account Description</u>				
10330-445000	EMERGENCY MGMT			\$	20,625
10531-590000	CAPITAL OUTLAY	\$	20,625		
		\$	20,625	\$	20,625
<b>Explanation:</b>	Emergency Management (10531) - 2020 Emergency Management Performance Grant to be used to purchase an event management trailer (message board, traffic cones, barricades, safety vests).				
<b>Net Budget Effect:</b>	Operating Fund (10) - Increased by \$20,625.				
		Debit		Credit	
		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
<u>Account Number</u>	<u>Account Description</u>				
25607-545000	Guinea Mill Contracted Services	\$	20,000		
25390-499900	Appropriated Fund Balance			\$	20,000
		\$	20,000	\$	20,000
<b>Explanation:</b>	Guinea Mill Watershed Improvements District (25607) - Provide funds in Contracted Services for clear/snag and bush hog mowing ditch improvements project along Guinea Mill Canal.				
<b>Net Budget Effect:</b>	Guinea Mill Watershed Improvements District (25) - Increased by \$20,000.				

Communication: Minutes for June 1, 2020 (Approval Of Minutes for June 1, 2020)



		Debit		Credit	
		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
<u>Account Number</u>	<u>Account Description</u>				
10790-545000	CONTRACTED SERVICES	\$	3,000		
10790-514800	FEES PAID TO OFFICIALS	\$	100		
10790-532001	LIBRARY MATERIALS	\$	1,500		
10790-513000	UTILITIES			\$	3,100
10790-511000	TELEPHONE & POSTAGE			\$	1,500
		\$	4,600	\$	4,600

**Explanation:** Library (10790) - Transfer fund for operations for the remainder of this fiscal year.

**Net Budget Effect:** Operating Fund (10) - No change.

		Debit		Credit	
		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
<u>Account Number</u>	<u>Account Description</u>				
10795-532000	Supplies	\$	13,900		
10795-576001	Baseball/Softball/Tball			\$	13,900
		\$	13,900	\$	13,900

**Explanation:** Parks & Recreation (10795) - Transfer funds for operations for the remainder of this fiscal year.

**Net Budget Effect:** Operating Fund (10) - No change.

		Debit		Credit	
		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
<u>Account Number</u>	<u>Account Description</u>				
10441-511010	Data Transmission	\$	1,500		
10441-557100	Software License Fees	\$	1,000		
10441-502000	Salaries			\$	2,500
		\$	2,500	\$	2,500

**Explanation:** Information Technology (10441) - Transfer funds for operations for the remainder of this fiscal year.

**Net Budget Effect:** Operating Fund (10) - No change.

Communication: Minutes for June 1, 2020 (Approval Of Minutes for June 1, 2020)

		Debit		Credit	
		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
<u>Account Number</u>	<u>Account Description</u>				
10512-516200	Vehicle Maintenance	\$	1,500		
10512-557100	Software License Fees			\$	1,351
10512-561000	Professional Services			\$	149
		\$	1,500	\$	1,500

**Explanation:** Animal Services & Control (10512) - Transfer for operations.

**Net Budget Effect:** Operating Fund (10) - No change.

		Debit		Credit	
		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
<u>Account Number</u>	<u>Account Description</u>				
10606-511000	Postage/Telephone			\$	200
10606-532000	Supplies	\$	200		
		\$	200	\$	200

**Explanation:** Soil & Water Conservation (10606) - Transfer funds for printer ink cartridges.

**Net Budget Effect:** Operating Fund (10) - No change.

		Debit		Credit	
		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
<u>Account Number</u>	<u>Account Description</u>				
10750-557701	Low Income Energy Assist Program	\$	2,163		
10330-431600	Low Income Energy Assist Program			\$	2,163
10760-585000	Donations	\$	5,000		
10330-484000	Disaster Relief Donations			\$	5,000
		\$	7,163	\$	7,163

**Explanation:** Social Services Administration (10750); County Services (10760) - Increase appropriations for additional allocation of Low Income Energy Assistance Funding and donations for "Operation Love Thy Neighbor" both results of COVID19 assistance.

**Net Budget Effect:** Operating Fund (10) - Increased \$7,163.

### 3. Surplus Resolution-Chairs, Department of Social Services

Communication: Minutes for June 1, 2020 (Approval Of Minutes for June 1, 2020)

RESOLUTION			
<p><b>WHEREAS</b>, THE Board of Commissioners of the County of Currituck, North Carolina during its regularly scheduled meeting authorized the following, pursuant to G.S. 160A and 270(b) that the property listed below will be sold at auction, negotiated sale or will be disposed of if not sellable.</p>			
County			
Asset	Description	Serial Number	DEPT
2830	Capella Chairs w/arms	A,C,D,F,G,H,I,J,K,L,M,P Q,R,S,T,U,V,X & 1 no tag	DSS
838	Blue wooden chairs (received from the library)	no tags - 8 total	DSS
<p><b>NOW, THEREFORE, BE IT RESOLVED</b>, that the Board of Commissioners of the County of Currituck reserves the</p>			
<p><b>ADOPTED</b>, this 6th day of June, 2020.</p>			
<p>Bob White, Chairman Currituck County Board of Commissioners</p>			
<p>Leeann Walton Clerk to the Board</p>		(Seal)	

- 4. Purchase of an Air Compressor System-Corolla Volunteer Fire Department
- 5. Purchase of Used Air Compressor System from Corolla VFD-Carova Beach VFD
- 6. Public Utilities Payment Plan per Exec. Order 124-Covid-19

**AMENDED-CONSIDERATION OF FIREWORKS RESOLUTIONS (PULLED FROM CONSENT AGENDA)**

**F. Amended Item: Resolution Approving the Exhibition, Use and Discharge of Pyrotechnics at Eagle Creek Golf Course, Moyock, North Carolina.**

Communication: Minutes for June 1, 2020 (Approval Of Minutes for June 1, 2020)

Ben Stikeleather, County Manager, reiterated the Sheriff's manpower concerns if both fireworks displays were to be held on the same day. Commissioners discussed those concerns and considered how much manpower would be needed for each event. Mr. Stikeleather said there was no issue with Eagle Creek having a fireworks display but not on the same day as the County's. Commissioner J. Owen Etheridge was looking for some flexibility. Mr. Stikeleather suggested language that would allow one evening within a date range.

Following discussion, Commissioner J. Owen Etheridge moved that the Resolution be revised to approve one night of fireworks to take place from July 4th through July 11, 2020. Commissioner Payment seconded the motion. The motion carried.

**RESOLUTION  
APPROVING THE EXHIBITION, USE AND DISCHARGE OF  
PYROTECHNICS AT EAGLE CREEK GOLF COURSE IN MOYOCK,  
NORTH CAROLINA**

**WHEREAS**, pursuant to N.C. Gen. Stat. §14-410 and §14-413, a display operator that will exhibit, use or discharge pyrotechnics at a public exhibition must receive written authority for such public exhibition from the Board of Commissioners for a county; and

**WHEREAS**, a display operator must provide proof of insurance in the amount of at least \$500,000.00; and

**WHEREAS**, Kevin Wetzel, aka. Wetzel Pyrotechnics is under contract with Tim Paasch to provide an exhibition of fireworks in Moyock at property known as Eagle Creek Golf Course, 109A Greenview Road, in Moyock, North Carolina, on a single night, not to take place before Saturday, July 4, 2020 or after Saturday, July 11, 2020. The fireworks display will be a public exhibition, and Wetzel Pyrotechnics has provided a certificate of insurance evidencing insurance coverage in an amount in excess of \$500,000.00.

**NOW, THEREFORE, BE IT RESOLVED** by the Board of Commissioners for Currituck County, North Carolina, that:

Section 1. Wetzel Pyrotechnics is authorized to exhibit, use or discharge pyrotechnics for public exhibition at that property known as Eagle Creek Golf Course, 109A Greenview Road, Moyock, North Carolina, on a single night, not to take place before Saturday, July 4, 2020 or after Saturday, July 11, 2020.

Section 2. This Resolution shall be effective upon adoption.

**ADOPTED** this 1st day of June, 2020.

\_\_\_\_\_  
Bob White, Chairman

ATTEST:

\_\_\_\_\_  
Leeann Walton

Communication: Minutes for June 1, 2020 (Approval Of Minutes for June 1, 2020)

Clerk to the Board

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	J. Owen Etheridge, Commissioner
<b>SECONDER:</b>	Mike H. Payment, Vice Chairman
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

**G. Resolution Approving the Exhibition, Use and Discharge of Pyrotechnics in Historic Corolla Park, Corolla, North Carolina.**

Ben Stikeleather, County Manager, discussed the annual Independence Day Celebration in Corolla and presented several options for the Board to consider for this year's event in light of the Covid-19 pandemic. Commissioners discussed event options and spectator viewing areas, challenges, health concerns, and staff needs. Commissioners chose to have a fireworks display only, with no vendors, and the majority of the Board voted to allow both cars and pedestrians to attend. Commissioner Mary Etheridge and Commissioner Jarvis preferred to allow pedestrians only. Staff was asked to reach out to the Shawboro Ruritans to see if they are willing to manage event parking.

Discussion concluded and Commissioner Beaumont moved for approval of the Resolution. Commissioner McCord seconded. The motion carried.

**RESOLUTION  
APPROVING THE EXHIBITION, USE AND DISCHARGE OF  
PYROTECHNICS AT WHALEHEAD IN HISTORIC COROLLA  
COROLLA, NORTH CAROLINA**

**WHEREAS**, pursuant to N.C. Gen. Stat. §14-410 and §14-413, a display operator that will exhibit, use or discharge pyrotechnics at a public exhibition must receive written authority for such public exhibition from the Board of Commissioners for a county; and

**WHEREAS**, a display operator must provide proof of insurance in the amount of at least \$500,000.00; and

**WHEREAS**, Dominion Fireworks, Inc. is under contract with Currituck County to provide an exhibition of fireworks at the county's property known as Whalehead in Historic Corolla, Corolla, North Carolina, on Friday, July 3, 2020, which will be a public exhibition, and Dominion Fireworks, Inc. has provided a certificate of insurance evidencing insurance coverage in an amount in excess of \$500,000.00.

**NOW, THEREFORE, BE IT RESOLVED** by the Board of Commissioners for Currituck County, North Carolina, that:

Section 1. Dominion Fireworks, Inc., is authorized to exhibit, use or discharge pyrotechnics for public exhibition at that property known as Whalehead in Historic Corolla, 1100 Club Road, Corolla, North Carolina on Friday, July 3, 2020.

Section 2. This resolution shall be effective upon adoption.

**ADOPTED** this 1st day of June, 2020.

\_\_\_\_\_  
Bob White, Chairman

ATTEST:

\_\_\_\_\_  
Leeann Walton  
Clerk to the Board

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Paul M. Beaumont, Commissioner
<b>SECONDER:</b>	Kevin E. McCord, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

### **RECESS REGULAR MEETING**

Chairman White recessed the regular meeting to hold Special Meetings of the Tourism Development Authority and the Ocean Sands Water and Sewer District Board.

### **SPECIAL MEETING-TOURISM DEVELOPMENT AUTHORITY**

The Currituck County Board of Commissioners held a Special Meeting during a recess of the 6:00 PM Regular Meeting to sit as the Tourism Development Authority. The Special Meeting took place in the Board Room of the Historic Courthouse at 153 Courthouse Road, Currituck, North Carolina. The purpose of the meeting was to consider budget amendments and to present the Tourism Development Authority proposed budget for Fiscal Year 2020-2021.

#### **Tourism Annual Budget Presentation for FY 2020-2021**

Chairman White opened the Special Meeting and County Manager, Ben Stikeleather, presented the proposed annual budget for Occupancy Tax spending for Fiscal Year 2020-2021. He reviewed the breakdown for promotions and tourism related spending per state statute, and how funds can be used. Projected revenues and funds budgeted for promotions and tourism related spending were presented. A date of June 15, 2020, was set for Public Hearing and possible adoption with an effective date of July 1, 2020.

#### **H. TDA Budget Amendment-iPads**

Ben Stikeleather, County Manager, reviewed the first budget amendment for Board consideration to purchase iPads for use by staff to process purchases. Chairman White moved for approval. Commissioner McCord seconded the motion. The motion carried and the amendment passed.



		Debit		Credit	
		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
<u>Account Number</u>	<u>Account Description</u>				
15442-511010	Data Transmission	\$	3,000		
15442-503500	Temporary Services			\$	3,000
		\$	3,000	\$	3,000
<b>Explanation:</b>	Occupancy Tax - Promotions (15442) - Transfer funds for increased data transmission costs for the remainder of this fiscal year.				
<b>Net Budget Effect:</b>	Occupancy Tax Fund (15) - No change.				

		Debit		Credit	
		Decrease Revenue or Increase Expense		Increase Revenue or Decrease Expense	
<u>Account Number</u>	<u>Account Description</u>				
15447-516002	Outer Banks Ramps	\$	3,000		
15447-531000	Fuel	\$	500		
15447-539000	Unemployment Insurance	\$	900		
15447-545000	Contract Services			\$	4,940
15447-557100	Software License Fees	\$	540		
		\$	4,940	\$	4,940
<b>Explanation:</b>	Occupancy Tax - Tourism Related Expenditures (15447) - Transfers for operations for the remainder of this fiscal year.				
<b>Net Budget Effect:</b>	Occupancy Tax Fund (15) - No change.				

**RESULT:** APPROVED [UNANIMOUS]  
**MOVER:** Bob White, Chairman  
**SECONDER:** Selina S. Jarvis, Commissioner  
**AYES:** Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

### ADJOURN TDA SPECIAL MEETING

There was no further business and Commissioner Beaumont moved to adjourn. The motion was seconded by Commissioner J. Owen Etheridge. The motion carried and the Special Meeting of the Tourism Development Authority adjourned at 8:13 PM.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Paul M. Beaumont, Commissioner
<b>SECONDER:</b>	J. Owen Etheridge, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

### **SPECIAL MEETING-OCEAN SANDS WATER & SEWER DISTRICT**

The Currituck County Board of Commissioners held a Special Meeting during a recess of the 6:00 PM Regular Meeting to sit as the Ocean Sands Water and Sewer District Board. The Special Meeting took place in the Board Room of the Historic Courthouse at 153 Courthouse Road, Currituck, North Carolina. The purpose of the meeting was to consider budget amendments and to present the Ocean Sands Water and Sewer District proposed budget for Fiscal Year 2020-2021.

#### **Ocean Sands Water & Sewer District Annual Budget Presentation for FY 2020-2021**

Ben Stikeleather, County Manager, presented the proposed budget for the Ocean Sands Water and Sewer District for Fiscal Year 2020-2021. Expected revenues, expenses, and funds for capital spending were reviewed.

Following presentation a date of June 15, 2020, was set for Public Hearing and possible adoption of the budget, effective July 1, 2020.

#### **Budget Amendments-OSWSD**

County Manager, Ben Stikeleather, reviewed the budget amendment to cover operational expenses for the remainder of the current fiscal year. Chairman White moved for approval and the motion was seconded by Commissioner Mary Etheridge. The motion carried.



<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Paul M. Beaumont, Commissioner
<b>SECONDER:</b>	Selina S. Jarvis, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

## CLOSED SESSION

**Closed Session pursuant to G.S. 143-318.11(a)(3) to preserve the attorney-client privilege and to consult with the County Attorney and preserve attorney-client privilege in the matter entitled 85 and Sunny, LLC v. Currituck County.**

Regular session resumed and Chairman White moved to enter Closed Session pursuant to G.S. 143-318.11(a)(3) to preserve the attorney-client privilege and to consult with the County Attorney and preserve the attorney-client privilege in the matter entitled 85 and Sunny, LLC, v. Currituck County. The motion was seconded by Commissioner Beaumont. The motion carried and the Board of Commissioners entered Closed Session.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	Bob White, Chairman
<b>SECONDER:</b>	Paul M. Beaumont, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner

## ADJOURN

### Motion to Adjourn Meeting

Commissioners returned from Closed Session and had no further business. Commissioner J. Owen Etheridge moved to adjourn. Commissioner Beaumont seconded the motion. The motion carried and the meeting of the Board of Commissioners adjourned at 8:52 PM.

<b>RESULT:</b>	<b>APPROVED [UNANIMOUS]</b>
<b>MOVER:</b>	J. Owen Etheridge, Commissioner
<b>SECONDER:</b>	Paul M. Beaumont, Commissioner
<b>AYES:</b>	Bob White, Chairman, Mike H. Payment, Vice Chairman, Paul M. Beaumont, Commissioner, J. Owen Etheridge, Commissioner, Mary "Kitty" Etheridge, Commissioner, Selina S. Jarvis, Commissioner, Kevin E. McCord, Commissioner



Number 20200162

# BUDGET AMENDMENT

The Currituck County Board of Commissioners, at a meeting on the 15th day of June 2020, passed the following amendment to the budget resolution for the fiscal year ending June 30, 2020.

<u>Account Number</u>	<u>Account Description</u>	Debit		Credit	
		<u>Decrease Revenue or Increase Expense</u>		<u>Increase Revenue or Decrease Expense</u>	
12541-554005	Insurance - Lower Currituck VFD	\$	46,275		
12541-588000	Contingency			\$	2,588
12390-499900	Appropriated Fund Balance			\$	43,687
		<u>\$ 46,275</u>		<u>\$ 46,275</u>	

**Explanation:** Fire Services (12541) - Increase appropriations for insurance renewal for Lower Currituck VFD. Renewal date is June 16 and funds were appropriated for July 1 in the next fiscal year.

**Net Budget Effect:** Fire Services Fund (12) - Increased by \$43,687.

Minute Book # \_\_\_\_\_, Page # \_\_\_\_\_

Journal # \_\_\_\_\_

\_\_\_\_\_  
Clerk to the Board

Attachment: 2020-06-15-BAs\_General Meeting-Amended Item (Amended-Budget Amendments)

Number 20200163

# BUDGET AMENDMENT

The Currituck County Board of Commissioners, at a meeting on the 15th day of June 2020, passed the following amendment to the budget resolution for the fiscal year ending June 30, 2020.

<u>Account Number</u>	<u>Account Description</u>	Debit		Credit	
		<u>Decrease Revenue or Increase Expense</u>		<u>Increase Revenue or Decrease Expense</u>	
30850-506000	Insurance Expense	\$	58,000		
30380-481000	Investment Earnings			\$	24,000
30390-499900	Fund Balance Appropriated			\$	34,000
		<u>\$ 58,000</u>		<u>\$ 58,000</u>	

**Explanation:** Post-employment Benefits (30850) - Increase appropriations for retiree insurance expense due to increase in number of retirees this fiscal year.

**Net Budget Effect:** Post-employment Benefits Fund (30) - Increased by \$58,000

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\_\_\_\_\_  
Clerk to the Board

Attachment: 2020-06-15-BAs\_General Meeting-Amended Item (Amended-Budget Amendments)